



City of McMinnville  
 Community Development Department  
 231 NE Fifth Street  
 McMinnville, OR 97128  
 (503) 434-7311  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

## EXHIBIT 5 - STAFF REPORT

**DATE:** November 2, 2023  
**TO:** Planning Commission Members  
**FROM:** Tom Schauer, Senior Planner  
**SUBJECT:** Public Hearing – Planned Development Amendment PDA 5-23, Three Mile Lane Review TML 4-23, Landscape Plan Review L 38-23, Variance 3-23

### STRATEGIC PRIORITY & GOAL:



### GROWTH & DEVELOPMENT CHARACTER

Guide growth & development strategically, responsively & responsibly to enhance our unique character.

**OBJECTIVE/S:** Strategically plan for short and long-term growth and development that will create enduring value for the community

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### Report in Brief:

This proceeding is a quasi-judicial public hearing of the Planning Commission to consider concurrent review of four applications for a proposal to develop a new 138-unit apartment development “Norton Landing” on a parcel of approximately 4.93 acres located at the south end of SE Norton Lane on the west side (Tax Lot R4427 00701). **See Vicinity Map (Figure 1) and Zoning Map (Figure 2). The site plan is provided below as Figure 3. For the landscape plan, building elevations, floor plans, and all detailed plans and drawings, please see the decision document and the application submittal.**

The requests are summarized below:

**PDA 5-23.** The subject property is subject to an existing Planned Development Overlay Ordinance (Ordinance 4709 as amended by Ordinances 4863 and 5072). A Planned Development requires overlay zone provisions and also approval of a master plan. This proposal would approve a master plan for the property consistent with the applicable provisions of the Comprehensive Plan, Zoning Ordinance, and the Planned Development Overlay Ordinances.

**TML 4-23.** The subject property is within the Three Mile Lane Planned Development Overlay, established by Ordinance 4131 and subsequently revised by Ordinances 4572, and other ordinances. The proposed development is subject to policies and standards of the Three Mile Lane Planned Development Overlay Ordinance.

**L 38-23.** The proposal includes a landscape plan, which is required for multi-dwelling development, subject to the provisions of Chapter 17.57 of the Zoning Ordinance.

**VR 3-23.** As part of the master plan, the applicant is requesting flexibility for two of the multi-dwelling standards related to parking space setbacks and distance between parking lot landscape islands; for one standard for the setback of a trash enclosure from a property line; and to building height provision of the Planned Development Overlay Ordinance.

The applicant has requested flexibility regarding certain standards of the Zoning Ordinance, and one provision of the PD Overlay Ordinance 5072. Some flexibility can be granted as part of the Planned Development Review. Other flexibility requires a variance.

The applicant is requesting flexibility regarding the following provisions of Chapter 17.11. These items can be approved within the flexibility allowed for the PD approval.

- Allow 63' rather than 60' between parking lot islands.
- Allow parking spaces within 6' rather than 10' of north and south property lines. (Provide 10' from west property line that is residential). To the north are the abutting parking lots of those developments. To the south is outside the UGB.
- Allow trash enclosure less than 30' from west property line abutting residential zone. (Trash enclosures would be 6-feet of other interior property lines which abut non-residential zoning, and would abut screened parking lot areas).

The applicant is requesting a variance to the 35' height limit established in the PD ordinance 5072. This item requires approval of a variance subject to the standard variance criteria. Further, it is ineligible for an administrative variance even though it is less than 10% of the standard, because building height is specifically listed as an item which doesn't qualify for an administrative variance.

- Request to exceed 35' height limit of PD ordinance to allow height of highest ridge to 37'.

This is a consolidated review to consider these applications associated with the development of the subject property. There will be one public hearing to receive testimony, and then four separate votes will be taken: one vote for each application, based on the applicable criteria.

The consolidated review procedures specify that the decisions for all applications are subject to the procedure that affords the most opportunity for public hearing and notice:

17.72.070 Concurrent Applications. When a proposal involves more than one application for the same property, the applicant may submit concurrent applications which shall be processed simultaneously. In so doing, the applications shall be subject to the hearing procedure that affords the most opportunity for public hearing and notice.

The Planning Commission will make the decisions on the applications. That will be the final local decision unless the applications are appealed to City Council.

### **Background:**

#### **Applicable Criteria**

The applicable criteria and standards are summarized below:

- The applicable criteria for the Planned Development Amendment are specified in Section 17.74.070 of the Zoning Ordinance. The approval is also subject to the conditions in the PD Overlay Ordinance: Ordinance 4709 as amended by ordinances 4863 and 5072.
- The applicable criteria for the Landscape Plan Review are provided in Section 17.57.070 of the Zoning Ordinance.



- The applicable criteria for the Three Mile Lane Ordinance are provided in the policies and applicable provisions of Ordinance 4131 (as amended).
- The applicable criteria for the building height variance (for building height) are provided in Section 17.74.100
- Applicable Goals and Policies of the Comprehensive Plan are also criteria for land use decisions, including the Great Neighborhood Principles and policies of the Three Mile Lane Area Plan.

Applicable standards of the Zoning Ordinance related to these applications are addressed in the findings in the decision document.

**Discussion:**

Staff is recommending approval of the applications subject to conditions, and with some revisions. Items of note:

- Widen the on-site east-west sidewalk from 5 feet to 10 feet to better accommodate both bicycles and pedestrians.
- Revise the landscape plan (the area south of the open space between buildings B and D to address Policies 15 and 16 of the Three Mile Lane Area Plan, the provisions of the Great Neighborhood Principles, and provision of MMC 17.11.110.4.
- Reuse the proposed recessed planter strip along Norton Lane to accommodate street trees.
- Revise the plan to reconcile the discrepancy in the narrative as needed to achieve 65% minimum standard parking spaces minimum size of 9'x19'.

**Attachments:**

1. Decision Document
2. PDA 5-23 Original Application and Attachments
3. TML 4-23 Original Application and Attachments
4. L 38-23 Original Application and Attachments
5. VR 3-23 Original Application and Attachments
6. PDA 5-23 Revised Narrative
7. PDA 5-23 Revised Site Plan Sheets
8. TML 4-23 Revised Narrative
9. VR 3-23 Revised Narrative
10. Comments from Fire Marshal
11. TIA Review Comments from City Traffic Consultant
12. Comments from ODOT

**Planning Commission Options:**

1. **APPROVE** the applications as proposed by the applicant, per the decision document provided, which includes the findings of fact.
2. **CONTINUE** the public hearing to a specific date and time.

3. Close the public hearing, but **KEEP THE RECORD OPEN** for the receipt of additional written testimony until a specific date and time.
4. Close the public hearing and **DENY** the application, providing findings of fact for the denial, specifying which criteria are not satisfied, or specifying how the applicant has failed to meet the burden of proof to demonstrate all criteria are satisfied, in the motion to deny.

**Staff Recommendation:**

**PDA 5-23**

Staff has reviewed the proposal for consistency with the applicable criteria. Staff finds that, based on the findings in the attached Decision Document, the application submitted by the applicant and the record contain evidence that demonstrates that, with conditions, the application complies with the applicable criteria and that the applicant has met the burden of proof.

Staff **RECOMMENDS APPROVAL** of the application based on the findings and conditions in the attached Decision Document.

**TML 4-23**

Staff has reviewed the proposal for consistency with the applicable criteria. Staff finds that, based on the findings in the attached Decision Document, the application submitted by the applicant and the record contain evidence that demonstrates that, with conditions, the application complies with the applicable criteria and that the applicant has met the burden of proof.

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**L 38-23**

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**VR 3-23**

Staff has reviewed the proposal for consistency with the applicable criteria. Staff finds that, based on the findings in the attached Decision Document, the application submitted by the applicant and the record contain evidence that demonstrates that, with conditions, the application complies with the applicable criteria and that the applicant has met the burden of proof.

Staff **RECOMMENDS APPROVAL** of the application based on the findings and conditions in the attached Decision Document.

**Suggested Motion:**

**PDA 5-23**

**BASED ON THE FINDINGS OF FACT, THE CONCLUSIONARY FINDINGS FOR APPROVAL, THE MATERIALS SUBMITTED BY THE APPLICANT, AND EVIDENCE IN THE RECORD, I MOVE THAT THE PLANNING COMMISSION APPROVE THE DECISION DOCUMENT AND APPROVE**

PLANNED DEVELOPMENT AMENDMENT, PDA 5-23, SUBJECT TO THE CONDITIONS IN SECTION II OF THE DECISION DOCUMENT.

**TML 4-23**

**BASED ON THE FINDINGS OF FACT, THE CONCLUSIONARY FINDINGS FOR APPROVAL, THE MATERIALS SUBMITTED BY THE APPLICANT, AND EVIDENCE IN THE RECORD, I MOVE THAT THE PLANNING COMMISSION APPROVE THE DECISION DOCUMENT AND APPROVE THE THREE MILE LANE DESIGN REVIEW, TML 4-23, SUBJECT TO THE CONDITIONS IN SECTION II OF THE DECISION DOCUMENT.**

**L 38-23**

**BASED ON THE FINDINGS OF FACT, THE CONCLUSIONARY FINDINGS FOR APPROVAL, THE MATERIALS SUBMITTED BY THE APPLICANT, AND EVIDENCE IN THE RECORD, I MOVE THAT THE PLANNING COMMISSION APPROVE THE DECISION DOCUMENT AND APPROVE THE LANDSCAPE PLAN REVIEW, L 38-23. SUBJECT TO THE CONDITIONS IN SECTION II OF THE DECISION DOCUMENT.**

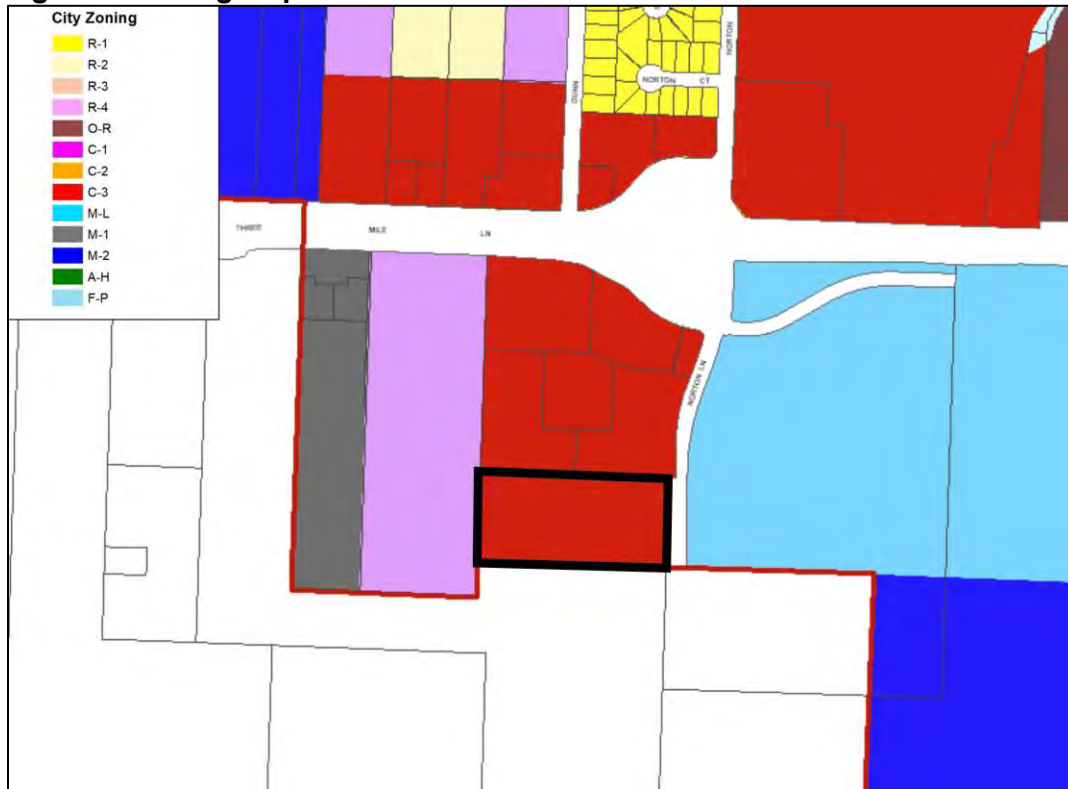
**VR 3-23**

**BASED ON THE FINDINGS OF FACT, THE CONCLUSIONARY FINDINGS FOR APPROVAL, THE MATERIALS SUBMITTED BY THE APPLICANT, AND EVIDENCE IN THE RECORD, I MOVE THAT THE PLANNING COMMISSION APPROVE THE DECISION DOCUMENT AND APPROVE THE VARIANCE, VR 3-23. SUBJECT TO THE CONDITIONS IN SECTION II OF THE DECISION DOCUMENT.**

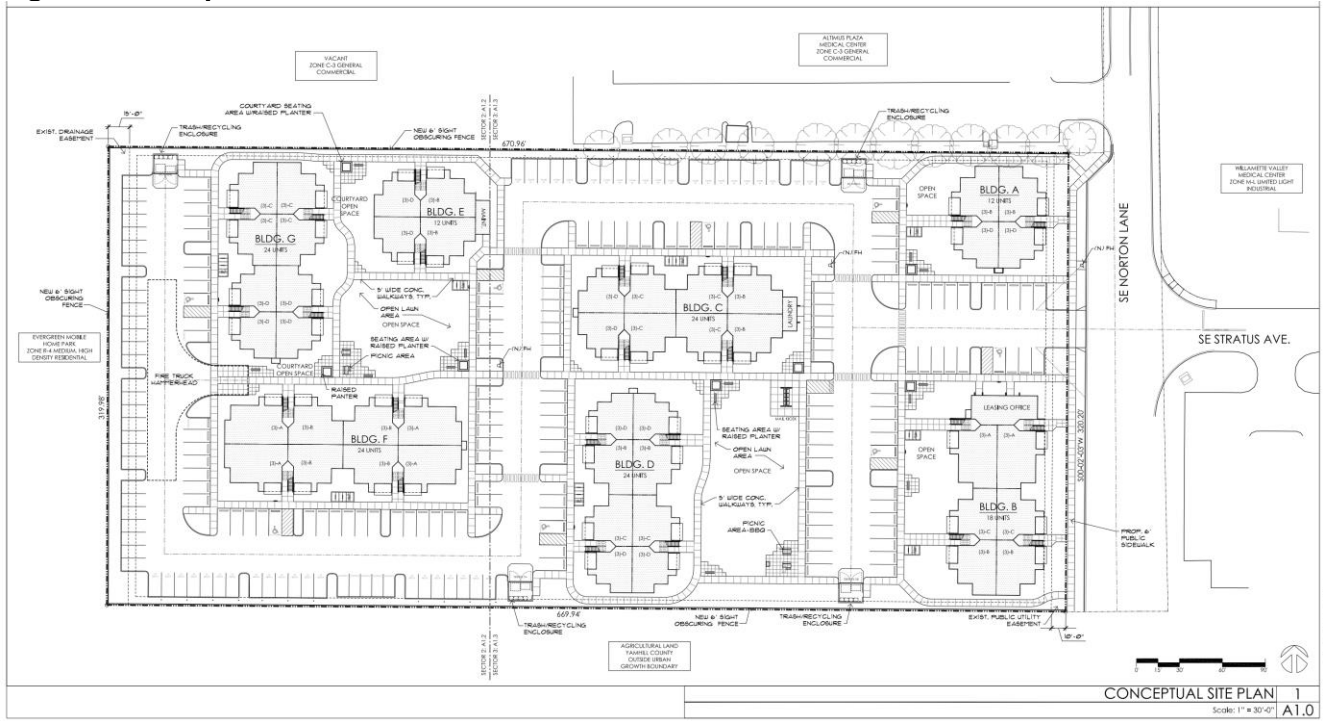
**Figure 1. Vicinity Map**



**Figure 2. Zoning Map**



**Figure 3a. Proposed Master Plan – Site Plan**





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**DECISION, CONDITIONS, FINDINGS OF FACT AND CONCLUSIONARY FINDINGS FOR THE APPROVAL OF A PLANNED DEVELOPMENT AMENDMENT, THREE MILE LANE REVIEW, LANDSCAPE PLAN REVIEW, AND VARIANCE FOR PROPERTY AT THE SOUTH END OF SE NORTON LANE ON THE WEST SIDE, TAX LOT R4427 00701**

**DOCKET:** PDA 5-23 (Planned Development Amendment)  
TML 4-23 (Three Mile Lane Review)  
L 38-23 (Landscape Plan Review)  
VR 3-23 (Variance)

**REQUEST:** Concurrent review of applications for a Planned Development Amendment (PDA 5-23), Three Mile Lane Review (TML 4-23), Landscape Plan Review (L 38-23), and Variance (VR 3-23).

The requests are summarized below. The applications are submitted as part of a proposal by the applicant to develop a new 138-unit apartment development, "Norton Landing," on a parcel of approximately 4.93 acres.

**PDA 5-23.** The subject property is subject to an existing Planned Development Overlay Ordinance (Ordinance 4709 as amended by Ordinances 4863 and 5072). A Planned Development requires overlay zone provisions and also approval of a master plan. This proposal would approve a master plan for the property consistent with the applicable provisions of the Comprehensive Plan, Zoning Ordinance, and the Planned Development Overlay Ordinances.

**TML 4-23.** The subject property is within the Three Mile Lane Planned Development Overlay, established by Ordinance 4131 and subsequently revised by Ordinances 4572, and other ordinances. The proposed development is subject to policies and standards of the Three Mile Lane Planned Development Overlay Ordinance.

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**VR 3-23.** As part of the master plan, the applicant is requesting flexibility for two of the multi-dwelling standards related to parking space setbacks and distance between parking lot landscape islands; for one standard for the setback of a trash enclosure from a property line; and to building height provision of the Planned Development Overlay Ordinance.

**LOCATION:** Address: South end of SE Norton Lane, on the west side  
Map & Tax Lot: R4427 00701

**ZONING:** C-3 PD (General Commercial with Planned Development Overlay)

**APPLICANT:** Reiter Design Architect Incorporated c/o Scott Reiter, on behalf of property owner KWDS, LLC c/o Chad Juranek

**STAFF:** Tom Schauer, Senior Planner

**DATE DEEMED COMPLETE:** October 2, 2023

**DECISION MAKING**

**BODY & ACTION:** The McMinnville Planning Commission makes the decision on the application. The Planning Commission's decision is the final local decision unless their decision is appealed to City Council.

**HEARING DATE & LOCATION:** November 2, 2023

This will be a hybrid meeting with the opportunity to join an in-person meeting at Civic Hall or virtually on a zoom meeting.

**Meeting Location:**

McMinnville Civic Hall, 200 NE 2<sup>nd</sup> Street, McMinnville, OR 97128

**Zoom Online Meeting:**

<https://mcminnvilleoregon.zoom.us/j/84808603865?pwd=WE03Ukt3bDU5VkUwRUhla1Jnb2w0QT09>

**Meeting ID:** 848 0860 3865 **Passcode:** 166748

The public may also join the Zoom meeting by phone by using the phone number and meeting ID below:

**Phone:** +1 253 215 8782, **Meeting ID:** 848 0860 3865

**PROCEDURE:** For consolidated application review, the applications are processed in accordance with the procedures in Section 17.72.120 of the Zoning Ordinance. The applications are reviewed by the Planning Commission in accordance with the quasi-judicial public hearing procedures specified in Section 17.72.130 of the Zoning Ordinance. The Planning Commission makes the final local decision, unless their decision is appealed to City Council.

**CRITERIA:** McMinnville Municipal Code (MMC) and Other Ordinance Sections:  
-Planned Development Amendment: MMC 17.74.070 & Ord. 5072  
-Landscape Plan: MMC 17.57.070  
-Three Mile Lane Review: Ord. 4131 as subsequently amended  
- Variance: MMC 17.74.080-110

Applicable Comprehensive Plan Goals and Policies are criteria for land use decisions.

**APPEAL:** The Planning Commission's decision may be appealed to the City Council within 15 calendar days of the date the written notice of decision is mailed as specified in Section 17.72.180 of the Zoning Ordinance. The City's final decision is subject

to a 120-day processing timeline, including resolution of any local appeal, per MMC 17.72.050 and ORS 227. A decision of the City Council is appealable to LUBA as specified in Section 17.72.190.

**COMMENTS:**

This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Engineering Department, Building Department, Parks Department, Public Works Department, Waste Water Services, City Manager, and City Attorney; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Frontier Communications; Comcast; Recology; Oregon Department of State Lands; and Northwest Natural Gas. Their comments are provided in Section IV this document.

**DECISION**

Based on the findings and conclusionary findings, the Planning Commission finds the applicable criteria are satisfied with conditions and **APPROVES** the Planned Development Amendment (PDA 5-23), Three Mile Lane Review (TML 4-23), Landscape Plan Review (L 38-23), and Variance (VR 3-23) **subject to the conditions of approval provided in Section II of this document.**

- PDA 5-23: APPROVAL WITH CONDITIONS
- TML 4-23: APPROVAL WITH CONDITIONS
- L 38-23: APPROVAL WITH CONDITIONS
- VR 3-23: APPROVAL WITH CONDITIONS

Planning Commission: \_\_\_\_\_  
Sidonie Winfield, Chair of the McMinnville Planning Commission

Date: \_\_\_\_\_

Planning Department: \_\_\_\_\_  
Heather Richards, Planning Director

Date: \_\_\_\_\_



## **I. APPLICATION SUMMARY:**

### ***Subject Property & Request***

The subject property is approximately 4.93 acres located at the south end of SE Norton Lane on the west side. Evergreen mobile home park is located to the west, Stratus Village and Altimus Plaza to the north and property outside the UGB to the south. The parking lot for the hospital is located across Norton lane to the east. **See Figure 1 for Vicinity Map & Aerial Photo and Figure 2 for Zoning Map.**

Four applications were submitted as part of this proposal, which are reviewed through a consolidated concurrent review process. The requests are summarized below. The applications are submitted as part of a proposal by the applicant to develop a new 138-unit apartment development, "Norton Landing," on properties totaling approximately 4.93 acres.

Concurrent review of applications for a Planned Development Amendment (PDA 5-23), Three Mile Lane Review (TML 4-23), Landscape Plan Review (L 38-23), and Variance (VR 3-23).

**PDA 5-23.** The subject property is subject to an existing Planned Development Overlay Ordinance (Ordinance 4709 as amended by Ordinances 4863 and 5072). A Planned Development requires overlay zone provisions and also approval of a master plan. This proposal would approve a master plan for the property consistent with the applicable provisions of the Comprehensive Plan, Zoning Ordinance, and the Planned Development Overlay Ordinances.

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The applicant will apply for sign permits at a later time. Upon submittal of the application, signage will be reviewed for compliance with applicable provisions including MMC 17.62, specific provisions regarding signage addressed in Ordinance 5072 and sign provisions of the Three Mile Lane Planned Development Overlay.

Ordinance 5072 includes conditions, including provisions addressing the Planned Development master plan requirements. The property is also located within the Three Mile Lane Planned Development Overlay (Ordinance 4131/4572), within the boundary of the new Three Mile Lane Area Plan (adopted by Ordinance 5126), and the Horizontal Surface Area of Airport Overlay Zone (MMC 17.52).

Figure 1. Vicinity Map



Figure 2. Zoning Map

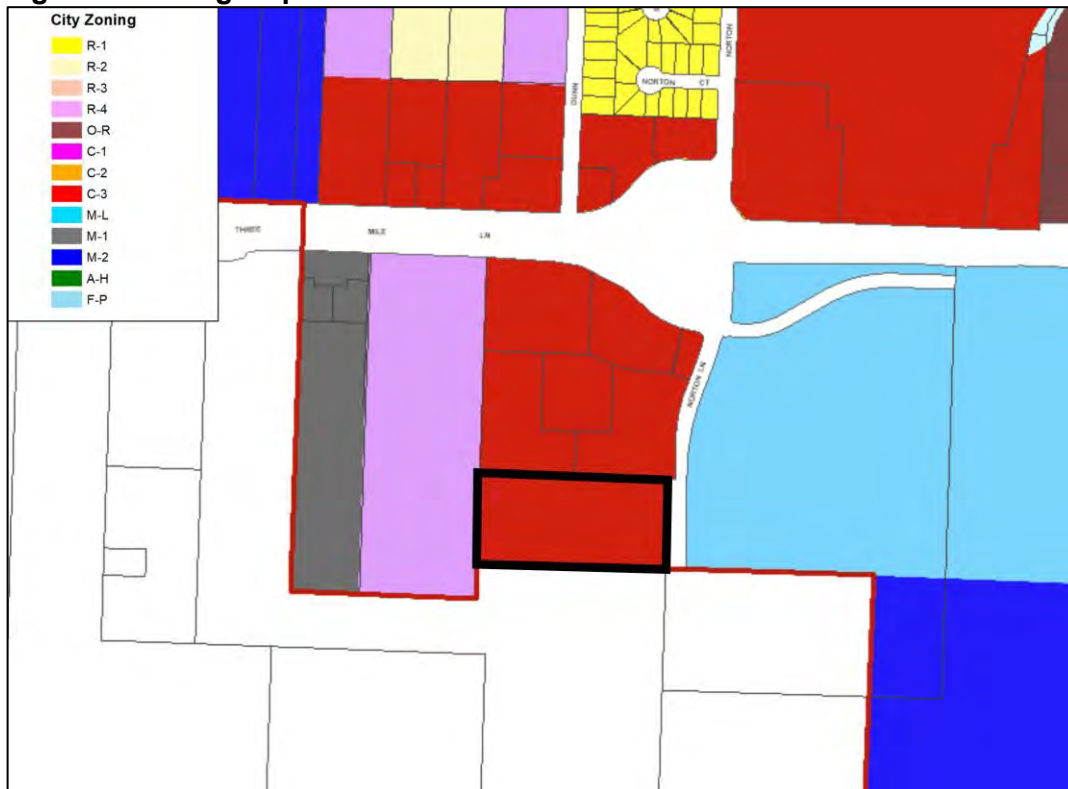
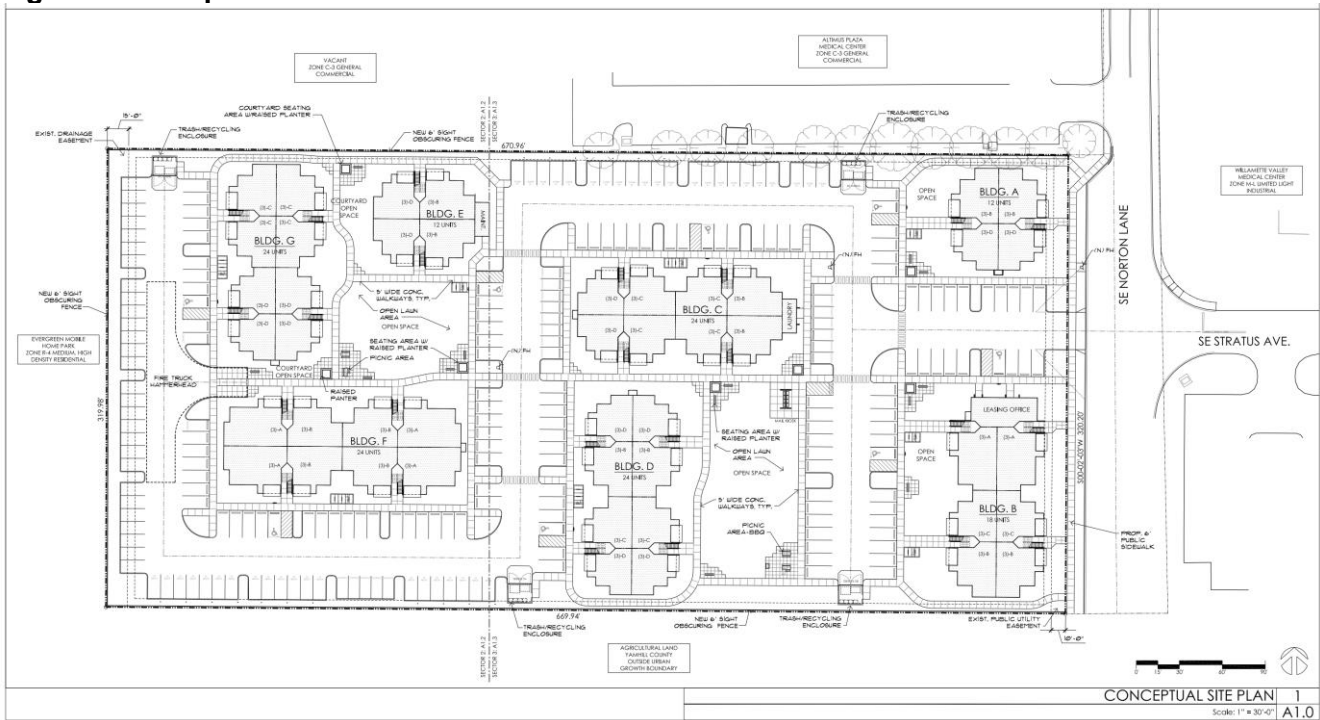


Figure 3a. Proposed Master Plan – Site Plan



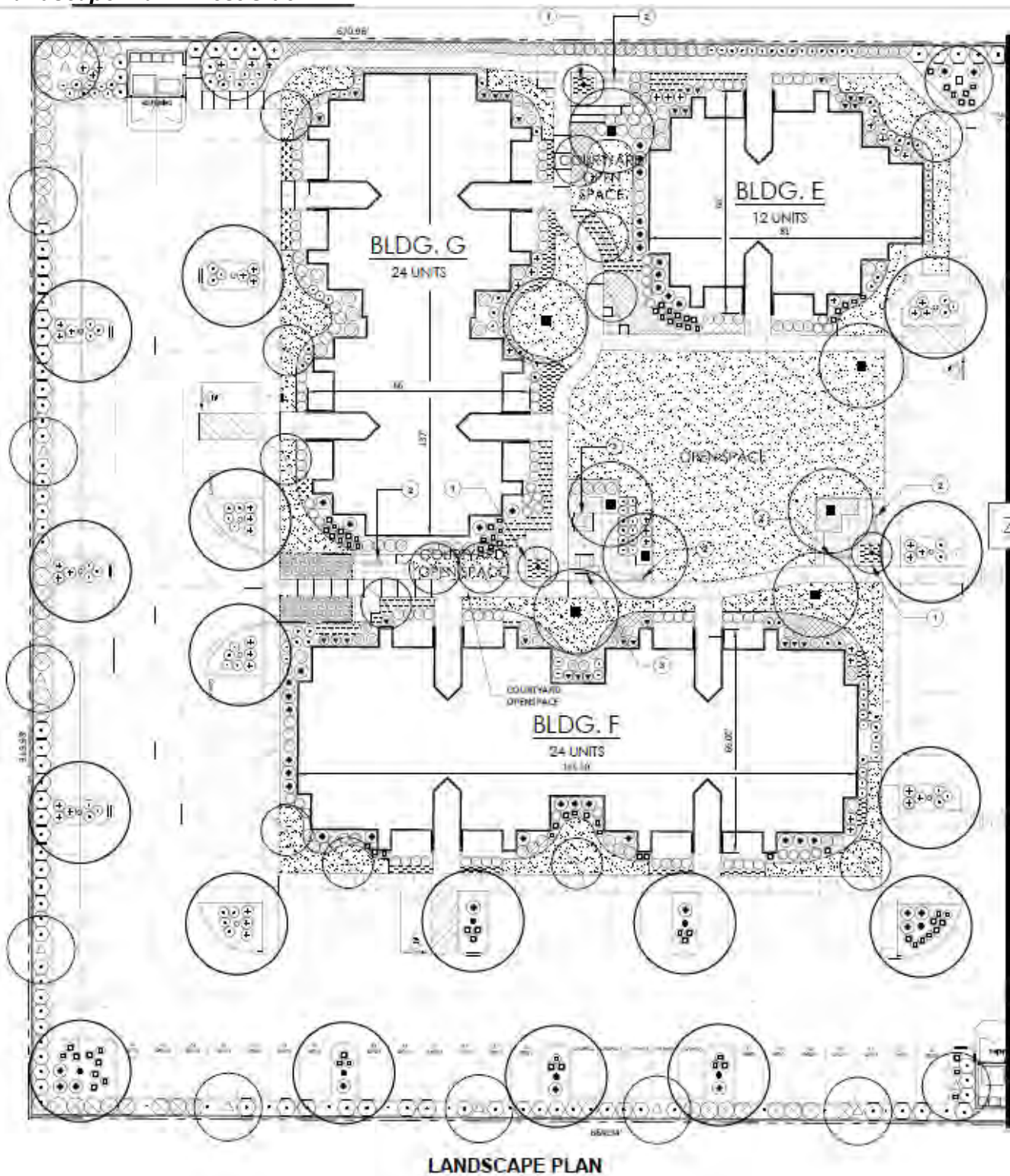
**Note: This aerial/site plan is to show surrounding context only. The site plan has had minor changes not reflected below such as the 10' setback to the west:**



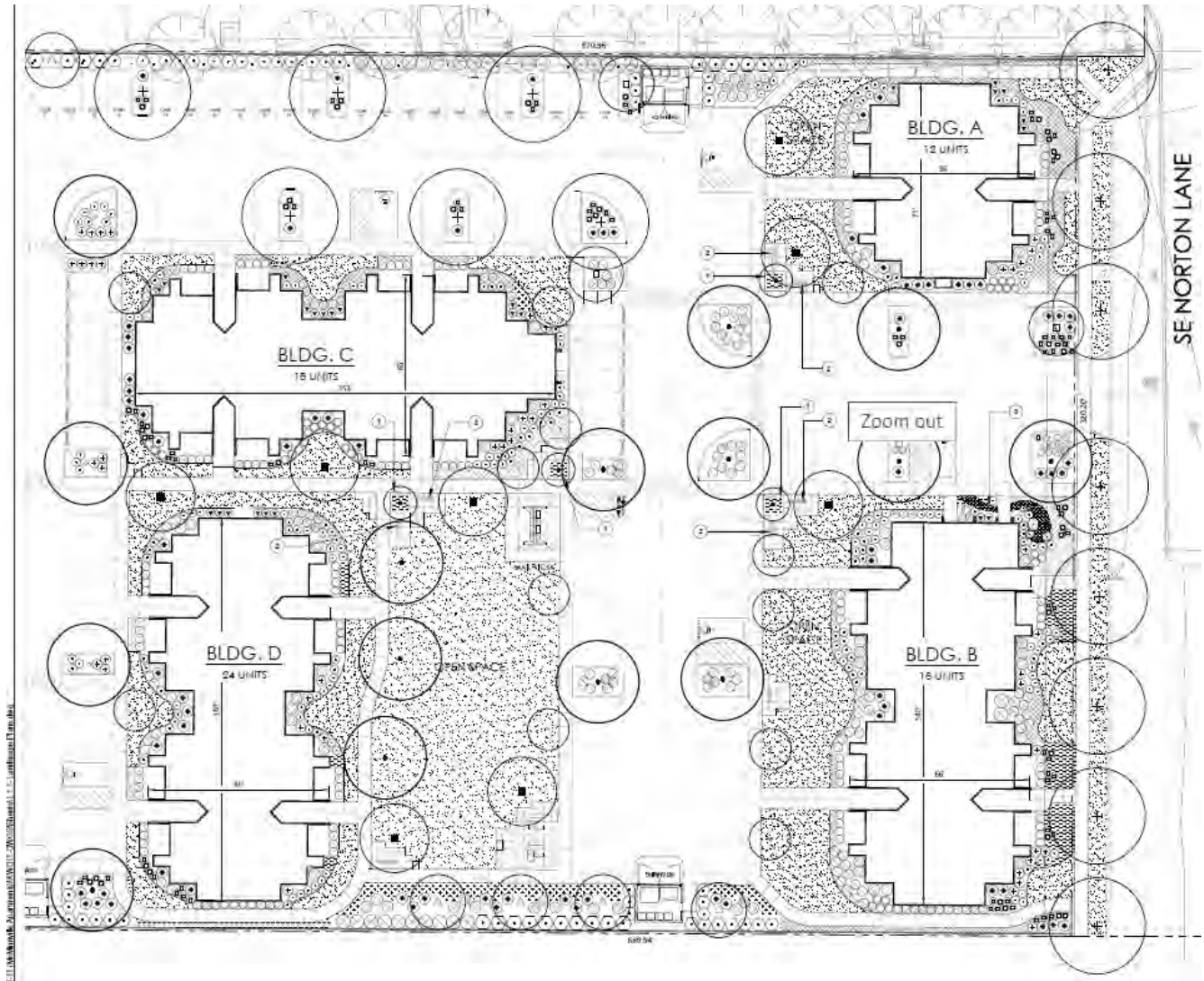


**Landscape Plan:** Note the landscape plan hasn't been updated to match the revised site plan which has a 10-foot setback on the west property line. (See applications for detailed plans and plant list.

**Landscape Plan - West Side**



**Landscape Plan - East Side**



**Figure 4. Typical Elevations (Building E is similar to Building A shown below; Buildings C, D, F, G are similar to Building B shown below). Please see applications for full set of drawings – floorplans and elevations.**



## **Public Comments**

Written testimony is listed in Section IV.

## **Agency Comments**

Notice of the proposal was sent to affected agencies and departments. Comments received from agencies are provided in Section IV of this Decision Document.

## **II. CONDITIONS:**

The applications are **approved subject to the following conditions:**

### **PDA 5-23 Conditions of Approval**

1. The Planned Development Amendment PDA 5-23 authorizes the proposed master plan for the 138-unit apartment development, consistent with the site plan, landscape plan, and building elevations as approved herein, upon approval of revisions by the City as addressed in these conditions of approval. The Master Plan shall be part of the Planned Development and Zone of the property and a copy of the approved plans shall be placed on file with the Planning Department.

This approval is contingent on Planning Commission concurrent approval of the requested flexibility to the Zoning Ordinance provisions of 17.11 and the variance to the 35' height limit established in Ordinance 4709 as amended by Ordinances 4863 and 5072, to allow 37'.

2. The applicant shall submit a revised site plan that addresses the following:
  - a. Revise the site plan to achieve 65% minimum standard parking spaces at 9'x19'. Several of the spaces counted as standard spaces in the calculations are shown on the site plan as 17 feet deep. Alternatively, provide a calculation and request Planning Commission approval of a plan that has less than 65% standard spaces.
  - b. Increase the width of the sidewalk that bisects the site from east to west to 10-feet in width to accommodate both bicycles and pedestrians since there is no other direct route for bicycles other than using the indirect meandering parking lot aisle.
  - c. The property is zoned commercial but is proposed for multi-family development. Rather than commercial frontage improvements to minor collector standards, provide a frontage improvement to residential standards with a sidewalk and planter strip. Sidewalks along Norton Lane are a combination of 5-foot and 6-foot sidewalk, and sidewalks on Stratus Avenue are 6-feet. As there are no more other more direct alternative pedestrian connections to existing and planned commercial areas to the north, the sidewalks along the Norton Lane frontage shall be a minimum of 6-foot wide with a planter strip.
3. Clarify accessibility/universal design of amenities for residents in common area open spaces consistent with Great Neighborhood Principles. **Note:** *The applicant is encouraged to exceed minimum accessibility requirements for recreational amenities.*
4. The applicant shall apply for and obtain all applicable permits from the City of McMinnville and other agencies.
5. The plan shall comply with the vision clearance standards of Chapter 17.54.

6. The applicant shall provide verification of whether any building mounted “wall-pack” type lights are proposed and the nature of any downcast cut-off shielding, for review and approval.
7. No sign shall be installed without first applying for applicable sign permits, building permits, and electrical permits. Signs shall comply with all applicable provisions of MMC 17.62, Planned Development Ordinance 5072, and the provisions of the Three Mile Lane Planned Development Ordinance 4131 as subsequently amended by Ordinance 4572 and subsequent ordinances.
8. The Planning Director may authorize minor amendments to the plans without requiring review of a Planned Development Amendment by the Planning Commission. The Planning Director shall determine what constitutes a minor or major amendment.
9. Prior to issuance of a building permit, the applicant shall comply with all requirements of Ordinance 4709 as amended by Ordinances 4863 and 5072.
10. The applicant shall submit utility plans and constrictor agreements for review and approval of the respective standards. The applicant shall complete installation of utilities per approved plans prior to occupancy, or shall provide financial surety as may be authorized with deferred completion.
11. As part of the building permit/site development review process, address the requirements of the Engineering Department, including the following:
  - a. The applicant will enter into a Construction Permit Agreement with the City's Engineering Department.
  - b. Provide the City with an approved 1200C Permit from DEQ.
  - c. ADA Sidewalk and Driveway Standards are now being applied to all new construction and remodels. These standards are intended to meet the current ADA Standards as shown in the "PROWAG" Design Guidelines. The standards can be found at the following webpage: <https://www.access-board.gov/files/prowag/PROW-SUP-SNPRM-2013.pdf> prior to final occupancy, the applicant shall construct new driveways and sidewalks in the right-of way that conform to these standards.
  - d. Street grades and profiles shall be designed and constructed to meet the adopted Land Division Ordinance standards and the requirements contained in the Public Right-of-Way Accessibility Guidelines (PROWAG). Additionally, corner curb ramps shall be constructed to meet PROWAG requirements. ADA Ramps are required at the SE Stratus Ave and SE Norton Ln intersection.
  - e. SE Norton Ln shall be improved to minor collector standard per the Transportation System Plan and have the City's typical crowned section.
  - f. The applicant shall secure all required state and federal permits, including, if applicable, those related to construction of the storm drain outfalls, the federal Endangered Species Act, Federal Emergency Management Act, and those required by the Oregon Division of State Lands, U.S. Army Corp of Engineers, and DEQ. Copies of the approved permits shall be submitted to the City prior to the City issuing permits.
  - g. That the applicant submit evidence that all fill placed in the areas where building sites are expected is engineered. Evidence shall meet with the approval of the City Building Division and the City Engineering Division.
  - h. That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to



issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private, rather than public, and private maintenance agreements must be approved by the City for them.

- i. The final development plans for the subject site include a detailed sanitary sewerage collection plan which incorporates the requirements of City's Collection Systems Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the structures located within the subject site are required to connect to the sanitary sewer systems as soon as service is available.
  - j. As identified on sheet C1.0 SE Norton Ln – Preliminary Plan page 292 of the PDA in detail "Typical Section – Norton Lane (North)" the frontage improvement will include a 2" grind and inlay to the existing crown of the street.
  - k. As identified on sheet C1.0 SE Norton Ln – Preliminary Plan page 292 of the PDA in detail "Typical Section – Norton Lane (South)" the frontage improvement will include a three quarter Street improvement consisting of a 40' from curb to curb, and curb and gutter on the east side of the street.
  - l. Width of improved SE Norton Ln curb to curb should be typically 40'.
  - m. Existing gate at south terminus of Norton Ln will need to be relocated or replaced to the new terminus to provide access for the farmland. Additionally, a new barricade may need to be installed at terminus per City Standard Detail 29.
12. The applicant shall contact the appropriate utility-locate service (dial 811 or 800-332-2344) prior to any excavation to ensure that underground utilities are not damaged.
  13. As part of the building permit/site development review process, address the requirements of the Fire Marshal, including the Fire Code requirements provided as an attachment. The Fire Marshal shall review and approve mountable curb or other transition between the parking lot surface and the raised hammerhead turnaround.
  14. Provide documentation that Recology has approved the proposed trash enclosure plan where the layout does not meet all dimension requirements specified in the Zoning Ordinance.
  15. As part of the building permit/site development review process, address applicable requirements of McMinnville Water and Light for provision of water and power service, including any necessary extension agreements, final power design, and minimum clearances from water facilities and electrical equipment.

#### **TML 4-23 Conditions of Approval**

1. Approval of the TML 4-23 design review is contingent on approval of the proposed Planned Development Amendment/master plan amendment PDA 5-23 and associated variances.
2. The approved master plan for PDA 5-23, subject to its conditions of approval, and as revised to address conditions of PDA 5-23, demonstrates compliance with the requirements of the Three Mile Lane Review.
3. At the time of submittal of a sign permit application, in addition to other applicable standards the sign permit application shall be reviewed for compliance with the sign provisions of the Three Mile Lane Planned Development Overlay Ordinance (Ordinance 4131 as amended by ordinance 4572 and other ordinances).

4. Because the plans become part of the Planned Development master plan, this approval doesn't expire unless the Master Plan undergoes a future major amendment, in which case a new concurrent Three Mile Lane Review would be required.

### **L 38-23 Conditions of Approval**

1. Approval of the landscape plan is contingent on approval of the proposed Planned Development Amendment/master plan amendment PDA 5-23 and associated variances.
2. The applicant shall submit a revised landscape plan addressing the following:
  - a. The landscape plan shall be revised to be consistent with the approved site plan, including the wider 10-foot landscape area along the west property line.
  - b. Revise the proposed design of the frontage for the design of the planter strip to be consistent with McMinnville standards, with a standard rather than recessed planter strip that allows for installation of street trees.
  - c. Revise the planting at the south side of the open space area between buildings B and D to incorporate landscaping influenced by agricultural themes consistent with policies 15 and 16 of the Three Mile Lane Area Plan. It is recommended that area or other are also incorporate food cultivation to meet 17.11.110.4.e. Otherwise, demonstrate which four items in 17.11.110.4 are being addressed.
  - d. If any revisions are required to the plans for PDA 5-23 and TML 4-23, the applicant shall submit a revised landscape plan consistent with the revised site plan for review and approval, incorporating any required revisions of the site plan.
3. The plan shall comply with the vision clearance standards of Chapter 17.54. The applicant shall show how proposed landscaping will screen the parking lot and also comply with the vision clearance height limits (not to exceed three feet) within vision clearance areas at driveway approaches.
4. The applicant shall install landscaping, including street trees, as shown on the revised and approved landscape plan, and shall comply with required conditions of approval. Landscaping shall be installed prior to occupancy or may be secured for up to six months as specified in Chapter 17.57.
5. The applicant shall contact the appropriate utility-locate service (dial 811 or 800-332-2344) prior to any planting excavation to ensure that underground utilities are not damaged.
6. The applicant shall maintain proper clearances around the existing and future water and electrical services that will be located on the site.
7. The planting of street trees shall be subject to the design drawings and specification developed by the City in May 2014 as revised. The applicant shall provide root barrier protection per standards in order to minimize sidewalk and tree root conflicts to a depth of eighteen (18) inches. In addition, street trees shall be staked and provided with two (2) deep watering tubes to promote deep root growth per the specification.
8. The applicant is reminded that trees are not to be planted within:
  - a. Five (5) feet of a private driveway or alley;
  - b. Ten (10) feet of a fire hydrant, transformer, power or water vault, water meter box, utility pole, sanitary sewer, storm or water line; or
  - c. Twenty (20) feet of street light standards or street intersections.

9. All street trees shall be a minimum of two (2) inches in caliper measured at six (6) inches above grade. All trees shall be healthy grown nursery stock with a single straight trunk, a well-developed leader with tops and roots characteristic of the species cultivar or variety. All trees must be free of insects, diseases, mechanical injury, and other objectionable features when planted.
10. The applicant shall schedule an inspection with the McMinnville Public Works Superintendent of the installed root barrier and water tubes prior to any street tree planting. Trees intended for planting shall be on-site and available for inspection. The applicant shall contact the McMinnville Public Works Superintendent, at (503) 434-7316 to schedule a planting inspection prior to backfilling.
11. All approved landscaping and street trees shall be continually maintained, including necessary watering, weeding, pruning, and replacement, by the developer or property owner. Maintenance of the street trees shall be the continuing obligation of the abutting property owner.
12. The approved landscape plan shall not expire unless the Planned Development is substantially amended, at which time a new landscape plan would be required. As provided in Section 17.57.070(C), minor changes in the landscape plan, such as like-for-like replacement of plants, shall be allowed, as long as they do not alter the character and aesthetics of the original plan. It shall be the Planning Director's decision as to what constitutes a major or minor change. Major changes to the landscape plan shall be reviewed and approved by the Landscape Review Committee.

### **VR 3-23 Conditions of Approval**

1. The variance to the 35' height limit in Ordinance 4709 is approved for a maximum of 37' to the buildings as proposed. The variances are specific to the approved plan for PDA 5-23, incorporating any revisions and conditions of approval. Any major changes to the plan shall require reconsideration of the variances.
2. Approval of the requested flexibility to the standards in Chapter 17.11 is based on the proposed plan. Any major changes to the plan shall require reconsideration of the approved reductions.

### **III. ATTACHMENTS:** *(on file with the Planning Department)*

1. PDA 5-23 Application and Attachments
2. TML 4-23 Application and Attachments
3. L 38-23 Application and Attachments
4. VR 3-23 Application and Attachments
5. Comments from Fire Marshal
6. TIA Review Comments from City Traffic Consultant
7. Comments from ODOT

### **IV. COMMENTS:**

#### ***Agency Comments***

This matter was referred to the following public agencies for comment: McMinnville Fire District, Police Department, Engineering Department, Building Department, Parks Department, Public Works Department, Waste Water Services, City Manager, and City Attorney; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Frontier Communications; Comcast; Recology; Oregon Department of State Lands; Oregon Department of Transportation; and Northwest Natural Gas. The following comments were received:

- **ODOT**

Attached are ODOT's comments for the Norton Landing proposal (*See attachment*). We don't need any further analysis for this location.

- **McMinnville Engineering Department**  
**Suggested Conditions of Approval**

1. The applicant will enter into a Construction Permit Agreement with the City's Engineering Department.
2. Provide the City with an approved 1200C Permit from DEQ.
3. ADA Sidewalk and Driveway Standards are now being applied to all new construction and remodels. These standards are intended to meet the current ADA Standards as shown in the "PROWAG" Design Guidelines. The standards can be found at the following webpage: <https://www.access-board.gov/files/prowag/PROW-SUP-SNPRM-2013.pdf> prior to final occupancy, the applicant shall construct new driveways and sidewalks in the right-of way that conform to these standards.
4. Street grades and profiles shall be designed and constructed to meet the adopted Land Division Ordinance standards and the requirements contained in the Public Right-of-Way Accessibility Guidelines (PROWAG). Additionally, corner curb ramps shall be constructed to meet PROWAG requirements. ADA Ramps are required at the SE Stratus Ave and SE Norton Ln intersection.
5. SE Norton Ln shall be improved to minor collector standard per the Transportation System Plan and have the City's typical crowned section.
6. The applicant shall secure all required state and federal permits, including, if applicable, those related to construction of the storm drain outfalls, the federal Endangered Species Act, Federal Emergency Management Act, and those required by the Oregon Division of State Lands, U.S. Army Corp of Engineers, and DEQ. Copies of the approved permits shall be submitted to the City prior to the City issuing permits.
7. That the applicant submit evidence that all fill placed in the areas where building sites are expected is engineered. Evidence shall meet with the approval of the City Building Division and the City Engineering Division.
8. That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private, rather than public, and private maintenance agreements must be approved by the City for them.
9. The final development plans for the subject site include a detailed sanitary sewerage collection plan which incorporates the requirements of City's Collection Systems Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the structures located within the subject site are required to connect to the sanitary sewer systems as soon as service is available.
10. As identified on sheet C1.0 SE Norton Ln – Preliminary Plan page 292 of the PDA in detail "Typical Section – Norton Lane (North)" the frontage improvement will include a 2" grind and inlay to the existing crown of the street.

11. As identified on sheet C1.0 SE Norton Ln – Preliminary Plan page 292 of the PDA in detail “Typical Section – Norton Lane (South)” the frontage improvement will include a three quarter Street improvement consisting of a 40’ from curb to curb and curb and gutter on the east side of the street.
12. Width of improved SE Norton Ln curb to curb should be typically 40’.
13. Existing gate at south terminus of Norton Ln will need to be relocated or replaced to the new terminus to provide access for the farmland. Additionally, a new barricade may need to be installed at terminus per City Standard Detail 29.
14. The sidewalk width on Norton Ln should be 10 feet.

- **McMinnville Fire District**

*See attachment for full comments. Excerpts are summarized below.*

- The Fire District has requested verification of the east to west dimension of the “hammerhead” turnaround. 70’ minimum is required.
- Development of multiple-family residential projects with more than 100 dwelling units...and other factors shall be provided with not less than two approved means of fire department access. Exceptions may be allowed for an approved automatic sprinkler system. The installation of attic fire sprinkles will negate this requirement.

- **McMinnville Building Department**

N/A

- **McMinnville Water & Light**

N/A

- **Comcast**

Comcast has no conflict or comment for this project.

### ***Public Comments***

No written public comments have been received as of October 26, 2023.

### **V. FINDINGS OF FACT - PROCEDURAL FINDINGS**

1. The applicant held a noticed neighborhood meeting on June 28, 2023.
2. The applicant submitted the applications with payment made on August 14, 2023.
3. The applications were deemed incomplete on September 13, 2023. The applicant submitted additional information, and the applications were deemed complete on October 2, 2023.
4. On October 2, 2023, notice of the applications was referred to the following public agencies for comment in accordance with Section 17.72.120 of the Zoning Ordinance: McMinnville Fire Department, Police Department, Engineering Department, Building Department, Parks Department, Public Works Department, Waste Water Services, City Manager, and City Attorney; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Frontier Communications; Comcast; Recology; Oregon Department of State Lands; Oregon Department of Transportation; and Northwest Natural Gas.

Comments received from agencies are addressed in Section IV of this Decision Document.

5. On October 3, 2023, notice of the applications and the November 2, 2023 Planning Commission public hearing was mailed to property owners within 300 feet of the subject property in accordance with Section 17.72.120 of the Zoning Ordinance.
6. Notice of the applications and the November 2, 2023 Planning Commission public hearing was published in the News Register on Friday, October 27, 2023, in accordance with Section 17.72.120 of the Zoning Ordinance.
7. On November 2, 2023, the Planning Commission held a duly noticed public hearing to consider the applications.

## **VI. FINDINGS OF FACT - GENERAL FINDINGS**

1. **Location:**
  - a. Address: South end of SE Norton Lane on the west side
  - b. Map & Tax Lot: R4427 00701
2. **Size:** Approximately 4.93 acres
3. **Comprehensive Plan Map Designation:** Commercial
4. **Zoning:** C-3 PD (General Commercial with Planned Development Overlay, Ordinance 4709 as amended by Ordinances 4863 and 5072)
5. **Overlay Zones/Special Districts/Area Plans:**
  - a. Three Mile Lane Planned Development Overlay
  - b. Airport Overlay Zone, Horizontal Surface
  - c. Three Mile Lane Area Plan
6. **Current Use:** Undeveloped
7. **Inventoried Significant Resources:**
  - a. **Historic Resources:** None
  - b. **Other:** None identified
8. **Other Features:**
  - a. **Slopes:** The site is generally flat.
  - b. **Easements:** Aviation easement recorded in 2002, instrument 200211460, 15' storm drainage easement along west property line; 10' PUE along frontage.
9. **Utilities:** Sanitary sewer is present in Norton Lane along the northerly portion of the frontage. Water extension will be necessary to serve the property.
10. **Transportation:** The subject property has frontage on the SE Norton Lane right-of-way, which is a minor collector. The street improvement end near the southerly access to the hospital.

## **VII. CONCLUSIONARY FINDINGS:**

The Conclusionary Findings are the findings regarding consistency with the applicable criteria and standards for the application.

### **McMinnville Zoning Ordinance**

The following Sections of Title 17, Zoning Ordinance, of the McMinnville Municipal Code provide criteria and standards applicable to the requests:

**Planned Development Amendment PDA 5-23 - Criteria**

The applicable criteria for a Planned Development Amendment are specified in Section 17.74.070 of the Zoning Ordinance.

**17.74.070 Planned Development Amendment - Review Criteria.** *An amendment to an existing planned development may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Major changes to an adopted site plan shall be processed in accordance with Section 17.72.120, and include the following:*

- *An increase in the amount of land within the subject site;*
- *An increase in density including the number of housing units;*
- *A reduction in the amount of open space; or*
- *Changes to the vehicular system which results in a significant change to the location of streets, shared driveways, parking areas and access.*

*An amendment to an existing planned development may be authorized, provided that the proposal satisfies all relevant requirements of this ordinance, and also provided that the applicant demonstrates the following:*

**FINDING: SATISFIED WITH CONDITIONS.** A Planned Development includes two components: (1) establishment of the Planned Development Overlay Zone with any special standards and provisions that modify any provisions of the underlying zoning regulations, and (2) approval of a Master Plan for the site. These can be adopted concurrently or the overlay zone can be established first followed by later approval of the master plan. For this property, the overlay zone was approved, and this amendment is to approve the master plan as required by the Zoning Ordinance and the Planned Development Overlay Ordinance. The criteria for applying a Planned Development with special provisions to this property were already met when the Planned Development Ordinance was adopted and subsequently amended. Conditions are approval are included to satisfy the criteria and ensure consistency with applicable zoning regulations and overlay zone provisions.

- A. *There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;*

**Applicant Response:** The current Planned Development Overlay and previously approved Planned Development Amendment, Ordinance 5072, did not include a Master Plan, or specific Site Plan of the Subject Site. Therefore, the proposed Site Plan and Development is subject to the review and the approval criteria for a Planned Development Amendment.

**FINDING, SUBSECTION (A): SATISFIED WITH CONDITIONS.** The Planned Development overlay was established concurrent with a rezone to commercial zoning. In general, the Planned Development ordinance established provisions that were more restrictive than the standard regulation requirements of the C-3 zone, including more restrictive provisions governing permitted uses and building height.

With this proposed amendment to adopt a Master Plan, the applicant is proposing a plan that substantially addresses the Zoning Ordinance requirements for the proposed use and development, the provisions of the underlying zone, and the more restrictive Overlay Zone provisions. The application is also subject new regulations that weren't in effect at the time the Planned Development overlay was established, including new residential design and development standards and new policies in the recently adopted Three Mile Lane Area Plan.

As part of the Planned Development Amendment the applicant is requesting flexibility for minor deviation from some measurable standards and those are addressed under the applicable provisions of these findings.

- B. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;*

**Applicant Response:** The proposed Planned Development Amendment and Development application has been designed to be consistent with the Comprehensive Plan objectives, Goals and Policies. Narrative addressing compliance of the Goals and Policies is included as a part of this overall application.

**FINDING: SATISFIED WITH CONDITIONS.** In the respective sections below, findings have been made regarding consistency with the Goals and Policies in Volume II of the Comprehensive Plan and the objectives of the area as provided in the Three Mile Lane Area Plan and the Three Mile Lane Planned Development Overlay Ordinance.

- C. The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;*

**Applicant Response:** The subject site is adjacent to existing development and the City of McMinnville city limits and the Urban Growth Boundary. The existing Altimus Medical Plaza Building complex is adjacent to the North, the Willamette Valley medical Center Hospital is adjacent Across SE Norton Lane to the East, the Evergreen Mobile Home Park is adjacent to the West and the city limits and UGB is adjacent to the South.

The proposed Site Plan and site design provides for the extension and Public improvements to SE Norton Lane for anticipated future development to the South.

**FINDING: SATISFIED WITH CONDITIONS.** Surrounding properties within the UGB are already developed or approved for development, including provision of services. Conditions of approval address requirements for utility connections and frontage improvements.

There is an existing 10' PUE along the frontage and a storm drain easement along the west side of the property.

This is the last property at the end of Norton Lane within the UGB. The property has frontage on existing Norton lane right-of-way and will need to make frontage improvements. The sewer in Norton Lane is relatively shallow at this location, and any future plans for service further to the south if land is added to the UGB at that location would need to be addressed should such land be added.

- D. The plan can be completed within a reasonable period of time;*

**Applicant Response:**

The proposed Development will be completed in a timely manner. The owners anticipate the start of construction as soon as permits can be obtained following the land-use approval process. Construction is anticipated to begin spring of 2024.

**FINDING: SATISFIED.** The applicant's timeline demonstrates completion within a reasonable period of time.



- E. *The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;*

**Applicant Response:** The subject site is located on SE Norton Lane at the intersection of SE Stratus Avenue. SE Norton Lane, which is classified as a minor collector, provides direct access to Highway 18, which is classified as a major arterial. This network of roadways provides adequate access for vehicular traffic serving the proposed development.

A Transportation Impact Study has been prepared and is included with the overall land-use application and Planned Development Amendment application.

**FINDING: SATISFIED WITH CONDITIONS.** The applicant has conducted a Traffic Impact Analysis (TIA) that demonstrates compliance with applicable performance standards. The City's transportation consultant has reviewed the TIA and found that it met technical requirements and addressed required criteria. While the property doesn't have frontage on an ODOT facility, ODOT also reviewed the TIA. The review letters from the City's consultant and ODOT are included as attachments. Conditions of approval are included regarding frontage improvements.

- F. *Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;*

**Applicant Response:** The proposed Site Plan and proposed site improvements will provide adequate utility and drainage services for the scope of the project and proposed number of living units.

There is an existing 10" public water main located with the SE Norton Lane right-of-way. A proposed 4" domestic water service and 6" fire suppression service will be installed on-site connecting to the existing public water main.

There is an existing 8" public sanitary service line located within the SE Norton Lane right-of-way. A new private sanitary service line will be installed throughout the site to serve the proposed buildings and living units. Due to existing grade elevations, a sanitary lift station will be provided on the subject site for gravity drainage to the public line.

There is an existing 15-foot private drainage easement running north-south along the westerly boundary of the subject site. A new on-site storm drainage system is proposed beneath the drive aisles, including required underground stormwater storage-detention before discharging into a riprap stormwater discharge facility. The stormwater discharge will be at pre-development flow rates in accordance with the State of Oregon Drainage laws.

**FINDING: SATISFIED WITH CONDITIONS.** Affected agencies and departments have reviewed the proposed development plans. Subject to conditions of approval, including requirements for provision of utilities and requirement addressing drainage, the development will have adequate utility and drainage facilities.

- G. *The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.*

**Applicant Response:** The proposed development of multi-family apartments units is consistent with the anticipated uses of the subject site as identified in the Comprehensive

Plan and the Three Mile Lane Area Plan. As a proposed multifamily development, the use will not generate or produce excessive noise or air pollution nor will it generate water pollution having adverse effects upon the surrounding community of the city.

**FINDING: SATISFIED.** There are no aspects of this apartment development as a Planned Development that are substantively different than would otherwise occur if the site was developed without a Planned Development overlay as a permitted use in the underlying C-3 zone for this property. There are no unique noise, air, or water pollutants associated with this residential development.

**Planned Development Ordinance (Ordinance 4709, Amended by Ordinances 4863 and 5072):**

**ORDINANCE 4709, AS AMENDED BY ORDINANCE 4863 AND 5072: CONDITIONS OF APPROVAL:**

1. *That the zone change request shall not take effect until and unless CPA 6-99 is approved by City Council.*

**Applicant Response:** This condition of approval is no longer applicable and Ordinance 4709 was approved by City Council.

**FINDING: SATISFIED.** This action was previously completed.

2. *That the development and use of the site is subject to the provisions of the Three Mile Lane Planned Development Ordinance, as amended. Further, that a detailed Master Plan for the subject site shall be submitted to the McMinnville Planning Commission for review and approval, pursuant to the procedures of McMinnville Zoning Ordinance 17.51, Planned Development Overlay, prior to any development occurring on the site. The plan shall include, at a minimum, proposed land uses and their locations, building locations, proposed circulation patterns, proposed open spaces, grading and drainage information, location and size of public utilities and services, off-street parking areas, direct pedestrian access, and other information deemed necessary to convey the details of the proposed development plans to the Planning Commission.*

**Applicant Response:** This current application is for a Planned Development Amendment to include the proposed Master Plan for the site. The application includes the detailed drawings and design as outlined in the above condition of approval. This application also includes the application for the Three Mile Lane Design Review.

**FINDING: SATISFIED.** The proposal addresses the applicable submittal requirements and procedures. The proposal is reviewed for consistency with all applicable requirements include the Three Mile Lane Ordinance 4131 as amended.

3. *That detailed plans for the proposed commercial development showing site layout, proposed circulation pattern, signage, building elevations, landscaping, parking, and lighting must be submitted to and approved by the Three Mile Lane Design Review Committee prior to issuance of any building permits for said development. Approval or denial of such plans shall be based upon findings that, to the extent possible, the building and site design employs principles that will ensure compatibility with adjacent development, and provide an architectural style appropriate to a "gateway" or entrance to the City of McMinnville. In addition, approval or denial of the exterior building design shall be based on a finding that, to the extent possible, the building design incorporates design and architectural features that would serve to break up the building's horizontal plane and provide visual interest. This may include, but is not limited to, the use of vertical columns, gables, variety of compatible and*

*complementary building materials, providing openings in the building façade, and landscaping at the building perimeter. Submitted plans should include detail as regard to building colors and materials (provide texture and relief), building height, planting design, window treatment, vertical and horizontal articulation, massing, voids to solids relationships, and other elements appropriate to ensure that the building and site design complies with the objectives and requirements of this planned development approval.*

*The provisions of Chapter 17.51 of the McMinnville Zoning Ordinance may be used to place conditions of any development and to determine whether or not specific uses are permissible. The applicant may appeal the decisions of the Three Mile Lane Design Review Committee to the Planning Commission if notice of the appeal is filed in the Planning Commission office within 15 days of the Committee's decision.*

**Applicant Response:** This application is for multifamily housing and is not a commercial proposal. Therefore this condition of approval is not applicable.

**FINDING: SATISFIED WITH CONDITIONS.** This condition was established before the Ordinance was amended to include multi-dwelling development as a permitted use for the property. The applicable issues above for multi-dwelling development are addressed through consistency with the provisions of the Three Mile Lane Overlay Zone Ordinance 4131 as amended, the policies of the Three Mile Lane Area Plan, and the residential design and development standards. The TML review is part of this concurrent review.

4. *That final development plans include landscape plans submitted to and approved by the McMinnville Landscape Review Committee and Three Mile Lane Design Review Committee. A minimum of 14 percent of the site must be landscaped. An arborvitae hedge or similar type of planted visual screen shall be required along the property lines adjacent to the residentially zoned land to the west. Landscaping emphasis shall exist along the site's eastern edge, adjacent to Norton lane, including required street trees, with particular emphasis at the site's eastern driveway intersections at Norton Lane. In addition, landscape islands are required to be located throughout the proposed off-street parking areas.*

*Street trees within a curbside planting strip along the Norton Lane frontage are required to have a two-inch minimum caliper, exhibit size and growing characteristics appropriate for the particular planting strip and be spaced as appropriate for the selected species and as may be required for the locations of above ground utility vaults, transformers, light poles, and hydrants. All street trees shall be of good quality and shall conform to American Standards for Nursery Stock (ANSI Z60.1). The Planning Director reserves the right to reject any plant material which does not meet this standard.*

**Applicant Response:** This proposed development and application includes the Landscape Plan Review application. The landscape design for the site includes the required screening and buffering to the adjacent properties along the North, West and South property lines. There are also enhanced plantings along the SE Norton Lane frontage.

The proposed development application also includes the SE Norton Lane street frontage improvements. These include the required street trees. The proposed street trees meet the specified requirements.

**FINDING: SATISFIED WITH CONDITIONS.** This condition is addressed as part of the Landscape Plan Review. The 14% coverage requirement is higher than required for commercial development but less than required for multi-dwelling development, and the landscape plan review addresses all applicable requirements, including the most restrictive provisions. That review is part of this concurrent review.

5. *That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private, rather than public, and private maintenance agreements must be approved by the City for them.*

**Applicant Response:** This application includes a Preliminary Grading and Drainage Plan, drawing C2.0. It is understood that complete drainage design must be provided in accordance with the City of McMinnville Storm Drainage Master Plan. Detailed drainage design and stormwater analysis will be provided for permit submittal to the City Engineering Department.

**FINDING: SATISFIED WITH CONDITIONS.** As a condition of approval, the detailed drainage plan with all applicable calculations must be submitted for review and approval prior to issuance of development permits.

6. *The final development plans for the subject site include a detailed sanitary sewerage collection plan which incorporates the requirements of City's Collection Systems Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the structures located within the subject site are required to connect to the sanitary sewer systems as soon as service is available.*

**Applicant Response:** This application includes a Preliminary Utilities Plan, drawing C3.0. It is understood that complete sanitary sewerage design must be provided in accordance with the City of McMinnville Collection Systems Plan. Detailed sanitary sewerage design and analysis will be provided for permit submittal to the City Engineering Department.

**FINDING: SATISFIED WITH CONDITIONS.** As a condition of approval, the detailed sanitary plan must be submitted for review and approval prior to issuance of development permits.

7. *That the developer secure from the Department of Environmental Quality (DEQ) applicable storm runoff and site development permits prior to construction of the required site improvements. Evidence of such permit shall be submitted to the City Engineer.*

**Applicant Response:** It is understood that the owner/developer must secure the applicable Department of Environmental Quality (DEQ) stormwater runoff permits. Evidence of the permits will be submitted to the City Engineer as required.

**FINDING: SATISFIED WITH CONDITIONS.** This requirement is incorporated as a condition of approval.

8. *That the developer enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and grading permit for a lot fill agreement from the City Building Division. All fill placed in the areas where construction is expected shall be engineered and shall meet with approval of the City Building Division and the City Engineering Department.*

**Applicant Response:** It is understood that the owner/developer must enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and grading permit for a lot fill agreement from the City Building Division. It is also understood that all fill placed in the areas where construction is expected

shall be engineered and shall meet with approval of the City Building Division and the City Engineering Department.

**FINDING: SATISFIED WITH CONDITIONS.** This requirement is incorporated as a condition of approval.

9. *That the developer extend water and power services to the subject site in accordance with McMinnville Water and Light requirements, including any necessary contracts and/or easements. Said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.*

**Applicant Response:** It is understood that the owner/developer extend water and power services to the subject site, if required by McMinnville Water and Light. The required improvements, if any, will be designed and installed to the requirements of McMinnville Water and Light. Although, there is an existing public water main located within the NE Norton Lane right-of-way and power currently serves the adjacent property to the north.

It is further understood that said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.

**FINDING: SATISFIED WITH CONDITIONS.** This requirement is incorporated as a condition of approval.

10. *That utilities shall be extended to the property boundaries by the applicant, as may be required by the City Engineer or McMinnville Water and Light.*

**Applicant Response:** It is understood that McMinnville Water and Light may require utility extensions to the subject site, if not currently provided. The required improvements, if any, will be designed and installed to the requirements of McMinnville Water and Light. Although, there is an existing public water main located within the NE Norton Lane right-of-way in front of the subject site and power currently serves the adjacent property to the north.

**FINDING: SATISFIED WITH CONDITIONS.** This requirement is incorporated as a condition of approval.

11. *That no building shall exceed the height of 35 feet.*

**Applicant Response:** The Applicant is proposing a Variance through the Planning Commission Authority to request approval to allow a roof height of 37 feet to the highest ridge line. The detailed response to the requested Variance is included within the Variance Application Narrative.

**FINDING: SATISFIED WITH CONDITIONS.** The Planned Development Ordinance establishes a more restrictive height limit than the height limits of the C-3 zone. In the variance application, the applicant has requested a variance to exceed this 35' height limit to 37' to the top of highest ridgeline. Administrative variances allow up to a 10% deviation from certain standards, but can't be applied to building height. Therefore, the applicant has addressed the standard variance criteria, provided in the respective section of the findings. Compliance with this condition is contingent on approval of the requested variance.

12. *That if outside lighting is provided, it must be directed down and away from residential areas and public streets.*

**Applicant Response:** The proposed site lighting design is included in this application, drawing ME1.0. The proposed lighting design provides full cut-off light fixture that prevent light from entering the adjacent properties and the lighting design does not shine light onto the SE Norton Lane right-of-way.

**FINDING: SATISFIED WITH CONDITIONS.** As noted, the applicant has submitted a lighting plan addressing the items noted above. As a condition, the applicant shall also verify there are no outward shining building mounted "Wall-Pack" types of fixtures unless appropriately shielded.

13. *That signs located on the site shall be subject to the requirements of McMinnville Ordinance 4572 (B).*

**Applicant Response:** It is understood that any proposed signage must be designed to be in compliance with the City of McMinnville Ordinance 4572 (B). A small site identification sign is proposed and the location is noted on the Site Plan, drawing A1.0 and A1.1.

**FINDING: SATISFIED WITH CONDITIONS.** At the time of sign permit application, any signage will be reviewed for consistency with the sign provisions of the Zoning Ordinance and Ordinance 4572. Please note that freestanding signs other than a monument sign are not authorized on the Norton Lane frontage.

14. *That all business, storage, or displays shall be conducted wholly within an enclosed building; except for off-street parking and loading.*

**Applicant Response:** The proposed development is for multifamily housing and this condition of approval is not applicable.

**FINDING: SATISFIED WITH CONDITIONS.** This condition continues to apply, so no outside storage is to occur.

15. *That the subject site is limited to professional office use or medical office use, or multiple family dwellings, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, multiple family dwellings, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site. (As amended by Ordinances 4863 and 5072)*

*If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped."*

**Applicant Response:** This condition of approval has been amended to include multifamily housing. The revised condition was included in Ordinance 5072 (PDA 6-18). A response to the amended condition of approval follows in the response to address Ordinance 5072 below.

**Applicant Response:** The proposed development is for 138 units of multiple family dwellings (apartments) and is therefore a complying use.

The subject site is 214, 759 sf. The proposed Site Plan provides 32,769 sf of common open space, which is 15% of the gross site area and is therefore compliant.

**FINDING: SATISFIED WITH CONDITIONS.** The use is a permitted use, and the site plan and calculations demonstrate consistency with the open space requirements.

16. *That if restrictive covenants are proposed for the development they must meet with the approval of the Planning Director.*

**Applicant Response:** There are no restrictive covenants proposed with this development application. This condition of approval is not applicable.

**FINDING: NOT APPLICABLE.**

17. *That the approved Master Plan as approved by the Planning Commission shall be placed on file with the Planning Department and become part of the zone and binding to the owner and developer. The developer will be responsible for requesting permission of the Planning Commission for any major changes of the details of the final development plans. Minor changes to the details of the adopted plan may be approved by the City Planning Director. It shall be the Planning Director's decision as to what constitutes a minor or major change. An appeal from a ruling by him may be made only to the Commission. Review of the Planning Director's decision by the Planning Commission may be initiated at the request of any one of the commissioners.*

**Applicant Response:** The requirements of this condition of approval are understood by the developer/owner.

**FINDING: SATISFIED WITH CONDITIONS.** The final plan, addressing any conditions or approval or revisions shall be placed in file.

18. *That improvements to the Norton Lane frontage, to the southernmost entry drive, or as may otherwise be required by the City of McMinnville, shall be done at the developer's expense and be finalized prior to release of any occupancy permits. Plans for improvement to Norton Lane shall be submitted to the City Engineer for review and approval prior to its construction.*

**Applicant Response:** It is understood that SE Norton Lane frontage improvements are required for the proposed development and that those improvements will be at the owner/developers expense. It is further understood that the design of the proposed SE Norton Lane improvements shall be submitted to the City engineer for review and approval. It is also understood that the frontage improvements must be completed before any occupancy permits will be issued.

**FINDING: SATISFIED WITH CONDITIONS.** Detailed civil plans shall be submitted for frontage improvements to North Lane for the full frontage of the property for a "3/4" street improvement as a condition of approval.

19. *The developer/owner of the subject site must grant to the Willamette Valley Medical Center and the City of McMinnville the right to cause all airspace above the surface of the subject site such as noise, vibrations, fumes, dust, fuel particles and all other effects as may be caused by the operations of aircraft landing at or taking off or operating at or on the Medical Center*

*property and the McMinnville Municipal Airport. The owner must fully waive any right or cause action which he may now or in the future arise against the Willamette Valley Medical Center and the City of McMinnville due to such circumstances.*

**Applicant Response:** The owner/developer understands the requirements of this condition of approval.

**FINDING: SATISFIED WITH CONDITIONS.** This requirement shall be addressed as a condition of approval prior to development.

20. *That prior to the development of the site, the applicant shall sign a waiver of remonstrance against the future development of Norton Lane. The waiver shall be prepared by the City.*

**Applicant Response:** The owner/developer understands the requirements of this condition of approval.

**FINDING: SATISFIED WITH CONDITIONS.** This shall be required as a condition of approval unless the City determines that the frontage improvements to Norton Lane sufficiently fulfill this obligation.

#### **ORDINANCES 4863 AND 5072: CONDITIONS OF APPROVAL:**

Ordinance 5072 amended the conditions of approval in Ordinance 4709, as previously amended by Ordinance 4863. Those revisions and responses are incorporated above. Ordinance 5072 also specified that all other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect. Those provisions are addressed above.

#### **Three Mile Lane Review (TML 4-23), Subject to Ordinance 4131, as amended by Ordinance 4572 and subsequent ordinances**

*Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit "A":*

- A. The goals and policies of the McMinnville Comprehensive Plan, Volume II, applicable regulations and standards in Volume III, and other City codes shall be adhered to.*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED WITH CONDITIONS.** Findings regarding the Goals and Policies in Volume II of the Comprehensive Plan are addressed in the respective sections of this decision document. Findings regarding applicable implementing regulations and standards that comprise Volume III of the Comprehensive Plan are also addressed in the respective sections of this decision document.

- B. A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.*

**APPLICANT RESPONSE:** No response.

**FINDING: NOT APPLICABLE.** The subject property is not within 120 feet of the centerline of the highway.

- C. Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:*



1. *The minimization of entrances onto Three Mile Lane;*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED.** The property doesn't have frontage on the highway or frontage road. The only proposed access is a single driveway approach to Norton Lane.

2. *The development of on-site circulation systems, connecting to adjoining properties, including public frontage roads;*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED.** The pattern of existing development and development approvals limits opportunities for connections to adjoining properties. Property to the west is developed, property to the north/east is developed, and property to the north/west is approved for development. Property to the south is outside the UGB.

Staff, Fire District staff, and representatives of the property owner and property owners to the north previously met and evaluated potential opportunities for connectivity but those were subsequently ruled out.

3. *The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED/NOT APPLICABLE.** The TIA didn't find need for required mitigation along the property frontage or off-site

4. *The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED WITH CONDITIONS.** The property doesn't have access to a frontage road. Norton Lane is a minor collector. To the north, Norton Lane is narrower than the current standards. Along this frontage, the street improvement width to minor collector standards includes width for bike lanes. The route to the future greenway system identified in the Three Mile Lane Area Plan would be from the property via Norton Lane to the greenway.

For on-site circulation, the plan has an S-shaped parking lot layout. There aren't dedicated bike facilities. The sidewalk configuration includes a direct 5-foot sidewalk connection that bisects the site with an east-west connection from the west end of the site to Norton Lane. As a condition of approval, this shall be 10-feet to accommodate both bikes and pedestrians.

- D. Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.*

**APPLICANT RESPONSE:** No response.

**FINDING: NOT APPLICABLE.** The property doesn't have frontage on the highway or frontage road.

*E. Mixed housing-type residential developments shall be allowed and encouraged in those areas designated as residential.*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED.** The subject property is zoned C-3 PD, and the Planned Development Ordinance 5072 and underlying C-3 zone permit apartment/multi-dwelling development.

*F. Temporary signage shall be allowed as per Section 17.62.060(B) (3) of the McMinnville Zoning Ordinance.*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED/NOT APPLICABLE.** No temporary signage is proposed, but would be authorized as specified in the Zoning Ordinance.

*Section 5. Signs.*

...

*Section 6. Procedures for Review*

...

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED WITH CONDITIONS.** The applicant will need to submit a sign permit application prior to any signage. The Planning Director will review signage for consistency with the provisions of the Three Mile Lane Planned Development Ordinance as well as the provisions of Ordinance 5072 and Chapter 17.62 of the Zoning Ordinance.

### **Landscape Plan (L 38-23)**

#### **17.57.050(C)**

*The landscaping plan shall be approved if it is found to be compatible with the purpose, intent, and requirements of this chapter.*

**17.57.010. Purpose and intent.** *The purpose and intent of this Chapter is to encourage and, where appropriate, require the use of landscape elements, particularly plant materials, in proposed developments in an organized and harmonious manner that will enhance, protect and promote the economic, ecological and aesthetic environment of McMinnville. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:*

*A. Provide guidelines and standards that will:*

- 1. Reduce soil erosion and the volume and rate of discharge of storm water runoff.*
- 2. Aid in energy conservation by shading structures from energy losses caused by weather and wind.*
- 3. Mitigate the loss of natural resources.*
- 4. Provide parking lot landscaping to reduce the harmful effects of heat, noise and glare associated with motor vehicle use.*
- 5. Create safe, attractively landscaped areas adjacent to public streets.*
- 6. Require the planting of street trees along the City's rights-of-way.*
- 7. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character and value of existing neighborhoods.*
- 8. Provide shade, and seasonal color.*

9. *Reduce glare, noise and heat.*

- B. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.*
- C. Unify development and enhance and define public and private places.*
- D. Preserve existing mature trees.*
- E. Enhance the urban forest and tree canopy.*
- F. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.*
- G. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.*
- H. Support McMinnville as a community that cares about its appearance.*

*It is further recognized that landscaping increases property values, attracts potential residents and businesses to McMinnville, and creates safer, more pleasant living and working environments for all residents and visitors to the city.*

*The guidelines and standards contained in this chapter serve to help McMinnville realize the objectives noted above. These guidelines and standards are intended as minimum standards for landscape treatment. Owners and developers are encouraged to exceed these in seeking more creative solutions both for the enhanced value of their land and for the collective health and enjoyment of all citizens of McMinnville. The landscaping provisions in Section 17.57.070 are in addition to all other provisions of the zoning ordinance which relate to property boundaries, dimensions, setback, vehicle access points, parking provisions and traffic patterns. The landscaping objectives shall also seek to accomplish the purposes set forth in Section 17.03.020. (Ord. 5027 §2, 2017; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).*

**APPLICANT'S RESPONSE:**

- A1. Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.
- A2. Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.
- A3. Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.
- A4. The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.
- A5. Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.
- A6. Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.

A7. The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adjacent to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.

A8. The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.

A9. The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.

Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.

B. The proposed Landscape Plan and design provides trees, buffer and screening plantings along the site perimeter. This will buffer the effects of on-site parking area and building lighting from the adjacent abutting properties. The overall landscape plan and development also aids in buffering sound both from the subject site and from the adjacent abutting properties.

C. The proposed Landscape Plan and design will provide street trees along the SE Norton Lane street frontage. The landscape design also provides screening of private spaces for the living units and the proposed open common areas on site.

D. The existing undeveloped site was historically agricultural uses and there are no existing trees.

E. The proposed Landscape Plan and design provides a variety of trees that will enhance and contribute to the overall urban tree canopy.

F. The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.

G. A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.

The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically branched and high canopy structure for the interphase between public and private space.

Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

H. The proposed Landscape Plan design provides an esthetically pleasing and attractive landscape design. This overall site and landscape design enhances the character of the existing neighborhood and provides a livable community environment.

**FINDING: SATISFIED.** The proposed landscape plan is reviewed against the standards of Chapter 17.57 to address consistency with, and demonstrate compliance with the purpose, intent, and objectives identified in Section 17.57.010. There are no existing trees on site to be preserved.

17.57.070 Area Determination—Planning factors.

A. *Landscaping shall be accomplished within the following ranges:*

...

*2. Commercial, at least seven percent of the gross area. This may be reduced to not less than five percent upon approval of the review committee. (The gross area to be landscaped may only be reduced by the review committee if there is a showing by the applicant that the intent and purpose of this chapter and subsection B of this section are met.)*

*3. Multiple-dwelling, twenty-five percent of the gross area. This may be reduced to not less than fifteen percent upon approval of the review committee. (The gross area to be landscaped may only be reduced by the review committee if there is a showing by the applicant that the intent and purpose of this chapter and subsection B of this section are met.)*

...

**Applicant Response:** The proposed Site Plan and Landscape Plan provide 30 percent of the site area in landscaping.

**FINDING: SATISFIED.** The applicant's plans and calculations demonstrate compliance.

*4. A parking lot or parking structure built in any zone providing parking spaces as required by the zoning ordinance shall be landscaped in accordance with the commercial requirements set forth above in subsection 2 of this section.*

**Applicant Response:** The proposed Site Plan and Landscape Plan provide parking lot landscaping as identified in 17.11.090 Residential Design Standards for Apartments.

**FINDING: SATISFIED.** The analysis and plans provided by the applicant demonstrate the site exceeds the 7% commercial requirement as well as the applicable multi-dwelling landscape requirement and the landscaping requirements in Chapter 17.11 for multi-dwelling residential development.

17.57.070 Area Determination—Planning factors (cont.)

B. *The following factors shall be considered by the applicant when planning the landscaping in order to accomplish the purpose set out in Section 17.57.010. The Landscape Review Committee shall have the authority to deny an application for failure to comply with any or all of these conditions:*

1. *Compatibility with the proposed project and the surrounding and abutting properties and the uses occurring thereon.*

**Applicant Response:** The adjacent uses: residential, commercial and agriculture would not be adversely affected by the proposed multi-family development. Uses to the north, south and west of the site will be sufficiently screened with a site obscuring fence and evergreen hedge. Additionally, the residential use to the west will be buffered with more than 15' between the property line and any structure.

**FINDING: SATISFIED.** The applicant's plans and the narrative regarding the purpose and intent as well as the above response demonstrate compatibility.

2. *Screening the proposed use by sight-obscuring, evergreen plantings, shade trees, fences, or combinations of plantings and screens.*

**Applicant Response:** Screening is proposed on north, south and west perimeters of the site. Screening will be achieved with a site obscuring fence and evergreen hedge.

**FINDING: SATISFIED WITH CONDITIONS.** At locations where screening of abutting properties is required and/or proposed, the application is proposing sight-obscuring fencing and hedge. **Note:** *the residential standards specify that the fencing is not to be chain link.*

3. *The retention of existing trees and natural areas that may be incorporated in the development of the project. The existing grade should be preserved to the maximum practical degree. Existing trees shall be provided with a watering area equal to at least one-half the crown area.*

**Applicant Response:** The site was previously open for agricultural use and there are no existing trees on site.

**FINDING: SATISFIED/NOT APPLICABLE.**

4. *The development and use of islands and plantings therein to break up parking areas.*

**Applicant Response:**

A total of 37 parking lot islands are used to break up parking areas. Each island has a shade tree and shrubs / grasses to help break up the parking areas.

**FINDING: SATISFIED WITH CONDITIONS.** The plan includes islands and plantings with trees to break up parking areas. The applicant has requested a minor increase to the applicable parking requirement for multi-dwellings to increase the island spacing from 60 feet to 63 feet. That request would need to be approved by the Planning Commission to approve the plan as proposed.

5. *The use of suitable street trees in the development of new subdivisions, shopping centers and like developments. Certain trees shall be prohibited in parking areas: poplar, willow, fruit, nut, birch, conifer, and ailanthus.*

**Applicant Response:** None of the proposed trees are prohibited. Many of the trees are listed on the City of McMinnville street tree list, including the Green Vase Zelkova proseed along SE Norton Lane.

**FINDING: SATISFIED.** The applicant's plan and response address this requirement.

6. *Suitable watering facilities or irrigation systems must be included in or near all planted areas;*

**Applicant Response:** The proposed development will have an automatic irrigation system with a water efficient controller and rain sensor. The irrigation system will be commercial grade with separate zones for lawn and shrub areas.

**FINDING: SATISFIED.** The applicant's plan and response address this requirement

17.57.070 Area Determination—Planning factors (cont.)

C. *All landscaping approved through the Landscape Review Committee shall be continually maintained, including necessary watering, weeding, pruning, mowing, and replacement. Minor changes in the landscape plan, such as like-for-like replacement of plants, shall be allowed, as long as they do not alter the character and aesthetics of the original plan. It shall be the Planning Director's decision as to what constitutes a major or minor change. Major changes to the landscape plan shall be reviewed and approved by the Landscape Review Committee. (Ord. 5027 §2, 2017; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).*

**APPLICANT'S RESPONSE: N/A.**

**FINDING: SATISFIED WITH CONDITIONS.** A condition of approval is included to ensure compliance with this requirement.

**Comprehensive Plan Volume II:**

The implementation of the goal, policy, and proposal statements in Volume II of the Comprehensive Plan shall occur in one of two ways. First, the specific goal, policy, or proposal shall be applied to a land use decision as a criterion for approval, denial, or modification of the proposed request. In this case the goal, the policy, or the proposal is directly applied. The second method for implementing these statements is through the application of provisions and regulations in ordinances and measures created to carry out the goals and policies. This method involves the indirect application of the statements.

Certain Goals, Policies, and Proposals from Volume II of the Comprehensive Plan provide criteria applicable to this request. The implementation of many of the goals, policies, and proposals as they apply to quasi-judicial land use applications are accomplished through the provisions, procedures, and standards in the city codes and master plans, which are sufficient to adequately address applicable goals, policies, and proposals as they apply certain applications, and are not addressed below.

The following findings are made relating to specific Goals and Policies:

**CHAPTER V. HOUSING AND RESIDENTIAL DEVELOPMENT**

**GOAL V.1: TO PROMOTE DEVELOPMENT OF AFFORDABLE, QUALITY HOUSING FOR ALL CITY RESIDENTS.**

General Housing Policies



*Policy 58. City land development ordinances shall provide opportunities for development of a variety of housing types and densities.*

**Applicant Response:** The proposed development provides multifamily housing that has been designed to be in compliance with the applicable sections of the City of McMinnville Zoning Ordinance and applicable ordinances within the planned development area of the subject site.

*Policy 59. Opportunities for multiple dwelling and mobile home developments shall be provided in McMinnville to encourage lower-cost renter and owner-occupied housing. Such housing shall be located and developed according to the residential policies in this plan and the land development regulations of the city.*

**Applicant Response:** The proposed multifamily development has been designed to be in compliance with the applicable sections of the City of McMinnville Zoning Ordinance, including the Residential Design Standards for apartments, 17.11.090 and the applicable ordinances within the planned development area of the subject site.

Low-Cost Housing Policies:

*Policy 64. The City of McMinnville shall work in cooperation with other governmental agencies, including the Mid-Willamette Valley Council of Governments and the Yamhill County Housing Authority, and private groups to determine housing needs, provide better housing opportunities and improve housing conditions for low and moderate income families.*

*Policy 65. The City of McMinnville shall coordinate with the Mid-Willamette Valley Council of Governments to develop a “fair share” plan to allocate low-cost housing throughout the tri-county region.*

*Policy 66. The City of McMinnville shall continue to allow development of its fair share of the region’s low-cost housing. The share accepted will be based on quantifiable studies which take into account the amount of the low-cost housing already in the community and the overall housing opportunities in the city and region.*

*Policy 67. Subsidized low-cost housing shall be dispersed throughout the McMinnville urban area. Dispersal plans shall be coordinated with appropriate agencies.*

**Applicant Response:** The proposed development is for market-rate multifamily housing and is not proposed to be a low-income or subsidized development.

**FINDING (HOUSING GOAL V.1 AND APPLICABLE POLICIES): SATISFIED.** Most of these policies are directives to the City. The City previously approved an amendment to the Planned Development Overlay to allow multi-dwelling (apartment) development as a permitted use. The applicant is proposing housing consistent with the permitted uses authorized in the zone and the Planning Development Overlay.

**GOAL V.2: TO PROMOTE A RESIDENTIAL DEVELOPMENT PATTERN THAT IS LAND INTENSIVE AND ENERGY-EFFICIENT, THAT PROVIDES FOR AN URBAN LEVEL OF PUBLIC AND PRIVATE SERVICES, AND THAT ALLOWS UNIQUE AND INNOVATIVE DEVELOPMENT TECHNIQUES TO BE EMPLOYED IN RESIDENTIAL DESIGNS.**

Policies:

*68.00 The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center, to designated neighborhood activity centers, and to*

*those areas where urban services are already available before committing alternate areas to residential use. (Ord. 5098, December 8, 2020)*

**Applicant Response:** The subject site is designated C-3 Commercial and has been identified for Medium-High Density Residential under the Three Mile Lane Planned Development Amendment to Ordinance 4709.

The subject site has existing urban services available to the site.

*Planned Development Policies:*

*72.00. Planned developments shall be encouraged as a favored form of residential development as long as social, economic, and environmental savings will accrue to the residents of the development and the city.*

**Applicant Response:** The subject site is located within the Three Miler Lane Planned Development Overlay. The proposed development has been designed to be in compliance with the applicable ordinances within the Three Mile Lane PD overly including Ordinance 4709 and Ordinance 5072.

*Policy 73. Planned residential developments which offer a variety and mix of housing types and prices shall be encouraged.*

*Policy 74. Distinctive natural, topographic, and aesthetic features within planned developments shall be retained in all development designs.*

**Applicant Response:** The subject site does not contain any distinctive natural or topographic features.

*Policy 75. Common open space in residential planned developments shall be designed to directly benefit the future residents of the developments. When the open space is not dedicated to or accepted by the city, a mechanism such as a homeowners association, assessment district, or escrow fund will be required to maintain the common area.*

**Applicant Response:** The proposed Site Plan provides Common Open Spaces in compliance with 17.11.090. The proposed Common Open Spaces occur within the private market-rate apartment development and will be maintained by the development

*Policy 76. Parks, recreation facilities, and community centers within planned developments shall be located in areas readily accessible to all occupants.*

**Applicant Response:** The proposed Site Plan provides Common Open for recreational use by the apartment residents. There are no other recreation facilities or community centers proposed within this development.

*Policy 77. The internal traffic system in planned developments shall be designed to promote safe and efficient traffic flow and give full consideration to providing pedestrian and bicycle pathways.*

**Applicant Response:** The proposed development and Site Plan is limited to a single 4.93 acre site and does not include any internal public streets. The proposed Site Plan does include internal drive aisles and parking areas that will provide safe and efficient access to parking and the buildings throughout the site. There is a network of internal sidewalks and pathways that connect the parking, buildings and common open spaces to the public street frontage.

*Policy 78. Traffic systems within planned developments shall be designed to be compatible with the circulation patterns of adjoining properties.*

**Applicant Response:** The subject site has limited public street frontage and is limited to a single driveway onto SE Norton Lane.

*Multiple Dwelling Development Policies:*

*Policy 86. Dispersal of new multi-dwelling housing development will be encouraged throughout the city in areas designated for residential and mixed-use development to encourage a variety of housing types throughout the community and to avoid an undue concentration of multi-dwelling development in specific areas of the community leading to a segregation of multi-dwelling development in McMinnville from residential neighborhoods. Dispersal policies will be consistent with the Great Neighborhood Principles.*

*In areas where there are the amenities, services, infrastructure and public facilities to support a higher density of multi-dwelling development, and the area is commensurate with a higher concentration of multi-dwelling development without creating an unintended segregation of multi-dwelling development, such as McMinnville's downtown, the area surrounding Linfield University and neighborhood activity centers, a higher concentration of multi-dwelling development will be encouraged.*

**Applicant Response:** As provided for by the adoption of Ordinance 4709, amended by Ordinance 5072, the subject site is located in an area of McMinnville that has the existing infrastructure, public facilities to support the density of the proposed development. The proposed development will provide needed housing for the support staff of the adjacent medical facilities and the Willamette Valley medical Center Hospital, as well as other nearby employment centers.

*87.00 Residential developments at densities beyond that normally allowed in the multiple-dwelling zone shall be allowed in the core area subject to review by the City. These developments will be encouraged for (but not limited to) the provision of housing for the elderly.*

**Applicant Response:** The proposed development is within the allowable density for the subject site. The subject site is not located within the core area and this does not apply.

*88.00 Deleted as per Ord. 4796, October 14, 2003.*

*89.00 Zoning standards shall require that all multiple-dwelling housing developments provide landscaped grounds. (Ord. 4796, October 14, 2003)*

**Applicant Response:** The proposed development includes detailed Landscape Plans that have been designed for review in conjunction with all aspects of the McMinnville Zoning Codes. Multiple locations with Zoning Code require landscaping, including the 17.11 the Residential Design Standards. The proposed landscape design provides landscaping throughout all areas of the site not covered by buildings, parking or drive aisles.

*Policy 90. Greater residential densities shall be encouraged to locate along collectors and minor arterials, within one-quarter mile from neighborhood and general commercial shopping centers or within neighborhood activity centers, and within a one-half-mile-wide corridor centered on existing or planned public transit routes.*

**Applicant Response:** The subject site is located on SE Norton Lane at the intersection of SE Stratus Avenue. SE Norton Lane, which is classified as a minor collector, provides direct access to Highway

18, which is classified as a major arterial. This network of roadways provides adequate access for vehicular traffic serving the proposed development.

Yamhill County Transit, Route 2, serves SE Norton Lane with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave.

*Policy 91. Multiple-dwelling housing developments, including condominiums, but excluding campus living quarters, shall be required to access off of arterials or collectors or streets determined by the city to have sufficient traffic carrying capacities to accommodate the proposed development.*

**Applicant Response:** The subject site and proposed development has access from SE Norton Lane which is a minor collector. Norton Lane connects directly to Highway 18 that is a major arterial. The existing network of public roadways has sufficient carry capacity for the traffic generated by the proposed development.

A Transportation Impact Study has been prepared and is included in this application.

*Policy 92. High-density housing developments shall be encouraged to locate along existing or potential public transit routes.*

**Applicant Response:** The subject site is located along an existing public transit route, Yamhill County Transit, Route 2, with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave.

*Policy 92.01. High-density housing shall not be located in undesirable places such as near railroad lines, heavy industrial uses, or other potential nuisance areas unless design factors are included to buffer the development from the incompatible use.*

**Applicant Response:** The subject site is not located near railroad lines or heavy industrial uses.

*Policy 92.02. High-density housing developments shall, as far as possible, locate within reasonable walking distance to shopping, schools, and parks, or have access, if possible, to public transportation.*

**Applicant Response:** The subject site is located along an existing public transit route, Yamhill County Transit, Route 2, with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave. The subject site is not located near schools, shopping or parks.

*92.03 Housing developments for the elderly shall, as far as possible, locate near community centers, parks, and shopping areas, or where transportation services can be provided to enable access to these activity areas. (Ord. 4796, October 14, 2003)*

**Applicant Response:** The proposed project is not an Elderly Housing development.

#### Urban Policies.

*Policy 99. An adequate level of urban services shall be provided prior to or concurrent with all proposed residential development, as specified in the acknowledged Public Facilities Plan. Services shall include, but not be limited to:*

- 1. Sanitary sewer collection and disposal lines. Adequate municipal waste treatment plant capacities must be available.*
- 2. Storm sewer and drainage facilities (as required).*

3. *Streets within the development and providing access to the development, improved to city standards (as required).*
4. *Municipal water distribution facilities and adequate water supplies (as determined by City Water and Light).*

**Applicant Response:** The subject site has existing water and sanitary sewer lines located within the SE Norton Lane right-of-way. The existing sanitary sewer is an 8" main, which has adequate capacity for the proposed development.

There is an existing 10" water main located in the SE Norton Lane right-of-way. This line has adequate capacity for domestic and fire line services for the proposed development and additional new fire hydrants.

The subject site is accessed by SE Norton Lane, which currently terminates just south of the intersections with SE Stratus Avenue. The proposed development will provide public street frontage improvements along the SE Norton Lane right-of-way to the southern boundary of the site and the current McMinnville City limits.

There are no public streets proposed or required within the proposed development. The Site Plan provides a network of drive aisles and parking areas serving the buildings and living units. A Fire Truck turnaround has been provided at the rear of the site.

**FINDING (HOUSING GOAL V.2 AND APPLICABLE POLICIES): SATISFIED WITH CONDITIONS.**

*General Policies:*

With Planned Development Ordinance 5072, the City has designated multi-dwelling development of this site as a permitted use.

*Planned Development Policies:*

Some of the policies specify locational considerations for uses, and the proposed use is already authorized for the subject property. The Planned Development proposal is for development of multiple apartment buildings on a single site. This is subject to the multi-dwelling residential standards in Chapter 17.11 of the Zoning Ordinance. Some of the above policies are more applicable to a master planned development that includes a subdivision and mix of housing types.

The Planned Development Ordinance for the property outlines any special requirements that apply to the use and development of the property, which the Master Plan must adhere to in addition to the other provisions of the Zoning Ordinance and other applicable ordinances. Some of the policies above would be addressed at time of designation of a Planned Development overlay zone and incorporated as conditions that specify what the Master Plan would need to perform to.

Many of the Planned Development policies have been implemented through the residential design and development standards regarding provision of open space and landscaping of the property and distributed throughout the site to serve all residents.

As noted by the applicant, the site doesn't have distinctive features to be incorporated into the site design.

Regarding the internal circulation system and the relationship to surrounding properties, findings were made regarding substantially the same provisions in the findings that address the Planned Development Amendment Criteria in this document.

*Multiple Dwelling Development Policies*

The multiple dwelling development policies are predominantly locational criteria. These locational considerations and policies were addressed in conjunction the Planned Development Ordinance when it was amended to authorize multi-dwelling development as a permitted use of this property.

Policy 89.00 requires the city to establish standards for landscaped grounds for multi-dwelling developments. The City has implemented these standards, and the master plan addresses them. This includes provisions of the PD Overlay Zone, the Landscape Standards of the Zoning Ordinance, and the Multi-Dwelling Residential Standards of Chapter 17.11

*Urban Policies:*

Applicable urban policies require adequate public facilities. The proposal will be required to comply with conditions of approval to provide required public facilities constructed to applicable standards. Impacts on applicable systems have been evaluated.

**CHAPTER IX URBANIZATION**

**GOAL IX 1: TO PROVIDE ADEQUATE LANDS TO SERVICE THE NEEDS OF THE PROJECTED POPULATION TO THE YEAR 2023, AND TO ENSURE THE CONVERSION OF THESE LANDS IN AN ORDERLY, TIMELY MANNER TO URBAN USES.**

**GOAL IX 2: TO ESTABLISH A LAND USE PLANNING FRAMEWORK FOR APPLICATION OF THE GOALS, POLICIES, AND PROPOSALS OF THE McMinnville Comprehensive Plan**

**GREAT NEIGHBORHOOD PRINCIPLES:**

*187.10 The City of McMinnville shall establish Great Neighborhood Principles to guide the land use patterns, design, and development of the places that McMinnville citizens live, work, and play. The Great Neighborhood Principles will ensure that all developed places include characteristics and elements that create a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood with enduring value, whether that place is a completely new development or a redevelopment or infill project within an existing built area.*

*187.20 The Great Neighborhood Principles shall encompass a wide range of characteristics and elements, but those characteristics and elements will not function independently. The Great Neighborhood Principles shall be applied together as an integrated and assembled approach to neighborhood design and development to create a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood, and to create a neighborhood that supports today's technology and infrastructure, and can accommodate future technology and infrastructure.*

*187.30 The Great Neighborhood Principles shall be applied in all areas of the city to ensure equitable access to a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood for all McMinnville citizens.*

*187.40 The Great Neighborhood Principles shall guide long range planning efforts including, but not limited to, master plans, small area plans, and annexation requests. The Great Neighborhood Principles shall also guide applicable current land use and development applications.*

*Policies:*

187.50 *The McMinnville Great Neighborhood Principles are provided below. Each Great Neighborhood Principle is identified by number below (numbers 1 – 13), and is followed by more specific direction on how to achieve each individual principle*

*1. Natural Feature Preservation. Great Neighborhoods are sensitive to the natural conditions and features of the land.*

*a. Neighborhoods shall be designed to preserve significant natural features including, but not limited to, watercourses, sensitive lands, steep slopes, wetlands, wooded areas, and landmark trees.*

**Applicant Response:** The existing subject site is vacant agricultural land and the no existing natural features or trees present.

**FINDING: SATISFIED.** The site is generally level without distinctive natural features such as those described above.

*2. Scenic Views. Great Neighborhoods preserve scenic views in areas that everyone can access.*

*a. Public and private open spaces and streets shall be located and oriented to capture and preserve scenic views, including, but not limited to, views of significant natural features, landscapes, vistas, skylines, and other important features.*

**Applicant Response:** The existing subject site does not have any scenic views or existing natural features. The proposed site design does create large open space areas within the subject site for residents to recreate.

**FINDING: SATISFIED.** The site plan provides opportunities for views from the buildings to the distant terrain, predominantly from the site to the south and east.

*3. Parks and Open Spaces. Great Neighborhoods have open and recreational spaces to walk, play, gather, and commune as a neighborhood.*

*a. Parks, trails, and open spaces shall be provided at a size and scale that is variable based on the size of the proposed development and the number of dwelling units.*

*b. Central parks and plazas shall be used to create public gathering spaces where appropriate.*

*c. Neighborhood and community parks shall be developed in appropriate locations consistent with the policies in the Parks Master Plan.*

**Applicant Response:** The proposed site plan creates a variety of open space types for the residents. There are large open spaces, space courtyard style gathering and sitting areas and picnic and barbeque areas. There is a network of onsite pathways that connect the building entries, parking areas and the public street frontage to the open spaces throughout the site.

**FINDING: SATISFIED WITH CONDITIONS.** The property will be developed as a single property with apartments and on-site amenities described by the applicant above. The amenities include a variety of features consistent with this policy which provide opportunities to walk, play, gather, and commune as a neighborhood.

*4. Pedestrian Friendly. Great Neighborhoods are pedestrian friendly for people of all ages and abilities.*



*a. Neighborhoods shall include a pedestrian network that provides for a safe and enjoyable pedestrian experience, and that encourages walking for a variety of reasons including, but not limited to, health, transportation, recreation, and social interaction.*

*b. Pedestrian connections shall be provided to commercial areas, schools, community facilities, parks, trails, and open spaces, and shall also be provided between streets that are disconnected (such as cul-de-sacs or blocks with lengths greater than 400 feet).*

**Applicant Response:** The subject site provides a network of walkways and pathways throughout the site connecting the living units to the open spaces, parking areas and the public street frontage. The subject site is also served by Yamhill County Transit with a transit stop at the corner of SE Norton Lane and SE Stratus Avenue.

The network of pedestrian walkways and pathways connect to all existing adjacent public sidewalks and street frontage. There are not existing adjacent trails.

**FINDING: SATISFIED WITH CONDITIONS.** The property will be developed as a single parcel. As described above, the design provides for internal pedestrian circulation as well as connections from the site to external pedestrian facilities and connections to Stratus Avenue and Norton Lane. The on-site circulation provides multiple pedestrian routes to and through the site and its amenities and pedestrian gathering places. As a condition of approval, the applicant will also construct sidewalk frontage improvements along Norton Lane.

*5. Bike Friendly. Great Neighborhoods are bike friendly for people of all ages and abilities.*

*a. Neighborhoods shall include a bike network that provides for a safe and enjoyable biking experience, and that encourages an increased use of bikes by people of all abilities for a variety of reasons, including, but not limited to, health, transportation, and recreation.*

*b. Bike connections shall be provided to commercial areas, schools, community facilities, parks, trails, and open spaces.*

**Applicant Response:** The proposed street frontage improvements along the SE Norton Lane Frontage includes a bike lane.

**FINDING: SATISFIED WITH CONDITIONS.** The property will be developed as a single site. As a condition of approval, the east-west sidewalk that bisects the site shall be increased to 10-feet in width to accommodate both bicycles and pedestrians.

*6. Connected Streets. Great Neighborhoods have interconnected streets that provide safe travel route options, increased connectivity between places and destinations, and easy pedestrian and bike use.*

*a. Streets shall be designed to function and connect with the surrounding built environment and the existing and future street network, and shall incorporate human scale elements including, but not limited to, Complete Streets features as defined in the Comprehensive Plan, grid street networks, neighborhood traffic management techniques, traffic calming, and safety enhancements.*

*b. Streets shall be designed to encourage more bicycle, pedestrian and transit mobility with a goal of less reliance on vehicular mobility.*

**Applicant Response:** The subject site has limited street frontage. There are no additional current or future rights of way adjacent the site.

**FINDING: SATISFIED WITH CONDITIONS.** The site will develop as a single property with access and frontage on a single public street. There are no new streets proposed.

*7. Accessibility. Great Neighborhoods are designed to be accessible and allow for ease of use for people of all ages and abilities.*

*a. To the best extent possible all features within a neighborhood shall be designed to be accessible and feature elements and principles of Universal Design.*

*b. Design practices should strive for best practices and not minimum practices.*

**Applicant Response:** The proposed on-site network of walkways and pathways will be designed and built to be fully accessible to all residents of the proposed development and the surrounding community.

**FINDING: SATISFIED WITH CONDITIONS.** The applicant shall clarify accessibility features of outdoor play areas, outdoor fitness areas, and other amenities.

*8. Human Scale Design. Great Neighborhoods have buildings and spaces that are designed to be comfortable at a human scale and that foster human interaction within the built environment.*

*a. The size, form, and proportionality of development is designed to function and be balanced with the existing built environment.*

*b. Buildings include design elements that promote inclusion and interaction with the right-of-way and public spaces, including, but not limited to, building orientation towards the street or a public space and placement of vehicle-oriented uses in less prominent locations.*

*c. Public spaces include design elements that promote comfortability and ease of use at a human scale, including, but not limited to, street trees, landscaping, lighted public areas, and principles of Crime Prevention through Environmental Design (CPTED).*

**Applicant Response:** The proposed site and building design provides a variety of scales for the buildings and usable open spaces. The building forms are heavily articulated to reduce mass and scale. The street frontage building are oriented along the street frontage and provide direct access to the building entries. The on-site parking areas are screened from the public street frontage and the buildings are oriented around the common open spaces within the site.

**FINDING: SATISFIED.** This proposal will be developed as a single site, with amenities to serve the residents. There are common use open space areas and amenities, but these are not public. The development and shared/common amenities are consistent with this policy, and the interface with the public right-of-way is also consistent with this policy and related residential design and development standards.

*9. Mix of Activities. Great Neighborhoods provide easy and convenient access to many of the destinations, activities, and local services that residents use on a daily basis.*

*a. Neighborhood destinations including, but not limited to, neighborhood-serving commercial uses, schools, parks, and other community services, shall be provided in locations that are easily accessible to surrounding residential uses.*

*b. Neighborhood-serving commercial uses are integrated into the built environment at a scale that is appropriate with the surrounding area.*

*c. Neighborhoods are designed such that owning a vehicle can be optional.*

**Applicant Response:** The existing subject site is vacant is located along the existing of the current urban growth boundary and the McMinnville city limits. The subject site is near the Highway 18 and Norton Lane intersections, and adjacent to the Yamhill County Transit bus line ant stop. A large employment center, Willamette Valley Medical Center across SE Norton Lane from the subject site.

**FINDING: SATISFIED.** The property itself will be a single property of approximately 4.93 acres in one ownership developed with 138 apartments and amenities for the residents. Currently, there are limited amenities nearby, but the Three Mile Lane Area Plan includes opportunities for additional mixed-use development opportunities and public spaces in proximity to the property.

*10. Urban-Rural Interface. Great Neighborhoods complement adjacent rural areas and transition between urban and rural uses.*

*a. Buffers or transitions in the scale of uses, buildings, or lots shall be provided on urban lands adjacent to rural lands to ensure compatibility.*

**Applicant Response:** The subject site is located adjacent the current McMinnville City Limits and the Urban Growth Boundary. The scale and mass of the proposed development serves as a transition from the higher density larger scale buildings adjacent to the site, to the agricultural lands outside the Urban Growth Boundary that is adjacent to the site.

The proposed site plan does not place buildings adjacent the southerly property line to the south. This “steps down” the massing of the site to the UGB. The proposed site plan places the parking area adjacent to the UGB to soften the transition. Parking is proposed within 6 feet of this property line. A six-foot sight obscuring fence and landscape screening and buffering with continuous shrubs and trees are proposed along this boundary to mitigate the proximity of the parking to the UGB.

**FINDING: SATISFIED WITH CONDITIONS.** The applicant’s narrative accurately describes the conditions for the west side of the property; however, on the east half, Buildings B and D are located closer to the south property line, with sidewalk between the building and property line, landscaping on both sides of the sidewalk, and a fence along the south property line. See conditions of approval for revisions to the landscape plan for the south area between buildings B and D, which will also address this criterion.

The area between buddings B and D would have a wider buffer along the south property line, but the screening between each of those buildings and the property line would be provided by the fencing. The proposed landscaping between those is groundcover south of the sidewalk and shrubs generally 4’ in height at maturity north between the sidewalks and buildings.

*11. Housing for Diverse Incomes and Generations. Great Neighborhoods provide housing opportunities for people and families with a wide range of incomes, and for people and families in all stages of life.*

*a. A range of housing forms and types shall be provided and integrated into neighborhoods to provide for housing choice at different income levels and for different generations.*

**Applicant Response:** The proposed development is for market rate housing and will be located adjacent to the proposed low-income housing project, Status Village. It is anticipated that the residents of both projects will represent a wide diversity of residents from young people just leaving home to live on their own, young families with small children, middle class working families and senior citizens.

**FINDING: SATISFIED.** This development will contribute additional housing types to the neighborhood and surrounding area further diversifying the housing in the area.

*12. Housing Variety. Great Neighborhoods have a variety of building forms and architectural variety to avoid monoculture design.*

*a. Neighborhoods shall have several different housing types.*

*b. Similar housing types, when immediately adjacent to one another, shall provide variety in building form and design.*

**Applicant Response:** The subject site is located adjacent to a variety of uses and subsequent building scales and building forms. The proposed development is for needed multi-family housing apartment units. The proposed development is for market rate housing and will be located adjacent to the proposed low-income housing project, Status Village. This accomplishes the Great Neighborhood Principle of providing a variety of housing types within the subject neighborhood.

**FINDING: SATISFIED.** This development introduces additional housing types into the neighborhood. On-site, each building generally represents the same building design. However, some of the buildings are a smaller version of the plan, approximately half the width. The buildings facing the street are different sizes, and the buildings interior to the site have an L or U shaped arrangement defining the edges of the adjacent open spaces, with buildings oriented at 90 degrees to one another.

*13. Unique and Integrated Design Elements. Great Neighborhoods have unique features, designs, and focal points to create neighborhood character and identity. Neighborhoods shall be encouraged to have:*

*a. Environmentally friendly construction techniques, green infrastructure systems, and energy efficiency incorporated into the built environment.*

*b. Opportunities for public art provided in private and public spaces.*

*c. Neighborhood elements and features including, but not limited to, signs, benches, park shelters, street lights, bike racks, banners, landscaping, paved surfaces, and fences, with a consistent and integrated design that are unique to and define the neighborhood.*

**APPLICANT RESPONSE:** No response.

**FINDING: SATISFIED.** While this will be a single property, the common elements for the residents serve to provide a sense of community and create a unique sense of character and identity. The common open space areas include a combination of landscape open space with shaded hardscape areas at the edges with seating, planters, and barbeque features which provide a place for socializing.

## **CHAPTER X: CITIZEN INVOLVEMENT AND PLAN AMENDMENT**

**GOAL X.1. TO PROVIDE OPPORTUNITIES FOR CITIZEN INVOLVEMENT IN THE LAND USE DECISION MAKING PROCESS ESTABLISHED BY THE CITY OF MCMINNVILLE.**

### ***Policies***

**188.00** *The City of McMinnville shall continue to provide opportunities for citizen involvement in all phases of the planning process. The opportunities will allow for review and comment by community residents and will be supplemented by the availability of information on planning*

*requests and the provision of feedback mechanisms to evaluate decisions and keep citizens informed.*

## **APPLICANT'S RESPONSE: N/A**

**FINDING: SATISFIED.** The proposal includes a proposed amendment to a Planned Development to approve the master plan, which requires a Planning Commission decision. The Zoning Ordinance provides for a concurrent consolidated review process when there are multiple applications associated with a development proposal, using the procedure that provides the greatest opportunity for public involvement. The Three Mile Lane Review, Variance, and Landscape Plan Review are included in this concurrent review which provides greater opportunity for public involvement.

In addition, the procedures require the applicant to conduct a neighborhood meeting prior to submitting the application. The applicant conducted the required neighborhood meeting and submitted the required documentation with the application.

### **Three Mile Lane Area Plan (Ordinance 5126) – Policies and Urban Design Elements**

*1. Require future development to be consistent with the design elements of the Three Mile Lane Area Plan.*

**Applicant Response:** The subject site lies within the area identified as Medium-High Density Residential in the Area Plan. The applicable Urban Design Elements identified in the Area Plan for the subject site are addressed as follows:

#### *Health Care Area*

##### *Key Urban Design Elements*

- *Transitions between uses: Health care facilities and surrounding residential areas. Health care facilities are often active around the clock with bright lighting and they generate significant vehicle traffic. They also require a lot of delivery traffic and, in the case of a major medical center, helicopter use. Buffering between uses should be considered, particularly senior housing or market-rate apartments with trees, landscaping and other treatments. Assisted living or nursing care facilities, however, would benefit from close proximity to the hospital.*

**Applicant Response:** The subject site abuts a large medical office building on the North side of the site. The Willamette Valley Medical Center Hospital is located on the East side of SE Norton Lane. The hospital is set back from SE Norton Lane a significant distance providing a significant buffer from the subject site.

The medical office building to the North has a row of mature trees along the property line abutting the subject site. The proposed Landscape Plan provides additional landscape buffering including trees, shrubs and a sight obscuring fence adjacent the property line.

**FINDING: SATISFIED,** as described above.

- *Transitions between uses: Health care facilities and other commercial uses. The scale and orientation of existing uses, as related to future uses should be considered. For example, while Senior Housing might benefit from a location within walking distance of a retail center, there should be careful site planning to ensure the housing isn't directly adjacent to loading or parking facilities. It may be most feasible to place health-care related housing with an orientation south towards views and the river.*

**Applicant Response:** This is not applicable to the subject site.

- *Walkability between uses. Convenient, safe connections between a variety of uses in this area will be important to current and future users.*

**Applicant Response:** The subject site has street frontage along SE Norton Lane. The proposed development includes Public Street Frontage improvements that will extend along the entire SE Norton Lane Frontage. This sidewalk provides a direct link and connection to the existing sidewalks along SE Norton Lane and Status Avenue. This existing sidewalk network connects the Medical Office Building (Altimus Plaza), the Willamette Valley Medical Center Hospital and up to the intersection of Cumulus Avenue and Highway 18.

The proposed and existing public sidewalk network also provides direct access to public transit and the Yamhill County Transit stop at the intersection of SE Norton Lane and SE Stratus Avenue.

- *Visual quality of buildings facing OR 18. New development should avoid placing loading docks or creating blank walls visible from passing vehicles.*

**Applicant Response:** This is not applicable to the subject site.

**FINDING (POLICY 1): SATISFIED WITH CONDITIONS.** As described in the Three Mile Lane Area Plan: “The [Area] is a unique district in the southeast portion of the City of McMinnville. The area contains approximately 1,340 acres of land with a variety of existing land uses and several large vacant parcels. The Three Mile Lane Area Plan is intended to create an implementable vision for the area’s future land uses and multi-modal transportation system.”

The policies are intended to guide development and future planning decisions in the Three Mile Lane area. These policies implement the Three Mile Lane Area Plan goals and describe how Great Neighborhood Principles are expected to be expressed in the future growth and development of the Three Mile Lane Area.

The Health Care Area, in which the site is located states the key urban design elements listed below. Apartments are a permitted use on the subject property. The relationship to surrounding land uses and the internal design of the site addresses these elements. The visual quality of the buildings facing Norton Lane have “faces” toward the street, with architectural features, windows and openings, and landscaping, which are appealing from the public right-of-way. Screening is provided between the residential use and adjoining uses.

*2. Public improvements and private development shall strive to protect tree groves and mature individual trees.*

**Applicant Response:** There are no existing mature trees that will be impacted by the proposed SE Norton Lane street frontage improvements.

*3. Riparian corridors and adjacent native landscape shall be protected.*

**Applicant Response:** The subject site is not adjacent any riparian corridors and does not contain any native landscape materials.

The proposed Landscape Plan does propose the use and incorporation of native plant materials and hybrids in the landscape design.

**FINDING: (Policies 2 and 3): SATISFIED.** There are no existing tree groves, mature individual trees, riparian corridors, or adjacent native landscaping.

4. *The built environment will be designed to provide and protect views to rolling hills and volcanoes and to enhance visual and physical access to the North Yamhill River. New streets and open spaces will be oriented to capture views.*

**Applicant Response:** The subject site does not have exposure to any existing views of the surrounding area. The upper floor living units may have the opportunity to capture views of the surroundings.

**FINDING: SATISFIED.** No new streets are proposed. Internally, the residential units have internal views of the common open space and landscaped areas, while external facing windows face toward the distant terrain to the south and east.

5. *Enhancing connections to existing trails and open space, such as connections into Joe Dancer Park and McBee Park, and creating a public greenway along South Yamhill River with trails and connections to the Three Mile Lane Area is a priority.*

**Applicant Response:** While the subject site does not have existing adjacent trails or open space, the proposed Site Plan and internal network of sidewalks, provides the availability of connection to future development and trails to the south of the site.

**FINDING: SATISFIED.** This site doesn't have direct connections to the parks or river, but provides pedestrian connectivity to the Norton Lane frontages that would provide relatively direct pedestrian routes to connect to a greenway system as described in this policy.

6. *New gathering spaces will be designed to incorporate natural areas and views.*

**Applicant Response:** The proposed Site Plan provides a variety of common open spaces and gathering spaces.

**FINDING: SATISFIED.** There are no existing natural areas to be incorporated into gathering spaces, but the proposed open spaces will be landscaped with a variety of landscape treatments. The locations of open spaces provide opportunities to maintain views of distant terrain in some directions.

7. *Require native landscape plantings with seasonal variation and tree plantings that include shade streets with mature tree canopy.*

**Applicant Response:** The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.

**FINDING: SATISFIED WITH CONDITIONS.** The landscape plan includes a variety of trees, shrubs, and groundcover, both evergreen and deciduous, including seasonal variation and shade trees.

8. *A network of sidewalks and trails will connect people to key locations within the Three Mile Lane Area.*

**Applicant Response:** The proposed Site Plan provides a network of internal sidewalks connecting the buildings, parking and common open spaces to the SE Norton Lane frontage. The proposed development includes Public Street Frontage improvements that will extend along the entire SE Norton Lane Frontage. This sidewalk provides a direct link and connection to the existing sidewalks



along SE Norton Lane and Status Avenue. This existing sidewalk network connects the Medical Office Building (Altimus Plaza), the Willamette Valley medical Center Hospital and up to the intersection of Cumulus Avenue and Highway 18.

**FINDING: SATISFIED.** The internal site design and connectivity to the adjacent street provide opportunities for linkages to the sidewalk and trails system.

*9. The Three Mile Lane Area will have safe bicycle routes for residents and touring cyclists.*

**Applicant Response:** The proposed development includes the extension of SE Norton Lane. The proposed public street frontage improvements includes a road section with a bike lane. This roadway improvements and bike lane will lead to the proposed future network of bikeways and trails identified in the Three Mile Lane Area Plan.

*10. Proposed new streets will connect to the existing local street grid, consistent with the conceptual designs in the Three Mile Lane Area Plan and in compliance with Transportation System Plan standards.*

**Applicant Response:** The proposed development includes the extension of SE Norton Lane. The proposed public street frontage improvements includes a road section with a bike lane. These improvements follow the proposed alignment of future roadways, bikeways and trails identified in the Three Mile Lane Area Plan.

**FINDING: SATISFIED WITH CONDITIONS.** No new streets are proposed. The applicant will need to comply with the provisions for frontage improvements along Norton Lane, a minor collector which provides for bike lanes.

*11. New commercial developments should be designed to be at a walkable, human scale and for ease of use by all ages and abilities.*

**Applicant Response:** The proposed development does not contain commercial uses and this is not applicable.

**FINDING: NOT APPLICABLE.** The proposal is not a commercial development.

*12. New commercial, office, mixed-use, and multi-family developments should be designed to reflect the micro-climate and enhance outdoor life through the incorporation of features such as porches, balconies, courtyards, plazas, etc.*

**Applicant Response:** The proposed multifamily development includes covered decks and patios for all of the living units. These covered areas will allow residents the opportunity to have access to private, weather protected outdoor living areas.

**FINDING: SATISFIED.** The plans and applicant's response demonstrate compliance with this policy.

*13. New commercial, office, mixed-use, and industrial campus developments should promote inclusion and interaction within the right-of-way.*

**Applicant Response:** The proposed development does not contain commercial, office, mixed-use or industrial uses and this is not applicable.

*14. Encourage mixed-use development where feasible.*

**Applicant Response:** The proposed development does not contain commercial or mixed-use and this is not applicable.

**FINDING: NOT APPLICABLE.** The proposal doesn't include these uses.

*15. Proposed site landscape for new development should strive to reflect patterns of wine industry—eg, rows of vines, southern orientation, shelter belts of trees – and consider functional site planning of vineyard and farm complexes as conceptual models.*

**Applicant Response:** The proposed development and Landscape Plan maximizes the usable site area for landscaping and common outdoor open space. Given the target density necessary for market rate apartments, the site development does not afford the opportunity to develop mini-vineyards or farm related uses.

*16. New development should consider adjacency to agricultural fields and respect this heritage through careful transitions.*

**Applicant Response:** The subject site abuts the McMinnville City Limit and urban growth boundary to the South. The adjacent land remains under agricultural uses. The proposed Site Plan provides a significant separation of the proposed buildings to the adjacent Southerly property line.

**FINDING (POLICIES 15 and 16): SATISFIED WITH CONDITIONS.** The applicant's response to Policy 16 reflects the west side of the site, but on the east side, buildings are closer to the south property line.

The southerly landscape area between Buildings B and D provides an opportunity to address both policies 15 and 16 by modifying that area to design a portion of the landscaping consistent with Policy 15, also helping to respect the heritage through careful transitions by use of landscape treatment and meet the provisions of 17.11.110.4.

*17. Architectural building design that includes simple roof forms (industrial and agricultural) is encouraged in the Three Mile Lane Area.*

**Applicant Response:** The proposed building and elevation design utilizes simple hip roof and gable roof forms. The building articulation provides relief in the wall and roof. The proposed building materials include lap siding and panel siding with battens are traditional to residential developments and are compatible with other residential developments in the neighborhood.

**FINDING: SATISFIED.** There aren't specific standards implementing this policy. The architectural design is balancing the requirements of Chapter 17.11 for variation and articulation of facades and roofing with the requirements of this policy for simple roof forms. While the site doesn't exhibit strong agricultural or industrial themes, it balances this policy with the residential standards.

*18. Encourage a diversity of future housing forms, types, and design that respect the current character of the area.*

**Applicant Response:** The proposed development is for multiple family housing in a varying scale of buildings. The variety of building scale is compatible with existing adjacent development.

**FINDING: SATISFIED.** The plans and applicant's response address this policy. See also finding for Policy 17 above.

*19. Ensure that new commercial and industrial campus development creates a welcoming and visible interface with Three Mile Lane.*

**Applicant Response:** The proposed development does not contain commercial or industrial use and this is not applicable.

**FINDING: NOT APPLICABLE.** This project is not commercial or industrial campus development.

*20. Encourage site design and architecture that visibly convey the historic or current industry on the site (e.g., aviation, wine-making).*

**Applicant Response:** The subject site is currently, and historically vacant. The subject site have been historically been farmed agricultural land.

**FINDING: SATISFIED WITH CONDITIONS.** Above, staff recommended conditions to the landscape plan to address this and related policies.

*21. New commercial, mixed-use, office, and industrial campus development should consider using local materials for cladding and building structure (timber, corrugated steel cladding, red brick), and incorporating vibrant color.*

**Applicant Response:** The proposed development does not contain commercial, office, mixed-use or industrial uses and this is not applicable.

**FINDING: NOT APPLICABLE.** The proposal doesn't include these uses.

*22. Public safety services shall be considered as part of the master planning, including access, response times, and opportunity for substations if needed.*

**Applicant Response:** The subject site has limited frontage and access to public streets. The proposed Site Plan provides direct access to SE Norton Lane for fire and emergency service vehicles. The onsite circulation provides adequate maneuvering and turning radii for fire and EMS vehicles and provides a Fire Truck turnaround at the rear of the site.

**FINDING: SATISFIED WITH CONDITIONS.** While this policy appears to address larger master planning aspects of Three Mile Lane, this application has been reviewed by the Fire Marshal, and the site development will need to be consistent with applicable fire codes.

*23. Ensure that no incompatible heavy industrial uses are allowed along Highway 18 in the Three Mile Lane Area or as part of the Innovation Campus.*

**Applicant Response:** The proposed development does not contain industrial uses and this is not applicable.

**FINDING: NOT APPLICABLE.** No industrial use is proposed.

*24. Significant natural features shall be inventoried and protected as much as possible within new development plans.*

**Applicant Response:** The subject site is existing farmed agricultural land and does not contain significant natural features.

**FINDING: NOT APPLICABLE.** No significant natural features are present.

### **ZONING ORDINANCE**

**Chapter 17.11. Residential Design and Development Standards.****17.11.090 Apartments.**

*Apartments are a type of attached housing within single-story or multi-story buildings. Apartment dwelling units may share common walls, ceilings, or floors.*

**A. Characteristics.**

1. *Site Sizes: Single walk-ups, block apartments, and many courtyard apartments can fit on a 100 x 100 foot lot. Bigger developments with multiple walk-up buildings may be as large as 250,000 square feet, or 500 x 500 foot lots.*

**Applicant Response:**

The subject site is 214,759 sf and within the range identified for walk-up apartments and is therefore compliant.

2. *Height Range: Apartment heights vary depending on the type and the location.*

**Applicant Response:**

The proposed development is for three-story apartment buildings which is fully compatible with the existing and proposed adjacent development.

3. *Density Ranges: Apartment densities vary depending on building type and site design layout.*

**Applicant Response:**

The proposed development has a density of just under 28 units per acre which is typical of "Walk-up Apartments".

**B. Types of Apartments.****2. Walk-Up Apartments.**

- a. *Description: Buildings are limited to three stories, and consist of about four to 12 units each, accessible from a single open-air stairwell. Dwelling units are typically constructed in Type V frame construction with fire sprinklers. Individual apartment buildings are arranged around common open space and shared parking areas.*

**Applicant Response:**

The proposed development is 12-unit to 24-unit three-story buildings with open-air stairways that provide access to the living units. The buildings are proposed to be Type V-B wood-framed construction with fire sprinklers. The buildings are located adjacent to several common open spaces.

- b. *Appropriate Context: Walk-up apartments are appropriate adjacent to or within a single dwelling neighborhood depending on site design, orientation to the street, location of parking, and the massing and scale of buildings.*

**Applicant Response:**

The subject site and proposed development is located within a C-3 Commercial zoned area to the North, an R-4 Medium, High Density parcel adjacent on the West side and the M-L Limited Light Industrial zone to the East. The existing neighborhood has a variety of building scales from the multi-story Willamette Valley Medical Center to the East, single story Medical Offices and a three-story motel to the North.

- c. *Also Named: Woody Walk-Ups, Single Stair Walk-Ups*

**Applicant Response:**

The proposed development is also referred to as a “woody walkup” and provides open air stairways to the upper living units.

- d. *Variations: May have an internal stair. Generally, in this case, the maximum number of units per floor are four. They can be designed with front and back windows for cross ventilation. Buildings can be separated to offer access to light and air on three sides.*

**Applicant Response:**

The proposed building design provides open air stairways on each side of the building to serve the upper floor units. The stairways serve as access to two ground floor and four upper floor units per stairway.

- e. *Lot Sizes: Vary widely, from 10,000 to 250,000 square feet*

**Applicant Response:**

The subject site is 214,759 sf which is within the identified range.

- f. *Density Range: 15 - 30 units per acre. (Note, maximum density will be governed by McMinnville’s municipal code.)*

**Applicant Response:**

The proposed density is just under 28 units per acre which is within the identified allowable density range.

- g. *Building Height: Usually 3 stories; can be 2 stories. (Note, maximum height will be governed by McMinnville’s Municipal Code.)*

**Applicant Response:**

The proposed building height is three-stories.

- h. *Construction Type and Building Code Issues: Typically Type V frame construction. Sprinklers for fire suppression are required.*

**Applicant Response:**

The proposed Construction Type is Type V-b, non-rated with NFPA 13-R Fire sprinklers.

**C. Development Standards. The applicable development standards are as follows:**

**Applicant Response:**

| Required:                    | Proposed:                 |
|------------------------------|---------------------------|
| Lot Width: 50 feet min.,     | Subject Site: 320.20 feet |
| Lot Depth: 100 feet min.     | Subject Site: 670.96 feet |
| Lot Size: 5,000 sf Min.,     | Subject Site: 214,759 sf. |
| Front Setback: 15 feet min., | Proposed: 15 feet         |
| Side Setback: 10 feet min,   | Proposed: 10 feet min.    |
| Rear Setback: 20 feet min.,  | Proposed: 79’-6”          |
| Building Height:             | Proposed: 36’-4”          |

**D. Design Standards. The Apartment Design Standards for multi-dwelling housing are standards that apply to apartment housing types. These standards are related to site design and building frontage, parking, compatibility with neighboring homes, open space, and private space.**

1. ***Context and Site Design.*** *Site design standards are intended to facilitate the development of attractive multi-dwelling housing. They encourage good site and building design, which contributes to livability, safety, and sustainability; helps create a stronger community, and fosters a quality environment for residents and neighbors.*
  - a. *Mirror the scale of blocks and the block-like structure of the surrounding neighborhood.*

**Applicant Response:**

The existing surrounding neighborhood has a variety of building sizes, scales and heights that range from mobile homes to the west, large multi-story hospital to the east, large single story medical office building to the north and a three-story hotel to the north. The proposed Site Plan provides a variety of three-story buildings ranging from 12 living units to 24 living units. There are portions of three buildings that have single story and two story pop-outs providing additional scale and variety.

- b. *Connect the internal network of streets and paths to those of the surrounding area where possible.*

**Applicant Response:**

Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive aisles provide access to the parking areas and buildings. There is a network of pathways and sidewalks that connect the parking areas, opens spaces and building entrances.

- c. *Configure apartments, parking areas, and common open space in clusters that mirror the scale of blocks of the surrounding neighborhood or are no more than 10,000 square feet in area per cluster.*

**Applicant Response:**

The neighborhood immediately adjacent to the subject site consists of large blocks, large sites and large scale buildings. The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters. This proposed Site Plan is compatible with existing adjacent development and provide smaller “clusters” of development.

Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.

Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.

Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.

- d. *Residential units must be oriented to a common open space, including a common green, a plaza, or a pocket park.*

**Applicant Response:**

The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The buildings on the proposed Site Plan are all oriented to common open spaces. The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provide the areas of each.

Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. Buildings A and B are directly adjacent to common open space. Building A is adjacent Open Space 01, Building B is adjacent Open Space 02.

Buildings C and D are grouped in a cluster and are oriented around Common Open Space 03.

Buildings E, F and G are also grouped in a cluster oriented around Common Open Space 04. There is also a Courtyard Open Space 05 between Building E and Building G.

- e. *Orient all buildings around a shared open space that meets the requirements of a Common Open Space.*

**Applicant Response:**

The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provides the areas of each space. There are a variety of uses proposed for the Common Open Spaces. These include both passive and active Open Space. There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.

- f. *Align buildings to surrounding streets.*

**Applicant Response:**

There is only one public street, SE Norton Lane, that abuts the subject site. There are two buildings that are adjacent to the SE Norton Lane frontage, Building A and Building B. These two buildings are aligned, on the long building axis, along the street frontage.

- g. *Connect to surrounding neighborhoods, schools, parks, and other neighborhood destinations.*

**Applicant Response:**

The subject site has limited street frontage and the adjacent properties do not have existing site circulation systems to connect to. The existing sidewalk along SE Norton Lane will be extended with the public road extension. The proposed Site Plan provides several connections from the internal site circulation system to the street frontage and new public sidewalk. There are sidewalks along the norther property boundary and the southern property boundary that will provide opportunities for connections to future development of the adjacent properties.

**2. Large Site Design Requirements.**

1. *Break up parking into smaller areas and access from side streets when possible.*

**Applicant Response:**

The Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive break up the parking areas into

smaller sections preventing long rows of parking. The narrow portion of the site is on the street frontage preventing additional site access points.

2. *Connect parking areas, building entries, and open spaces with paved walkways.*

**Applicant Response:**

The proposed Site Plan provides a network of paved walkways that connect the building entries, common open spaces and parking areas.

3. *Buffer parking areas with landscaping.*

**Applicant Response:**

The proposed Site Plan provides landscaped areas to serve as buffers to the exterior perimeter and to buffer the buildings from the parking. There are landscape islands throughout all parking areas that provide shade trees, ground cover and shrubbery to buffer the parking area.

4. *Minimize the width and number of driveways and curb cuts.*

**Applicant Response:**

The proposed Site Plan has a single 26-foot-wide driveway/curb cut onto SE Norton Lane.

5. *Provide different types of open space throughout site, both active and passive, including playgrounds, trails, volleyball courts, bocce ball courts, community gardens, etc.*

**Applicant Response:**

The proposed Site Plan provides a variety of common open spaces. They vary in size, shape and their proposed uses. There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some areas are more confined courtyard spaces and can provide a more intimate quiet setting.

There is a summary of the proposed Common Open Space areas on the Open Space Analysis Plan, drawing A1.0A.

6. *Align buildings to surrounding streets.*

**Applicant Response:**

The subject site has street frontage along SE Norton Lane. The proposed Site Plan orients two buildings along the street frontage. The long axis of these buildings provides the required alignment to the street frontage.

7. *Centrally locate common buildings and spaces*

**Applicant Response:**

The proposed Site Plan has buildings oriented around the common open spaces. There are three areas of the site where buildings orient to the common open spaces.

8. *Group apartments, parking, and open space into smaller clusters.*



**Applicant Response:**

The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters.

Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.

Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.

Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.

**3. Pedestrian Access.** *On-site pedestrian circulation system shall include the following:*

- a. *Continuous connections between the primary buildings, streets abutting the site, ground-level entrances, common buildings, common open space, and vehicle and bicycle parking areas.*

**Applicant Response:**

The proposed Site Plan provides a network of sidewalks and walkways that connect parking areas, building entries and common open spaces.

- b. *At least one pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.*

**Applicant Response:**

The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk. The buildings that are located along the street frontage provide a sidewalk connection from the building entries to the new public sidewalk along SE Norton Lane.

- c. *Pedestrian walkways shall be separated from vehicle parking and maneuvering areas by physical barriers such as planter strips, raised curbs, or bollards.*

**Applicant Response:**

Pedestrian walkways that are located adjacent vehicle parking are separated by concrete wheel stops and the walkways are of concrete which is contrasting to the asphaltic concrete paving.

- d. *Walkways shall be constructed with the hard surface material, shall be permeable for stormwater, and shall be no less than 3 feet to 5 feet wide. If adjacent to a parking area where vehicles will overhang the walkway, a 7-foot-wide walkway shall be provided. The walkways shall be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials.*

**Applicant Response:**

Pedestrian walkways that are located adjacent vehicle parking are proposed to be concrete which is contrasting to the asphaltic concrete paving. The walkways are all

5'-0" wide. Concrete wheel stops are located 2 feet from the sidewalk in the parking spaces and will provide the 5'-0" minimum clear walkway width.

- e. *Spacing requirement: No further than 200 feet apart, on center. At least 1 pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.*

**Applicant Response:**

The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk.

- f. *May be co-located with a common green.*

**Applicant Response:**

The proposed network is walkways on the Site Plan provides circulation to all parking areas, building entries and the common green open spaces.

**4. Parking Lot Location and Design.**

**a. Characteristics.**

1. *A parking lot is a storage space for cars and should provide secure storage.*
2. *It is also a place where everyone is a pedestrian while getting to or from their car. Therefore, it should be designed primarily for the ease, safety and comfort of a person rolling or on foot.*
3. *Clearly defined pathways through parking lots and garages to building entrances, surrounding sidewalks, and transit stops enhance pedestrian safety. These pathways also provide an opportunity to improve the appearance of parking lots.*
4. *Design parking lots and garages so that vehicles are not the dominant feature.*
5. *To encourage bicycling as a mode choice, bike parking areas should include bike repair, maintenance, and cleaning stations.*

**b. Universal Design Standards for Nine Parking Spaces or More.**

1. *Parking lot pathways should be designed as part of the seamless accessibility network described in Apartment Standards, particularly the required Through Connection.*

**Applicant Response to (a) and (b):**

The proposed Site Plan provides a network of parking area sidewalks and pathways that provide convenient access for the residents to all areas of the site include the common open spaces, common facilities and the public street frontage.

**2. Driveways to shared parking areas are:**

1. *Limited to one driveway per street frontage.*
2. *Parallel parking is permitted on a driveway that crosses a front, side or rear yard abutting a street, but not within the required yard setback.*

**Applicant Response:**

The subject site and proposed Site Plan does not have any shared parking areas or site access.

- c. **Parking Lots For Small Multi-Dwelling Sites - Containing More Than Nine Parking Spaces but Fewer Than 16 Parking Spaces. Off-street parking may be arranged in clusters, subject to the following standards:**

1. Residential developments with fewer than 16 dwellings are permitted parking clusters of not more than five contiguous spaces.
2. Residential developments with 16 dwellings or more are permitted parking clusters of not more than eight contiguous spaces.
3. Parking clusters must be separated from other spaces by at least four feet of landscaping.
4. Clustered parking areas may be covered.

**Applicant Response:**

The subject site and proposed Site Plan provides a parking area greater than 16 spaces therefore this is not applicable.

**d. Parking Lots For Medium-to-Large Multi-Dwelling Sites - Containing More Than 16 Parking Spaces.**

1. Interior landscaping, minimum area
2. Interior landscaping shall be required for off-street parking areas 5,000 square feet or greater in size.

**Applicant Response:**

Parking area landscaping is proposed to be provided for all parking areas and is shown on the proposed Landscape Plan.

3. For parking lots less than 50,000 square feet, the minimum landscaped area is 5 percent.

**Applicant Response:**

Parking area landscaping proposed is greater than the 5% minimum and is shown on the proposed Landscape Plan.

4. For parking lots 50,000 square feet and greater, the minimum landscaped area is 8 percent.

**Applicant Response:**

The proposed Site Plan does not include parking areas over 50,000 sf.

5. Planted areas may take the form of landscape areas and planter bays.

**Applicant Response:**

This is understood and the landscape islands have been included in the overall parking lot landscape area.

6. Landscaped areas along a through connection count toward required interior landscaping.

**Applicant Response:**

This is understood and the landscape areas along through connections have been included in the overall interior landscape area.

7. Landscaped islands and peninsulas shall be evenly distributed throughout all parking areas and separated no more than 60 feet from another. Such islands shall be provided with raised curbs, be a minimum of five feet in width, and shall each contain at least one deciduous tree. To achieve the maximum canopy coverage, all trees shall be non-columnar and have root systems that form deep before spreading to decrease the episodes of buckled pavement.

**Applicant Response:**

Landscape islands have been incorporated into the proposed Site Plan and parking area design. Parking area landscape islands and planting beds will be contained with raised concrete curbs. The spacing of the landscape islands is proposed to exceed the 60 foot maximum spacing in several locations primarily due to the parking stall width and spacing standards. An Administrative Variance is being requested to increase the spacing to 63 feet maximum.

8. *Trees may line the required Through Connection, and/or be clustered within landscape islands or planter bays, and/or shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.*

**Applicant Response:**

Trees are proposed within all of the landscape islands and planter bays throughout the parking areas providing the design tree canopy effect and shading.

9. *When a parking area abuts property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line.*

**Applicant Response:**

Six-foot-high Sight obscuring fences are proposed along the side and rear property lines. Buffer plantings and trees are also proposed along these property lines.

- e. *Parking Lot Setbacks Adjacent to Buildings and Structures. Where an off-street parking or vehicular use area is located adjacent to a building or structure, the off-street parking or vehicular use area shall be set back from the exterior wall of the building or structure by a minimum five-foot-wide landscape strip, or by a minimum five-foot-wide paved pedestrian walkway.*

**Applicant Response:**

The proposed Site Plan maintains a minimum 10-foot separation of parking areas from the closest point of any building or structure.

- f. *Parking Lot Location. Off-street parking spaces and vehicle maneuvering areas shall not be located:*

1. *Within of 20 feet from any street property line, except alley property lines;*

**Applicant Response:**

The parking areas on the proposed Site Plan are located at a distance greater than 20 feet from the street frontage property line.

2. *Between a street property line and the front façade of cottages located closest to the street property line. This standard does not apply to alleys.*

**Applicant Response:**

This is not applicable to this proposed Site Plan and development.

3. *Off-street parking spaces shall not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.*

**Applicant Response:**

The parking areas on the proposed Site Plan are located at a distance of 6-feet from the North and South (side) property lines and 10 feet from the West (rear)property line. For the areas less than 10 feet, an Administrative Variance is being requested to the reduced setback. This is compatible with the existing adjacent development that also has parking located 6 feet from the adjacent property lines.

An Administrative Variance under separate application is being requested for the non-compliance of this issue.

4. *Landscaping, fencing, or walls at least three feet tall shall separate clustered parking areas and parking structures from common courtyards and public streets.*

**Applicant Response:**

The is not applicable to the proposed Site Plan as there are no parking areas proposed adjacent to common courtyards and public streets.

5. *Garages and carports (whether shared or individual) must not abut common courtyards.*

**Applicant Response:**

This is not applicable as no carports or garages are proposed.

6. *Individual attached garages up to 200 square feet shall be exempt from the calculation of maximum building footprint for cottages.*

**Applicant Response:**

This is not applicable as no garages are proposed.

7. *Individual detached garages must not exceed 400 square feet in floor area.*

**Applicant Response:**

This is not applicable as no garages are proposed

8. *Garage doors for attached and detached individual garages must not exceed 20 feet in width.*

**Applicant Response:**

This is not applicable as no garages are proposed

- g. *Parking Lot Required Through Connections. Through Connections may be multi-modal or used exclusively for bicycle and pedestrian access and need to meet the standards in Table 1.*

**Applicant Response:**

The proposed Site Plan and parking area layout does not create “through connections”.

## 5. Common Open Space

### a. Characteristics.

1. *Provide opportunities for formal and informal recreational use by residents of all ages. This could be a shared recreational facility including sports fields, play structures, bike tracks, courts, swimming pool, or other options.*

#### **Applicant Response:**

The proposed common open areas provide formal and informal recreations opportunities for the residents. There are formal seating areas and courtyards with benches and raised planters with seating walls, large open lawn areas for informal sports activities and formal picnic areas with picnic tables and barbeques.

2. *Provide tall deciduous trees for summer shade and winter solar access. When possible, preserve and incorporate large existing trees at least 9 inches in diameter as a focal point of open spaces.*

#### **Applicant Response:**

The landscape design includes deciduous shade trees adjacent the formal sitting areas and picnic areas and they are adjacent to the larger open lawn areas. These will provide the desired summer shading and will permit winter sun exposure. The existing subject site does not contain any existing trees.

3. *Enhance the usability of the space through the inclusion of elements including seating, outdoor lighting, weather protection and/or shade structures, and art, among other features.*

#### **Applicant Response:**

The proposed common open areas provide seating areas and courtyards with benches and raised planters with seating walls. The formal picnic areas include picnic tables and barbeques.

4. *Incorporate landscaping that receives at least 50 percent of its irrigation from harvested rainwater.*

#### **Applicant Response:**

As this is an identified characteristic it is not a requirement. The proposed design does not have the ability to store and harvest rain water.

5. *Provide opportunities for food cultivation including a community garden and/or incorporating cultivated species into the landscaping.*

#### **Applicant Response:**

Community gardens are not proposed.

6. *A maximum of 50 percent of common open space may be provided in a rooftop deck that includes shared amenities, weather protection, and landscaping, and is accessible to all residents.*

#### **Applicant Response:**

Rooftop deck areas are not proposed.

7. *A shared outdoor courtyard or shared street/woonerf that is fronted by individual entrances, windows, and balconies There should be a combination of hardscape and landscaped space and/or planters.*

**Applicant Response:**

The proposed Common Open Space Courtyards between Buildings E and G and between Buildings F and G have building entries and ground floor patios adjacent to them. There is also decorative hardscape concrete paving with an enhanced score joint pattern. Benches and raised planters with seat height walls help to define the courtyard spaces. Enhanced landscape plantings also define the courtyard spaces.

**b. Required Elements, General.**

1. *A common open space shall be provided that is centrally located and designed with a clear function that enhances the livability of residents. These functions shall include passive and active uses. The open space shall be accessible to all residents and if possible be fronted by clearly defined unit entrances. The common open space shall serve as the focus of surrounding buildings. Entries and windows shall face the common open space to provide informal surveillance. Common open spaces shall be accessible to all residents.*

**Applicant Response:**

The proposed Common Open Spaces are centrally located in three primary building cluster groups. The on-site walkway systems connects all of the open spaces, building entries and parking areas throughout the site making accessible to all residents of the development.

There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.

2. *Common open space shall be a minimum of 15 percent of the site.*

**Applicant Response:**

The amount of total proposed Common Open Space provided is greater than the 15% of the site area required. The total site area is 214,759 sf, thus 32,214 sf of Common Open Space is required. 32,769 sf of Common Open Space is provided.

An Open Space Analysis Plan has been prepared that delineates each proposed Common Open Space and identifies and summarizes the areas of each. See drawing A1.0A included within this application.

**6. Private Open Space.**

**a. Characteristics.**

1. *Every dwelling needs private open space for relief from indoors and to provide access to fresh air, light, and nature. Private open space may take many forms based on the size of unit.*

2. *They should translate into a perception of an increase in living space and the ability to invite the outdoors in. Additionally, these open spaces can provide environmental benefits with plants that consume carbon dioxide and help reduce stormwater runoff. Spaces should be adequate to be usable, allowing space for a chair to sit in, a place to barbecue or hang clothes to dry, or for a pet to curl up.*
3. *Private open space should enhance the residential function of the building while also improving the appearance of the building. They should be integrated into the overall architectural form and add detail to the façade.*
4. *Placement can vary based on privacy concerns. It can be combined across multiple floors.*

**Applicant Response to (a) 1-4:**

The proposed living unit and building design provides private open space for each unit. The ground floor living units have on-grade patios that are 96 sf each. The ground floor patios are directly adjacent the living rooms and provide a door onto the patio with adjacent living room windows. This blends the living room space and the patios into usable living areas. The ground floor patios are screened with landscaping to maintain privacy from the adjacent walkways and parking areas.

The upper-level living units each have 72 sf private decks that are directly adjacent the living rooms with the living room windows and a door opening directly onto the deck area. This also extends the living space from the living room onto the private deck.

The placement of the decks and patios provides privacy to adjacent living units and provides greater articulation on the building facades improving the overall appearance and character of the buildings.

**b. Required Elements.**

1. *All units shall have a minimum of 36 square feet of private open space that allows for personalization and private use of the space and contributes to the livability and function of the dwelling. Any exterior private open spaces shall be supplemented with operable windows to allow for cross-ventilation, increase airflow and provide the ability to control access to the outdoors.*

**Applicant Response:**

The ground floor living unit patios are 96 sf each and the upper floor living unit decks are 72 sf each. Both exceed the minimum areas required.

2. *At least 50 percent of upper units shall have a balcony that is accessible from the interior of the unit that is a minimum of 60 square feet with no dimension less than 6 feet. These balconies can be designed to be up to the full width of the apartment in order to provide adequate space for use and allow greater indoor/outdoor flow. Balconies can be cantilevered, semi-recessed, or fully recessed. They should be located based on privacy and environmental concerns. If balconies are transparent, adequate storage should be provided within the unit or the larger building so that balconies do not become informal storage spaces.*

**Applicant Response:**



All of the upper floor living units have decks that are 72 sf each with a minimum dimension of 6 feet. This exceeds the minimum number of units and the minimum deck area required. The decks are placed to maintain privacy between adjacent living units.

3. *Private outdoor space at the ground level must meet the requirements of Universal Standards: Front Yard regardless of whether the private outdoor space is in the front, side, or rear of a building.*

**Applicant Response:**

The ground floor living unit patios are all screened with landscaping and are setback from walkways, street frontage and adjacent parking areas as required.

- c. Supplemental Elements. *In addition to meeting the required elements above, projects must provide private open space in the form of one of the options listed below.*
  1. *A "Juliet-style" balcony of 12-inch dimension that allows residents to bring a sense of the outdoors into the unit. Must have doors that can open inwards or full height sliding glass doors to allow the introduction of fresh air and sunlight. If this item is selected, units must also include operable windows to increase airflow/ability to control access to the outdoors.*
  2. *An upper story rooftop deck or terrace that may include space for outdoor seating, dining, and planters for cultivation. This terrace may be stepped back on structures over two stories so as to reduce the visual impact of upper floors.*
  3. *Alternative option that meets the concept and guiding principles.*

**Applicant Response:**

All upper floor living units provide decks that are 72 sf each, thus meeting the supplementary requirements. All unit decks are directly adjacent to the living rooms and provide operable windows and a deck access door directly onto the deck.

7. Alleys.

**Applicant Response:**

The subject site does not have the opportunity for providing alleys and this is not applicable.

8. Landscaping

a. Characteristics.

1. *Use landscape elements, particularly plant materials, in an organized and harmonious manner that will enhance, protect and promote the economic, ecological, and aesthetic environment of McMinnville.*

**Applicant Response:**

The proposed landscape design provides a cohesive design that utilizes plant materials to enhance the overall site appearance, enhance the outdoor living environment and provide an overall aesthetically pleasing site character. The choice and selection of plant materials incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity.

The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Shade trees will soften the built environment, reduce heat island, and provide habitat for birds. Flowering Shrubs will soften the buildings and enhance the aesthetic value of the site and provide food for local pollinators.

Lawn areas will provide a pastoral aesthetic reminiscent of the Willamette Valley, promoting harmony with adjacent landscapes. These areas will provide opportunities outdoor activities that promote the mental and physical health of the residential community, and in turn, the greater community of McMinnville. Finally, the regular maintenance of the landscape will help provide jobs for the local economy.

2. *Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:*

a. *Reduce soil erosion and the volume and rate of discharge of stormwater runoff.*

**Applicant Response:**

Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.

b. *Aid in energy conservation by shading structures from energy losses caused by weather and wind.*

**Applicant Response:**

Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.

c. *Mitigate the loss of natural resources.*

**Applicant Response:**

Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.

d. *Provide parking lot landscaping to reduce the harmful effects of heat, noise, and glare associated with motor vehicle use.*

**Applicant Response:**

The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects. The combination of landscape plant materials and sight obscuring fences buffer noise from the properties.

e. *Create safe, attractively landscaped areas adjacent to public streets.*

**Applicant Response:**

Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.

- f. *Require the planting of street trees along the City's rights-of-way.*

**Applicant Response:**

Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.

- g. *Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character, and value of existing neighborhoods.*

**Applicant Response:**

The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adjacent to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.

- h. *Provide shade, and seasonal color.*

**Applicant Response:**

The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.

- i. *Reduce glare, noise, and heat.*

**Applicant Response:**

The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.

Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.

- j. *Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.*

**Applicant Response:**

Visual noise will be mitigated with the use of a site obscuring fence, trees, and evergreen shrubs.

Lighting impacts are mitigated by the selection and placement of light fixtures that provide full cut-off of light pollution to the abutting properties.

- k. *Unify development and enhance and define public and private places.*

**Applicant Response:**

The landscape design helps unify the development with the use of repetition of design elements in private and public areas. These elements include repetition of form, texture, and color of the plant materials.

Public spaces meant to be used by all residents are defined with lawn areas, walkways, benches, raised planters, and flowering trees.

Landscaping adjacent to private areas are defined by ornamental plantings of shrubs, grasses and groundcovers.

*l. Preserve existing mature trees.*

**Applicant Response:**

There are no existing trees onsite.

*m. Enhance the urban forest and tree canopy.*

**Applicant Response:**

The urban forest is enhanced with the planting of trees throughout the site; including shade trees and flowering trees. In total, 115 trees are proposed, including street trees.

*n. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.*

**Applicant Response:**

The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.

*o. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.*

**Applicant Response:**

A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.

The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane,

providing a vertically branched and high canopy structure for the interphase between public and private space.

Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

- p. Support McMinnville as a community that cares about its appearance. It is further recognized that landscaping increases property values, attracts potential residents and businesses to McMinnville, and creates safer, more pleasant living and working environments for all residents and visitors to the city.*

**Applicant Response:**

The proposed landscape design will use a variety of trees, shrubs, grasses and groundcovers to provide an aesthetically pleasing appearance that will increase the value of the property, attract residents and create a safe and pleasing living environment.

The proposed trees throughout the site will provide structure, shade, pleasing fall color and a net increase to the urban forest canopy. The ornaments landscape elements will soften the built environment and offer a pleasing appearance for those living and visiting the community.

**b. Required Elements.**

- 1. All areas of the site not occupied by the structures or paved areas shall be landscaped in an attractive and functional manner.*

**Applicant Response:**

The proposed landscape design provides landscape plant materials in all areas of the site not occupied by structures or parking and paved areas. The proposed landscape design provides an aesthetically pleasing design and use of a variety of plant materials.

- 2. A minimum of 20 percent of the net site shall be landscaped. Paved pedestrian paths, when integrated within the landscaped area, may satisfy up to 5 percent of this requirement. Landscaped setback areas, landscaped common open spaces, eco-roofs, vegetated stormwater facilities, preserved natural areas, and planter areas can be credited toward the minimum landscape standard.*

**Applicant Response:**

The proposed Site Plan and landscape design provides 30 percent of the subject site area in landscaping. More specifically, the site area is 214,759 sf. 64,157 sf of landscape area is provided, which is 30 percent.

## 9. Privacy and Screening.

- a. Characteristics. *Low walls or fences are encouraged to provide separation between private open space and common open space, streets, or internal circulation paths. Fences should be designed to integrate with the architecture of the building and add visual interest through the use of materials, color, and detail.*

### **Applicant Response:**

The proposed landscape design provides landscape screening and buffering to ground floor living unit patios (private open space). The plant materials used for screening these areas blends harmoniously with the adjacent landscape design providing an attractive and effective privacy screen.

## b. Required Elements.

1. *All fences on the interior of the development shall be no more than 3 feet high. Fences along the rear or side property lines of the development may be up to 6 feet high. Chain-link fences are prohibited.*

### **Applicant Response:**

There are no internal site fences proposed. There is a 6-foot sight obscuring fence proposed on the site side and rear property lines. The fence will not be chain-link fencing.

2. *Mechanical and communication equipment and outdoor garbage and recycling areas shall be screened so they are not visible from streets and common open spaces.*

### **Applicant Response:**

The proposed trash and recycling enclosures provide a solid evergreen screen along the sides of the facilities, as illustrated on the Landscape Plan.

3. *Utilities such as transformers, heating and cooling, electric meters, and other utility equipment shall not be located within 5 feet of a front entrance and shall be screened with sight-obscuring materials.*

### **Applicant Response:**

The proposed locations of any on-site transformers or other utility equipment has not yet been determined. The transformers and equipment will be screened as required.

4. *The placement of balconies above the first story shall not create a direct line of sight into the living spaces or backyards of adjacent residential properties.*

### **Applicant Response:**

The proposed Site Plan does not locate buildings that would orient living unit balconies that would create a line of sight into adjacent properties.

## 10. Front Yard.

### a. Characteristics.

1. *For all housing types the front setback—even when it is small or zero, should be designed to provide a transition from the public realm of the street to the private realm of the dwelling.*
2. *The front setback provides a vital transition between the public area of the street and the private spaces within the dwelling. The smaller the front setback is, the more important the concept of layering public to private spaces becomes. When*

*multi-dwelling units are on the ground floor of the building and face the perimeter of the site and surrounding streets, they must meet the standards of this section.*

**b. Required Elements.**

1. *Dwelling units located on the ground floor of the building and facing the perimeter of the site and surrounding streets must meet the requirements of either:*
  - a. *Front Yard Type 1: Neighborhood*

**Applicant Response:**

The proposed Site Plan and Landscape Plan provides a landscape design that meets or exceeds the requirements for a Type 1 Front Yard (Neighborhood Type). The "Gateway" area is landscaped at the minimum depth with grasses, the "Front Yard" is fully landscaped and provides a hedge screen at 3 feet to provide privacy to the unit patios.

**11. Compatibility.**

- a. **Characteristics.** *New multi-dwelling housing should be compatible with its surrounding context while introducing new shape, size, and detail variation, enabling different housing styles and types to sit side-by-side harmoniously.*

**Applicant Response:**

The existing developed neighborhood contains a wide variety of uses, building styles and scales. The existing uses include a medical office building, a three-story hotel, the large multi-story hospital (Willamette Valley Medical Center) and single-story mobile homes in a mobile home park. The proposed three-story apartment buildings are well within the range and scale of existing adjacent developments. The proposed buildings range in size from twelve units to twenty-four units also providing a variety in the scale of buildings within the proposed development.

**b. Required Elements.**

1. *On a site with multiple buildings of varying scales (or that vary from the surrounding context), provide a gradual transition between scales. For example, locate dwellings that are similar in scale and density along the street frontage and transition to lower scale and density buildings toward the rear of the site. Use rear driveways and landscaping as a buffer backing up to adjacent properties of a different scale.*

**Applicant Response:**

The proposed Site Plan locates similar scale buildings adjacent the street frontage and along the northerly property line that are compatible and in context with the existing adjacent development. The buildings in the rear of the site are set back nearly 80 feet to provide separation and a buffer from the single-story mobile home park adjacent the rear of the site.

2. *Arrange building volumes and setbacks in a way that reflects neighborhood patterns along street frontages and contributes to the desired character.*

**Applicant Response:**

The existing neighborhood character is quite diverse and does not have any established patterns to follow. The proposed Site Plan provides a street frontage compatible with similar developments within the community and provides a pedestrian friendly street scape.

3. *Arrange courtyard apartments so that end units reflect a neighborhood context of detached units along the street frontage.*

**Applicant Response:**

This element is not applicable to the garden style apartments.

4. *Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.*

**Applicant Response:**

The existing neighborhood has a variety of height and scale buildings. The existing large medical office building adjacent the site to the north is single story. However, the proposed Stratus Village to the West of the medical Office Building and directly adjacent the subject site to the North is proposed to be a three-story building.

5. *Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.*

**Applicant Response:**

The proposed site and building design does not propose stepped back upper floors. The building design, the proposed roof design and building articulation provides the desired human scale elements without stepping upper floors.

- c. *Supplemental Elements.* *In addition to meeting the required elements above, projects must respond to the compatibility requirement in the form of three of the options listed below.*

1. *Use roof forms and bays to break up the overall mass of larger residential structures.*

**Applicant Response:**

The proposed building design includes a variety of roof forms that include hipped sloped roofs, intersecting gable roof forms and large breaks in the overall roof at building indentations. The proposed roof design breaks up the overall building mass into smaller sections thus reducing the overall appearance of the building mass and scale.

2. *Walls incorporate vertical wall offsets, projections, or recesses to reduce building façades into smaller volumes and define visually distinct living unit modules.*

**Applicant Response:**

The proposed building design includes offsets in the façade wall planes. The deck forms project from the primary building wall plane and the intersecting gable roof form serves to break-up the overall building mass. The larger buildings have central indentations that serve to further break-up the building mass.

3. *Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.*

**Applicant Response:**

This element is not utilized in the building design.



4. *Mark a distinct physical transition between the base and upper floors of a building through a change in brick pattern, change in materials and/or wall surface pattern, articulation of a floor line, or change in window types.*

**Applicant Response:**

The proposed building design includes a variety of building materials and textures. The building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building.

5. *Use horizontal elements the entire width of the front façade to mark a break between floors or along the roofline including band course, band molding, bellyband, or belt course.*

**Applicant Response:**

The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building.

6. *Use a variation in roof forms on all four elevations of a structure to visually break up monotony including pitched or sloping roof elements, variations in pitch and height of roof planes, dormers, eaves, gale, or dormer end brackets, corbels, or decorative wood timbers.*

**Applicant Response:**

This element is not utilized in the building design.

7. *Limit continuous ridgelines to less than 40 feet in length and continuous eaves to 25 feet in length.*

**Applicant Response:**

This element is not utilized in the building design.

8. *Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.*

**Applicant Response:**

This element is not utilized in the building design.

12. Wall and Roof Design.

A. Characteristics.

1. *For buildings that front the street, avoid long, monotonous, uninterrupted walls. Modulate buildings walls and roofs to prevent large, uninterrupted walls and building mass.*

**Applicant Response:**

The proposed building design provides significant articulation and variety in walls planes. The longest wall plane is less than 20 feet before intersecting or recessed wall planes. Projecting or recessed building elements provide a great amount of articulation and variety of wall planes and roof forms.

2. *Differentiate between the base of the building and the top of the building to enhance the pedestrian realm. Make base treatment cohesive across façades and integrate with the architectural character of the building.*

**Applicant Response:**

The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building. This design concept is applied to all buildings on site.

3. *Multi-dwelling development must address the following design objectives:*
- A. *Articulation – All street-facing buildings shall incorporate design elements that break up façades into smaller planes.*
  - B. *Eyes on the street – A certain percentage of the area of each street-facing façade must be windows or entrance doors.*
  - C. *Main entrance – On street-facing façades, at least one main entrance must meet standards for location, orientation, and visibility.*
  - D. *Detailed Design – All street-facing buildings shall include several features.*

**Applicant Response:**

The proposed site and building design address the four design objectives. The building design provides building and roof forms to break up the facades into smaller planes; the street facing units provide windows and glazed doors that face the street frontage; the open entry stairway to the units faces the street frontage; and the street facing building elevations provide a variety of design features.

**B. Required Elements.****1. Articulation.**

- A. *For multi-dwelling buildings with 30 to 60 feet of street frontage, a minimum of one of the following elements shall be provided along the street-facing façades.*
- B. *For buildings with over 60 feet of street frontage, at least one element below shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.*
  - 1. *A porch at least 5 feet deep.*
  - 2. *A balcony that is at least 2 feet deep and is accessible from an interior room.*
  - 3. *A bay window that extends at least 2 feet.*
  - 4. *A section of the façade that is recessed by at least 2 feet deep and 6 feet long.*
  - 5. *A gabled dormer.*
- C. *Buildings under 30 feet in length are exempt from these requirements.*

**Applicant Response:**

The proposed street frontage buildings have over 60 feet of street frontage. The proposed building design does not have a wall plane over 20 feet between articulation or design features to break up the facades. These include the recessed front porch/patio and upper floor decks that are 6 feet in depth. These projecting elements also have gabled and hipped roof dormers.

**2. Eyes on The Street.**

- A. *At least 15 percent of the area of each street-facing façade must be windows or entrance doors. Windows used to meet this standard must be transparent and allow views from the building to the street. Glass blocks and privacy windows in bathrooms do not meet this standard.*

- B. *Window area is considered the entire area within the outer window frame, including any interior window grid.*
- C. *Doors used to meet this standard must face the street or be at an angle of no greater than 45 degrees from the street.*

**Applicant Response:**

The proposed building design provides 22% window area on the street facing facades. This includes the living room, bedroom and bath windows and the glazed door in the living room leading to the decks and ground floor patios.

3. *Main Entrances. Main entrances must meet both of the following standards.*

- A. *Be no further than 8 feet behind the longest street-facing wall of the building.*
- B. *Face the street, be at an angle of up to 45 degrees from the street, or open onto a porch. If the entrance opens up onto a porch, the porch must meet all of these additional standards.*
  - 1. *Be at least 25 square feet in area with a minimum 4 feet depth.*
  - 2. *Have at least one porch entry facing the street.*
  - 3. *Have a roof that is no more than 12 feet above the floor of the porch.*
  - 4. *Have a roof that covers at least 30 percent of the porch area.*

**Applicant Response:**

The proposed building provides open air enclosed stairways to serve the living units on that side of the building including the ground floor units. The stairway provides a roof covering.

4. *Detailed Design.*

- A. *For multi-dwelling buildings with up to 30 feet or more of street frontage, a minimum of two of the elements shall be provided along the street-facing façade or façades.*
- B. *For buildings with over 30 feet of street frontage, at least one element shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.*
  - 1. *Covered porch at least 5 feet deep, as measured horizontally from the face of the main building façade to the edge of the deck, and at least 5 feet wide.*

**Applicant Response:**

The proposed design for Building 'A' has 70 feet of street frontage. The façade is broken up into three primary articulated sections. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. The longest wall plane between these sections is 20 feet. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.

The proposed design for Building 'B' has 140 feet of street frontage. The façade is broken up into six articulated sections and the longest wall plane between these sections is 20 feet. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.

- 2. *Recessed entry area at least 2 feet deep, as measured horizontally from the face of the main building façade, and at least 5 feet wide.*

**Applicant Response:**

The proposed building recessed entries and open-air stairways to the upper floor living units. These entries are 8 feet wide and 18 feet deep.

3. *Offset on the building face of at least 16 inches from one exterior wall surface to the other.*

**Applicant Response:**

The proposed street facing building designs provide multiple changes in wall planes. The depth of the wall plane variations are 6 feet in several locations and 10 feet in additional locations. These variations occur multiple times on all street frontage building façade elevations.

4. *Dormer that is at least 4 feet wide and integrated into the roof form.*

**Applicant Response:**

This design element is not utilized in the building design.

5. *Roof eaves with a minimum projection of 12 inches from the intersection of the roof and the exterior walls.*

**Applicant Response:**

The proposed building roof design provides 24 inch, 18 inch and 12 inch roof overhangs.

6. *Roofline offsets of at least 2 feet from the top surface of one roof to the top surface of the other.*

**Applicant Response:**

The design element is not utilized in the building design.

7. *Horizontal lap siding between 3 to 7 inches wide (the visible portion once installed). The siding material may be wood, fiber-cement, or vinyl.*

**Applicant Response:**

The proposed building elevation design includes fiber cement horizontal lap siding with a 6-inch exposure.

8. *Brick, cedar shingles, stucco, or other similar decorative materials covering at least 40 percent of the street-facing façade.*

**Applicant Response:**

This design element is not utilized in the building design.

9. *Gable roof, hip roof, or gambrel roof design.*

**Applicant Response:**

The proposed building roof design includes hip roofs and gable roof forms.

10. *Window trim around all windows at least 3 inches wide and 5/8 inches deep.*

**Applicant Response:**

This design element is not utilized in the building design.

11. *Window recesses, in all windows, of at least 3 inches as measured horizontally from the face of the building façade.*

**Applicant Response:**

This design element is not utilized in the building design.

12. *Balcony that is at least 3 feet deep, 5 feet wide, and accessible from an interior room.*

**Applicant Response:**

The proposed building and living unit design includes recessed upper floor decks that are 6 feet deep and 12 feet wide.

13. *Bay window at least 2 feet deep and 5 feet long.*

**Applicant Response:**

This design element is not utilized in the building design.

14. *One roof pitch of at least 500 square feet in area that is sloped to face the southern sky and has its eave line-oriented within 30 degrees of the true north/ south axis.*

**Applicant Response:**

This design element is not utilized in the building design.

**FINDING (Section 17.11.090): SATISFIED WITH CONDITIONS.** Except for those standards where the applicant has requested flexibility as part of the Planned Development, and/or a variance as applicable, the applicant's plans and narrative demonstrate compliance with the multi-dwelling standards of Section 17.11.090. Compliance with this Chapter is contingent on the Planning Commission approving the requested relief.

**17.11.100 Universal Design Standards.**

**APPLICANT RESPONSE: N/A.**

**FINDING (Section 17.11.100. Residential Design and Development Standards: Residential Universal Design Standards). NOT APPLICABLE.** This section doesn't apply to apartments.

**17.11.110 Planned Development Residential Design and Development Standards.**

*Chapter 17.51 of the McMinnville Municipal Code allows for planned development overlays in McMinnville as a means of providing greater flexibility and greater freedom of design in the development of land than may be possible under strict interpretation of the provisions of the zoning ordinance. McMinnville encourages residential planned developments as a means of achieving the City's adopted Great Neighborhood Principles.*

**A. Guiding Principles.**

*A1. Appropriate site size and scale. Small to medium sites (10,000 square feet to 40,000 square feet) should act as infill sites and respond to the surrounding neighborhood in scale, character, building design, details and materials. Large sites (over 40,000 square feet) should provide a variety of housing types, centralized common open space, and an interconnected system of streets and pathways that connect to the surrounding neighborhood.*

*A2. Livability. Residential Planned Developments should contribute to a livable neighborhood by incorporating visually pleasing design, minimizing the impact of vehicles, emphasizing pedestrian and bicycle connections, and providing public and private open spaces for outdoor use.*

*A3. Compatibility. Residential Developments should have a scale that is appropriate for the surrounding neighborhood and maintains the overall residential character of McMinnville.*

*A4. Safety and Functionality. Developments should be safe and functional, by providing visibility into and within a residential development and by creating a circulation system that prioritizes bicycle and pedestrian safety.*

*A5. Enduring quality. Residential Planned Developments should incorporate design and building practices that promote the economic, ecological, and aesthetic environment of McMinnville, such as energy conservation, preservation of trees and open space, and quality building materials.*

*B. Required Elements, Great Neighborhood Principles. All residential planned developments shall meet the Great Neighborhood Principles as identified below.*

*B1. Natural Feature Preservation.*

*B2. Scenic Views.*

*B3. Parks and Open Spaces.*

*B4. Pedestrian Friendly.*

*B5. Bike Friendly.*

*B6. Connected Streets.*

*B7. Accessibility.*

*B8. Human Scale Design.*

*B9. Mix of Activities.*

*B10. Urban-Rural Interface.*

*B11. Housing for Diverse Incomes and Generations.*

*B12. Housing Variety.*

*B13. Unique and Integrated Design Elements.*

*C. Required Elements, Common Open Space. All Residential Planned Developments over four units shall meet the fundamental and supplemental requirements for Common Open Space.*

*C1. Characteristics.*

*a. Common open spaces offer residents social and health benefits while also defining and bringing character to a development.*

*b. Common open spaces may include shared recreational facilities such as play areas, sports fields or swimming pools; rooftop decks that prompt interaction and include shared amenities such as grills, play space, or seating.*

*C2. Guiding Principles.*

*a. Common open space should be appropriately located so users feel safe and residents take ownership and responsibility for the shared space.*

*b. The design should take into account its relationship to units, entries, and windows, as well as how landscaping or other barriers may impact sight corridors.*

*c. Common open spaces should have clear intended uses with visual cues to inform users as to the desired function.*

*d. Avoid large, hard-surfaced or landscaped areas that lack furnishings or other design elements suggesting specific activities.*

- e. Break down large spaces into smaller, comfortable outdoor rooms through the use of fencing or low walls, furnishings and lighting, building placement, and plantings.*
- f. Sensitive design will produce greater benefits than expensive materials or furnishings and certainly more options for use than large undefined open areas.*

### **C3. Fundamental Requirements.**

- a. A common open space shall be provided that is centrally located and designed with a clear function that enhances the livability of residents in the planned development. These functions shall include passive and active uses. The open space shall be accessible to all residents and if possible be fronted by clearly defined unit entrances. The common open space shall serve as the focus of surrounding buildings. Entries and windows shall face the common open space to provide informal surveillance. Common open space shall be accessible to all residents.*
- b. Common open space shall be a minimum of 15 percent of the planned development. Passive open space shall not be more than 5 percent of the planned development.*
- c. When vehicular areas are located between dwellings and common open space, clearly defined pathways shall be provided to enhance pedestrian safety.*
- d. Pathway surface shall be clearly marked and differentiated from the vehicular area with paint or alternative paving material.*
- e. Common open space shall have a minimum dimension of 20 feet at the narrowest part.*
- f. Walkways are required between dwellings and common open space.*

### **C4. Supplemental Requirements. Provide at least four of the options listed below.**

- a. Provide opportunities for formal and informal recreational use by residents of all ages. This could be a shared recreational facility including sport fields, play structure, bike track, courts, swimming pool, or other options.*
- b. Provide tall deciduous trees for summer shade and winter solar access. When possible preserve and incorporate large existing trees at least 9 inches in diameter as a focal point of open spaces.*
- c. Enhance the usability of the space through the inclusion of elements including seating, outdoor lighting, weather protection and/or shade structures, and art, among other features.*
- d. Incorporate landscaping that receives at least 50 percent of its irrigation from harvested rainwater.*
- e. Provide opportunities for food cultivation. Include a community garden and/or incorporate cultivated species into the landscaping.*

### **D. Supplemental Elements, Consideration.**

#### **D1. Modular Block Layouts.**

- a. An intermix of housing types is possible if blocks are platted with a lot width module that can be aggregated. If lots are increments of 25 to 30 feet wide and can be*

*aggregated into lots that are 50 or 60 feet wide (or 75 or 90 feet wide), a wide variety of dwelling types can occupy the same block.*

*1. For example, narrow lot dwellings such as townhouses or tiny houses on 25 foot lots may sit next to larger lot dwellings such as courtyard apartments or cottage clusters on a 50- or 75-foot lot.*

*b. In a new planned development, the greatest flexibility for lot variety is provided by having an alley serve as parking and driveway to each lot (see Figure "Block with Alley").*

*c. When parking is accessed via a driveway from the front of the lot, the lot width is governed by frontage requirements of Universal Design Standards – Street Frontage, and the minimum lot width will be 40 feet (see Figure "Block without Alley").*

*D2. Block Lengths*

...

*D3. Partial Alley at End of Block*

...

*D4. Usable Side Yard Setback.*

...

**FINDING (Residential Design and Development Standards. Section 17.11.110. Planned Development Residential Design and Development Standards): SATISFIED WITH CONDITIONS.**

- As specified in Section 17.11.011, the standards in this chapter are applicable to new housing construction.
- The application is subject to the provisions of Section 17.11.090, Apartments.
- As specified in Section 17.11.100, the Universal Design Standards in that section are not applicable to apartments.
- Because the property is subject to a Planned Development Ordinance, the provisions of Section 17.11.110, Planned Development Residential Design and Development Standards, apply. Some are optional.

The applicant has provided a narrative with responses addressing each of the standards, in addition, the plans and elevations submitted by the applicant provide the information needed to determine compliance with the applicable provisions of Chapter 17.11. As part of the Planned Development Amendment, the applicant has also requested flexibility regarding certain multi-dwelling standards in Chapter 17.11, which are addressed under the provisions for Section 17.74 of this decision document.

**Chapter 17.33. C-3 Zone.**

**17.33.010 Permitted uses.**

In a C-3 zone, the following uses and their accessory uses are permitted:

6. Apartments, Multiple dwellings subject to the provisions of the R-4 zone, except that within the City Center Housing Overlay Zone designated in Chapter 17.66, density limitations of the R-4 zone shall not apply, and any special development standards of the Overlay Zone shall supersede those of the R-4 zone.

***Applicant Response:*** *The proposed development is for Apartments and is therefore subject to the R-4 zone*

**FINDING (Chapter 17.33): SATISFIED.** The property is subject to the provisions of the C-3 zone, as modified by the provisions of the Planned Development Overlay Ordinance. Apartments are a permitted use in the C-3 zone, subject to the provisions of the R-4 zone. Some of those provisions are governed by the provisions of Planned Development Overlay Ordinance, which contains some provisions which are more restrictive than those of the C-3 and R-4 zones. The R-4 zone specifies that density maximum



may not apply to permitted housing types other than single attached dwellings. In addition, some of the multi-dwelling standards in 17.11.090, including some standards for a lot over 14,000 square feet in size, supersede some provisions of the R-4 zone.

### **CHAPTER 17.21 R-4 MEDIUM, HIGH DENSITY RESIDENTIAL ZONE**

**Applicant Response:** Although the subject site is located within the C-3 Commercial Zone, Section 17.33.010 (6) provides that Apartments are subject to the provisions of the R-4 zone. Therefore the following narrative addresses the compliance with the applicable provisions of the R-4 zone.

#### **17.21.010 Permitted uses.**

In an R-4 zone, the following uses and their accessory uses are permitted:

- A. Tiny Houses, Single detached dwelling;
- B. Middle Housing
  1. Plexes: Duplex Dwelling; Triplex Dwelling; Quadplex Dwelling (minimum lot size of seven thousand square feet)
  2. Cottage Clusters
  3. Townhouses
- C. Apartments; Multiple dwelling subject to the following:
  1. Developments with five or more units.
  2. The property on which the use will be located has direct access from a major collector or minor arterial street, or a local collector street within 600' of a collector or arterial street; or
  3. The property is located within one-half mile of a planned or existing transit route; or
  4. The property is within one-quarter mile from a planned or existing neighborhood or commercial shopping area.

#### **Applicant Response:**

*The proposed development is for 138 apartment units located in seven three story buildings. The subject site does provide direct access to a local collector street that is within 600 feet of a designated collector.*

#### **17.21.030 Lot size.**

In an R-4 zone, the lot size shall not be less than five thousand square feet, except that the lot area for attached single dwelling lots shall average one thousand-five hundred square feet in area. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

#### **Applicant Response:**

*The subject site is 214,759 sf (4.93 Acres) and therefore meets the requirement.*

#### **17.21.040 Yard requirements.**

In an R-4 zone, each lot shall have yards of the following size unless otherwise provided for in Section 17.54.050:

- A. A front yard shall not be less than fifteen feet, except as provided in Section 17.11.030(C), Table 1(d), Cottage Clusters.
- B. A side yard shall not be less than six feet. An exterior side yard shall not be less than fifteen feet, except as provided in Section 17.11.030(C), Table 1(f), Cottage Clusters.
- C. A rear yard shall not be less than twenty feet, except as provided in Section 17.11.030(C), Table 1(c), Cottage Clusters.
- D. Whether attached to a residence or as a separate building, a covered storage facility for a vehicle on which the main opening is toward a street shall be located not less than twenty feet to the property line bordering the street;
- E. All yards shall be increased, over the requirements of this section, one foot for each two feet of building height over thirty-five feet. (Ord. 4912 §3, 2009; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

The yards on the proposed Site Plan are as follows:

- Front Yard: 15 feet provided, 15 feet required.
- Side Yard: 10 feet provided, 6 feet required.
- Rear Yard: 79'-6" provided, 20 feet required.

The proposed yards meet the requirements.

**17.21.050 Building height.**

In an R-4 zone, a building shall not exceed sixty feet in height. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

The proposed building height is 36'-4". The current underlying Three Mile Lane Planned Development Overlay Ordinance 4709 has a height limitation of 35 feet. The concurrent Planning Commission Variance Application, submitted concurrent within this application, is requesting a height variance to 37 feet. Additional compliance narrative addressing the proposed building height is in the applicant response to the Planning Commission Variance Application narrative.

**17.21.060 Density requirements.**

In an R-4 zone, the maximum density for single attached dwelling may not exceed four dwelling units per 5,000 square feet. Density maximum may not apply to any other permitted housing types, including accessory dwelling units. (Ord. 4796 §1(b), 2003; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

The allowable density for the 214,759 sf subject site is 210 living units. The Site Plan for the proposed development includes 138 apartment units. The proposed density is less than the allowable and is therefore compliant.

**[Staff Note: Density provisions only apply to single-attached dwellings, which are townhouses. Therefore, this is not applicable to the application.]**

**Chapter 17.52. Airport Overlay Zone**

...

17.52.030 General Restrictions. No use in the Airport Overlay Zone shall:

- A. Create electrical interference with navigational signals or radio communication between the airport and aircraft; or
- B. Otherwise endanger or interfere with the landing, takeoff, or maneuvering of aircraft intending to use the airport.

...

17.52.060 Horizontal Zone Restrictions. The following uses are prohibited in the Horizontal Zone:

Any structure which exceeds a height greater than 309 feet above MSL except that a structure may be constructed to a vertical height no greater than 35 feet above the ground in the Eola Hills.

...

**FINDING (Chapter 17.52): SATISFIED WITH CONDITIONS.** The subject property is within the Airport Overlay Zone, which is comprised of several sub-areas. The subject property is within the Horizontal Zone. 17.52.030, General Restrictions, and 17.52.060, Horizontal Zone Restrictions, apply to the subject property.

The general restrictions of the Airport Overlay Zone will continue to be applicable to the property and the operation of the use since it is located within the Airport Overlay Zone. There is nothing related to the residential use or development that is expected to create electrical interference or otherwise endanger or interfere with the landing, takeoff, or maneuvering of aircraft intending to use the airport.

The proposed structures will not exceed a height greater than 309 feet above mean sea level (MSL). The published airport elevation is 163 above MSL, so a building exceeding 309 feet above MSL would be approximately 146 feet tall.

#### **Chapter 17.54. General Provisions**

**FINDING (Chapter 17.54): SATISFIED WITH CONDITIONS.** This Chapter includes various provisions, including those regarding exceptions to building height, fences (recodified as MMC 8.10.210), yards, and clear vision areas. Fences within interior side and rear yards are limited to seven feet in height. Clear vision areas are to be maintained at driveway approaches and street intersections. A clear vision area shall contain no planting, fence, wall, structure or temporary or permanent obstruction exceeding three (3) feet in height, measured from the top of the curb or, where no curb exists, from three and one-half (3.5) feet above the edge of the pavement, or top of asphalt measured at the property line, except that the following may be allowed in a clear vision area.

1. Trees exceeding this height may be located in the clear vision area provided all branches and foliage are removed to a height of eight feet above the grade;
2. Telephone, power, and cable television pole, electrical junction boxes.
3. Government issued traffic safety signs.
4. Telephone switch boxes provided they are less than 10 inches wide at the widest dimension.

#### **Chapter 17.57. Landscaping**

**FINDING (Chapter 17.57):** *The findings regarding the applicable provisions of this chapter are addressed in the findings for the landscape plan review in this decision document.*

#### **Chapter 17.58. Trees**

...

**17.58.080 Street Tree Planting - When Required.** *All new multi-dwelling development, commercial or industrial development, subdivisions, partitions, or parking lots fronting on a public roadway which has a designated curb-side planting strip or planting island shall be required to plant street trees in accordance with the standards listed in Section 17.58.090.*

#### **17.58.100 Street Tree Plans.**

##### *A. Submittal.*

...

*2. Commercial, Industrial, Parking Lots, and Multi-dwelling Residential Development: Landscape plans, to include street tree planting as may be required by this ordinance, shall be submitted to the Landscape Review Committee for review and approval prior to the issuance of a building permit.*

#### **17.58.110 Street Tree Planting.**

...

##### *B. Commercial, Industrial, Multi-dwelling, Parking Lot Development.*

*1. Planting Schedule: Street trees required of a commercial, industrial, multi-dwelling, or parking lot development shall be installed at the time all other required landscaping is installed.*

**FINDING (Chapter 17.58):** *The findings regarding the applicable provisions of this chapter are addressed in the findings for the landscape plan review in this decision document.*

**Chapter 17.60. Off-Street Parking and Loading****17.60.050 Spaces—Location.**

- A. Except as provided below, required off-street parking spaces for dwellings shall be located on the same lot with the dwelling...

**Applicant Response:**

All required parking is provided on the same site as all proposed dwelling units.

**17.60.060 Spaces. Number required.**

Except for the southerly 100 feet of Block 10 and the northerly 100 feet of Block 11, Rowland's Addition and the area bounded by Second Street, Adams Street, Fourth Street, and Galloway Street, at the time of erection of a new structure or at the time of enlargement or change of use of an existing structure, off-street parking spaces shall be provided as follows unless greater requirements are otherwise established. Where square feet are specified, the area measured shall be the gross floor area primary to the functioning of the particular use of the property but shall exclude space devoted to off-street parking or unloading.

Section 17.60.060. Spaces, Number required.

**A. Residential land use category:**

3. Multiple dwelling: One and one-half spaces per dwelling with less than three bedrooms, two spaces per dwelling unit with three or more bedrooms, and one space per dwelling unit which is expressly reserved for senior or handicapped persons.

**Applicant Response:**

The Off-Street parking required and provided is as follows:

Parking Required:

One- and Two-Bedroom Units: 1.5 spaces/unit required, (120 units x 1.5 =180)

Three-Bedroom Units: 2 spaces per/unit required (18 units x 2 =18)

Total Parking Required: 216 spaces

Provided Parking:

Standard Spaces: 188 spaces

Compact Spaces: 22 spaces

ADA Accessible spaces: 8 spaces

Total Spaces Provided 218 spaces

**[Staff Note:** The site plan shows numerous parking spaces that are 17' deep and don't meet the dimensions for standard spaces, but are counted in the applicant's calculations as standard spaces].

**17.60.070 Off-street loading requirements.**

- A. Buildings or structures to be built or substantially altered which receive and distribute materials and merchandise by trucks shall provide and maintain off-street loading berths in sufficient number and size to adequately handle the needs of the particular use.

**Applicant Response:**

The proposed multifamily apartment development does not require off-street loading spaces. None are proposed to be provided.

**17.60.080 Design requirements.**

- A. *All parking lots and driving aisles shall be asphaltic cement concrete or Portland cement concrete with driving aisles, maneuvering aisle and parking spaces clearly marked, except that in an industrial zone, parking spaces which are in addition to those required by this chapter, may be surfaced with a minimum of treated gravel and maintained dust free.*

**Applicant Response:**

All parking areas and drive aisles on the proposed Site Plan are designed to be constructed of asphaltic concrete.

- B. *In a residential zone, a required front yard or a required side yard adjacent to the street shall not be used for any purpose except for off-street parking of motor vehicles, unless otherwise allowed by this ordinance, and such parking space shall not be less than twenty feet in depth from the property line.*

**Applicant Response:**

Parking areas or drive aisles are not proposed to be located within the front or side yard setbacks.

- C. *Safe access shall be provided as follows:*
1. *Access aisles shall be of sufficient width for all vehicular turning and maneuvering.*

**Applicant Response:**

The drive aisles on the proposed Site Plan are 26 feet wide. The turn radii on the drive aisles are 18-foot minimum inside radius. These proposed dimensions provide safe and adequate width and turn radii for vehicles and for fire trucks and emergency vehicles.

2. *Groups of more than four parking spaces shall be so located and served by a driveway that their use will require no backing movements or other maneuvering within a street right-of-way other than an alley.*

**Applicant Response:**

The proposed site plan provides a single driveway access to SE Norton Lane. No backing movements are proposed onto SE Norton Lane.

3. *Driveways to off-street parking areas shall be designed and constructed to facilitate the flow of traffic and to provide for maximum safety of pedestrians and vehicular traffic on the site.*

**Applicant Response:**

The proposed driveway access is 26 feet wide which provides ample width for two-way traffic into and from the subject site. The Public Street frontage improvements include setback sidewalks that cross the driveway and will be constructed in accordance with the City of McMinnville standards.

4. *Clear vision areas shall be provided at driveway exits for all uses except single-detached dwellings and plexes, shall have minimum dimensions of ten feet measured along the street right-of-way and the edge of the driveway. In commercial and industrial zones, buildings and signs may be constructed with cantilevers which extend out over the clear vision area at a height greater than eight feet when measured from the top of the curb, or where no curb exists, from the established centerline grade. Except for existing permanent buildings and structures (other than signs), nonconformities shall be made to comply with the provisions of this section within seven years from the date of its adoption.*

**Applicant Response:**

The proposed driveway access from SE Norton Lane provides the required clear vision area. A clear vision triangle is shown on the proposed Site Plan, drawings A1.0, A1.1, demonstrating compliance.

5. *Driveway cuts shall be a minimum of twenty feet from a street intersection.*

**Applicant Response:**

The proposed driveway location is centered on the intersection of SE Norton Lane and SE Stratus Avenue. This location is in compliance with City of McMinnville standards and policies.

- D. *Parking areas shall be made compatible with surrounding uses as follows:*

1. *Parking spaces along the outer boundaries of a parking area shall be contained by a curb or a bumper rail so placed to prevent a motor vehicle from extending over an adjacent property, sidewalk, or street.*

**Applicant Response:**

The parking areas on the proposed Site Plan provides concrete curbs or wheel stops preventing encroachment into adjacent properties. Additionally, there is a 6-foot sight obscuring fence along the property boundaries.

2. *When a parking area in a commercial or industrial zone abuts a property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line. The responsibility for placement of the fence or wall lies with the commercial or industrial property.*

**Applicant Response:**

The proposed Site Plan provides a 6-foot sight obscuring fence along the adjacent property boundaries.

3. *Artificial lighting which may be provided shall be so deflected as not to shine or create glare in any residential zone or on any adjacent dwelling.*

**Applicant Response:**

The proposed Site Lighting Plan, drawings ME 1.0 demonstrates the photometric footprint of the proposed site light fixtures. The proposed fixtures provide full cut-off of light on the adjacent properties preventing light and glare shining on the adjacent sites.

- E. *Space size minimum shall be as follows:*

1. *Handicap parking spaces shall be a minimum of twelve feet wide and 19 feet in length.*

**Applicant Response:**

Handicap Accessible parking spaces have been designed in compliance with Oregon Structural Specialty Code and the ANSI A117.1 standards and requirements. Van Accessible stalls are 9'x19' with an 8' wide access aisle and standard Handicap Accessible spaces are 9'x19' with an 6' wide access aisle. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.

2. *Compact and subcompact parking spaces shall be a minimum of eight feet by sixteen feet.*

**Applicant Response:**

The Compact Spaces on the proposed Site Plan are 8' wide x 16' long and are in compliance. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.

3. *Standard parking spaces shall be a minimum of eight feet six inches by nineteen feet.*

**Applicant Response:**

The Standard Spaces on the proposed Site Plan are 9' wide x 19' long and are in compliance. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.

**[Staff Note: Some spaces shown in the plan as 17 feet deep are counted as standard spaces.]**

**F. The type of space shall be set as follows:**

1. *Handicap spaces shall be required and designated as per current federal, state, and local regulations.*

**Applicant Response:**

The Handicap Accessible parking spaces have been designed to be in compliance with the Oregon Structural Specialty Code and the ANSI 117.1 standards and requirements.

2. *Standard spaces shall comprise not less than sixty-five percent of all newly constructed lot spaces.*

**Applicant Response:**

The total parking spaces provided is 218 spaces, of which 188 spaces are standard spaces. This represents 86 percent of the spaces provided.

**[Staff Note: Some spaces which are shown on the plan as 17' deep are counted in the above calculation as standard spaces. As shown, less than 65% of spaces meet the requirements for standard spaces.]**

**17.60.140 Bicycle Parking.**

**A. Bicycle parking facilities shall be required as follows:**

1. *In any commercial (C-1, C-2, and C-3) or office/residential (O-R) zone, bicycle parking facility requirements shall be based on the amount of automobile parking required. The minimum number of bicycle parking spaces provided shall be ten percent of the automobile parking spaces required.*
2. *The uses exempted from bicycle parking requirements include: residential uses, drive-in theaters, mortuaries, motels, hotels, and automobile service stations.*

**Applicant Response:**

As this is a multi-family residential use Bicycle Parking spaces are not required. However, the applicant is proposing to provide bicycle parking (bike racks) at each building. The proposed Site Plan includes 40 Bicycle Parking Spaces.

**FINDING (Chapter 17.60. Off-Street Parking and Loading): SATISFIED WITH CONDITIONS.** The applicant's narrative indicates the compliance with the minimum 65% standard spaces, but several of the spaces counted as standard spaces are 17 feet deep which is smaller than the dimensions of a standard space. The applicant shall modify the site plan to address this requirements and/or shall obtain approval from the Planning Commission to have a lower percentage of standard spaces. Other provisions comply with these standards.

**Chapter 17.61. Solid Waste and Recycling Enclosure Plan****17.61.030 Guidelines and Standards.**

- A. *The location of an enclosure must allow for collection agency drive-in access. A fifty-foot (50) access approach is recommended. In addition to the approach, either an exit that allows the truck to move forward or a turn area with a minimum radius of 46.5 feet is preferred. Both approach and location shall be unobstructed and free of over head wires and low hanging trees. An eighteen-foot (18) minimum height clearance above the enclosure approach is required and a thirty-two-foot (32) vertical clearance is required above the container itself. The enclosure shall be of sufficient size to store trash and recycling receptacles, the size of which shall be determined by the collection agency and will be based on proposed use. A minimum distance of two-feet (2) is required between the container and existing or proposed structures. The enclosure shall be a minimum of six-feet (6) tall or six inches (6) higher than the top of the tallest container.*

**Applicant Response:**

The proposed Site Plan provides the required front access and turn radii. There are no overhead wires located within the subject site.

Due to limited access and site constraints, a Fire Truck Turnaround is provided at the rear of the site. This turnaround may be used by the trash collection vehicles and provides adequate maneuvering for vehicle turnaround. The proposed Site Plan has been submitted to Recology Western Oregon (collection service) for review and approval.

All proposed Trash Enclosure structures are located in excess of the 20-foot minimum.

The proposed Trash Enclosure has been designed in accordance with Recology Western Oregon Solid Waste Plan Guide and Standards.

- B. *Solid waste enclosures shall not be located within twenty-feet (20) of a required front or exterior yard and should be placed at the rear of a building whenever possible. Should an enclosure be placed within a required landscaped front or exterior yard, additional landscaping must be provided elsewhere on the property to compensate for the encroachment into the required landscaped yard. Any modifications to the required landscaping must meet the approval of the Landscape Review Committee.*

**Applicant Response:**

The proposed Site Plan does not locate trash enclosures within the front yard setbacks and the subject site does not contain exterior yards. The proposed Site Plan does locate Trash Enclosures 5 feet from the adjacent side yards. A 6' high sight obscuring fence is proposed to be located along the property line.

- C. *Any trash or recycling enclosure which is visible from the street must provide landscaping around three (3) sides of the structure. Climbing vines and screening shrubs or hedges are appropriate and landscaping must be a minimum of three-feet (3) in height at the time of planting.*

**Applicant Response:**

There are no Trash Enclosures that will be visible from the street therefore this is not applicable.

- D. *Where a commercial or industrial zone abuts a residential zone, enclosures must be placed a minimum of thirty-feet (30) from any residential structure or as otherwise approved by the Planning Director.*



**Applicant Response:**

The adjacent properties to the North of the subject site are zoned C-3 Commercial. The property adjacent the subject site on the South is outside the City of McMinnville and the Urban Growth Boundary. The property to the West of the subject site is zoned residential and there are no trash enclosures proposed to be located within so feet of this adjacent residential property. The proposed trash enclosure at the rear of the site adjacent the North property line is 28'-4" East of this Westerly property line. Additional landscaping and screening is proposed adjacent this enclosure. There is also a six-foot sight obscuring fence proposed along westerly property line.

An Administrative Variance is being requested for this reduced setback.

- E. Generally, the design of the structure should match the exterior surface of the building and can be constructed of masonry, wood or concrete blocks in combination with plant material capable of forming a complete evergreen hedge. The floor of the enclosure shall be a concrete holding pad which must extend eight-feet (8) beyond the gates.*

**Applicant Response:**

The proposed Trash Enclosures are to be constructed of split-faced concrete masonry units with a solid cap. Landscape screening is proposed along the sides of the trash enclosures to provide the required evergreen hedge.

The concrete slab under the enclosure and in front of the enclosure will be reinforced and extend a minimum of 10 feet in front of the enclosure.

- F. Gates that screen the containers are required and must remain closed at all times except at times of service.*

**Applicant Response:**

Painted steel gates are proposed on all trash enclosures. This is illustrated in the Trash Enclosure Plan 5/A1.4, and the Trash Enclosure Elevations, 6/A1.4.

- G. Parking is prohibited in front of the enclosure and all parked vehicles must be located at a safe distance. A "No Parking" sign must be visibly placed on the gates of the enclosure.*

**Applicant Response:**

The proposed Site Plan shows "NO PARKING" pavement markings directly in front of the trash enclosures.

- H. Solid waste and recycling enclosures must be placed in a location that is compatible with the City of McMinnville's Fire Code.*

**Applicant Response:**

All proposed trash enclosure locations provide substantial separation from buildings as required by the Fire Code.

**FINDING (CHAPTER 17.61. SOLID WASTE AND RECYCLING ENCLOSURE PLAN): SATISFIED WITH CONDITIONS.** As provided in Section 17.61.020, this chapter applies to new multi-dwelling developments of three or more dwelling units. The plans and applicant's responses demonstrate compliance with these standards. The drawings and narrative materials demonstrate the materials meet requirements. The applicant shall obtain approval for the minor reduction of distance of the enclosure from the west property line, and the applicant shall provide a response from Recology approving the layout where any of the above dimensional standards aren't achieved.

**Chapter 17.62. Signs****APPLICANT RESPONSE:** N/A

**FINDING (Chapter 17.62. Signs): SATISFIED WITH CONDITIONS.** As a condition of approval, the applicant shall separately submit an application for a sign permit. The application will be reviewed for consistency with the applicable sign regulations of this Chapter and the specific provisions of Planned Development Overlay Ordinance 5072. The sign permit application will also be reviewed for consistency with the provisions of the Three Mile Lane Planned Development Overlay Ordinance, which is also referenced in Ordinance 5072.

**Variances VR 3-23**

The applicant has requested flexibility regarding certain standards of the Zoning Ordinance, and one provision of the PD Overlay Ordinance 5072. Some flexibility can be granted as part of the Planned Development Review. Other flexibility requires a variance.

The applicant is requesting flexibility regarding the following provisions of Chapter 17.11. These items can be approved within the flexibility allowed for the PD approval.

- Allow 63' rather than 60' between parking lot islands.
- Allow parking spaces within 6' rather than 10' of north and south property lines. (Provide 10' from west property line that is residential). To the north are the abutting parking lots of those developments. To the south is outside the UGB.
- Allow trash enclosure less than 30' from west property line abutting residential zone. (Trash enclosures would be 6-feet of other interior property lines which abut non-residential zoning, and would abut screened parking lot areas).

**Note: The applicant didn't request flexibility regarding the minimum percentage of standard parking spaces, but several spaces counted as standard spaces are smaller than required. The applicant will also need to modify the site plan and/or request flexibility to allow a greater percentage of compact spaces.**

The applicant is requesting a variance to the 35' height limit established in the PD ordinance 5072. This item requires approval of a variance subject to the standard variance criteria. Further, it is ineligible for an administrative variance even though it is less than 10% of the standard, because building height is specifically listed as an item which doesn't qualify for an administrative variance.

- Request to exceed 35' height limit of PD ordinance to allow height of highest ridge to 37'.

**CHAPTER 17.74 REVIEW CRITERIA****17.74.090 Administrative Variance-Review Criteria.**

*The Planning Director may grant limited adjustments to the terms of this title as follows:*

- A. *Lot area: Maximum possible adjustment of one percent of the minimum lot area, but not more than 90 (ninety) square feet;*
- B. *Setbacks: Maximum adjustment of 10 (ten) percent of the required setback.*
- C. *These provisions shall be used sparingly and shall not be exceeded except by regular referral to the Planning Commission.*
- D. *Special conditions may be attached to adjustments if such conditions relate directly to the adjustments.*

**Applicant Response:**

The applicant is seeking approval of the following Administrative Variances:

1. 17.11.090, D (4), d (7): Landscape Islands shall be separated no more than 60 feet from another.

Proposed:

The Site Plan proposes a maximum distance of 63 feet between parking area landscape islands. This is an increase of 3 feet which is less than 10% of the required dimension. Standard parking stalls are 9 feet wide and the 60 foot limitation does not coincide with standard parking stall dimensions affecting an efficient parking layout and the best use of the site area to achieve the densities identified by the zoning codes. The requested increase of 5% has no adverse impacts upon the site or the amount of landscaping proposed on the Landscape Plan.

2. 17.11.090, D (4) f (3): *Off-street parking spaces shall not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.*

Proposed:

The parking areas on the proposed Site Plan are located at a distance of 6-feet from the North and South (side) property lines. This is less than the 10 feet required from the adjacent side property lines. This is compatible with existing adjacent development that also has parking located 6 feet from the adjacent property lines.

**Applicant Response:**

The requested adjustment to the landscape island spacing in parking areas is affected by the recently adopted Chapter 17.11 Residential Design Standards. Previously development in the vicinity of the subject site was not developed under these requirements. As noted above, the 60-foot spacing identified in the newly adopted standard does not take into account the standard or compact parking stall spacing. The net result is inefficient utilization of the available site area for parking thus affecting the overall net achievable density.

The requested adjustment for proposed parking areas to be located less than 10 feet from the adjacent property lines is compatible with the existing adjacent developments as they have parking located at 6 feet from the adjacent property lines. As the subject site has a designated C-3 General Commercial zoning designation, the parking setback is not required. As a multiple family apartment use, the Residential Design Standards of 17.11 are required. Previous adjacent built developments have not been required to meet this standard. The proposed variance is fully compatible with the adjacent developments.

3. 17.61.130 (D): *Where a commercial or industrial zone abuts a residential zone, enclosures must be placed a minimum of thirty-feet (30) from any residential structure or as otherwise approved by the Planning Director*

Proposed:

The property West of the subject site is zoned R-4 Medium-High Density Residential. The proposed trash enclosure at the rear of the site adjacent the North property line is 28'-4" East of this Westerly property line. The rear yard of the existing mobile home is more than 15 feet west of this property line therefore the proposed trash enclosure location is more than 30 feet from the adjacent residential structure.

Additional landscaping and screening is proposed adjacent this enclosure. There is also a six-foot sight obscuring fence with landscape buffering and screening proposed along westerly property line.

**FINDING: SATISFIED WITH CONDITIONS.** Staff recommends finds the requested relief consistent with the intent of the policies intended to be implemented by these standards. Staff recommends approval with conditions, based on the plans submitted by the applicant, and that the variances are specific to this plan. The requested relief either meets the requirements for administrative variances and/or is within the authority of the Planning Commission to grant as part of the Planned Development Amendment.

**17.74.100 Variance-Planning Commission Authority.**

*The Planning Commission may authorize variances from the requirements of this title where it can be shown that, owing to special and unusual circumstances related to a specific piece of property, strict application of this title would cause an undue or unnecessary hardship, except that no variance shall be granted to allow the use of property for a purpose not authorized within the zone in which the proposed use would be located. In granting a variance, the Planning Commission may attach conditions which it finds necessary to protect the best interests of the surrounding property or neighborhood and otherwise achieve the purposes of this title.*

**Applicant Response:**

The applicant is seeking approval of the following Variance through the Planning Commission Authority:

1. Planned Development Overlay Ordinance 4709, Condition 11:  
This Condition of Approval limits the Building height to 35' to the highest point of the roof. The applicant is seeking a Variance to increase the roof height to 37'-0" to the highest ridge line. The proposed building design with sloped and hip roof forms, maintains a low-profile roof line and the requested additional height to 37'-0" will not have any adverse impacts on the existing development in the surrounding area.

**17.74.110 Conditions for Granting Variance.**

*A variance may be granted only in the event that the following circumstances substantially exist:*

- A. *Exceptional or extraordinary circumstances apply to the property which do not apply generally to other properties in the same zone or vicinity, and result from lot size or shape legally existing prior to the date of the ordinance codified in this title, topography, or other circumstance over which the applicant has no control;*

**Applicant Response:**

The allowable building height within the underlying C-3 General Commercial Zone is 80 feet. As this is a residential development, the requirements of the R-4 Medium-High Density Zone govern which allows a building height of 68 feet. The existing adjacent development includes numerous buildings that exceed the 35' height limitation. These include the Willamette Valley Medical Center Hospital and the Comfort Inn Motel which is also three-stories and exceeds 35' in building height.

Other properties within the City of McMinnville with the same zoning do not have this height restriction and would allow for a greater density of needed housing to be developed.

- B. *The variance is necessary for the preservation of a property right of the applicant substantially the same as owners of other property in the same zone or vicinity possess;*

**Applicant Response:**

The imposed height restriction limits the density and number of units that can be built on this site. The proposed development needs to achieve three story building heights to be financially feasible. Other adjacent developed properties have buildings exceeding the 35' height limitation and the proposed height preserves a comparable development opportunity already provided to the adjacent developments.

- C. *The variance would not be materially detrimental to the purposes of this title, or to property in the zone or vicinity in which the property is located, or otherwise conflict with the objectives of any city plan or policy;*

**Applicant Response:**

The proposed sloped roof forms have an eave height of 26'-9" with a ridge height just under 37 feet. With the sloped roofs and hip roof design, the impacts of the requested 37' height is not noticeable and results in less impact than a building with a vertical roof parapet wall at 35 feet. The proposed roof eave line provides a much lower perceived overall building height. Additionally, the adjacent properties in the vicinity of the subject site have roof heights far greater than 35 feet. The proposed height adjustment is fully compatible with the adjacent developed properties.

- D. *The variance requested is the minimum variance which would alleviate the hardship.*

**Applicant Response:**

The proposed three-story building design uses 8' high ceilings within the living units, which is considered minimum for market rate apartment units. Given the floor structure and the required heel trusses due to the new energy code requirements, the requested height of 37 feet is the minimum height to accommodate the three-story units. As previously addressed, the project is not feasible if three story building cannot be utilized.

**FINDING (17.64.110(A)-(D)): SATISFIED WITH CONDITIONS.** This variance is based on the plans submitted by the applicant. The requested relief is a minor deviation from the standards. The allowed Building height would be 80' if not limited to 35' by the PD Ordinance. The applicant is requesting a minor increase that is consistent with the intent and purpose of the condition and height limitation. The predominant building volume will be within the 35' height limit and the eave lines are well within the height limit. Only the highest, narrow portion of the highest eave line will exceed 35' to approximately 37'.

TS



Planning Department
231 NE Fifth Street o McMinnville, OR 97128
(503) 434-7311 Office o (503) 474-4955 Fax
www.mcminnvilleoregon.gov

Office Use Only:
File No. PDA 5-23
Date Received 8/14/23
Fee \$4,127.00
Receipt No. 208596
Received by AW

569-23-000457-PLNG

Planned Development Amendment Application

Applicant Information

Applicant is: [ ] Property Owner [ ] Contract Buyer [ ] Option Holder [ ] Agent [x] Other Architect/Planner
Applicant Name Reiter Design Architect Incorporated Phone (503) 574-3036
Contact Name Scott Reiter Phone (503) 789-6461
Address 6107 SW Murray Blvd., #480
City, State, Zip Beaverton, OR. 97008
Contact Email ScottRDG@aol.com

Property Owner Information

Property Owner Name KWDS, LLC Phone (503) 781-5685
Contact Name Chad Juranek Phone
Address PO Box 145
City, State, Zip Wilsonville, OR. 97070
Contact Email Cjuranek@jkmanage.com

Site Location and Description

(If metes and bounds description, indicate on separate sheet)
Property Address SE Norton Lane at SE Stratus Ave.
Assessor Map No. R4 4 - 4 - 27 Total Site Area 214,759 sf, 4.93 acres
Subdivision Block Lot 701
Comprehensive Plan Designation Commercial Zoning Designation C-3 General Commercial

1. Show in detail how your request seeks to amend the existing planned development overlay. State the reason(s) for the request and the intended use(s) of the property: \_\_\_\_\_

See Attached Narrative

2. Show in detail, by citing specific goals and policies, how your request is consistent with applicable goals and policies of the McMinnville Comprehensive Plan (Volume II): \_\_\_\_\_

See Attached Narrative

3. Considering the pattern of development in the area and surrounding land uses, show, in detail, how the proposed amendment is orderly and timely: \_\_\_\_\_

See Attached Narrative

4. Describe any changes in the neighborhood or surrounding area which might support or warrant the request: \_\_\_\_\_

See Attached Narrative



5. Document how the site can be efficiently provided with public utilities, including water, sewer, electricity, and natural gas, if needed, and that there is sufficient capacity to serve the proposed use: \_\_\_\_\_

See Attached Narrative

6. Describe, in detail, how the proposed use will affect traffic in the area. What is the expected trip generation? \_\_\_\_\_

See Attached Narrative

In addition to this completed application, the applicant must provide the following:

- A site plan (drawn to scale, legible, and of a reproducible size). The site plan should show existing and proposed features such as: access; lot and street lines with dimensions in feet; distances from property lines; improvements; north direction arrow, and significant features (slope, vegetation, adjacent development, drainage, etc.).
- A copy of the current planned development overlay ordinance.
- A legal description of the subject site, preferably taken from the deed.
- Compliance of Neighborhood Meeting Requirements.
- Payment of the applicable review fee, which can be found on the Planning Department web page.

I certify the statements contained herein, along with the evidence submitted, are in all respects true and are correct to the best of my knowledge and belief.

Applicant's Signature

8/10/23  
Date

Property Owner's Signature

Date

manages KWDS, LLC

8-11-23

NORTON LANDING  
APARTMENTS  
McMinnville, Oregon

**LAND-USE APPLICATION**

**PLANNED DEVELOPMENT AMENDMENT**

AUGUST 14, 2023

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070

**RDA**  

---

REITER DESIGN ARCHITECT  
INCORPORATED

# NORTON LANDING APARTMENTS

## PLANNED DEVELOPMENT AMENDMENT TABLE OF CONTENTS

1. LAND-USE APPLICATION: Planned Development Amendment
2. PROJECT DESIGN TEAM
3. TAX MAP, LEGAL DESCRIPTION, VICINITY MAP
4. PRELIMINARY TITLE REPORT
5. APPLICATION NARRATIVES:
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  - Conditions of Approval, Ord. 4709, Ord. 5072 page 04
  - Comprehensive Plan-Volume II, Goals and Policies page 10
  - McMinnville Zoning Code Compliance
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  - Ordinance 4131 (Three Mile Lane Planned Development Overlay),
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8. NEIGHBORHOOD MEETING MATERIALS
9. APPLICATION DRAWINGS: Architectural, Civil and landscape

**LAND-USE APPLICATION**  
Planned Development Amendment

**1**

# Planned Development Amendment Information & Submittal Requirements



Planning Department  
231 NE Fifth Street ◦ McMinnville, OR 97128  
(503) 434-7311 Office ◦ (503) 474-4955 Fax  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

## Overview

A planned development is applied to property as a vehicle to encourage variety in the development pattern of the community; encourage mixed uses in a planned area; encourage developers to use a creative approach and apply new technology in land development; preserve significant man-made and natural features; facilitate a desirable aesthetic and efficient use of open space; and create public and private open spaces. A planned development is not intended as a guise to circumvent the intent of the Zoning Ordinance. Once adopted and applied to a property, the planned development – in concert with the Zoning Ordinance – guides development within the subject property. The City recognizes that changes to the details of a planned development may be necessary on occasion due to circumstances that may be beyond the control of the property owner. This application is used to request such amendments.

An amendment to an existing planned development overlay may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Any text amendments or major changes to an adopted site plan must be approved by the Planning Commission. Major changes to an adopted site plan include the following:

- A. An increase in the amount of land within the subject site;
- B. An increase in density, including the number of housing units;
- C. A reduction in the amount of open space; or
- D. Changes to the vehicular system which results in a significant change to the location of streets, shared driveways, parking areas, and access.

For further clarification as to what constitutes a major or minor amendment, you are encouraged to contact the Planning Department before submitting an application.

## Application Submittal

The following materials must be provided at the time of submittal, or the application will not be accepted for processing.

- A completed Planned Development Amendment application form. If additional explanation or materials would assist or support the request, include them with the application form.
- A site plan (drawn to scale, with a north direction arrow, legible, and of a reproducible size). The site plan should show existing and proposed features such as: access; lot and street lines with dimensions in feet; distances from property lines; improvements; and significant features (slope, vegetation, adjacent development, drainage, etc.).
- A legal description of the subject site, preferably taken from the deed.



- A copy of the current planned development overlay ordinance.
- Compliance of Neighborhood Meeting Requirements.
- Payment of the applicable review fee.

## **Review Process**

Upon receipt of a complete application for a major change to an existing planned development, the Planning Department will schedule a date and time for the Planning Commission's public hearing on the request, and provide notification of the proposed amendment to property owners within 300 feet of the subject site. The Planning Commission's public hearing will follow the procedures as stated in Sections 17.72.120 (Applications – Public Hearings) and 17.72.130 (Public Hearing Process) of the Zoning Ordinance.

Approval of an amendment to an existing planned development requires that the applicant demonstrate that the following criteria, as stated in Section 17.74.070 (Planned Development Amendment – Review Criteria) of the Zoning Ordinance have been met:

- A. There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;
- B. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;
- C. The development shall be designed so as to provide for adequate access to, and efficient provision of, services to adjoining parcels;
- D. The plan can be completed within a reasonable period of time;
- E. The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;
- F. Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;
- G. The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.

Following the close of the hearing, the Commission will vote to forward a recommendation to the City Council to approve the requested amendment, or approve it with a different form. If the Commission recommends the request be denied, no further proceedings shall be held, unless an appeal of the Commission's decision is filed, as stated in Section 17.72.180 (Appeal from Ruling of Planning Commission) of the Zoning Ordinance.

Upon receipt of the decision of the Planning Commission to recommend approved the Council shall:

- A. Based on the material in the record and the findings adopted by the Commission and transmitted to the City Council, adopt an ordinance effecting the proposed change, or;
- B. Call for a public hearing on the proposal subject to the notice requirements stated in Section 17.72.120(D-F) (Applications – Public Hearings) of the Zoning Ordinance.

# **PROJECT DESIGN TEAM**

# **2**

# NORTON LANDING APARTMENTS

## PROJECT DESIGN TEAM

### **OWNER**

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070  
(503) 781-5685  
Owner's Representative: Chad Juranek

### **ARCHITECT / PLANNER**

REITER DESIGN ARCHITECT, INCORPORATED  
6107 SW Murray Blvd., #480  
Beaverton, Oregon 97008  
(503) 574-3036  
Project Architect, Project Planner: Scott A. Reiter

### **CIVIL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
250 NW Franklin Avenue, Suite 404  
Bend, Oregon 97703  
(541) 318-1161  
Project Civil Engineer: Jennifer VanCamp

### **LANDSCAPE ARCHITECT**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Landscape Architect: Jeffery Creel

### **STRUCTURAL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Engineer: Jeff Schwindt

### **TRANSPORATION ENGINEER**

LANCASTER-MOBLEY  
321 SW 4<sup>th</sup> Ave., Suite 400  
Portland, Oregon 97204  
(503) 248-0313  
Project Engineer: Daniel Stumpf



**TAX MAP  
LEGAL DESCRIPTION  
VICINITY MAP**

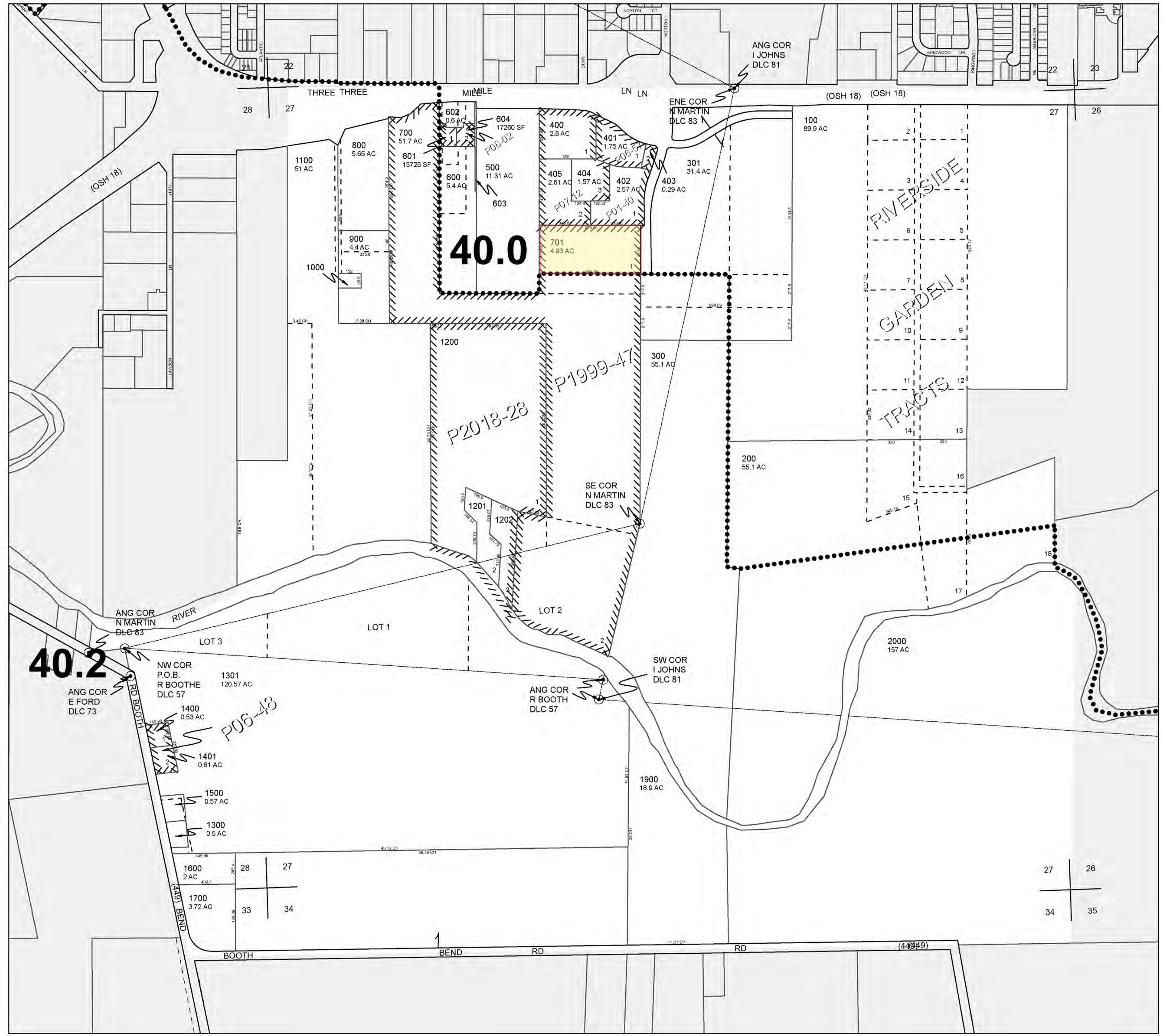
**3**





ASSESSMENT & TAX  
CARTOGRAPHY

SECTION 27 T.4S. R.4W. W.M.  
YAMHILL COUNTY OREGON  
1" = 400'



- CANCELLED TAXLOTS:
- 1800
  - 801
  - 302
  - 201

DATE PRINTED: 4/12/2019

This product is for Assessment and Taxation (A&T) purposes only and has not been prepared or is suitable for legal, engineering, surveying or any purposes other than assessment and taxation.



**EXHIBIT "A"**  
**LEGAL DESCRIPTION**

Parcel 1 of Partition Plat 99-47, recorded November 02, 1999 as Instrument No. 199921786, Deed and Mortgage Records, Yamhill County, Oregon.



Evergreen  
Mobile Home  
Park

Comfort Inn

Altimus Plaza

Willamette Valley  
Medical Center

Subject Site

THREE MILE LANE (HIGHWAY 18)



# **PRELIMINARY TITLE REPORT**

**4**



320 Church St. NE, Salem, OR 97301  
PHONE (503)581-1431 FAX (503)364-8716

---

To: Stewart Title Company  
4900 Meadows Rd, Ste. 150  
Lake Oswego, OR 97035  
Attn: Zach Bay

Date: July 31, 2023  
Order No. 601760AM  
Reference: APN: 515923  
McMinnville, OR 97128

Your File No.: 2091909

We have enclosed our Preliminary Title Report pertaining to order number 601760AM.

***Thank you for the opportunity to serve you. Your business is appreciated!***

If you have any questions or need further assistance, please do not hesitate to contact your Title Officer listed below.

Sincerely,

***Teri Due***

teri.due@amerititle.com  
Title Officer

**NOTICE: Please be aware that, due to the conflict between federal and state laws concerning the legality of the cultivation, distribution, manufacture or sale of marijuana, the Company is not able to close or insure any transaction involving land that is associated with these activities.**



AmeriTitle, LLC  
 320 Church St. NE, Salem, OR 97301  
 PHONE (503)581-1431 FAX (503)364-8716

---

July 31, 2023  
 File Number: 601760AM  
 Report No.: 1  
 Title Officer: Teri Due Email: teri.due@amerititle.com

**PRELIMINARY TITLE REPORT**

**Property Address:** APN: 515923, McMinnville, OR 97128

| <u>Policy or Policies to be issued:</u>              | <u>Liability</u> | <u>Premium</u> |
|--|------------------|----------------|
| ALTA LENDER'S COMMERCIAL ( X ) EXTENDED ( ) STANDARD | \$TBD            | \$0.00         |
| Proposed Insured:                                    |                  |                |
| Endorsements: OTIRO End 209.10-06                    |                  | \$100.00       |
| Local Government Lien Search                         |                  | \$50.00        |

We are prepared to issue ALTA (07/01/21) title insurance policy(ies) of Stewart Title Guaranty Company, in the usual form insuring the title to the land described as follows:

**Legal description attached hereto and made a part hereof marked Exhibit "A"**

and dated as of 28th day of July, 2023 at 7:30 a.m., title is vested in:

**KWDS LLC, an Oregon limited liability company**

The estate or interest in the land described or referred to in this Preliminary Title Report and covered herein is:

FEE SIMPLE



**Except for the items properly cleared through closing, Schedule B of the proposed policy or policies will not insure against loss or damage which may arise by reason of the following:**

**GENERAL EXCEPTIONS:**

1. Taxes or assessments which are not shown as existing liens by the records of any taxing authority that levies taxes or assessments on real property or by the Public Records; proceedings by a public agency which may result in taxes or assessments, or notices of such proceedings, whether or not shown by the records of such agency or by the Public Records.
2. Facts, rights, interests or claims which are not shown by the Public Records but which could be ascertained by an inspection of the Land or by making inquiry of persons in possession thereof.
3. Easements, or claims of easement, not shown by the Public Records; reservations or exceptions in patents or in Acts authorizing the issuance thereof; water rights, claims or title to water.
4. Any encroachment (of existing improvements located on the subject Land onto adjoining Land or of existing improvements located on adjoining Land onto the subject Land) encumbrance, violation, variation, or adverse circumstance affecting the Title that would be disclosed by an accurate and complete land survey of the subject Land.
5. Any lien, or right to a lien, for services, labor, material, equipment rental, or workers compensation heretofore or hereafter furnished, imposed by law and not shown by the Public Records.

EXCEPTIONS 1 THROUGH 5 ABOVE APPLY TO STANDARD COVERAGE POLICIES AND MAY BE MODIFIED OR ELIMINATED ON AN EXTENDED COVERAGE POLICY.

**SPECIAL EXCEPTIONS:**

**Tax Information:**

Taxes assessed under Code No. 40.0 Account No. 515923 Map No. R4427 00701

NOTE: The 2022-2023 Taxes: \$13,715.66, are Paid

6. [Taxes](#) assessed under Code No. 40.0 Account No. 515923 [Map](#) No. R4427 00701  
The 2023-2024 Taxes: A lien not yet due or payable.
7. City liens, if any, of the City of McMinnville.
8. The property lies within and is subject to the levies and assessments of the Yamhill Soil and Water Conservation District.
9. Right, title and interest of the public in and to those portions of the Land lying within roads, streets or highways.
10. Limited access provisions contained in Deed from Richard A. Moyer and Phyllis A. Folkel Moyer and Gene A. McMullen and Dorothy D. Folkel Moyer to State of Oregon, by and through its State Highway Commission, which provided that no right or easement of right of access to, from or across the State Highway other than expressly therein provided for shall attach to the abutting property,  
Recorded: March 27, 1989  
Instrument No.: [Film Volume: 230 Page: 1748](#)



11. An easement including the terms and provisions thereof, affecting the portion of said Land and for the purposes stated therein as set forth in instrument:  
Granted To: Burch Feero Inc., an Oregon Corporation  
Recorded: August 25, 1998  
Instrument No.: [1998-016670](#)
  
12. An easement including the terms and provisions thereof, affecting the portion of said Land and for the purposes stated therein as set forth in instrument:  
Granted To: Aviation Easement  
Recorded: June 11, 2002  
Instrument No.: [2002-11461](#)
  
13. Easements as shown on the Partition [Plat](#) No. 1999-047.
14. Personal property taxes, if any.
15. Unrecorded leaseholds, if any, and the rights of vendors and holders of security interest in personal property of tenants to remove said personal property at the expiration of the term.
16. The Company will require a copy of the Operating Agreement (including any approvals of withdrawal of member(s) or acceptance of new member(s)) and the Articles of Organization of KWDS, LLC for its examination prior to closing. Any conveyance or encumbrance of the Limited Liability Company's property must be executed by all of the members unless otherwise provided for in the Operating Agreement.

The Company reserves the right to add additional items or make further requirements after review of the requested documentation.

17. Any lien, or right to a lien, for services, labor, material, equipment rental, or workers compensation heretofore or hereafter furnished, imposed by law and not shown by the Public Records.

To remove this item, the Company will require an affidavit and indemnity on a form supplied by the Company.

IF THE ABOVE EXCEPTION IS TO BE REMOVED FROM A FORTHCOMING POLICY PRIOR TO THE EXPIRATION OF THE STATUTORY LIEN PERIOD, THE COMPANY MUST BE CONTACTED REGARDING ITS UNDERWRITING REQUIREMENTS FOR EARLY ISSUE.

18. Persons in possession or claiming the right of possession.

To remove this item, the Company will require an affidavit and indemnity on a form supplied by the Company.

#### **INFORMATIONAL NOTES:**

- NOTE: Our examination of the title to the subject property discloses no open Deeds of Trust or Mortgages of record. The accuracy of this conclusion should be confirmed in writing prior to closing of the proposed transaction.
- NOTE: This report does not include a search for financing statements filed in the office of the Secretary of State in this or any other State, or in a county other than the county wherein the premises are situated, and no liability is assumed if a financing statement is filed in the office of the County Clerk (Recorder) covering fixtures on the premises wherein the lands are described other than by metes and bounds or under the rectangular survey system by recorded lot and block.

NOTE: As of the date hereof, there are no matters against the party(ies) shown below which would appear as exceptions to coverage in a title insurance product:

Parties:

KWDS LLC, an Oregon limited liability company

NOTE: The following is the last deed of record affecting said Land,

Document: Statutory Warranty Deed

Grantor: Gregory K. McMuliin and Cheryl L. McMuliin, Trustees of the GK & CL Trust, established under a Declaration of Trust dated March 25, 2011, as to an undivided one-half interest, Phyllis A. Moyer, Trustee of the Phyllis A. Moyer Living Trust dated September 10, 1992, as to an

undivided one-half interest, as tenants in common

Grantee: KWDS LLC, an Oregon limited liability company

Recorded: July 12, 2019

Instrument No.: 2019-09282

NOTE: Any map or sketch enclosed as an attachment herewith is furnished for information purposes only to assist in property location with reference to streets and other parcels. No representation is made as to accuracy and the company assumes no liability for any loss occurring by reason of reliance thereon.

NOTE: Your application for title insurance was placed by reference to only a street address or tax identification number. Based on our records, we believe that the legal description in this report covers the parcel(s) of Land that you requested. If the legal description is incorrect, the parties to the transaction must notify the Company and/or the settlement company in order to prevent errors and to be certain that the correct parcel(s) of Land will appear on any documents to be recorded in connection with this transaction and on the policy of title insurance.

NOTE: Due to current conflicts or potential conflicts between state and federal law, which conflicts may extend to local law, regarding marijuana, if the transaction to be insured involves property which is currently used or is to be used in connection with a marijuana enterprise, including but not limited to the cultivation, storage, distribution, transport, manufacture, or sale of marijuana and/or products containing marijuana, the Company declines to close or insure the transaction, and this Preliminary Title Report shall automatically be considered null and void and of no force and effect.

THIS PRELIMINARY TITLE REPORT IS NOT AN ABSTRACT OF TITLE, REPORT OF THE CONDITION OF TITLE, LEGAL OPINION, OPINION OF TITLE, OR OTHER REPRESENTATION OF THE STATUS OF TITLE. THE PROCEDURES USED BY THE COMPANY TO DETERMINE INSURABILITY OF THE TITLE, INCLUDING ANY SEARCH AND EXAMINATION, ARE PROPRIETARY TO THE COMPANY, WERE PERFORMED SOLELY FOR THE BENEFIT OF THE COMPANY, AND CREATE NO EXTRACTIONAL LIABILITY TO ANY PERSON, INCLUDING A PROPOSED INSURED.

This report is preliminary to the issuance of a policy of title insurance and shall become null and void unless a policy is issued and the full premium paid.

**End of Report**

*"Superior Service with Commitment and Respect for Customers and Employees"*

**EXHIBIT "A"**  
**LEGAL DESCRIPTION**

Parcel 1 of Partition Plat 99-47, recorded November 02, 1999 as Instrument No. 199921786, Deed and Mortgage Records, Yamhill County, Oregon.

Kirk Bass  
40 CHATEAU REALTY SIMONS  
1103 N BAKER  
McMinnville, OR 97128

OFFICIAL YAMHILL COUNTY RECORDS  
CHARLES STERN, COUNTY CLERK



\$61.00

200211461

1:20:59 PM 6/11/2002

DMR-EDMR Cnt=1 Stn=1 KAREN  
\$40.00 \$10.00 \$11.00

Upon recording, please return to:  
Locke Liddell & Sapp LLP  
2200 Ross Avenue, Suite 2200  
Dallas, Texas 75201  
Attention: Brian R. Forbes

### AVIATION EASEMENT

THIS AVIATION EASEMENT ("Easement") is granted this 22 day of NOVEMBER, 1999, by PHYLLIS A. MOYER, Trustee of the Phyllis A. Moyer Living Trust dated September 10, 1992, as to an Undivided 1/2 Interest, and GENE A. MCMULLIN AND DOROTHY D. FOLKEL MCMULLIN, Co-Trustees of the McMullin Family Trust by Agreement dated May 8, 1996, as to an Undivided 1/2 Interest (herein referred to collectively as "Owner"), and WILLAMETTE VALLEY MEDICAL CENTER, LLC, an Oregon limited liability company, d/b/a Willamette Valley Medical Center (herein referred to as "WVMC").

The parties recite and declare that:

A. Owner is the owner of real property more particularly described on Exhibit A attached hereto and incorporated herein (the "Property").

B. WVMC is the owner of that certain piece of property more particularly described on Exhibit B attached hereto and incorporated herein (the "Hospital").

In consideration of the sum of Ten Dollars (\$10.00) and other valuable consideration, the receipt and sufficiency of which are hereby acknowledged and of the mutual promises and covenants contained herein, the parties hereto agree as follows:

#### SECTION I

#### GRANT OF EASEMENT

Owner, for the above recited consideration and in consideration of a zoning change, ZC11-99, approved by the Planning Commission on August 19, 1999 and passed by the City Council through Ordinance No. 4609 on September 28, 1999 which is hereby acknowledged and for the benefit of the public in its use of the airport, does hereby grant and convey to the WVMC, its successors and assigns a perpetual easement and right of way across the Property for the passage of all aircraft (for purposes of this Easement, the term "aircraft" shall mean any contrivance now known or hereafter invented, used, or designed for navigation of or flight in the air, including, but not limited to, helicopters).

PAGE 1/8 - EASEMENT

99998:00141:DALLAS:645909.2  
11/19/99 4:01 PM

## SECTION II

### DESCRIPTION OF EASEMENT

This Easement grants to WVMC, its employees, agents, independent contractors, invitees, visitors and guests the rights to operate aircraft in the air space one hundred fifty (150) feet or more above the surface elevation of Owner's Property and includes the right to cause in all said air space one hundred fifty (150) feet or more above the surface elevation of Owner's Property, such noise, vibrations, fumes, dust, fuel particles, and all other effects that may be caused by the operation of any aircraft landing at, or taking off from, or operating at or on the Hospital.

Owner further covenants and agrees to perpetually restrict the following activities or uses of the Property:

1. Prohibit radio or electromagnetic interference that interferes in the operation of aircraft landing on or at the Hospital.
2. Prohibit the construction of buildings or any structures, including radio antennae or any other antennae or poles, or of natural vegetation that interferes in the operation or flight path of aircraft landing at the Hospital and specifically restricts the height of all buildings, structures, antennae, towers and vegetation to not exceed one hundred fifty (150) feet above the surface of the Property.
3. Prohibit lights, lighted signs, and other lighted objects that interfere in the operation of aircraft landing or taking off at the Hospital.
4. Prohibit hazardous or unreasonable objectionable smoke, fumes, or vapors that interfere in the operation of aircraft to and from the Hospital.
5. Owner agrees to secure all tables, chairs, awnings and all other structures or personal property located on the Property in a manner which will not be lifted or moved by the use of aircraft landing at the Hospital. Owner acknowledges that the use of helicopters may lift such loose items off the ground and obstruct the helicopter's approach to the Hospital.

## SECTION III

### DUTY TO INFORM TENANTS

Owner also agrees to advise future tenants and occupiers of said described property of the terms and conditions of this Easement and the covenants contained herein by including notice of said Easement in any rental agreements executed.

PAGE 2 <sup>18</sup> EASEMENT

99998:00141:DALLAS:645909.2  
11/19/99 4:01 PM

SECTION IV

RELEASE AND WAIVER

Owner does hereby fully waive, remise, and release any right or cause of action which it may now have or which it may have in the future against WVMC, its successors and assigns, due to such noise, vibrations, fumes, dust, fuel particles, and all other effects that may be caused or may have been caused by the operation of (i) aircraft using the easement granted herein or (ii) aircraft landing at, or taking off from, or operating at or on the Hospital.

SECTION V

COVENANTS RUNNING WITH THE LAND

This Easement and right-of-way and all rights appertaining thereto, together with all covenants contained herein, shall be held by WVMC, its successors and assigns. Owner further understands and agrees that the covenants and agreements contained in this Easement shall run with the land and shall be binding on the successors and assigns of Owner.

This Easement shall bind the parties hereto, their heirs, administrators, executors, successors and assigns, and each and every one of them as though specifically named herein.

IN WITNESS WHEREOF, the parties have executed this Easement as of the date first written above.

PHYLLIS A. MOYER, Trustee of the Phyllis A. Moyer Living Trust dated September 10, 1992 as to an Undivided 1/2 Interest

by: \_\_\_\_\_  
Phyllis A. Moyer, Trustee

and GENE A. MCMULLIN AND DOROTHY D. FOLKEL MCMULLIN, Co-Trustees of the McMullin Family Trust by Agreement dated May 8, 1996 as to an Undivided 1/2 Interest

by: Gene A. McMullin  
Gene A. McMullin, Trustee

by: Dorothy D. Folkel McMullin  
Dorothy D. Folkel McMullin, Trustee

SECTION IV

RELEASE AND WAIVER

Owner does hereby fully waive, remise, and release any right or cause of action which it may now have or which it may have in the future against WVMC, its successors and assigns, due to such noise, vibrations, fumes, dust, fuel particles, and all other effects that may be caused or may have been caused by the operation of (i) aircraft using the easement granted herein or (ii) aircraft landing at, or taking off from, or operating at or on the Hospital.

SECTION V

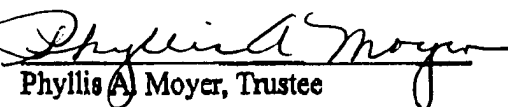
COVENANTS RUNNING WITH THE LAND

This Easement and right-of-way and all rights appertaining thereto, together with all covenants contained herein, shall be held by WVMC, its successors and assigns. Owner further understands and agrees that the covenants and agreements contained in this Easement shall run with the land and shall be binding on the successors and assigns of Owner.

This Easement shall bind the parties hereto, their heirs, administrators, executors, successors and assigns, and each and every one of them as though specifically named herein.

IN WITNESS WHEREOF, the parties have executed this Easement as of the date first written above.

PHYLLIS A. MOYER, Trustee of the Phyllis A. Moyer Living Trust dated September 10, 1992  
as to an Undivided 1/2 Interest

by:   
Phyllis A. Moyer, Trustee

and GENE A. MCMULLIN AND DOROTHY D. FOLKEL MCMULLIN, Co-Trustees of the  
McMullin Family Trust by Agreement dated May 8, 1996 as to an Undivided 1/2 Interest

by: \_\_\_\_\_  
Gene A. McMullin, Trustee

by: \_\_\_\_\_  
Dorothy D. Folkel McMullin, Trustee

STATE OF Arizona )  
County of Maricopa )

ss.

On November 22, 1999 before me, the undersigned, a Notary Public in and for said State, personally appeared GENE A. MCMULLIN, Co-Trustee of the McMullin Family Trust, personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) are subscribed to the within instrument and acknowledged to me that they executed the same in their authorized capacity(ies), and that by their signatures on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal  
 

NOTARY PUBLIC FOR \_\_\_\_\_

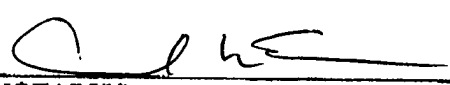
Anne M Smith  
(Notary's name must be typed or legibly printed)

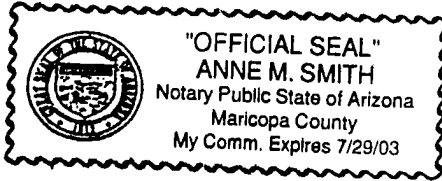
STATE OF Arizona )  
County of Maricopa )

ss.

On November 22, 1999 before me, the undersigned, a Notary Public in and for said State, personally appeared DOROTHY D. FOLKEL MCMULLIN, Co-Trustee of the McMullin Family Trust, personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) are subscribed to the within instrument and acknowledged to me that they executed the same in their authorized capacity(ies), and that by their signatures on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal.

  
NOTARY PUBLIC FOR \_\_\_\_\_



Anne M Smith  
(Notary's name must be typed or legibly printed)



STATE OF CALIFORNIA )  
 )  
County of PLACER )

88.

On NOVEMBER 22, 1999 before me, the undersigned, a Notary Public in and for said State, personally appeared PHYLLIS A. MOYER, Trustee of the Phyllis A. Moyer Living Trust, personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) are subscribed to the within instrument and acknowledged to me that they executed the same in their authorized capacity(ies), and that by their signatures on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal.

  
\_\_\_\_\_  
NOTARY PUBLIC FOR CALIFORNIA

ROSS OHRENSCHALL  
(Notary's name must be typed or legibly printed)



WILLAMETTE VALLEY MEDICAL CENTER, INC.,  
an Oregon limited liability company

By: Rosemari Davis, CEO  
Name: Rosemari Davis  
Its: CEO

STATE OF Oregon )  
COUNTY OF Yamhill ) SS.

On November 23, 1999 before me, the undersigned, a Notary Public in and for said State, personally appeared Rosemari Davis, the CEO of WILLAMETTE VALLEY MEDICAL CENTER, LLC, an Oregon limited liability company, personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) are subscribed to the within instrument and acknowledged to me that they executed the same in their authorized capacity(ies), and that by their signatures on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal.

Janet L. Quinn  
NOTARY PUBLIC FOR State of Oregon  
Janet L. Quinn  
(Notary's name must be typed or legibly printed)



# Exhibit A-1

## LEGAL DESCRIPTION

-----PARCEL 1: Part of the Nehemiah Martin Donation Land Claim No. 83 and part of the Isaiah M. John Donation Land Claim No. 81 in Section 27, Township 4 South, Range 4 West of the Willamette Meridian in Yamhill County, Oregon, described as follows:

BEGINNING at a stone at the Southeast corner of said Donation Land Claim of Nehemiah Martin, said point also being on the West line of that certain tract described as Parcel I in deed to Evergreen Agricultural Enterprises, Inc., an Oregon Corporation, recorded December 4, 1987 in Film Volume 218, Page 1257, Deed and Mortgage Records; thence North 00°04' East along said West line, 43.59 chains, more or less, to a point of intersection with the Southerly right of way line of the relocated Three Mile Lane Highway and the True Point of Beginning of the tract herein to be described; thence East along the Southerly right of way of relocated Three Mile Lane Highway 1000 feet to a point; thence South 00°04' West, 1089 feet to a point; thence West 1000 feet to a point on the West line of said Evergreen Agricultural Enterprises, Inc., an Oregon Corporation, said point being South 00°04' West, 1089 feet from the point of beginning; thence North 00°04' East 1089 feet to the True Point of Beginning.

-----

PURCHASE AGREEMENT

8/8

**COVER SHEET FOR RECORDING**

**Name of Document:** DRAINAGE WAY EASEMENT  
**Name of Parties:** Phyllis A. Moyer, Trustee  
Gene A. McMullin, Co-Trustee  
Dorothy D. Folkel McMullin, Co-Trustee

"Grantors"

Burch Feero, Inc., "Grantee"

**Consideration:** -0-

**Send Tax Statements:** No Change

**After recording, return to:**

Walter R. Gowell, Esq.  
P.O. Box 480  
McMinnville, OR 97128

Recorded in Yamhill County, Oregon  
CHARLES STERN, COUNTY CLERK



60.00

199816670 11:32am 08/25/98

001 9030216 09 05

1 0 D13 6 30.00 10.00 20.00 0.00 0.00 0.00

8-25-98

V6

## DRAINAGEWAY EASEMENT

KNOW ALL MEN BY THESE PRESENTS, that, GENE A. MCMULLIN and DOROTHY D. FOKEL MCMULLIN, as Co-Trustees of the The McMullin Revocable Living Trust dated May 8, 1996, and PHYLLIS A. MOYER, as Trustee of the Phyllis A. Moyer Living Trust dated September 10, 1992, "Grantor" being the owners, for the mutual covenants and construction activities hereinafter described, and other good and valuable consideration, do forever grant unto BURCH FEERO, INC., an Oregon corporation, a temporary construction easement and a permanent storm drain utility easement over, under and along the full width and length of the premises described as follows, to-wit:

See Exhibit "A" attached hereto for a description of the permanent easement premises:

**TO HAVE AND TO HOLD** the above-described permanent storm drainage utility easement unto said grantee in accordance with the conditions and covenants as follows:

The easement shall include the right to the said Grantee to excavate for and to construct, place, operate, maintain, repair, replace, relocate, inspect, and remove a drainageway pipeline and/or ditch with all appurtenances incident thereto or necessary therewith, including manholes, for the purpose of conveying drainage waters under or across said premises, and make excavations therefor from time to time, in, under, and through the above-described premises within said easement, and to cut and remove from said easement any trees and other obstructions which may endanger the safety or interfere with the use of said drainageway, pipelines or appurtenances attached to or connected therewith; and the right of ingress and egress to and over said above-described easement premises at any and all times for the purpose of repairing, renewing, excavating, replacing, inspecting, maintaining the drainageway pipelines and appurtenances, and for doing anything necessary, useful or convenient for the enjoyment of the easements hereby granted.

No building shall be constructed over the easements and no earthfill or embankment shall be placed within this easement, nor over this drainageway or pipeline without a specific written agreement between Grantee and Grantor, or their heirs, successors, or assigns.

Grantee, upon the initial installation and upon each and every occasion that the same be repaired, replaced, renewed, added to, or removed, shall reasonably restore the premises of the grantor, and any improvements disturbed by Grantee, to as good

Page 1 - PIPELINE EASEMENT

8-25-98

2/6

condition as they were prior to any such installation or work, and shall reimburse Grantor for any crop losses actually incurred through such maintenance activities. Grantee further covenants that the North-South leg of the easement area shall be improved with a buried storm drainage pipeline. The East-West leg of the easement area shall be improved either by a buried storm drainage pipeline or by an open drainageway constructed to the minimum required standards specified by the City of McMinnville and/or Yamhill County, Oregon.

Grantor and Grantee understand and agree that the easement herein granted is intended to benefit only those certain real premises described on Exhibit B attached hereto and by this reference made a part hereof.

WITNESS our hands and seats this 24<sup>th</sup> day of AUGUST, 1998.

Phyllis A. Moyer  
PHYLLIS A. MOYER, Trustee

Gene A. McMullin  
GENE A. MCMULLIN, Co-Trustee  
Dorothy D. Folkel McMullin  
DOROTHY D. FOLKEL MCMULLIN  
Co-Trustee

GRANTOR

Burch Ferro, Inc.

By: Steve Burch

President

GRANTEE

STATE OF OREGON     )  
                                  ) ss.  
County of Yamhill    )

On this 24<sup>th</sup> day of August, 1998, before me personally appeared PHYLLIS A. MOYER, as Trustee of the Phyllis A. Moyer Living Trust dated September 10, 1992 and proved to me on the basis of satisfactory evidence to be the person who signed the within instrument in my presence,

Page 2 - PIPELINE EASEMENT

8-25-98

3/6

IN WITNESS WHEREOF, I have hereunto set my hand and official seal on the day and year above written,

Julie A. Gettman  
Notary Public for Indiana Oregon  
My Commission Expires: 1/1/00

STATE OF OREGON )  
 ) ss.  
County of Yamhill )



On this 24th day of August, 1998, before me personally appeared GENE A. MCMULLIN and DOROTHY D. MCMULLIN, as Co-Trustees of the The McMullin Revocable Living Trust dated May 8, 1996, and proved to me on the basis of satisfactory evidence to be the persons who signed the within instrument in my presence,

IN WITNESS WHEREOF, I have hereunto set my hand and official seal on the day and year above written,

Julie A. Gettman  
Notary Public for Indiana Oregon  
My Commission Expires: 1/1/00

STATE OF OREGON )  
 ) ss.  
County of Yamhill )



On this 24th day of August, 1998, before me personally appeared Steve Burch President of Burch Ferro, Inc., an Oregon corporation, who proved to me on the basis of satisfactory evidence to be the person who, being duly authorized on behalf of said corporation signed the within instrument in my presence,

IN WITNESS WHEREOF, I have hereunto set my hand and official seal on the day and year above written,

Julie A. Gettman  
Notary Public for Indiana Oregon  
My Commission Expires: 1/1/00



Page 3 - PIPELINE EASEMENT

4/6

8-25-98

EXHIBIT   A    
PAGE   1  

Matt Duncel & Assoc.  
19010 Baker Creek Road  
McMinnville, Oregon. 97128  
Phone: 472-7904  
Fax : 472-0367

Date: 2 Jan. 1997

BURCH FEERO - Easement Legal Description

A 15' wide easement in Section 27, Township 4 South, Range 4 West, Yamhill County, Oregon, the perimeter of which is more particularly described as follows:

Beginning at the northwest corner of Parcel 2 of that tract of land described in deed from FAYE FOLKEL to RICHARD A. MOYER, PHYLLIS A. FOLKEL MOYER, GENE A. McMULLIN and DOROTHY D. FOLKEL McMULLIN and recorded in Volume 81 Page 1298, Yamhill County Deed Records; thence South 00°23'20" East 448.31 feet to the southwest corner of said Parcel 2 (basis of bearing CS-10482); thence South 89°40'58" West 660.00 feet along the north line of Parcel 1 of said MOYER and McMULLIN tract to the southwest corner of that tract of land described in deed from McELMURRY and FARNHAM to KURT FEERO and recorded in Film Volume 264 Page 1243; thence South 00°28'00" East 15.00 feet; thence North 89°40'58" East 675.00 feet parallel with and 15.00 feet south of the north line of said Parcel 1; thence North 00°23'20" West 463.28 feet to the north line of said Parcel 2; thence South 89°48'37" West 15.00 feet to the point of beginning.

8-25-98

EXHIBIT   A   5/6  
PAGE   1



PARCEL I

BEING a part of the Nehemiah Martin Donation Land Claim Notification No. 1236, Claim No. 83 in Section 27 in Township 4 South, Range 4 West of the Willamette Meridian in Yamhill County, Oregon. BEGINNING at point in the center of State Highway at a point 215.8 feet West of the Northeast corner of the Northwest Quarter of said Section 27; thence South 906.8 feet to an iron pipe for Southeast corner this tract; thence West parallel to center line of road 671.5 feet to an iron pipe in fence line now there; thence North 00° 19' West along old line fence now there, 906.8 feet to point in center of State Highway; thence East along center of Highway, 676.5 feet to BEGINNING.

EXCEPTING THEREFROM that portion acquired by the State of Oregon by and through its Department of Transportation by final judgment entered September 26, 1963 in the Circuit Court for Yamhill County in Suit No. 23303.

ALSO EXCEPTING THEREFROM that portion acquired by the State of Oregon by and through its Department of Transportation by judgment entered September 19, 1991 in the Circuit Court for Yamhill County in Suit No. CV89-137.

FURTHER EXCEPTING therefrom that portion conveyed to the State of Oregon by and through its Department of Transportation by deed recorded August 1, 1995 in Film Volume 1995, Page 10052.

Tax Lot No. 4427-400

PARCEL II

A tract of land in Section 27, Township 4 South, Range 4 West, Willamette Meridian, Yamhill County, Oregon, described as follows:

BEGINNING at the Northwest corner of the MCMINNVILLE HOSPITAL, INC. tract as described by deed recorded in Film Volume 291, Page 2084, Yamhill County Deed Records, said point being on the South margin of State Highway 18; thence South 00° 04' 00" West, 130.05 feet along the West line of said MCMINNVILLE HOSPITAL, INC. tract to the TRUE POINT OF BEGINNING, said point being the beginning of a non-tangent curve having a radius of 337.00 feet and being concave to the Northeast; thence 66.79 feet Southeasterly along said curve (Chord=South 75° 27' 04" East, 66.68 feet); thence South 81° 07' 44" East, 21.73 feet to the beginning of a curve having a radius of 26.00 feet and being concave to the Southwest; thence 42.78 feet Southeasterly along said curve (Chord=South 33° 59' 47" East, 38.11 feet) to the beginning of a curve having a radius of 730.16 feet and being concave to the Northwest; thence 75.91 feet Southwesterly along said curve (Chord=South 16° 06' 52" West, 75.88 feet); thence South 19° 05' 36" West, 112.60 feet to the beginning of a curve having a radius of 521.47 feet and being concave to the Southeast; thence 183.77 feet Southwesterly along said curve (Chord=South 08° 59' 51" West, 182.82 feet); thence South 01° 05' 53" East, 138.38 feet; thence South 88° 54' 07" West, 24.13 feet to an angle point in the West line of said MCMINNVILLE HOSPITAL, INC. tract; thence North 00° 04' 00" East, 550.39 feet along said West line to the TRUE POINT OF BEGINNING.

8-25-98

RETURN TO  
OREGON HIGHWAY DIVISION  
RIGHT OF WAY SECTION  
119 TRANSPORTATION BLDG.  
SALEM, OREGON 9731Q

Highway Division  
File 57054  
9B-29-18

ORIGINAL

WARRANTY DEED

RICHARD A. MOYER and PHYLLIS A. FOLKEL MOYER, husband and wife, and GENE A. McMULLIN and DOROTHY D. FOLKEL McMULLIN, husband and wife, Grantors, hereby convey unto the STATE OF OREGON, by and through its DEPARTMENT OF TRANSPORTATION, Highway Division, Grantee, fee title to the following described property, to wit:

PARCEL 1

A parcel of land lying in the Nehemiah Martin D.L.C. No. 83, Township 4 South, Range 4 West, W.M., Yamhill County, Oregon and being a portion of that property designated as Parcel 1 and described in that deed to Richard A. Moyer, Phyllis A. Folkel Moyer, Gene A. McMullin and Dorothy D. Folkel McMullin, recorded in Film Volume 81, Page 1298, Records of Yamhill County; the said parcel being that portion of said property included in a strip of land 23 feet in width, lying on the Southerly side of the "B" center line which center line is described as follows:

Beginning at Engineer's center line Station "B" 689+40, said station being 286.47 feet South and 360.78 feet East of the Southeast corner of Section 21, Township 4 South, Range 4 West, W.M.; thence North 75° 49' 57" East 246.58 feet; thence on a spiral curve right (the long chord of which bears North 77° 25' 57" East 239.93 feet) 240 feet; thence on a 1432.39 foot radius curve right (the long chord of which bears North 83° 59' 26.5" East 167.81 feet) 167.91 feet; thence on a spiral curve right (the long chord of which bears South 89° 27' 04" East 239.93 feet) 240 feet to Engineer's center line Station "B" 698+34.49 Back equals "B" 699+20.02 Ahead; thence South 87° 51' 04" East 552.79 feet; thence on a spiral curve right (the long chord of which bears South 86° 15' 04" East 239.93 feet) 240 feet; thence on a 1432.39 foot radius curve right (the long chord of which bears South 81° 12' 51.5" East 91.82 feet) 91.84 feet; thence on a spiral curve right (the long chord of which bears South 76° 10' 39" East 239.93 feet) 240 feet; thence South 74° 34' 39" East 184.08 feet; thence on a 67 foot radius curve left (the long chord of which bears North 53° 47' 08.5" East 105.07 feet) 120.77 feet; thence North 2° 08' 56" East 98.91 feet to Engineer's center line Station "B" 714+48.41.

ALSO that portion of said property lying Northerly of said center line.

Bearings are based upon the Oregon Co-ordinate System, North Zone.

The parcel of land to which this description applies contains 0.16 acre, more or less, outside of the existing right of way.

TOGETHER WITH all abutter's rights of access between the Salmon River Highway and Grantors' remaining real property, EXCEPT, however,

Grantors, their heirs and assigns, shall have access to a frontage road on the Southerly side of said highway.

11-28-88

4427-700  
21-04492  
TAX LOT

1989 MAR 27 PM 2:55

F 230 P 1748

3-27-89

Highway Division  
File 57054  
9B-29-18

Grantors also grant to Grantee, its successors and assigns, a permanent easement to construct and maintain slopes, and to construct, relocate, and maintain television cables, telephone and electric powerline and water line facilities, and appurtenances therefor, necessitated by the widening and improvement of the E. McMinnville Intchge.-Airport Rd. Section of the Salmon River Highway, over, under, across and upon the following described property, to wit:

**PARCEL 2**

A parcel of land lying in the Nehemiah Martin D.L.C. No. 83, Township 4 South, Range 4 West, W.M., Yamhill County, Oregon and being a portion of that property designated as Parcel 1 and described in that deed to Richard A. Moyer, Phyllis A. Folkel Moyer, Gene A. McMullin and Dorothy D. Folkel McMullin, recorded in Film Volume 81, Page 1298, Records of Yamhill County; the said parcel being that portion of said property included in a strip of land 30 feet in width, lying on the Southerly side of the "B" center line which center line is described in Parcel 1.

EXCEPT therefrom Parcel 1.

The parcel of land to which this description applies contains 0.03 acre, more or less.

IT IS UNDERSTOOD that the easement herein granted does not convey any right or interest in the above-described Parcel 2, except as stated herein, nor prevent Grantors from the use of said property; provided, however, that such use shall not be permitted to interfere with the rights herein granted or endanger the lateral support of the Salmon River Highway; that Grantee shall never be required to remove the slope materials placed by it upon said property, nor shall Grantee be subject to any damages to Grantors, their heirs and assigns, by reason thereof or by reason of any change of grade of the highway abutting thereon.

IT IS ALSO UNDERSTOOD that Grantors shall not place or erect any buildings or structures upon the easement area without the written consent of Grantee.

IT IS FURTHER UNDERSTOOD that nothing herein contained is intended to create any obligation on the part of Grantee for the maintenance of said utilities.

11-28-88

STATE OF OREGON  
County of Yamhill  
I hereby certify that the  
within was received and duly  
recorded by me in Yamhill  
County records.

1520  
62

02302

CHARLES STERN, COUNTY CLERK  
*M. Gabriel*

3-27-89

1989 MAR 27 PM 2:55

F 230P1750

Highway Division  
File 57054  
9B-29-18

Grantors hereby covenant to and with Grantee, its successors and assigns, that they are the owners of all of the above-described property which is free from encumbrances, except for easements, conditions, and restrictions of record, and will warrant and defend the property herein conveyed and the easement rights herein granted from all lawful claims whatsoever, except as stated herein.

THIS INSTRUMENT WILL NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY APPROVED USES.

The true and actual consideration received by Grantors for this conveyance is \$ 450.00.

Dated this 16 day of December, 1988.

*Richard A. Moyer*  
Richard A. Moyer

*Phyllis A. Folkel Moyer*  
Phyllis A. Folkel Moyer

*Gene A. McMullin*  
Gene A. McMullin

*Dorothy D. Folkel McMullin*  
Dorothy D. Folkel McMullin

CALIFORNIA  
STATE OF OREGON, County of PLACER

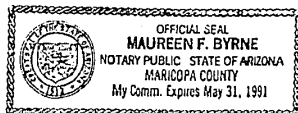
DECEMBER 28, 1988. Personally appeared the above named Richard A. Moyer and Phyllis A. Folkel Moyer, who acknowledged the foregoing instrument to be their voluntary act. Before me:



*Catherine J. Ruff*  
Notary Public for Oregon CALIFORNIA  
My Commission expires FEB 16, 1991

Ariza  
STATE OF OREGON, County of Maricopa

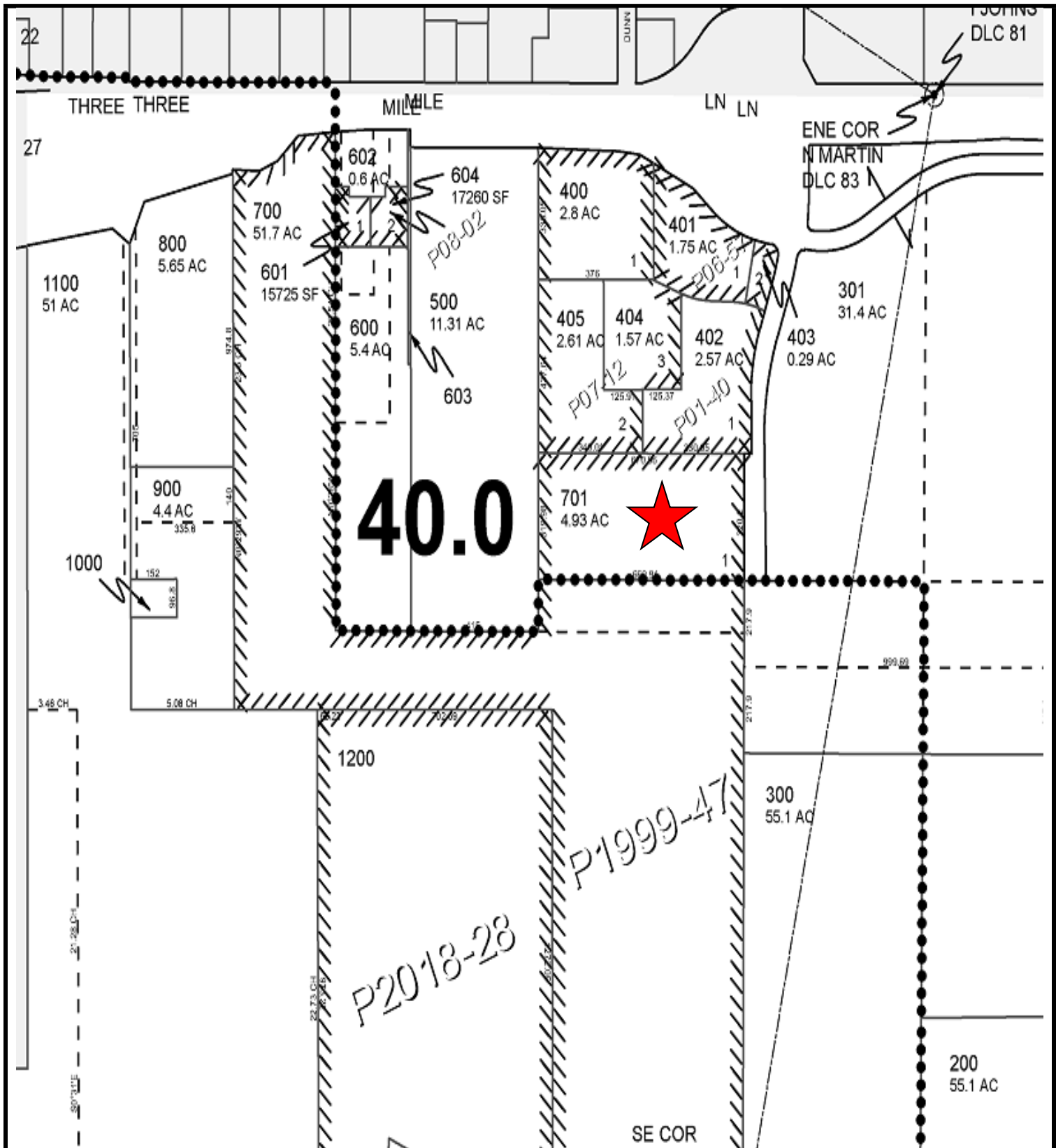
December 16, 1988. Personally appeared the above named Gene A. McMullin and Dorothy D. Folkel McMullin, who acknowledged the foregoing instrument to be their voluntary act. Before me:



*Maureen F. Edwards*  
Notary Public for Oregon Arizona NEE B-112  
My Commission expires May 31, 1991

11-28-88  
Page 3 - WD  
ael/nlp

3-27-89



APN: 515923  
McMinnville, OR 97128

THIS MAP IS FURNISHED AS AN ACCOMMODATION STRICTLY FOR THE PURPOSES OF GENERALLY LOCATING THE LAND. IT DOES NOT REPRESENT A SURVEY OF THE LAND OR IMPLY ANY REPRESENTATIONS AS TO THE SIZE, AREA OR ANY OTHER FACTS RELATED TO THE LAND SHOWN THEREOF





PROJECT SITE

500 ft



**SEE REVISED COPY**

**APPLICATION NARRATIVE**  
Planned Development Amendment

**5**

## **PRIOR LAND-USE DECISIONS**

ORDINANCE 4131

ORDINANCE 4709

ORDINANCE 5072

**6**



## ORDINANCE NO. 4131

### THREE MILE LANE

An Ordinance enacting conditions for development in an area hereinafter described, and commonly known as Three Mile Lane; now, therefore,

#### THE CITY OF McMinnville ORDAINS AS FOLLOWS:

Section 1. Statement of Purpose. The City of McMinnville finds that the development of lands on the north and south sides of Three Mile Lane (Oregon Highway 18) is acceptable if provisions are made to integrate the dual functions of the highway as a bypass around the City and a land service arterial for adjacent properties. The City also finds that development along Three Mile Lane is desirable if it is of good quality and design, if the living and working environments developed are compatible with each other, and if the living environment includes open spaces, parks, and features buffering of residential uses from the highway. The City recognized, during the comprehensive planning process, that to insure that these concerns are met, proper designations on the McMinnville Comprehensive Plan and Zoning Maps must be made, and, further, that a planned development overlay must be placed over the area establishing specific conditions for development.

Section 2. Planned Development Overlay. The planned development overlay which is created and implemented by this ordinance shall be placed over areas to the north and south of Oregon Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18. The affected area is further described by map in Exhibit "A". Areas within and without the city limits shall carry this designation. The policies and procedures set by this ordinance shall be applied to all land use decisions under the jurisdiction of the City of McMinnville. The overlay shall not impinge the legal jurisdiction of Yamhill County for areas outside the city limits.

Section 3. Notation on Zoning and Comprehensive Plan Maps. The perimeters of the areas affected by this ordinance shall be noted on the official McMinnville Comprehensive Plan Map of 1980 and the official McMinnville Zoning Map of 1980.

Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit "A":

- (a) The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- (b) A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- (c) Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:

1. The minimization of entrances onto Three Mile Lane;
  2. The development of on-site circulation systems, connecting to adjoining properties;
  3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
  4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.
- (d) Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- (e) Residentially designated areas covered by this ordinance shall be developed at R-1 zoning classification densities. Mixed housing-type residential developments shall be allowed and encouraged.
- (f) Residential developments shall provide parkland under the provisions of Section 35 of Ordinance No. 3702, unless safe and convenient pedestrian access is provided to an existing park. If no land is required, money in lieu of land shall be required.

The following policies shall apply to specified properties north and south of Three Mile Lane, described on the map in Exhibit "B". The written descriptions, uses, and written policies included below are to apply to the interpretation of the map and be of at least equal weight and importance.

#### Municipal Airport (Area 1)

The airport shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980 to accommodate aviation related facilities in keeping with the Airport Master Plan, as amended. Facilities identified in the plan for upgrading and expanding the aviation aspects of the airport shall be exempted from review by the provisions of the planned development overlay. Any private or public aviation related industrial development that may in the future be contemplated shall be subject to the provisions of this ordinance. The adoption of an airport zoning ordinance in the future may be appropriate, and additional requirements of such a zone shall be applied in conjunction with the requirements of this ordinance.

### West of Municipal Airport (Area 2)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The City shall encourage the location of aviation related industries or industries requiring extensive use of airport services, in this area. Zone changes to M-L (Limited Light Industrial), to M-1 (Light Industrial) and AH (Agricultural Holding) shall be allowed. Zone changes to M-2 (General Industrial) may be allowed upon findings that the intended use of the property shall be an aviation related industry, or an industry which requires extensive use of the airport services for its operation.

### West of Municipal Airport to Mobile Home Park (Area 3)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The City shall allow zone changes to M-L (Limited Light Industrial) and AH (Agricultural Holding) designations.

### East of Pacific Avenue - Industrial Area (Area 4)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980 to accommodate the existing industrial uses. Expansion of those uses may take place if compatibility with surrounding properties is insured.

### Airport Rendezvous (Area 5)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The areas designated AH (Agricultural Holding) on the map shall remain so designated until completion of the update of the Airport Master Plan. The adoption of an airport zoning ordinance, recommended land uses, and development controls for identified noise contours may be appropriate upon completion of the Airport Master Plan; any additional requirements enacted by the City shall be applied in conjunction with the requirements of this ordinance. Redesignation of the properties may be allowed after completion of the Airport Master Plan update and adoption of appropriate Master Plan recommendations, provided that the requirements of this ordinance are met, and upon determination that the proposed use is compatible with surrounding properties.

### Remainder of Planned Development Area

The remainder of the properties within the area covered by this ordinance shall be developed according to the designations on the McMinnville Comprehensive Plan Map. Site plan review as required in Chapter 17.51 of Ordinance No. 3380 shall include conditions to insure compatibility of allowed developments with proposed uses in surrounding areas.

Section 5. Procedures for Review.

- (a) Annexation to the City of lands within the area covered by this ordinance shall meet the requirements of the annexation ordinance.
- (b) Comprehensive Plan Map amendments shall be processed under procedures set out in Ordinance No. 4127.
- (c) Zone changes and land divisions submitted for approval shall be processed under the requirements and standards of Chapter 17.51 of Ordinance No. 3380 (Zoning Ordinance). If standards and requirements of Chapter 17.51 differ with those established elsewhere by the City, the more restrictive standards and requirements shall be adhered to.
- (d) Land division requests shall also be processed under the requirements of Ordinance No. 3702 (Land Division Ordinance).
- (e) Amendments to this ordinance shall be processed under procedures set out in Chapter 17.51 of Ordinance No. 3380 (Zoning Ordinance).

Passed by the City Council on April 7, 1981.



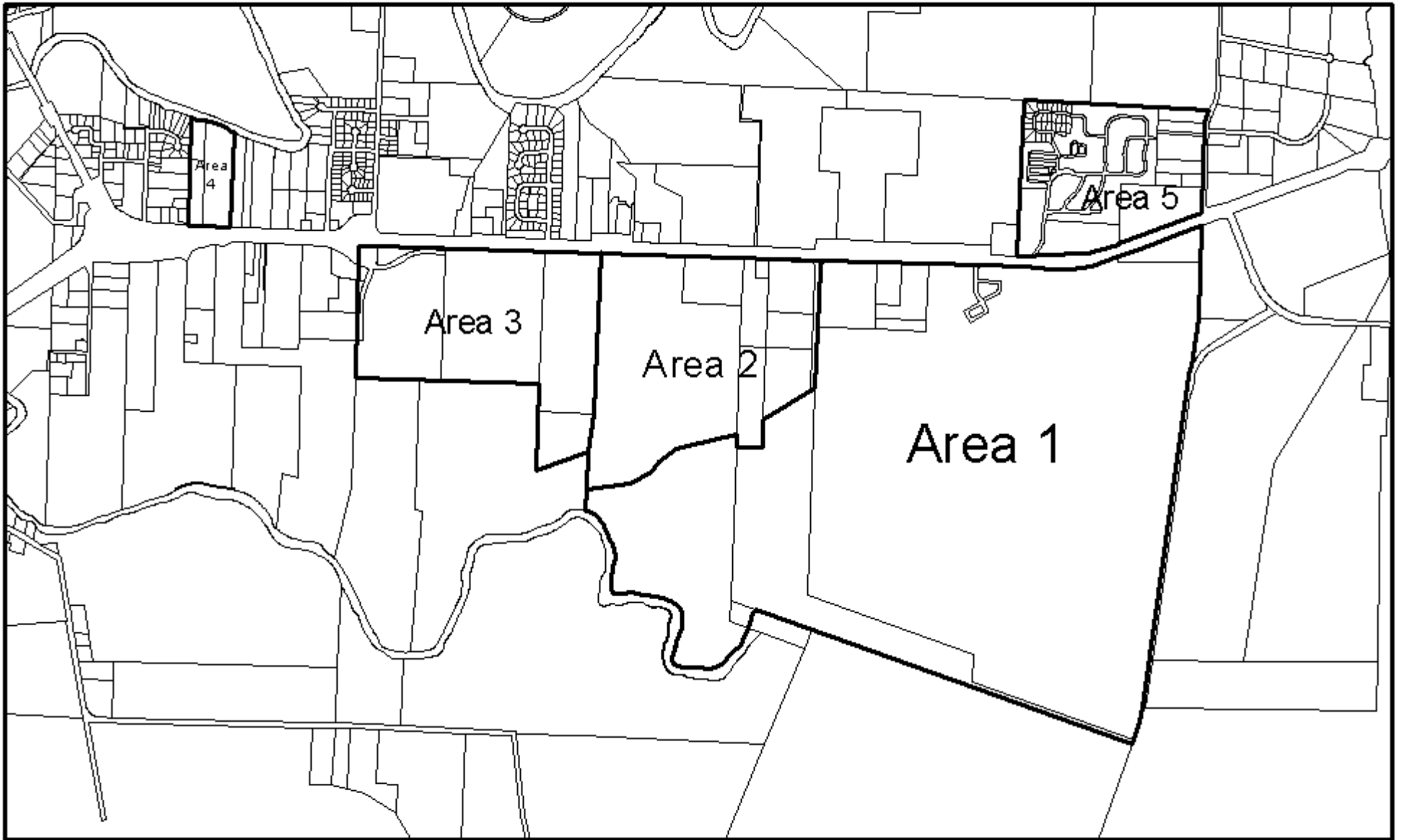
2000 0 2000 Feet



**EXHIBIT A**  
**Three Mile Lane**  
**Planned Development Overlay**

Ordinance No. 4131

**CITY OF McMINNVILLE**



2000 0 2000 Feet

**EXHIBIT B**  
**Three Mile Lane**  
**Planned Development Overlay**

Ordinance No. 4131

**CITY OF McMINNVILLE**

ORDINANCE NO. 4709

An Ordinance amending the City of McMinnville Comprehensive Plan Map from an existing Industrial designation to a Commercial designation and a zone change from a County EF-40 (Exclusive Farm Use - 40-acre minimum) zone to a City C-3 PD (General Commercial Planned Development) zone on a parcel of land approximately five acres in size located south of Three Mile Lane, and immediately west of Norton Lane and the Willamette Valley Medical Center.

RECITALS

The Planning Commission received an application (CPA 6-99 / ZC 11-99) from Gene and Dorothy McMullin and Phyllis Moyer dated July 2, 1999, requesting a comprehensive plan map amendment from an existing Industrial designation to a Commercial designation and a zone change from a County EF-40 (Exclusive Farm Use - 40-acre minimum) zone to a City C-3 PD (General Commercial Planned Development) zone on approximately five acres of land. The subject site is located south of Three Mile Lane, and immediately west of Norton Lane and the Willamette Valley Medical Center and is further described as a portion of Tax Lot 700, Section 27, T.4 S., R. 4 W., W.M.

A public hearing was held August 19, 1999, at 7:30 p.m. before the McMinnville Planning Commission after due notice had been given in the local newspaper on August 14, 1999, and written notice had been mailed to property owners within 300 feet of the affected property; and

At said public hearing, testimony was received, the application materials and a staff report were presented; and

The Planning Commission, being fully informed about said request, found that said changes conformed to the zone change review criteria listed in Chapter 17.72.035 of Ordinance No. 3380 based on the material submitted by the applicant and the findings of fact and the conclusionary findings for approval contained in the staff report, all of which are on file in the Planning Department, and that the plan amendment and zone change are consistent with the Comprehensive Plan; and

The Planning Commission approved said comprehensive plan map amendment and zone change and has recommended said changes to the Council, now, therefore,

THE CITY OF McMINNVILLE ORDAINS AS FOLLOWS:

Section 1. That the Council adopts the findings and conclusions of the Planning Commission, staff report on file in the Planning Department, and the application filed by Gene and Dorothy McMullin and Phyllis Moyer.

Section 2. That the Comprehensive Plan Map shall be amended from an Industrial designation to a Commercial designation for the property described in Exhibit "A" which is attached hereto and incorporated herein by this reference.

Section 3. That the property described in Exhibit "A" is hereby rezoned from a County EF-40 (Exclusive Farm use - 40-acre minimum) zone to a City C-3 PD (General Commercial Planned Development) zone subject to the following conditions:

1. That the zone change request shall not take effect until and unless CPA 6-99 is approved by the City Council.
2. That development and use of the site is subject to the provisions of the Three Mile Lane Planned Development Ordinance, as amended. Further, that a detailed Master Plan for the subject site shall be submitted to the McMinnville Planning Commission for review and approval, pursuant to the procedures of McMinnville Zoning Ordinance 17.51, Planned Development Overlay, prior to any development occurring on the site. The plan shall include, at a minimum, proposed land uses and their location(s), building locations, proposed circulation patterns, proposed open spaces, grading and drainage information, location and size of public utilities and services, off-street parking areas, direct pedestrian access, and other information deemed necessary to convey the details of the proposed development plans to the Planning Commission.
3. That detailed plans for the proposed commercial development showing site layout, proposed circulation pattern, signage, building elevations, landscaping, parking, and lighting must be submitted to and approved by the Three Mile Lane Design Review Committee prior to the issuance of any building permits for said development. Approval or denial of such plans shall be based on findings that, to the extent possible, the building and site design employs principles that will ensure compatibility with adjacent development, and provide an architectural style appropriate to a "gateway" or entrance to the City of McMinnville. In addition, approval or denial of the exterior building design shall be based on a finding that, to the extent possible, the building design incorporates design and architectural features that would serve to break up the building's horizontal plane and provide visual interest. This may include, but is not limited to, the use of vertical columns, gables, variety of compatible and complimentary building materials, providing openings in the building facade, and landscaping at the building perimeter. Submitted plans should include detail as regard building colors and materials (provide texture and visual relief), building height, planting design, window treatment, vertical and horizontal articulation, massing, voids to solids relationships, and other elements appropriate to ensure that the building and site design complies with the objectives and requirements of this planned development approval.

The provisions of Chapter 17.51 of the McMinnville Zoning Ordinance may be used to place conditions on any development and to determine whether or not specific uses are permissible. The applicant may appeal the decision of the Three Mile Lane Design Review Committee to the Planning Commission if



notice of intent to appeal is filed in the Planning Department office within fifteen (15) days of the Committee's decision.

4. That final development plans include landscape plans to be submitted to and approved by the McMinnville Landscape Review Committee and Three Mile Lane Design Review Committee. A minimum of 14 percent of the site must be landscaped. An arborvitae hedge or some similar type of planted visual screen shall be required along the property lines adjacent to the residentially zoned land to the west. Landscaping emphasis shall exist along the site's eastern edge, adjacent to Norton Lane, including required street trees, with particular emphasis at the site's eastern driveway intersection(s) at Norton Lane. In addition, landscape islands are required to be located throughout proposed off-street parking areas.

Street trees within a curbside planting strip along the Norton Lane frontage are required to have a two-inch minimum caliper, exhibit size and growing characteristics appropriate for the particular planting strip, and be spaced as appropriate for the selected species and as may be required for the location of above ground utility vaults, transformers, light poles, and hydrants. All street trees shall be of good quality and shall conform to American Standard for Nursery Stock (ANSI Z60.1). The Planning Director reserves the right to reject any plant material which does not meet this standard.

5. That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of the City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to the issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private rather than public, and private maintenance agreements must be approved by the City for them.
6. That final development plans for the subject site include a detailed sanitary sewage collection plan which incorporates the requirements of the City's Collection System Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to the issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. In addition, existing structures located within the subject site are required to connect to the sanitary sewer system as soon as service is available.
7. That the developer secure from the Department of Environmental Quality (DEQ) applicable storm runoff and site development permits prior to construction of the required site improvements. Evidence of such permit shall be submitted to the City Engineer.
8. That the developer enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and

grading permit for lot fill and grading from the City Building Division. All fill placed in the areas where construction is expected shall be engineered and shall meet with the approval of the City Building Division and the City Engineering Department.

9. That the developer extend water and power services to the subject site in accordance with McMinnville Water and Light requirements, including any necessary contracts and/or easements. Said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.
10. That utilities shall be extended to the property boundaries by the applicant, as may be required by the City Engineer or McMinnville Water and Light.
11. That no building shall exceed the height of 35 feet.
12. That if outside lighting is to be provided, it must be directed down and away from residential areas and public streets.
13. That signs located on the site shall be subject to the requirements of McMinnville Ordinance No. 4572 (B).
14. That all business, storage, or displays shall be conducted wholly within an enclosed building; except for off-street parking and loading.
15. That the subject site is limited to professional office use or medical office use, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility. Uses other than professional office use or medical office use may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the subject site.
16. That if restrictive covenants are proposed for the development they must meet with the approval of the Planning Director.
17. That the approved Master Plan as approved by the Planning Commission shall be placed on file with the Planning Department and become a part of the zone and binding on the owner and developer. The developer will be responsible for requesting permission of the Planning Commission for any major change of the details of the final development plans. Minor changes to the details of the adopted plan may be approved by the City Planning Director. It shall be the Planning Director's decision as to what constitutes a major or minor change. An appeal from a ruling by him may be made only to the Commission. Review of the Planning Director's decision by the Planning Commission may be initiated at the request of any one of the commissioners.

18. That improvements to the Norton Lane frontage, to the southernmost entry drive, or as may otherwise be required by the City of McMinnville, shall be done at the developer's expense and be finalized prior to release of any occupancy permits. Plans for the improvement of Norton Lane shall be submitted to the City Engineer for review and approval prior to its construction.
19. That the developer/owner of the subject site must grant to the Willamette Valley Medical Center and the City of McMinnville the right to cause all airspace above the surface of the subject site such as noise, vibrations, fumes, dust, fuel particles, and all other effects as may be caused by the operation of aircraft landing at or taking off from or operating at or on the Medical Center property and the McMinnville Municipal Airport. The owner must fully waive any right or cause of action which he may now or in the future raise against the Willamette Valley Medical Center and the City of McMinnville due to such circumstances noted above.
20. That prior to the development of the site, the applicant shall sign a waiver of remonstrance against the future improvement of Norton Lane. The waiver shall be prepared by the City.

Passed by the Council this 28<sup>th</sup> day of September 1999, by the following votes:

Ayes: Aleman, Kirchner, Payne, Rabe, Windle

Nays: \_\_\_\_\_

Approved this 28<sup>th</sup> day of September 1999.

Richard D Windle  
COUNCIL PRESIDENT

Attest:

Kenneth Taylor  
RECORDER PRO TEM

ORDINANCE NO. 5072

**AN ORDINANCE AMENDING AN EXISTING PLANNED DEVELOPMENT OVERLAY DISTRICT TO ADD MULTIPLE FAMILY RESIDENTIAL AS AN ALLOWABLE USE IN THE PLANNED DEVELOPMENT OVERLAY DISTRICT**

RECITALS:

The Planning Department received an application (PDA 6-18) from KWDS, LLC, on behalf of property owners Gene A. McMullin (representing McMullin Family Residual Trust) and Phyllis A. Moyer (representing Phyllis Moyer Living Trust), requesting approval of a Planned Development Amendment. The existing Planned Development Overlay District limits uses, and the Planned Development Amendment request was to add multiple family residential as an allowable use on the subject site; and

The subject site is located west of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.; and

A public hearing was held on May 16, 2019 at 6:30 p.m., before the McMinnville Planning Commission after due notice had been provided in the local newspaper on May 7, 2019, and written notice had been mailed to property owners within 300 feet of the affected property; and

At said public hearing, the application materials and a staff report were presented, and applicant and public testimony was received; and

The Planning Commission, being fully informed about said requests, found that the requested amendments conformed to the applicable Comprehensive Plan goals and policies, as well as the planned development amendment review criteria listing in Section 17.74.070 of the McMinnville Municipal Code based on the material submitted by the applicant and the findings of fact and conclusionary findings for approval contained in Exhibit A; and

The Planning Commission recommended approval of said planned development amendment to the Council;

The City Council having received the Planning Commission recommendation and staff report, and having deliberated; and

**NOW, THEREFORE, THE COMMON COUNCIL FOR THE CITY OF MCMINNVILLE ORDAINS AS FOLLOWS:**

1. That the Council adopts the Findings of Fact, Conclusionary Findings, Decision and Conditions of Approval as documented in Exhibit A; and
2. That the requested amendment to the existing Planned Development Overlay District is approved, subject to the following conditions:
  - 1) That Condition 15 of Ordinance 4709, as amended by Ordinance 4863, be amended as follows (text to be removed is shown with ~~strikeout~~, text to be added is **bold and underlined**):

"That the subject site is limited to professional office use or medical office use, or multiple family dwellings, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, multiple family dwellings, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.

If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped."

- 2) All other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect.

Passed by the Council this 25<sup>th</sup> day of June, 2019, by the following votes:

Ayes: Garvin, Geary, Peralta, Stassens

Nays: \_\_\_\_\_

Scott A. Hill  
MAYOR

Attest:

Melissa Biset  
CITY RECORDER

Approved as to form:

[Signature]  
CITY ATTORNEY



CITY OF MCMINNVILLE  
PLANNING DEPARTMENT  
231 NE FIFTH STREET  
MCMINNVILLE, OR 97128

503-434-7311  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

**DECISION, CONDITIONS, FINDINGS OF FACT AND CONCLUSIONARY FINDINGS FOR THE APPROVAL OF A PLANNED DEVELOPMENT AMENDMENT TO ADD MULTIPLE FAMILY RESIDENTIAL AS AN ALLOWABLE USE WITHIN AN EXISTING PLANNED DEVELOPMENT OVERLAY DISTRICT**

- DOCKET:** PDA 6-18 (Planned Development Amendment)
- REQUEST:** Approval to amend Planned Development Overlay Ordinance 4709, as amended by Ordinance 4863, by amending a condition of approval that currently limits use of the site to professional office, medical office, senior condominiums, senior apartments, assisted living facilities, and other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility. The requested amendment is to add multiple-family residential apartments to that list of allowable uses in the existing Planned Development Overlay condition of approval. The base C-3 zone allows multiple-family residential as a permitted use.
- LOCATION:** West of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.
- ZONING:** C-3 PD (General Commercial Planned Development)
- APPLICANT:** KWDS, LLC, on behalf of property owners Gene A. McMullin (representing McMullin Family Residual Trust) and Phyllis A. Moyer (representing Phyllis Moyer Living Trust)
- STAFF:** Chuck Darnell, Senior Planner
- DATE DEEMED COMPLETE:** March 22, 2019
- HEARINGS BODY & ACTION:** The McMinnville Planning Commission makes a recommendation for approval or denial to the City Council.
- HEARING DATE & LOCATION:** May 16, 2019, Civic Hall, 200 NE 2<sup>nd</sup> Street, McMinnville, Oregon.



**PROCEDURE:** An application for a Planned Development Amendment is processed in accordance with the procedures in Section 17.72.120 of the McMinnville Municipal Code. The application is reviewed by the Planning Commission in accordance with the quasi-judicial public hearing procedures specified in Section 17.72.130 of the McMinnville Municipal Code.

**CRITERIA:** The applicable criteria for a Planned Development Amendment are specified in Section 17.74.070 of the McMinnville Municipal Code. In addition, the goals, policies, and proposals in Volume II of the Comprehensive Plan are to be applied to all land use decisions as criteria for approval, denial, or modification of the proposed request. Goals and policies are mandated; all land use decisions must conform to the applicable goals and policies of Volume II. "Proposals" specified in Volume II are not mandated, but are to be undertaken in relation to all applicable land use requests.

**APPEAL:** As specified in Section 17.72.190 of the McMinnville Municipal Code, the City Council's decision may be appealed to the Land Use Board of Appeals (LUBA) within 21 (twenty-one) days of the date written notice of decision is mailed. The City's final decision is subject to a 120 day processing timeline, including resolution of any local appeal.

**COMMENTS:** This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Engineering Department, Building Department, Parks Department, City Manager, and City Attorney; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Public Works; Yamhill County Planning Department; Frontier Communications; Comcast; Northwest Natural Gas; and Oregon Department of Transportation. Their comments are provided in this document.

**RECOMMENDATION**

Based on the findings and conclusionary findings, the City Council finds the applicable criteria are satisfied with conditions and **APPROVES** the Planned Development Amendment (PDA 6-18), **subject to the conditions of approval provided in this document.**

RECOMMENDATION: APPROVAL WITH CONDITIONS

City Council: Scott Hill  
Scott Hill, Mayor of McMinnville

Date: 6/25/2019

Planning Commission: Roger Hall  
Roger Hall, Chair of the McMinnville Planning Commission

Date: 6/27/2019

Planning Department: Heather Richards  
Heather Richards, Planning Director

Date: 6/25/19

## **I. APPLICATION SUMMARY:**

The applicant has provided information in their application narrative and findings (attached as Attachment 1) regarding the history of land use decisions for the subject site(s) and the request(s) under consideration. City has found the information provided to accurately reflect the current Planned Development Amendment request and the relevant background, and excerpted portions are provided below to give context to the request, in addition to staff's comments.

### ***Subject Property & Request***

The application (PDA 6-18) is a request for a Planned Development Amendment to amend a condition of approval from Ordinances 4709 and 4863 that currently limits use of the site to professional office, medical office, senior condominiums, senior apartments, assisted living facilities, and other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility. The requested amendment is to add multiple-family residential apartments to that list of allowable uses in the existing Planned Development Overlay condition of approval. The base C-3 zone allows multiple-family residential as a permitted use.

The subject property is located west of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits and urban growth boundary. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.

Excerpts from Land Use Application Narrative and Findings:

#### **Existing Conditions**

The Subject Site is currently undeveloped, and used for farming. The site is generally flat, with a minor slope to the northwest. There are no significant or distinguishing natural features associated with this property.

Access to the property is from Highway 18 via the intersection with Norton Lane. There is a signalized intersection of Norton Lane (SE & NE) with Highway 18. Immediately south of the highway, there is a three-way stop intersection of SE Norton Lane and SE Stratus Avenue. Stratus Avenue loops around the Medical Center and intersects back with SE Norton Lane across from the subject site.

#### **Access and Utilities**

The property fronts on west side of SE Norton Lane, just south of the Altimus Plaza medical offices. The existing right-of-way for SE Norton Lane is 60 feet down to and through the Stratus Avenue Loop intersection, south of the Medical Center. The existing improvements extend through this intersection. The street is currently improved with curb and gutter and 28 foot paved section.

SE Norton Lane is improved to City standard south to the intersection with the Stratus Avenue loop. The street improvements extend approximately 180 feet along the frontage of the Subject Site [Staff would note that the street along the subject site's frontage do not include sidewalks, but sidewalks constructed to City standards would be required at the time of development of the site – see Conditions 18 and 20 of Ordinance 4709]. There is an additional 280 feet of site frontage that is currently not improved.



There are existing urban services and utilities within SE Norton Lane. These facilities are all available and were adequately sized to serve development of the subject property, consistent with the allowed commercial uses.

#### Surrounding Uses

North – The property immediately north of the subject property is the Altimus Plaza medical offices. Further to the north at the Stratus Avenue intersection are the Comfort Inn & Suites and the Diner restaurant. The land west of the motel is vacant land, designated for professional and medical offices, or other compatible uses.

East – The property immediately east of the subject property is developed with medical offices, specifically Willamette Valley Medical Center, including a Heliport. To the south of the medical center is vacant farm land, outside of the city limits and UGB.

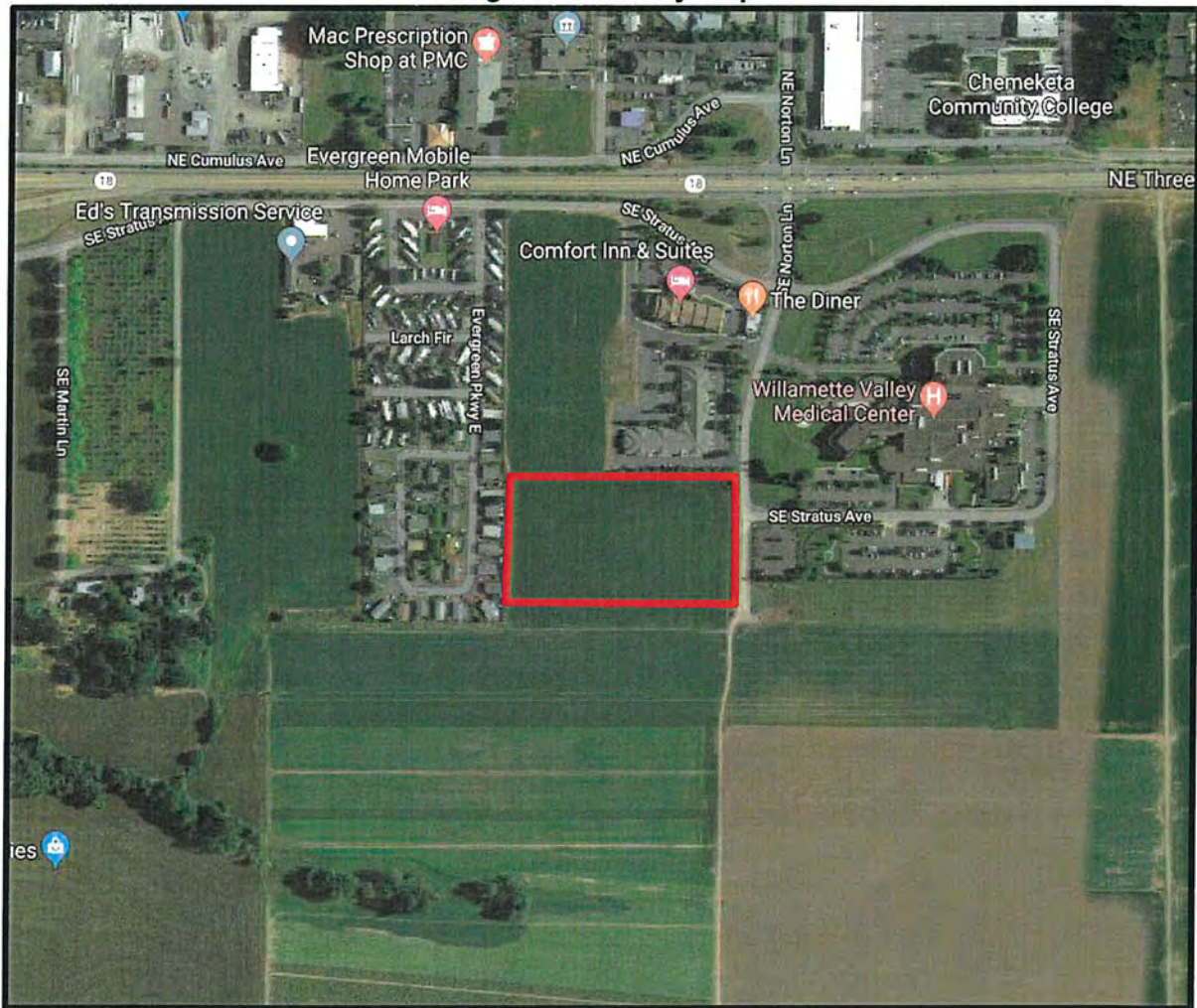
The Evergreen Aviation Museum, north of Hwy 18, and Municipal Airport, south of Hwy 18, are located just over a mile to the east.

South – The property immediately south of the subject property is vacant farm land, outside of the city limits and UGB.

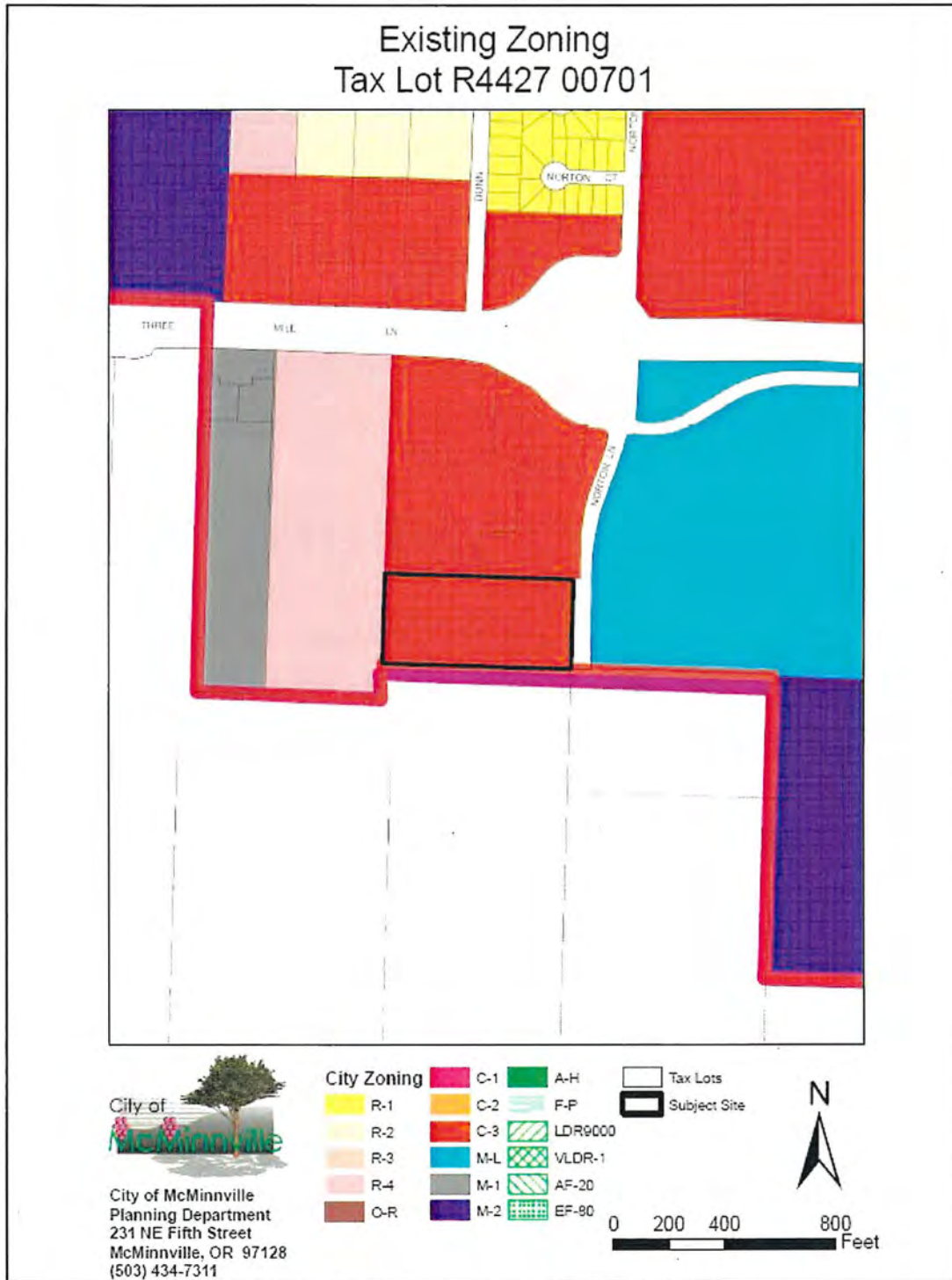
West – The property immediately west of the subject property is the Evergreen Estates Mobile Home Park, which is zoned R-4.

***See Vicinity Map (Figure 1) and Zoning Map (Figure 2) below.***

Figure 1. Vicinity Map



**Figure 2. Zoning Map**



**Background**

Excerpts from Land Use Application Narrative and Findings:

The subject site is within the Three Mile Lane Planned Development Overlay area, originally established by Ordinance 4131 in 1981

By Ordinance 4131, the City of McMinnville established Planned Development requirements for the Three Mile Lane area (Oregon Highway 18 Corridor). The area covered by this requirement extends north and south of Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18.

In 1994, Ordinance 4131 was amended to add specific development policies (Section 4) and signage regulations for Commercial lands (Section 5) by Ordinance 4572.

In 1999, the City amended the Comprehensive Plan re-designating a 5 acre industrial area south of Stratus Avenue for commercial use. With this Plan amendment, the 5 acre area, which is the Subject Site, was zoned C-3 PD, General Commercial Planned Development by Ordinance 4709.

Ordinance 4709 included 20 Conditions applicable to development within the PD boundary. These conditions remain in effect. However Condition 15 specifically limited uses as follows:

*“Condition 15. That the subject site is limited to professional office use or medical office use, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use or medical office use may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.”*

The record indicates the City’s general intent in limiting uses in this area was to avoid creation of strip commercial development, by restricting retail uses.

In 2006, the Three Mile Lane Planned Development (Ord. 4709) was amended, by Ordinance 4863. This amendment added senior condominiums, senior apartments, or assisted living facilities as permitted uses. Under Ordinance 4863 the current allowed uses for the subject property are limited as follows:

*“Condition 15. That the subject site is limited to professional office use or medical office use, **or senior condominiums, senior apartments, or assisted living facilities**, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.”*

The revised Condition 15 applies specifically to Tax Lot 701. In addition to the use restrictions per amended Condition 15, the specific development policies (Section 4) and signage regulations for Commercial lands (Section 5) established in Ordinance 4572 are applicable to the development of the Subject Property, as well as the other 19 Conditions set in Ordinance 4709.

The existing Planned Development originally adopted by Ordinance 4709 in 1999, and later amended by Ordinance 4863 in 2006, contains 19 other conditions of approval. Those conditions of approval are

still applicable to the property and are provided in Ordinance 4709, which is included as Attachment 2 to this Decision Document.

### **Summary of Criteria & Issues**

The application (PDA 6-18) is subject to Planned Development Amendment review criteria in Section 17.74.070 of the Zoning Ordinance. An amendment to an existing planned development may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Major changes to an adopted site plan shall be processed in accordance with Section 17.72.120. The goals and policies in Volume II of the Comprehensive Plan are also independent approval criteria for all land use decisions.

The specific review criteria for Planned Development Amendments in Section 17.74.070 of the McMinnville Zoning Ordinance require the applicant to demonstrate that:

- A. There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;
- B. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;
- C. The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;
- D. The plan can be completed within a reasonable period of time;
- E. The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;
- F. Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;
- G. The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.

The applicant has provided findings to support the request for a Planned Development Amendment. These will be discussed in detail in Section VII (Conclusionary Findings) below.

Generally, the purpose of a planned development is to provide greater flexibility and greater freedom of design in the development of land than may be possible under strict interpretation of the provisions of the zoning ordinance. Further, the purpose of a planned development is to encourage a variety in the development pattern of the community; encourage mixed uses in a planned area; encourage developers to use a creative approach and apply new technology in land development; preserve significant man-made and natural features; facilitate a desirable aesthetic and efficient use of open space; and create public and private common open spaces. A planned development is not intended to be simply a guise to circumvent the intent of the zoning ordinance.

Consideration of a planned development request includes weighing the additional benefits provided to the development and city as a whole through the planned development process that go above and beyond what would be provided through a standard land use application against the applicable zoning requirements. It should be noted in this case that the subject site is already regulated by an existing Planned Development (Ordinance No. 4709), and the request is only to expand the list of uses that are listed as permitted on the subject site. Overall, the proposed planned development amendment would



provide additional benefits to the community and the City as a whole. The proposal would provide an opportunity for increased variety in the development pattern of the community, and would encourage mixed uses in a planned area, specifically allowing for the incorporation of a use that is identified as needed in the community.

## **II. CONDITIONS:**

1. That Condition 15 of Ordinance 4709, as amended by Ordinance 4863, be amended as follows (text to be removed is shown with ~~strikeout~~, text to be added is **bold and underlined**:

"That the subject site is limited to professional office use or medical office use, or **multiple family dwellings**, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, **multiple family dwellings**, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.

**If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.**"

2. All other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect.

## **III. ATTACHMENTS:**

1. PDA 6-18 Application and Attachments (on file with the Planning Department)
2. Ordinance No. 4709 (on file with the Planning Department)
3. Ordinance No. 4863 (on file with the Planning Department)

## **IV. COMMENTS:**

### **Agency Comments**

This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Parks and Recreation Department, Engineering and Building Departments, City Manager, and City Attorney, McMinnville School District No. 40, McMinnville Water and Light, Yamhill County Public Works, Yamhill County Planning Department, Recology Western Oregon, Frontier Communications, Comcast, Northwest Natural Gas, Oregon Department of Transportation. The following comments were received:

- McMinnville Engineering Department

We have completed our review of PDA 6-18. A recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposal.

Thus, we have no comments re: the proposed planned development amendment.

At the time of development, the applicant will need to acquire all necessary permits (including any required by the Oregon Department of Transportation), and will need to construct the appropriate street and utility improvements necessary to support the development.

- McMinnville Fire Department

No comments received

- McMinnville Water and Light

Regarding the above planning development amendment there are no comments from the water or power side on our end.

- Oregon Department of Transportation

Since the site isn't adjacent to state highway, the only thing we would consider are impacts to a state highway intersection. Normally our threshold for a TIA is 50 peak hour trips or 300 ADT at a state highway intersection. This would potentially include the intersections at Stratus/Norton, OR-18/Norton, and maybe farther down OR-18 depending on what the trip distribution looks like.

## **Public Comments**

Notice of this request was mailed to property owners located within 300 feet of the subject site. Notice of the public hearing was also provided in the News Register on Tuesday, May 7, 2019. As of the date of the Planning Commission public hearing on May 16, 2019, no public testimony had been received by the Planning Department.

## **V. FINDINGS OF FACT - PROCEDURAL FINDINGS**

- A. The applicant, KWDS, LLC, held a neighborhood meeting on November 7, 2018.
- B. The applicant submitted the Planned Development Amendment application (PDA 6-18) on December 20, 2018.
- C. During the completeness review, it was determined that the neighborhood meeting had not be held in accordance with Section 17.72.095 of the McMinnville Municipal Code. It was also determined that findings had not been provided for all applicable Comprehensive Plan goals and policies. The application was deemed incomplete on January 18, 2019.
- D. The applicant held a second neighborhood meeting in accordance with Section 17.72.095 of the McMinnville Municipal Code on February 13, 2019.
- E. The applicant submitted a revised application on March 14, 2019 that included evidence of the neighborhood meeting held on February 13, 2019 and the applicable Comprehensive Plan goals and policies identified in the incomplete notification.
- F. The application was deemed complete on March 22, 2019. Based on that date, the 120 day land use decision time limit expires on July 20, 2019.

- G. Notice of the application was referred to the following public agencies for comment in accordance with Section 17.72.120 of the Zoning Ordinance: McMinnville Fire Department, Police Department, Parks and Recreation Department, Engineering and Building Departments, City Manager, and City Attorney, McMinnville School District No. 40, McMinnville Water and Light, Yamhill County Public Works, Yamhill County Planning Department, Recology Western Oregon, Frontier Communications, Comcast, Northwest Natural Gas, Oregon Department of Transportation.  
Comments received from agencies are addressed in the Decision Document.
- H. Notice of the application and the May 16, 2019 Planning Commission public hearing was mailed to property owners within 300 feet of the subject property in accordance with Section 17.72.120 of the Zoning Ordinance on Tuesday, April 23, 2019.
- I. Notice of the application and the May 16, 2019 Planning Commission public hearing was published in the News Register on Tuesday, May 7, 2019, in accordance with Section 17.72.120 of the Zoning Ordinance.
- J. No public testimony was submitted to the Planning Department prior to the Planning Commission public hearing.
- K. On May 16, 2019, the Planning Commission held a duly noticed public hearing to consider the request.

**VI. FINDINGS OF FACT – GENERAL FINDINGS**

- 1. **Location:** West of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.
- 2. **Size:** Approximately 4.93 acres.
- 3. **Comprehensive Plan Map Designation:** Commercial
- 4. **Zoning:** C-3 PD (General Commercial Planned Development)
- 5. **Overlay Zones/Special Districts:** Three Mile Lane Planned Development Overlay District (Ordinance No. 4131 and Ordinance No. 4572)
- 6. **Current Use:** Undeveloped
- 7. **Inventoried Significant Resources:**
  - a. **Historic Resources:** None
  - b. **Other:** None
- 8. **Other Features:** The site is described by the applicant as being generally flat, with a minor slope to the northwest. There are no significant or distinguishing natural features associated with this property.
- 9. **Utilities:**
  - a. **Water:** Water service is available to the subject site.
  - b. **Electric:** Power service is available to the subject site.



- c. **Sewer:** Sanitary sewer service is available to the subject site.
- d. **Stormwater:** Storm sewer service is available to the subject site.
- e. **Other Services:** Other utility services are available to the subject site. Northwest Natural Gas and Comcast is available to serve the site.

10. **Transportation:** The site is adjacent to SE Norton Lane, which is identified as a minor collector in the McMinnville Transportation System Plan. Section 17.53.101 of the McMinnville Municipal Code identifies the right-of-way width for minor collector streets as 56 feet (with no bike lane) or 66 feet (with bike lane). The existing right-of-way width of SE Norton Lane adjacent to the subject site is 71.47 feet, as shown on Partition Plat 99-47. Therefore, no additional right-of-way dedications are necessary. Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development.

**VII. CONCLUSIONARY FINDINGS:**

The Conclusionary Findings are the findings regarding consistency with the applicable criteria for the application. The applicable criteria for a Planned Development Amendment are specified in Section 17.74.070 of the Zoning Ordinance.

In addition, the goals, policies, and proposals in Volume II of the Comprehensive Plan are to be applied to all land use decisions as criteria for approval, denial, or modification of the proposed request. Goals and policies are mandated; all land use decisions must conform to the applicable goals and policies of Volume II. "Proposals" specified in Volume II are not mandated, but are to be undertaken in relation to all applicable land use requests.

**Comprehensive Plan Volume II:**

The following Goals, Policies, and Proposals from Volume II of the Comprehensive Plan provide criteria applicable to this request:

The implementation of most goals, policies, and proposals as they apply to this application are accomplished through the provisions, procedures, and standards in the city codes and master plans, which are sufficient to adequately address applicable goals, polices, and proposals as they apply to this application.

The following additional findings are made relating to specific Goals and Policies:

GOAL IV 1: TO ENCOURAGE THE CONTINUED GROWTH AND DIVERSIFICATION OF McMINNVILLE'S ECONOMY IN ORDER TO ENHANCE THE GENERAL WELL-BEING OF THE COMMUNITY AND PROVIDE EMPLOYMENT OPPORTUNITIES FOR ITS CITIZENS.

GOAL IV 2: TO ENCOURAGE THE CONTINUED GROWTH OF McMINNVILLE AS THE COMMERCIAL CENTER OF YAMHILL COUNTY IN ORDER TO PROVIDE EMPLOYMENT OPPORTUNITIES, GOODS, AND SERVICES FOR THE CITY AND COUNTY RESIDENTS.

*Policy 21.01 The City shall periodically update its economic opportunities analysis to ensure that it has within its urban growth boundary (UGB) a 20-year supply of lands designated for commercial and industrial uses. The City shall provide an adequate number of suitable, serviceable sites in appropriate locations within its UGB. If it should find that it does not have an adequate supply of lands designated for commercial or industrial use it shall take corrective actions which may include, but are not limited to, redesignation of lands for such*

*purposes, or amending the UGB to include lands appropriate for industrial or commercial use. (Ord.4796, October 14, 2003)*

**APPLICANT'S RESPONSE:** The comprehensive plan map is already consistent with these Goals and this Policy. The subject site and surrounding properties to the north and east are designated commercial and zoned C-3PD.

The proposed amendment does not change this designation, and the property remains zoned C-3PD. Additional vacant commercial land remains north and east of the subject property, to accommodate additional office or senior housing uses. The amendment only adds multi-family apartments to the list of allowed uses, which already includes senior apartments.

**FINDING: SATISFIED.** City concurs with the applicant's findings, and adds that the proposed amendment to Condition 15 would not remove any ability to develop commercial uses on the subject site. Currently, and after the proposed amendment, professional office, medical office, senior condominiums, senior apartments, assisted living facilities, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility, as allowed under the revised condition of approval that was approved by Ordinance No. 4863.

**GOAL IV 3: TO ENSURE COMMERCIAL DEVELOPMENT THAT MAXIMIZES EFFICIENCY OF LAND USE THROUGH UTILIZATION OF EXISTING COMMERCIALY DESIGNATED LANDS, THROUGH APPROPRIATELY LOCATING FUTURE COMMERCIAL LANDS, AND DISCOURAGING STRIP DEVELOPMENT.**

*Policy 24.50 The location, type, and amount of commercial activity within the urban growth boundary shall be based on community needs as identified in the Economic Opportunities Analysis. (Ord.4796, October 14, 2003)*

*Policy 25.00 Commercial uses will be located in areas where conflicts with adjacent land uses can be minimized and where city services commensurate with the scale of development are or can be made available prior to development.*

*Policy 29.00 New direct access to arterials by large-scale commercial developments shall be granted only after consideration is given to the land uses and traffic patterns in the area of development as well as at the specific site. Internal circulation roads, acceleration/deceleration lanes, common access collection points, signalization, and other traffic improvements shall be required wherever necessary, through the use of planned development overlays.*

**APPLICANT'S RESPONSE:** As noted, the Subject Property is designated commercial, consistent with these policies. The designation does not change with this proposed amendment. In approving the prior amendment (Ordinance 4863) the City has already determined that residential use, in the form of senior housing, would be compatible with the underlying C-3 commercial zoning, which allows multi-family housing. The applicant is simply requesting that the residential use not be restricted to senior housing, by also allowing traditional multi-family apartment.

The provisions of Policy 29.00 remain in effect, and have been implemented by the existing transportation improvements in the immediate area. As demonstrated by the traffic impact report attached with this application, the proposed multi-family use will generate less traffic impacts than the base commercial office uses.

**FINDING: SATISFIED.** The existing property is already designated on the Comprehensive Plan as Commercial, and is zoned C-3 PD (General Commercial Planned Development) as determined to meet Goal IV 3 and Policies 24.50 and 25.00 by the previous actions to amend the Comprehensive Plan (CPA 6-99) and adopt the existing Planned Development (ZC 11-99) by Ordinance 4709. The proposed amendment to add multiple family residential as an allowed use would not remove any ability to develop commercial uses on the subject site. Currently, and after the proposed amendment, professional office, medical office, senior condominiums, senior apartments, assisted living facilities, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility, would still be allowed on the subject site under the revised condition of approval that was approved by Ordinance No. 4863.

The existing property has frontage on SE Norton Lane, which is identified as a minor collector in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010. Therefore, the consideration of the impacts of a new direct access on land uses and traffic patterns in the area of the development is not required by Policy 29.00 because the adjacent roadway is not an arterial. However, the applicant provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use.

*Proposal 6.00 A planned development overlay should be placed on the large cluster commercial development areas and the entrances to the City to allow for review of site design, on-site and off-site circulation, parking, and landscaping. The areas to be overlaid by this designation shall be noted on the zoning map and/or comprehensive plan map.*

**APPLICANT'S RESPONSE:** The Three Mile Lane Planned Development is consistent with this proposal. The PD Overlay remains in effect, the amendment simply adds multi-family apartments as an allowed uses, and will maintain consistency with the underlying C-3 zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings, and adds that the existing conditions of approval from Ordinance No. 4709 remain in effect. Conditions 2, 3, and 4 require site plan and landscape plan review and approval by the Planning Commission and Landscape Review Committee, respectively, prior to any development of the subject site. These reviews would ensure review of site design, on-site and off-site circulation, parking, and landscaping.

**GOAL V 1:** TO PROMOTE DEVELOPMENT OF AFFORDABLE, QUALITY HOUSING FOR ALL CITY RESIDENTS.

*Policy 58.00 City land development ordinances shall provide opportunities for development of a variety of housing types and densities.*

*Policy 59.00 Opportunities for multiple-family and mobile home developments shall be provided in McMinnville to encourage lower-cost renter and owner-occupied housing. Such housing shall be located and developed according to the residential policies in this plan and the land development regulations of the City.*

*Policy 61.00 The City of McMinnville shall monitor the conversion of lands to residential use to ensure that adequate opportunities for development of all housing types are assured. Annual reports on the housing development pattern, housing density and mix shall be prepared for city review.*

**APPLICANT'S RESPONSE:** The City has implemented these Policies through adoption of the Zoning Code, which includes residential zones accommodating various densities and housing types. Further, the underlying C-3 zone allows multi-family development, consistent with R-4 standards, per Section 17.33.010.3. The proposed amendment simply adds this allowed use, maintaining consistency with the C-3 code.

As addressed herein, the evidence provided demonstrates that there is no greater need for senior housing over other type of housing. In fact, the City's Code does not even distinguish senior housing as a separate use category. Any senior housing built would be approved consistent with the C-3 and R-4 zoning for multi-family housing.

Further, there is residential R-4 land immediately to the west of the subject site, so there is a basis for concluding that R-4 residential is compatible with the professional and medical office commercial designation for the remainder of the Planned Development Area.

The requested change will not alter the current density allocated for the property. The allowed density will remain consistent with the R-4 standards for multi-family housing.

Therefore the requested amendment maintains consistency with the applicable zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

**GOAL V 2:** TO PROMOTE A RESIDENTIAL DEVELOPMENT PATTERN THAT IS LAND INTENSIVE AND ENERGY-EFFICIENT, THAT PROVIDES FOR AN URBAN LEVEL OF PUBLIC AND PRIVATE SERVICES, AND THAT ALLOWS UNIQUE AND INNOVATIVE DEVELOPMENT TECHNIQUES TO BE EMPLOYED IN RESIDENTIAL DESIGNS.

*Policy 68.00 The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center and to those areas where urban services are already available before committing alternate areas to residential use.*

*Policy 71.00 The City of McMinnville shall designate specific lands inside the urban growth boundary as residential to meet future projected housing needs. Lands so designated may be developed for a variety of housing types. All residential zoning classifications shall be allowed in areas designated as residential on the Comprehensive Plan Map.*

*Policy 71.05 The City of McMinnville shall encourage annexations and rezoning which are consistent with the policies of the Comprehensive Plan so as to achieve a continuous five-year supply of buildable land planned and zoned for all needed housing types. (Ord.4840, January 11, 2006; Ord. 4243, April 5, 1983; Ord. 4218, November 23, 1982)*

**APPLICANT'S RESPONSE:** The area of the proposed amendment is zoned C-3 Commercial and therefore is not committed to low density development. The existing C-3 zoning is consistent with Goal V2 and the Policies 68, 71.05 and 71.09.

**FINDING: SATISFIED.** The proposed Planned Development Amendment would result in the addition of multiple family residential to the list of allowed uses in the existing Planned Development area. The subject property is not designated as residential, but has an underlying zone of C-3 (General Commercial). The underlying C-3 zone allows multiple family residential uses as a permitted use, so the proposed amendment is not inconsistent with the underlying zone and provides an opportunity for the development of residential uses.

The most recently acknowledged Residential Buildable Lands Inventory, which was prepared in 2001, that identified a need for additional land for housing uses. That inventory, which was titled the McMinnville Buildable Land Needs Analysis and Growth Management Plan, identified a deficit of over 1,000 acres of land for housing in Table B-11 of Appendix B. More specifically, the analysis identified a need of 162 acres of R-4 (higher density) zoned land. The proposed amendment to allow multiple family residential on the subject site would not increase the amount of land designated specifically for higher density residential use, but it would increase the potential for the development of higher density residential uses on a property with an existing, underlying zone that already permits multiple family residential use (C-3 General Commercial zone).

*Policy 71.09 Medium and High-Density Residential (R-3 and R-4) – The majority of residential lands in McMinnville are planned to develop at medium density range (4 – 8 units per net acre). Medium density residential development uses include small lot single-family detached uses, single family attached units, duplexes and triplexes, and townhouses. High density residential development (8 – 30 dwelling units per net acre) uses typically include townhouses, condominiums, and apartments. The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center and to those areas where urban services are already available before committing alternate areas to residential use.*

*1. Areas that are not committed to low density development;*

**APPLICANT'S RESPONSE:** The area of the proposed amendment is zoned C-3 Commercial and therefore is not committed to low density development. The existing C-3 zoning is consistent with Goal V2 and the Policies 68, 71.03, and 71.09.

The current Planned Development Overlay is consistent with the criteria for location of medium and high density residential, which is allowed under Policy 71.09 and also specifically allowed in the C-3 zone.

However, Ordinance 4863 imposed a specific limitation restricting residential development to various types of senior housing, per Condition 15. Except for this specific conditioned limitation, the C-3 zone would otherwise allow multi-family development, consistent with R-4 standards, as a permitted use.

The applicant's intent is to develop high density apartments consistent with the R-4 standards, as required by the C-3 zone. So technically, under the C-3 zoning, the density is not being increased, as the R-4 standards will be applied.

**FINDING: SATISFIED.** City concurs with the applicant's findings, specifically that the existing underlying zone of C-3 (General Commercial) is not committed to low density development.

*2. Areas that have direct access from collector or arterial streets;*

**APPLICANT'S RESPONSE:** The subject site has direct access from the Highway (E. Salmon River) via SE Norton Lane. Therefore this criterion is met.

**FINDING: SATISFIED.** The subject site has frontage on SE Norton Lane, which is identified as a minor collector in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010. Therefore, the subject site has direct access from a collector street.

3. *Areas that are not subject to development limitations such as topography, flooding, or poor drainage;*

**APPLICANT'S RESPONSE:** The subject site is an open generally flat field, with no development restrictions. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

4. *Areas where the existing facilities have the capacity for additional development;*

**APPLICANT'S RESPONSE (Provided December 20, 2018):** The commercial area along Norton Lane is served by a full range of urban services including sanitary sewer, water, storm drainage and franchise utilities. All of these services generally have sufficient capacity to serve the proposed development.

However, the City Engineer indicated that the existing sanitary sewer line serving the subject site was designed based on, "*an assumption in the modeling for undeveloped commercial/industrial properties at an equivalent of 12 dwelling units per acre.*"

Consequently, in coordination with the City Engineer, CH2M has conducted an analysis of the sewer system capacity for the immediate area to determine that there is capacity. The system was designed based on medium density residential calculated to serve 59 units (4.93 acres x 12 units per acre).

The applicant has agreed to cover the cost for CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development.

CH2M findings will be submitted separately.

**Revised Response from Applicant (Provided March 14, 2019):** The applicant agreed to cover the cost of CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development

However, Mike Bisset, City Engineer, has informed us that the City has engaged CH2M to conduct a system-wide analysis, rather than just to Norton Lane area.

**FINDING: SATISFIED.** The Engineering Department provided comments (provided on April 5, 2019) on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site. Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction.



Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

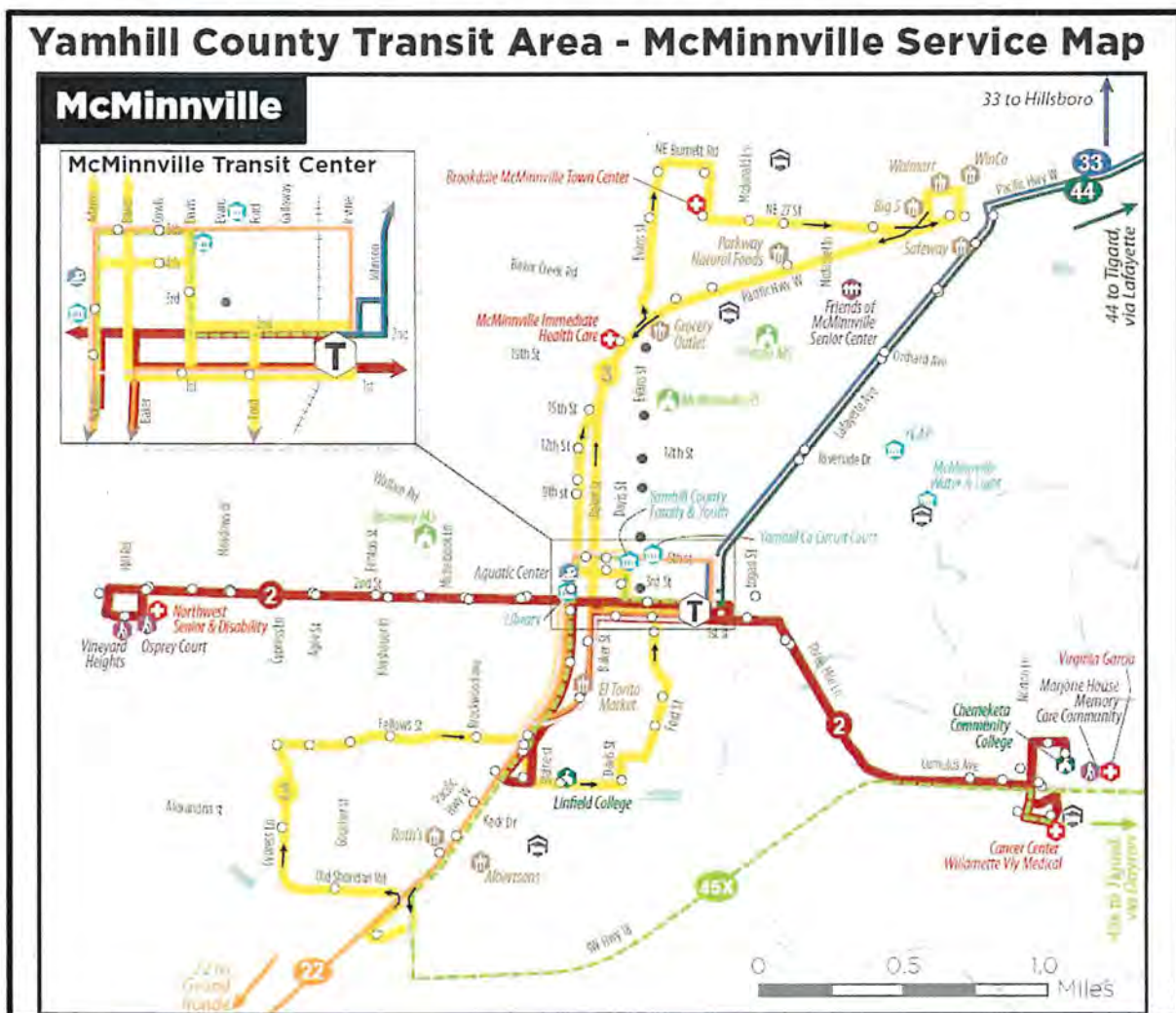
5. Areas within one-quarter mile of existing or planned public transportation; and,

**APPLICANT'S RESPONSE:** Local Transit Routes 2 runs on the Highway and on SE Norton Lane to the Willamette Valley Medical Center, which is located across the street from the subject site. See the attached Route Map and Schedule, at the end of this Compliance Narrative.

Route 2 provides regularly scheduled weekday service. This route connects to the Transit Center for connection to the Yamhill County Transit route for access to the other parts of town. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would add that Route 2 provides roughly hourly weekday service, and connects to the McMinnville Transit Center, where riders could transfer to other routes, including commuter routes to surrounding cities.

A view of the McMinnville transit map, which identifies Routes 2 near the Willamette Valley Medical Center, is provided below:



6. *Areas that can be buffered from low density residential areas in order to maximize the privacy of established low density residential areas.*

**APPLICANT'S RESPONSE:** And, there is residential R-4 land immediately to the west of the subject site, which establishes the buffer of lower density residential per criteria 6.

The requested change will not alter the current density allocated to the property. The density will remain as is consistent with the R-4 standards for multi-family housing.

Therefore all of the criteria in Policy 71.09 are met.

**FINDING: SATISFIED.** The subject site is not located adjacent to any existing low density residential areas. Therefore the subject site and area does not require any buffering from low density residential areas.

*Policy 71.13 The following factors should serve as criteria in determining areas appropriate for high-density residential development:*

1. *Areas which are not committed to low or medium density development;*

**APPLICANT'S RESPONSE:** The subject property is zoned C-3 General Commercial with a PD overlay. The C-3 zone allows high density residential use. Therefore it is not committed to low or medium density development.

**FINDING: SATISFIED.** City concurs with the applicant's findings, specifically that the existing underlying zone of C-3 (General Commercial) is not committed to low density development.

2. *Areas which can be buffered by topography, landscaping, collector or arterial streets, or intervening land uses from low density residential areas in order to maximize the privacy of established low density residential areas;*

**APPLICANT'S RESPONSE:** There is no designated low density residential in the surrounding neighborhood. There is R-4 zoning to the west, which provides a buffer between the site and other zoning. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

3. *Areas which have direct access from a major collector or arterial street;*

**APPLICANT'S RESPONSE:** The subject site has direct access from the Highway (E. Salmon River) via SE Norton Lane. Therefore this criterion is met.

**FINDING: SATISFIED.** While Highway 18 (Three Mile Lane) is identified as a major arterial in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010, the subject site does not have direct access from Highway 18. The subject site has frontage and direct access only onto SE Norton Lane, which is identified as a minor collector in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010. However, the applicant provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development



conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use. Therefore, the use allowed by the proposed amendment would not be more detrimental to the surrounding street network or transportation system than other uses currently allowed on the subject site by Ordinance No. 4709 and Ordinance No. 4863.

4. *Areas which are not subject to development limitations;*

**APPLICANT'S RESPONSE:** The subject site is an open generally flat field, with no development restrictions. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

5. *Areas where the existing facilities have the capacity for additional development;*

**APPLICANT'S RESPONSE (Provided December 20, 2018):** The City Engineer indicates that the existing sanitary sewer line serving the subject site was designed based on, "*an assumption in the modeling for undeveloped commercial/industrial properties at an equivalent of 12 dwelling units per acre.*" Whereas, the applicant is proposing high density multi-family development up to 120 units.

Consequently, in coordination with the City Engineer, CH2M has conducted an analysis of the sewer system capacity for the immediate area to determine that there is capacity. The system was designed based on medium density residential calculated to serve 59 units (4.93 acres x 12 units per acre).

The applicant has agreed to cover the cost for CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development.

CH2M findings will be submitted separately.

**Revised Response from Applicant (Provided March 14, 2019):** The applicant agreed to cover the cost of CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development

However, Mike Bisset, City Engineer, has informed us that the City has engaged CH2M to conduct a system-wide analysis, rather than just to Norton Lane area.

**FINDING: SATISFIED.** The Engineering Department provided comments (provided on April 5, 2019) on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site. Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction.

Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

6. *Areas within a one-half mile wide corridor centered on existing or planned public transit routes;*

**APPLICANT'S RESPONSE:** Local Transit Route 2 runs on the Highway and on SE Norton Lane to the Willamette Valley Medical Center. The Medical Center is located across the street from the subject site. See the attached Route Map and Schedule, at the end of this Compliance Narrative. Route 2 provides regularly scheduled weekday service. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would add that Route 2 provides roughly hourly weekday service, and connects to the McMinnville Transit Center, where riders could transfer to other routes, including commuter routes to surrounding cities.

A view of the McMinnville transit map, which identifies Routes 2 near the Willamette Valley Medical Center, is provided in the finding for Policy 71.09 above.

7. *Areas within one-quarter mile from neighborhood and general commercial shopping centers; and*

**APPLICANT'S RESPONSE:** The City's zoning for Neighborhood Commercial is C-1, which General Commercial is C-2.

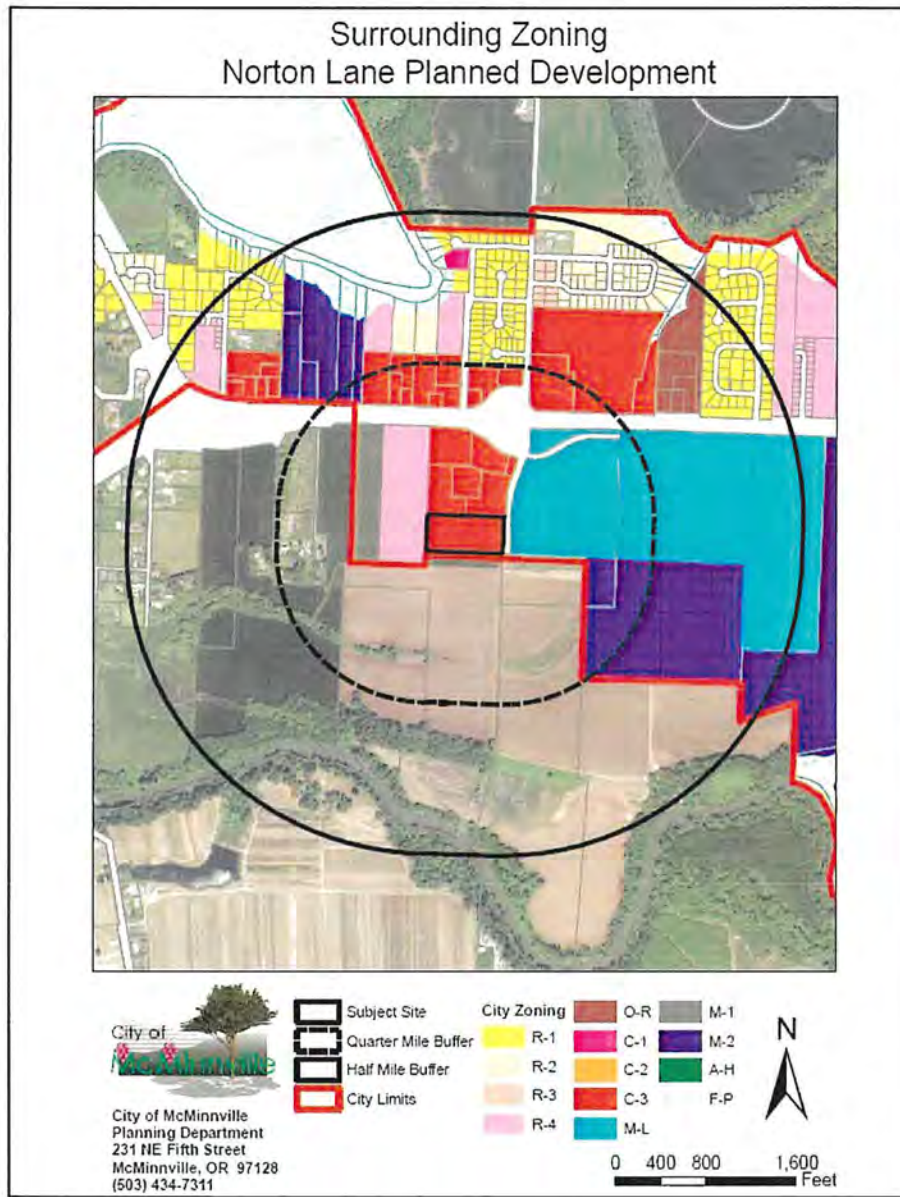
The subject property and adjacent property to the north (770 feet) to Stratus Avenue is zoned C-2, General Commercial, with a PD overlay. There is also C-2 zoning on the north side of the highway on both sides of NE Norton Lane.

The closest C-1 zoning is approximately ¼ mile east of the Norton Lane intersection with the Highway.

Therefore this criterion is met.

**FINDING: SATISFIED.** While the McMinnville Municipal Code (MMC) does have a Neighborhood Business Zone (C-1), that zoning district has been applied very narrowly and is only designated on two parcels in the entire city limits (the two parcels that the applicant notes approximately ¼ mile east of the Norton Lane intersection with Highway 18). Also, the MMC does not have any definition of neighborhood or general commercial shopping centers. Therefore, the commercial designation of property is used in analyzing this Comprehensive Plan policy and high density residential locational factor. The subject site is located within one-quarter mile of commercially zoned property. The commercially zoned property within one-quarter mile of the subject site is zoned C-3 (General Commercial) – which is a correction on the applicant's response where General Commercial is identified as C-2 – to allow general retail, service, and shopping type uses.

A map identifying the subject site and the distances to commercially zoned properties is provided below:



8. *Areas adjacent to either private or public permanent open space.*

**APPLICANT'S RESPONSE:** The closest public open space to the site is Joe Dancer Park, which is located on E. Brooks Street, approximately 1.4 miles to the northwest.

The proposed development will however be designed to provide on-site open space for passive and active recreation for tenants. Therefore this criterion is met.

Therefore all of the criteria in Policy 71.13 are met.

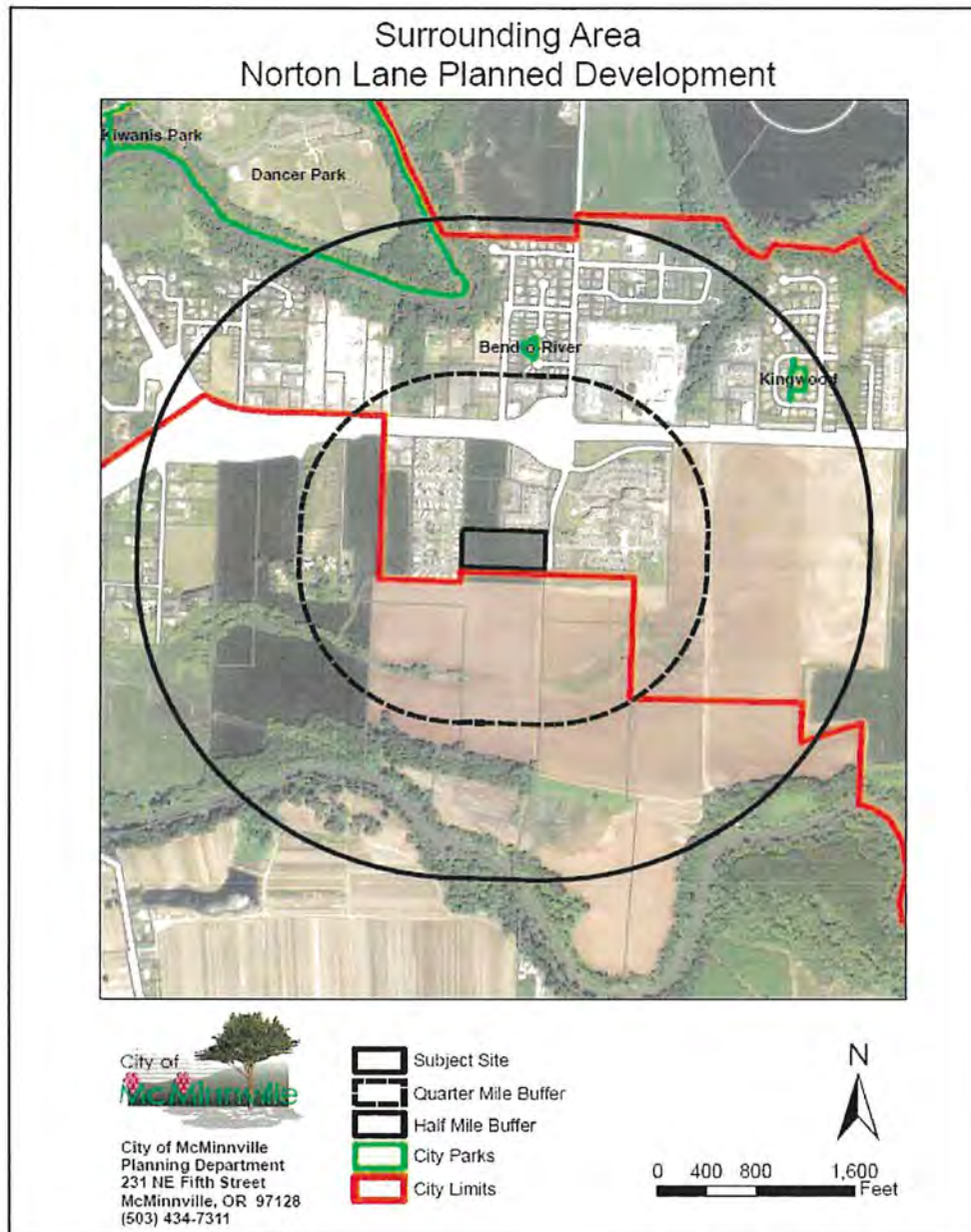
**FINDING: SATISFIED WITH CONDITION #1.** The subject site is not located adjacent to private or public open space. There are only three public parks within ½ mile of the subject site, and two of those parks (Bend-o-River and Kingwood) are what would be classified as Mini-Parks in the McMinnville Parks, Recreation, and Open Space Master Plan. The other park within ½ mile of the subject site is Joe Dancer Park, which is identified as a Community Park in the McMinnville

Parks, Recreation, and Open Space Master Plan. However, Joe Dancer Park is located across the Yamhill River and is not actually accessible within a ½ mile distance of the subject site.

The factor for high density residential development calls for open space to be adjacent to the site, not within a certain distance. While there some opportunities for access to open space within one-half mile of the subject site, there is not any usable open space adjacent to the site. Not only is there no open space adjacent to the site, there is a lack of connectivity to open space in the surrounding area. As a planned development, open space needs to be provided that provides a benefit to the residents of the site. Therefore, language is included in the amended condition of approval to require that, if the site is developed with multiple-family residential uses, an area equivalent to 10 percent of the gross size of the site shall be reserved for usable open space for residents of the multiple-family development site. The condition of approval states that the usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.

A map identifying the subject site and the distances to surrounding parks is provided below:





*Planned Development Policies*

*Policy 72.00 Planned developments shall be encouraged as a favored form of residential development as long as social, economic, and environmental savings will accrue to the residents of the development and the city.*

**APPLICANT’S RESPONSE:** The existing Three Mile Lane Planned Development has been established consistent with these Planned Development Policies, per Ordinance 4709. The 20 conditions included with the ordinance remain in effect, and are applicable to any development of the Subject Site.

**FINDING: SATISFIED.** City concurs with the applicant’s findings. The existing Planned Development overlay, approved by Ordinance No. 4709, is not specifically residential, but is of an underlying commercial zone that allows for some types of residential uses, including the proposed multiple family residential use. The proposed amendment to allow multiple family

residential use, along with the other commercial and senior living residential uses currently allowed by the existing Planned Development condition of approval, which would provide an opportunity for a wider variety of housing types than is currently allowed.

*Policy 73.00 Planned residential developments which offer a variety and mix of housing types and prices shall be encouraged.*

**APPLICANT'S RESPONSE:** The requested amendment will add opportunity for a wider variety of housing types, than is currently allowed. However, this general area is not identified by the City for lower density housing alternatives, just medium and high density residential, consistent with the C-3 zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would add that the existing Planned Development overlay, approved by Ordinance No. 4709, is not specifically residential, but is of an underlying commercial zone that allows for some types of residential uses, including the proposed multiple family residential use. The proposed amendment to allow multiple family residential use, along with the other commercial and senior living residential uses currently allowed by the existing Planned Development condition of approval, which would provide an opportunity for a wider variety of housing types than is currently allowed.

*Policy 74.00 Distinctive natural, topographic, and aesthetic features within planned developments shall be retained in all development designs.*

**APPLICANT'S RESPONSE:** There are no particularly distinctive natural, topographic or aesthetic features associated with the Subject Site. The property is relatively flat, and currently abuts farm land to the south, which is outside of the city limits and UGB.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

*Policy 75.00 Common open space in residential planned developments shall be designed to directly benefit the future residents of the developments. When the open space is not dedicated to or accepted by the City, a mechanism such as a homeowners association, assessment district, or escrow fund will be required to maintain the common area.*

**APPLICANT'S RESPONSE:** No specific development plans are being proposed at this time. However, the applicant is proposing an apartment complex. Their typical development plans provide landscaped open space for passive recreational use, and depending upon the specific target market, other amenities may be provided. Two examples of the applicant's most recent apartment projects have been provided to show how open space and amenities are typically provided.

Specific details regarding site amenities will be provided at the time of development review, assuming this requested PD amendment is approved. Such facilities or amenities are always located for convenient use of the tenants, as reflected in the site plan examples.

The applicant will maintain ownership and management of the facilities, with costs covered by rents. Therefore no HOA, assessment district or escrow funds are necessary to ensure proper maintenance.

**FINDING: SATISFIED WITH CONDITION #1.** City concurs with the applicant's findings. City would add that, while there are some opportunities for access to open space within one-half mile of the subject site, there is not any usable open space adjacent to the site. Not only is there no

open space adjacent to the site, there is a lack of connectivity to open space in the surrounding area. As a planned development, open space needs to be provided that provides a benefit to the residents of the site. Therefore, a condition of approval related to Policy 71.13 will require that, if the site is developed with multiple-family residential uses, an area equivalent to 10 percent of the gross size of the site shall be reserved for usable open space for residents of the multiple-family development site. The condition of approval states that the usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.

*Policy 76.00 Parks, recreation facilities, and community centers within planned developments shall be located in areas readily accessible to all occupants.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED WITH CONDITION #1.** While there some opportunities for access to open space within one-half mile of the subject site, there is not any usable open space adjacent to the site. Not only is there no open space adjacent to the site, there is a lack of connectivity to open space in the surrounding area. As a planned development, open space needs to be provided that provides a benefit to the residents of the site, and in areas readily accessible to all occupants. Therefore, a condition of approval related to Policy 71.13 will require that, if the site is developed with multiple-family residential uses, an area equivalent to 10 percent of the gross size of the site shall be reserved for usable open space for residents of the multiple-family development site. The condition of approval states that the usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.

Further, the existing conditions of approval from Ordinance No. 4709 remain in effect. Conditions 2, 3, and 4 require site plan and landscape plan review and approval by the Planning Commission and Landscape Review Committee, respectively, prior to any development of the subject site. These reviews would ensure review of site design, on-site and off-site circulation, parking, and landscaping, which will allow an opportunity to review whether the required open space is located in an area readily accessible to all occupants.

*Policy 77.00 The internal traffic system in planned developments shall be designed to promote safe and efficient traffic flow and give full consideration to providing pedestrian and bicycle pathways.*

**APPLICANT'S RESPONSE:** Any development of the subject property will be required to meet these design criteria. So this policy will be equally implemented with or without the proposed amendment. Again, the two site plans provided show how site circulation is typically provided.

**FINDING: SATISFIED.** The existing conditions of approval from Ordinance No. 4709 remain in effect. Conditions 2, 3, and 4 require site plan and landscape plan review and approval by the Planning Commission and Landscape Review Committee, respectively, prior to any development of the subject site. These reviews would ensure review of site design, on-site and off-site circulation, parking, and landscaping, which will allow an opportunity to review whether the internal traffic system promotes safe and efficient traffic flow and gives consideration to providing pedestrian and bicycle pathways.

*Policy 78.00 Traffic systems within planned developments shall be designed to be compatible with the circulation patterns of adjoining properties.*

**APPLICANT'S RESPONSE:** Traffic system improvements have been developed in the immediate neighborhood, including the Highway 18 intersection with Norton Lane, and the SE Norton Lane intersection with SE Stratus Avenue. These improvements provide basic and adequate circulation into, out of, and throughout the immediate neighborhood. These street improvements were made to meet the anticipated development within the Three Mile Lane Planned Development area.

As reflected in Table 1 herein, and the accompanying Traffic Impact Report, the traffic impacts of the proposed multi-family development will be slightly more than those associated with senior housing, but will be considerably lower than traffic generated by allowed professional and medical offices, thereby maintaining consistency with this criterion.

**FINDING: SATISFIED.** The site is adjacent to SE Norton Lane, which is identified as a minor collector in the McMinnville Transportation System Plan. Section 17.53.101 of the McMinnville Municipal Code identifies the right-of-way width for minor collector streets as 56 feet (with no bike lane) or 66 feet (with bike lane). The existing right-of-way width of SE Norton Lane adjacent to the subject site is 71.47 feet, as shown on Partition Plat 99-47. Therefore, no additional right-of-way dedications are necessary. Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure the traffic system within planned developments will be compatible with adjoining properties. In addition, the applicant provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

#### *Residential Design Policies*

*Policy 79.00 The density allowed for residential developments shall be contingent on the zoning classification, the topographical features of the property, and the capacities and availability of public services including but not limited to sewer and water. Where densities are determined to be less than that allowed under the zoning classification, the allowed density shall be set through adopted clear and objective code standards enumerating the reason for the limitations, or shall be applied to the specific area through a planned development overlay. Densities greater than those allowed by the zoning classification may be allowed through the planned development process or where specifically provided in the zoning ordinance or by plan policy. (Ord. 4796, October 14, 2003)*

**APPLICANT'S RESPONSE:** The requested amendment to allow multi-family development, only amends Condition 15 of Ord. 4709, and does not alter the allowed density, which will remain consistent with the R-4 standards and the base C-3 zoning.



**FINDING: SATISFIED.** City concurs with the applicant's findings. The proposed amendment to the existing Planned Development condition would add multiple family residential as an allowed use. Multiple family residential is a permitted use in the underlying C-3 (General Commercial) zone, subject to the standards and provisions of the R-4 (Multiple Family Residential) zone. Therefore, the future development, if it includes multiple family residential dwelling units, will be subject to the density requirements of the R-4 zone.

Further, existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services.

#### *Urban Policies*

*Policy 99.00 An adequate level of urban services shall be provided prior to or concurrent with all proposed residential development, as specified in the acknowledged Public Facilities Plan. Services shall include, but not be limited to:*

- 1. Sanitary sewer collection and disposal lines. Adequate municipal waste treatment plant capacities must be available.*
- 2. Storm sewer and drainage facilities (as required).*
- 3. Streets within the development and providing access to the development, improved to city standards (as required).*
- 4. Municipal water distribution facilities and adequate water supplies (as determined by City Water and Light). (as amended by Ord. 4796, October 14, 2003)*
- 5. Deleted as per Ord. 4796, October 14, 2003.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

Specifically related to sanitary sewer service, the Engineering Department provided comments on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site.

**GOAL VI 1:** TO ENCOURAGE DEVELOPMENT OF A TRANSPORTATION SYSTEM THAT PROVIDES FOR THE COORDINATED MOVEMENT OF PEOPLE AND FREIGHT IN A SAFE AND EFFICIENT MANNER.

## Streets

*Policy 117.00 The City of McMinnville shall endeavor to insure that the roadway network provides safe and easy access to every parcel.*

*Policy 118.00 The City of McMinnville shall encourage development of roads that include the following design factors:*

- 1. Minimal adverse effects on, and advantageous utilization of, natural features of the land.*
- 2. Reduction in the amount of land necessary for streets with continuance of safety, maintenance, and convenience standards.*
- 4. Emphasis placed on existing and future needs of the area to be serviced. The function of the street and expected traffic volumes are important factors.*
- 5. Consideration given to Complete Streets, in consideration of all modes of transportation (public transit, private vehicle, bike, and foot paths). (Ord.4922, February 23, 2010)*

*Policy 119.00 The City of McMinnville shall encourage utilization of existing transportation corridors, wherever possible, before committing new lands.*

*Policy 120.00 The City of McMinnville may require limited and/or shared access points along major and minor arterials, in order to facilitate safe access flows.*

*Policy 121.00 The City of McMinnville shall discourage the direct access of small-scale residential developments onto major or minor arterial streets and major collector streets.*

**APPLICANT'S RESPONSE:** The City's transportation Goal and Policies are further refined and implemented through the Transportation System Plan (TSP). The TSP provides the functional classification of streets, including arterial, collector and local designations.

The TSP identifies corridor improvements for the Highway 18 Corridor:

*ODOT, the City of McMinnville and Yamhill County have mutually approved the Highway 18 Corridor Refinement Plan, which was completed in 1996. The Plan includes a series of traffic control and frontage roads improvements north and south of Highway 18, to include closing of the existing Norton Lane intersection, construction of a new interchange near the Evergreen Air Museum, and redesign of the current East McMinnville (Three Mile Lane) interchange for full, directional access.*

*Some frontage road improvements have been completed since the 1996 Plan was adopted. Exhibit 4-10 illustrates the current state of the Plan. It is important to note, however, that the northernmost collector access road depicted in the Highway 18 Corridor Refinement Plan is not shown in the TSP due to the fact that it is located outside of McMinnville's current urban growth boundary. Under Oregon Administrative Rule (OAR 660-012), transportation facilities outside of urban growth boundaries are not permitted as part of a TSP unless a "reasons" exception to the applicable goal(s) has been approved by the City. In this case, McMinnville finds such action premature due to the lack of certainty as to the street corridor's location and design. An*

*amendment to this plan, and a Goal 2 (Land Use) exception, would be part of any future proposal to add this element to the TSP and permit its construction and use for urban purposes.*

Some of the corridor improvements have already been completed, including the Norton Lane intersection and Stratus Avenue frontage road. The future interchange discussed is further to the east, at Cumulus Avenue. However, no decision regarding an interchange has yet been made.

The proposed amendment does not substantially alter the demand for transportation and public service improvement to any greater degree than the currently allowed uses.

Primary access to the property is from Salmon River Highway 18, designated as an arterial street. SE Norton Lane intersects with Highway 18 north of the subject property. The property fronts on the west side of SE Norton Lane, just south of the intersection with SE Stratus Avenue. Stratus Avenue is classified as a collector.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would clarify that the site is adjacent to SE Norton Lane, which is identified as a minor collector in the McMinnville Transportation System Plan. Section 17.53.101 of the McMinnville Municipal Code identifies the right-of-way width for minor collector streets as 56 feet (with no bike lane) or 66 feet (with bike lane). The existing right-of-way width of SE Norton Lane adjacent to the subject site is 71.47 feet, as shown on Partition Plat 99-47. Therefore, no additional right-of-way dedications are necessary.

Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

Further, Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure review of site design and on-site and off-site circulation to ensure that the proposed access to serve future development will be compatible with the surrounding transportation system.

*Policy 122.00 The City of McMinnville shall encourage the following provisions for each of the three functional road classifications.*

*2. Major, minor collectors.*

*-Designs should minimize impacts on existing neighborhoods.*

*-Sufficient street rights-of-way should be obtained prior to development of adjacent lands.*

*-On-street parking should be limited wherever necessary.*

*-Landscaping should be required along public rights-of-way. (Ord.4922, February 23, 2010)*

*-As far as is practical, residential collector streets should be no further than 1,800 feet apart in order to facilitate a grid pattern of collector streets in residential areas.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

#### *Parking*

*Policy 126.00 The City of McMinnville shall continue to require adequate off-street parking and loading facilities for future developments and land use changes.*

*Policy 127.00 The City of McMinnville shall encourage the provision of off-street parking where possible, to better utilize existing and future roadways and rights-of-way as transportation routes.*

#### *Bike Paths*

*Policy 130.00 The City of McMinnville shall encourage implementation of the Bicycle System Plan that connects residential areas to activity areas such as the downtown core, areas of work, schools, community facilities, and recreation facilities. (Ord.4922, February 23, 2010)*

*Policy 131.00 The City of McMinnville shall encourage development of bicycle and footpaths in scenic and recreational areas as part of future parks and activities.*

*Policy 132.00 The City of McMinnville shall encourage development of subdivision designs that include bike and foot paths that interconnect neighborhoods and lead to schools, parks, and other activity areas. (Ord. 4922, February 23, 2010; Ord. 4260, August 2, 1983)*

*Policy 132.15 The City of McMinnville shall require that all new residential developments such as subdivisions, planned developments, apartments, and condominium complexes provide pedestrian connections with adjacent neighborhoods.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure review of site design, including off-street parking and bike and pedestrian connections associated with the future development plan.

#### *Supportive of General Land Use Plan Designations and Development Patterns*

*Policy 132.27.00 The provision of transportation facilities and services shall reflect and support the land use designations and development patterns identified in the McMinnville Comprehensive Plan. The design and implementation of transportation facilities and*

services shall be based on serving current and future travel demand—both short-term and long-term planned uses. (Ord. 4922, February 23, 2010)

**APPLICANT'S RESPONSE:** See response to Policies 117.00 – 121.00 above.

**FINDING: SATISFIED.** City concurs with the applicant's findings, along with the additional findings provided for Policies 117.00 – 121.00 above.

#### *Public Safety*

*Policy 132.32.00 The safe, rapid movement of fire, medical, and police vehicles shall be an integral part of the design and operation of the McMinnville transportation system. (Ord. 4922, February 23, 2010)*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will be completed to existing City standards, which are of a design and operation standard that allows for required movements for fire, medical, and police vehicles.

#### *Livability*

*Policy 132.35.00 Transportation facilities in the McMinnville planning area shall be, to the degree possible, designed and constructed to mitigate noise, energy consumption, and neighborhood disruption, and to encourage the use of public transit, bikeways, sidewalks, and walkways. (Ord. 4922, February 23, 2010)*

*Policy 132.41.20 Modal Balance – The improvement of roadway circulation must not impair the safe and efficient movement of pedestrians and bicycle traffic. (Ord. 4922, February 23, 2010)*

*Policy 132.41.25 Consolidate Access – Efforts should be made to consolidate access points to properties along major arterial, minor arterial, and collector roadways. (Ord. 4922, February 23, 2010)*

#### *Pedestrian Programs*

*Policy 132.54.00 Promoting Walking for Health and Community Livability – The City will encourage efforts that inform and promote the health, economic, and environmental benefits of walking for the individual and McMinnville community. Walking for travel and recreation should be encouraged to achieve a more healthful environment that reduces pollution and noise to foster a more livable community. (Ord. 4922, February 23, 2010)*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards.

Further, Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure review of site design and on-site and off-site circulation to ensure that the proposed access to serve future development will be compatible with the surrounding transportation system.

**GOAL VII 1:** TO PROVIDE NECESSARY PUBLIC AND PRIVATE FACILITIES AND UTILITIES AT LEVELS COMMENSURATE WITH URBAN DEVELOPMENT, EXTENDED IN A PHASED MANNER, AND PLANNED AND PROVIDED IN ADVANCE OF OR CONCURRENT WITH DEVELOPMENT, IN ORDER TO PROMOTE THE ORDERLY CONVERSION OF URBANIZABLE AND FUTURE URBANIZABLE LANDS TO URBAN LANDS WITHIN THE McMinnville Urban Growth Boundary.

#### *Sanitary Sewer System*

*Policy 136.00 The City of McMinnville shall insure that urban developments are connected to the municipal sewage system pursuant to applicable city, state, and federal regulations.*

*Policy 139.00 The City of McMinnville shall extend or allow extension of sanitary sewage collection lines within the framework outlined below:*

- 1. Sufficient municipal treatment plant capacities exist to handle maximum flows of effluents.*
- 2. Sufficient trunk and main line capacities remain to serve undeveloped land within the projected service areas of those lines.*
- 3. Public water service is extended or planned for extension to service the area at the proposed development densities by such time that sanitary sewer services are to be utilized.*
- 4. Extensions will implement applicable goals and policies of the comprehensive plan.*

#### *Storm Drainage*

*Policy 142.00 The City of McMinnville shall insure that adequate storm water drainage is provided in urban developments through review and approval of storm drainage systems, and through requirements for connection to the municipal storm drainage system, or to natural drainage ways, where required.*

*Policy 143.00 The City of McMinnville shall encourage the retention of natural drainage ways for storm water drainage.*

#### *Water System*

*Policy 144.00 The City of McMinnville, through McMinnville Water and Light, shall provide water services for development at urban densities within the McMinnville Urban Growth Boundary.*

**APPLICANT'S RESPONSE:** The surrounding development along SE Norton Lane has been constructed consistent with the Public Facilities Goal and these Policies for specific utilities. All required urban services are currently available and adequate to support development of the

property consistent with these policies, assuming that CH2M makes positive findings with their sewer system analysis, being paid for by the applicant.

The proposed amendment will not result in development that will create substantially greater demands on the available services than the types of development that is already allowed.

The City has confirmed that the sanitary sewer line has sufficient capacity to accommodate the proposed increase in density from senior housing to apartments. The net difference is 61 units.

**Revised Response from Applicant:** The applicant agreed to cover the cost of CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development

However, Mike Bisset, City Engineer, has informed us that the City has engaged CH2M to conduct a system-wide analysis, rather than just to Norton Lane area.

**FINDING: SATISFIED.** Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

Specifically related to sanitary sewer service, the Engineering Department provided comments on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site.

*Policy 145.00 The City of McMinnville, recognizing McMinnville Water and Light as the agency responsible for water system services, shall extend water services within the framework outlined below:*

- 1. Facilities are placed in locations and in such a manner as to insure compatibility with surrounding land uses.*
- 2. Extensions promote the development patterns and phasing envisioned in the McMinnville Comprehensive Plan.*
- 3. For urban level developments within McMinnville, sanitary sewers are extended or planned for extension at the proposed development densities by such time as the water services are to be utilized.*
- 4. Applicable policies for extending water services, as developed by the City Water and Light Commission, are adhered to.*

*Policy 147.00 The City of McMinnville shall continue to support coordination between city departments, other public and private agencies and utilities, and McMinnville Water and Light to insure*

*the coordinated provision of utilities to developing areas. The City shall also continue to coordinate with McMinnville Water and Light in making land use decisions.*

*Water and Sewer – Land Development Criteria*

*Policy 151.00 The City of McMinnville shall evaluate major land use decisions, including but not limited to urban growth boundary, comprehensive plan amendment, zone changes, and subdivisions using the criteria outlined below:*

- 1. Sufficient municipal water system supply, storage and distribution facilities, as determined by McMinnville Water and Light, are available or can be made available, to fulfill peak demands and insure fire flow requirements and to meet emergency situation needs.*
- 2. Sufficient municipal sewage system facilities, as determined by the City Public Works Department, are available, or can be made available, to collect, treat, and dispose of maximum flows of effluents.*
- 3. Sufficient water and sewer system personnel and resources, as determined by McMinnville Water and Light and the City, respectively, are available, or can be made available, for the maintenance and operation of the water and sewer systems.*
- 4. Federal, state, and local water and waste water quality standards can be adhered to.*
- 5. Applicable policies of McMinnville Water and Light and the City relating to water and sewer systems, respectively, are adhered to.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** An existing conditions of approval from Ordinance No. 4709, which remains in effect, will require that final development plans include the provision of water and power services (Condition 9). This will ensure that the density of the future development is shown to be able to be served by McMinnville Water and Light services. McMinnville Water and Light was provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

*Police and Fire Protection*

*Policy 153.00 The City shall continue coordination between the planning and fire departments in evaluating major land use decisions.*

*Policy 155.00 The ability of existing police and fire facilities and services to meet the needs of new service areas and populations shall be a criterion used in evaluating annexations, subdivision proposals, and other major land use decisions.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Emergency services departments were provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

*Parks and Recreation*



**GOAL VII 3:** TO PROVIDE PARKS AND RECREATION FACILITIES, OPEN SPACES, AND SCENIC AREAS FOR THE USE AND ENJOYMENT OF ALL CITIZENS OF THE COMMUNITY.

*Policy 163.00 The City of McMinnville shall continue to require land, or money in lieu of land, from new residential developments for the acquisition and/or development of parklands, natural areas, and open spaces.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Park fees shall be paid for each housing unit at the time of building permit application as required by McMinnville Ordinance 4282, as amended.

*Energy Conservation*

**GOAL VIII 1:** TO PROVIDE ADEQUATE ENERGY SUPPLIES, AND THE SYSTEMS NECESSARY TO DISTRIBUTE THAT ENERGY, TO SERVICE THE COMMUNITY AS IT EXPANDS.

*Energy Supply Distribution*

*Policy 173.00 The City of McMinnville shall coordinate with McMinnville Water and Light and the various private suppliers of energy in this area in making future land use decisions.*

*Policy 177.00 The City of McMinnville shall coordinate with natural gas utilities for the extension of transmission lines and the supplying of this energy resource.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** McMinnville Water and Light and Northwest Natural Gas were provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

**GOAL X 1:** TO PROVIDE OPPORTUNITIES FOR CITIZEN INVOLVEMENT IN THE LAND USE DECISION MAKING PROCESS ESTABLISHED BY THE CITY OF McMINNVILLE.

**GOAL X 2:** TO MAKE EVERY EFFORT TO ENGAGE AND INCLUDE A BROAD CROSS SECTION OF THE COMMUNITY BY MAINTAINING AN ACTIVE AND OPEN CITIZEN INVOLVEMENT PROGRAM THAT IS ACCESSIBLE TO ALL MEMBERS OF THE COMMUNITY AND ENGAGES THE COMMUNITY DURING DEVELOPMENT AND IMPLEMENTATION OF LAND USE POLICIES AND CODES.

*Policy 188.00 The City of McMinnville shall continue to provide opportunities for citizen involvement in all phases of the planning process. The opportunities will allow for review and comment by community residents and will be supplemented by the availability of information on planning requests and the provision of feedback mechanisms to evaluate decisions and keep citizens informed.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The process for a planned development amendment provides an opportunity for citizen involvement throughout the process through the neighborhood meeting provisions, the public notice, and the public hearing process. Throughout the process, there are

opportunities for the public to review and obtain copies of the application materials and the completed City report prior to the advertised public hearing(s). All members of the public have access to provide testimony and ask questions during the public review and hearing process.

### **McMinnville Zoning Ordinance**

The following Sections of the McMinnville Zoning Ordinance (Ord. No. 3380) provide criteria applicable to the request:

#### **Chapter 17.03. General Provisions**

**17.03.020 Purpose.** The purpose of this ordinance is to encourage appropriate and orderly physical development in the City through standards designed to protect residential, commercial, industrial, and civic areas from the intrusions of incompatible uses; to provide opportunities for establishments to concentrate for efficient operation in mutually beneficial relationship to each other and to shared services; to provide adequate open space, desired levels of population densities, workable relationships between land uses and the transportation system, and adequate community facilities; to provide assurance of opportunities for effective utilization of the land resource; and to promote in other ways public health, safety, convenience, and general welfare.

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The purpose of the Zoning Ordinance is met by the proposal as described in the Conclusionary Findings contained in this Decision Document.

#### **Chapter 17.06. Definitions**

##### **17.06.015 General Definitions**

**Apartment House** – See "Dwelling, Multifamily."

**Assisted Living** – A living arrangement where the elderly or other persons are provided assistance with daily activities such as dressing, grooming and bathing.

**Condominium** – Ownership of a single unit in a multi-unit structure that includes common areas and facilities; includes residential, commercial, and industrial condominiums and regulated, in part, by Oregon State Law (ORS Chapter 100). (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Dwelling, Multi-Family** – A building containing three or more dwelling units. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Dwelling Unit** – A residence containing one or more rooms designed for occupancy by one family and having not more than one cooking facility. This includes both buildings constructed on-site and off-site, such as manufactured homes. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Family** – For the purpose of this Zoning Ordinance, "family" refers to:  
An individual or two or more persons related by blood, marriage, adoption, or legal guardianship, or other duly-authorized custodial relationship, living together as one housekeeping unit using one kitchen, and providing means of lodging to not more than two additional persons, excluding servants, or a group of not more than five unrelated persons, excluding servants, living together as one housekeeping unit using one kitchen. (Ord. 4988 §1, 2015; Ord. 4479A §1, 1991; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**APPLICANT'S RESPONSE:** A general definition of senior housing is as follows (factfinder2.census.gov):

***Senior housing definition***

*Senior Housing Definition. Senior housing is housing that is suitable for the needs of an aging population. It ranges from independent living to 24-hour care. In senior housing there is an emphasis on safety, accessibility, adaptability, and longevity that many conventional housing options may lack.*

Relative to the proposed amendment, there are a few general factors that affect site development and potential impacts when comparing senior housing to general multi-family housing, as follows:

- Senior housing typically provides smaller 1 and 2 bedroom units, while multi-family facilities may include 3 or more bedroom units in the mix.
- Seniors (retired or assisted living) tend to have fewer cars, relying upon public transit or family to making necessary trips. Those seniors that do still drive tend to make fewer trips than younger tenants, particularly families with children and/or two working adults. Consequently senior housing typically has a lower demand for off-street parking, and tends to generate lower trip volumes, both daily and during peak hours, as reflected in Table 1 above [See Table 1 from Traffic Impact Report].
- The lower parking demand for senior housing, combined with smaller units, tends to yield a slightly higher density than general multi-family apartments.

While multi-family development will result in slightly higher trip generation, than senior housing, it is important to recognize that the office commercial uses allowed under the C-3PD zoning generally have higher trip generation than either form of housing.

**FINDING: SATISFIED.** City concurs with the applicant's clarification that senior condominiums or senior apartments, which are currently listed as allowed uses in Condition 15, are not specifically defined in the Section 17.06.015 of the McMinnville Zoning Ordinance. Section 17.60.060(A)(4) of the McMinnville Zoning Ordinance does regulate off-street parking differently for dwelling units that are "expressly reserved for senior or handicapped persons". However, the same development standards, including setbacks, density, and height, would apply to the development of apartments on the site, whether they were age-restricted or not.

The use that is added to the amended Condition 15 of Ordinance 4709 is "multiple-family dwelling" because that is the specific use that is listed as permitted in the C-3 (General Commercial) zone in Section 17.33.010 of the McMinnville Zoning Ordinance.

## **Chapter 17.21. R-4 Multiple-Family Residential Zone**

**17.21.030 Lot size.** In an R-4 zone, the lot size shall not be less than five thousand square feet, except that the lot area for common wall, single-family lots shall not be less than two thousand five hundred square feet per family. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**17.21.040 Yard requirements.** In an R-4 zone, each lot shall have yards of the following size unless otherwise provided for in Section 17.54.050:

- A. A front yard shall not be less than fifteen feet;
- B. A side yard shall not be less than six feet, except an exterior side yard shall not be less than fifteen feet;

- C. A rear yard shall not be less than twenty feet;
- D. Whether attached to a residence or as a separate building, a covered storage facility for a vehicle on which the main opening is toward a street shall be located not less than twenty feet to the property line bordering the street;
- E. All yards shall be increased, over the requirements of this section, one foot for each two feet of building height over thirty-five feet. (Ord. 4912 §3, 2009; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**17.21.050 Building height.** In an R-4 zone, a building shall not exceed sixty feet in height. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**17.21.060 Density requirements.** In an R-4 zone, the lot area per family shall not be less than fifteen hundred square feet for each unit with two bedrooms or less, and not less than seventeen hundred fifty square feet for each unit with three bedrooms, and an additional five hundred square feet for each additional bedroom in excess of three in any one unit. The above requirements may be waived if the provisions of Section 17.21.020(M) are utilized. (Ord. 4796 §1(b), 2003; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**APPLICANT’S RESPONSE:** The base C-3 zone allows for multi-family apartments as a permitted use, subject to the R-4 zoning standards and density, per Chapter 17.21.

Under Ordinance 4863, senior housing, including apartments and condominiums area an allowed use. However, the City’s code does not specifically define senior housing, differentiated from multi-family apartments, other than assisted living, per Chapter 17.06.015.

No specific site development plan is proposed at this time. Any housing development of the Subject Property will be subject to the R-4 zoning provisions, including density, and will be subject to Site Plan review for compliance with these standards.

**FINDING: SATISFIED.** City concurs with the applicant’s clarification that senior condominiums or senior apartments, which are currently listed as allowed uses in Condition 15, are not specifically defined in the Section 17.06.015 of the McMinnville Zoning Ordinance. Therefore, the same development standards, including setbacks, density, and height, would apply to the development of apartments on the site, whether they were age-restricted or not. Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure that any future development of multiple family residential uses would meet the necessary R-4 development standards.

### **Chapter 17.33. C-3 General Commercial Zone**

**17.33.010 Permitted Uses.** In a C-3 zone, the following uses and their accessory uses are permitted:

- 3. Multiple-family dwelling subject to the provisions of the R-4 zone; [..]

**APPLICANT’S RESPONSE:** The property is zoned C-3 PD. The underlying C-3 zone allows multi-family development, per 17.33.010.3, consistent with the R-4 standards.

There is residential R-4 land immediately to the west of the subject site, so there is a basis for concluding that R-4 residential is compatible with the medical commercial designation for the remainder of the Planned Area.

The requested change will not alter the current density allocated to the property. It will remain as is consistent with the R-4 standards, as set forth below.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

## **Chapter 17.51. Planned Development Overlay**

**17.51.010 Purpose.** The purpose of a planned development is to provide greater flexibility and greater freedom of design in the development of land than may be possible under strict interpretation of the provisions of the zoning ordinance. Further, the purpose of a planned development is to encourage a variety in the development pattern of the community; encourage mixed uses in a planned area; encourage developers to use a creative approach and apply new technology in land development; preserve significant man-made and natural features; facilitate a desirable aesthetic and efficient use of open space; and create public and private common open spaces. A planned development is not intended to be simply a guise to circumvent the intent of the zoning ordinance.

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The proposed amendment to allow multiple family residential use on the subject site would provide for greater flexibility and greater freedom of design in the development of the subject site than is currently allowed by the existing condition of approval. The proposed amendment to add multiple family residential as an allowed use would not remove any ability to develop other uses on the subject site. Currently, and after the proposed amendment, professional office, medical office, senior condominiums, senior apartments, assisted living facilities, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility, would still be allowed on the subject site under the revised condition of approval that was approved by Ordinance No. 4863, along with multiple family residential. This increases the opportunity for mixed uses in the planned development area. The proposed amendment to allow multiple family residential use on the subject site is also not a guise to circumvent the intent of the zoning ordinance, because multiple family residential is a permitted use in the existing, underlying C-3 (General Commercial) zoning district.

**17.51.020 Standards and requirements.** The following standards and requirements shall govern the application of a planned development in a zone in which it is permitted:

- A. The principal use of land in a planned development shall reflect the type of use indicated on the comprehensive plan or zoning map for the area. Accessory uses within the development may include uses permitted in any zone, except uses permitted only in the M-2 zone are excluded from all other zones. Accessory uses shall not occupy more than twenty-five percent of the lot area of the principal use;
- B. Density for residential planned development shall be determined by the underlying zone designations. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The proposed amendment to allow multiple family residential use on the subject site will not be inconsistent with the comprehensive plan or zoning designation of the subject site, because multiple family residential is a permitted use in the existing, underlying C-3 (General Commercial) zoning district. The density of any eventual residential development will be determined by the existing regulations in the underlying C-3 zone.

**17.51.030 Procedure.** The following procedures shall be observed when a planned development proposal is submitted for consideration:

- A. The Commission shall consider the preliminary development plan at a meeting at which time the findings of persons reviewing the proposal shall also be considered. In reviewing the plan, the Commission shall need to determine that:
  - a. There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;
  - b. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;
  - c. The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;
  - d. The plan can be completed within a reasonable period of time;
  - e. The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;
  - f. Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;
  - g. The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole;

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Section 17.51.030 is satisfied in that the Commission will have an opportunity to review a future development plan, per Conditions 2 and 3 of Ordinance 4709, which require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure that any future development of multiple family residential uses would meet the necessary R-4 development standards. Discussion of the criteria listed in subsection C is provided below, as those criteria are the review criteria for a Planned Development Amendment, as found in 17.74.070 of the Zoning Ordinance.

**17.74.070. Planned Development Amendment – Review Criteria.** An amendment to an existing planned development may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Major changes to an adopted site plan shall be processed in accordance with Section 17.72.120, and include the following:

- An increase in the amount of land within the subject site;
- An increase in density including the number of housing units;
- A reduction in the amount of open space; or
- Changes to the vehicular system which results in a significant change to the location of streets, shared driveways, parking areas and access.

**APPLICANT'S RESPONSE:** While none of the listed changes are specifically applicable, the Planning Director has determined that the proposed PD amendment, which changes allowed uses, is considered to be a Major amendment and is therefore subject to Section 17.72.120, as addressed herein.

**FINDING: SATISFIED.** The applicant has provided a Planned Development Amendment request to be reviewed by the Planning Commission in accordance with Section 17.72.120.

**17.74.070. Planned Development Amendment – Review Criteria.** [...] An amendment to an existing planned development may be authorized, provided that the proposal satisfies all relevant requirements of this ordinance, and also provided that the applicant demonstrates the following:

**17.74.070(A).** *There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;*

**APPLICANT'S RESPONSE:** The applicants desire to develop multi-family apartments. While senior housing remains an appropriate use, the applicant submits that the need for housing alternatives, within the Planned Development, is greater than just the narrow segment currently allowed for senior housing.

While there is a general modest trend of an aging population, the median age for Yamhill County, including McMinnville is 37.5 (2015). This is slightly higher than the state in general. Consequently the applicant submits that there is no greater demand for senior housing over general housing or non-age restricted apartments.

McMinnville's building permit activity over the past couple of years reflects this trend with similar permit activity rates for both single and multi-family residential construction,

#### Historic Perspective

Based on a review of the historic records associated with the Three Mile Lane Planned Development it appears that the City's intent was initially to limit development to professional and medical offices, or other compatible commercial uses. General commercial and retail were not allowed with the intent to avoid creating strip commercial development in this area.

The current development within this Planned Development is predominantly medical facilities and professional offices, as originally targeted. However a motel and restaurant have also been developed, considered as compatible uses.

The 2006 amendment (Ord 4683) allowed for various types of senior housing as permitted uses. This amendment was based on findings that this type of housing was compatible with the developed properties and other allowed uses under Ordinance 4709, in particular the compatibility of supportive native of medical services. The decision was also based on consideration that traffic impacts from senior housing would generally be lower than those created by professional or medical offices.

While the prior amendment allowed for senior housing, the decision does not appear to have been made based on any specific findings of need for this particular housing type relative to types of housing. Rather, the decision was based more on consistency with the underlying C-3 zone, and compatibility with the allowed professional and medical office uses.

However, since the amendment, the anticipated senior housing has not come to fruition, at least on the subject site. Consequently the land has remained vacant. While the subject site was not actually developed for the anticipated senior housing, the need for such housing has been met, in part, by other projects developed in the vicinity northeast of the site. These developments include:

1. Fircrest Community, assisted living and retirements apartments; and
2. Parkland Village, assisted living and retirements apartments; and
3. The Virginia Garcia Memorial Health Center has also been developed in close proximity to these senior housing projects. This facility provides various senior medical services.

#### Continued Need for Housing – General Multi-family

The City's 2001 Housing Needs Analysis projected growth from 2000 to 2020 to increase from 25,153 to 38,720. It is noted that the City has not completed an update of the Needs Analysis, but is working on an update. Consequently the 2001 projections of need are still considered valid.

The 2001 projects identified a need for both single family and multi-family units. City staff confirmed that the projected need remains unfulfilled. For example, staff noted that local business, such as the Medical Center, Chemeketa Community College, and others have identified a need for apartments for transitional housing for new employees and students. Generally people moving to McMinnville continue to create a need for more housing. This growth is consistent with the 2001 projects, just delayed due to the economic recessions over the past 18 years.

Based on data from Portland State University's Population Research Center (PRC) the City's population in 2016 was 33,405, below the 2020 projection. The PRC's most current data shows McMinnville's 2017 population at 34,293, but still below the 2020 projection.

The slower than projected growth is primarily attributed to the economic recessions of in the 1990s and the Great Recession 2008-2012. However, in-migration has also affected the County's and City's population, as noted by PSU research.

Since 2012, the end of the "Great Recession," the economy has shown considerable strength. Given the current continuing strong economy, growth is expected to increase over the rates of the previous 10 years, more on track with the projections prior to the recessions. The Center's updated 2035 projection is for McMinnville to grow to a population of 44,122.

*Portland State University's Population Research Center (PRC)*

*"Yamhill County's total population is forecast to grow by a little more than 70,000 persons from 2017 to 2067, which translates into a total countywide population of 177,170 in 2067 (Figure 15). The population is forecast to grow at the highest rate—just below one and a half percent per year—in the near-term (2017-2025). This anticipated population growth in the near-term is based on three core assumptions: (1) Yamhill County's economy will continue to strengthen in the next 10 years; (2) middle-aged persons will continue migrating into the county—bringing their families or having more children; and (3) empty nesters and retirees will continue migrating into the county, thus increasing deaths. The largest component of growth in this initial period is net in-migration. Over 1,300 more births than deaths are forecast for the 2017 to 2025 period. At the same time roughly 13,000 net in-migrants are also forecast, combining with a diminishing natural increase for continued population growth.*

*Yamhill County's two largest UGBs—McMinnville and Newberg—are forecast to experience a combined population growth of nearly 20,000 from 2017 to 2035 and nearly 37,000 from 2035 to 2067 (Figure 16). McMinnville is expected to increase by 9,829 persons from 2017 to 2035 (1.4% AAGR), growing from a total population of 34,293 in 2017 to 44,122 in 2035. Newberg's population is expected to increase at a slightly faster rate (1.9% AAGR), growing from 24,296 persons in 2017 to 34,021 in 2035. McMinnville and Newberg are forecast to grow more slowly during the second part of the forecast period at 1.1 and 1.3 percent, respectively. We expect both sub-areas to capture increasing shares of the county's total population."*

*Portland State University's Population Research Center (PRC) released the preliminary 2016 population estimates for Oregon and its cities and counties on November 15.*



*According to the preliminary July 1 population estimates, Oregon's population increased from 4,013,845 in 2015 to 4,076,350 in 2016, or by 62,505. This increase represents a 1.6 percent change, slightly higher than in the previous year (1.3 percent). The increase in 2016 is over 11,000 higher than added in 2015, and surpasses the peak pre-recession growth of 58,000 in 2006 and it is closer to growth experienced in the early and mid-1990s.*

*Population growth consists of two factors: natural increase (the number of births minus the number of deaths) and net migration (movers-in minus movers-out). From 2015 to 2016 net migration accounted for roughly 83 percent of Oregon's population growth.*

*Generally, net in-migration has either boosted population growth around the state or has prevented population losses. In counties where a natural decrease is occurring (over a third of the counties experience a natural decrease, meaning there are more deaths than births), net immigration has offset overall population decreases. Net in-migration this past year is estimated to have accelerated in most counties statewide from last year.*

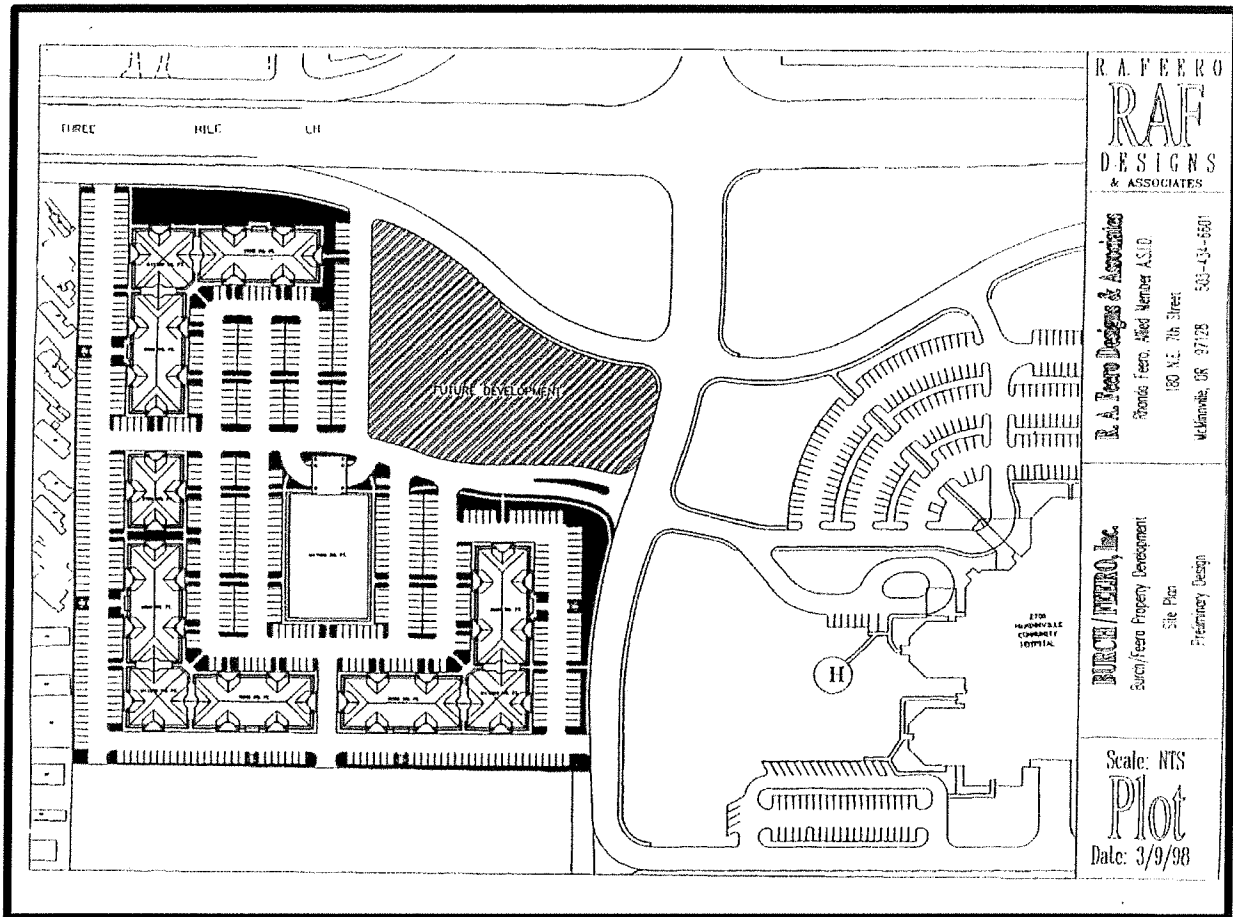
Vacancy rates in 1990 were 2.2% for single family and 3.7% for multi-family. The 2020 projections were based on 2.5% for single family and 5.0% for multi-family. PRC indicates the current vacancy rate is 3.32% for multi-family, which is below the earlier projected rate, thereby indicating a need for more multi-family housing.

For McMinnville currently 40.7% of households are renters. Generally, statewide, there is a trend for renter ship moving towards an even split 50/50 with home ownership. The City's building permit activity for multi-family development has followed a similar pattern as single family over the past couple of years, which is consistent with this trend towards a 50/50 split.

**FINDING: SATISFIED.** City would generally concur with the applicant's findings, but does provide some clarifying points and additional findings below.

The most recently acknowledged Residential Buildable Lands Inventory, which was prepared in 2001, that identified a need for additional land for housing uses. That inventory, which was titled the McMinnville Buildable Land Needs Analysis and Growth Management Plan, identified a deficit of over 1,000 acres of land for housing in Table B-11 of Appendix B. More specifically, the analysis identified a need of 162 acres of R-4 (higher density) zoned land. The City of McMinnville is currently updating its Residential Buildable Lands Inventory and Housing Needs Analysis, and while these reports are not finalized or acknowledged, the analyses are identifying a deficit of land zoned for residential dwelling units. The proposed amendment to allow multiple family residential on the subject site would not increase the amount of land designated specifically for higher density residential use, but it would increase the potential for the development of higher density residential uses on a property with an existing, underlying zone that already permits multiple family residential use (C-3 General Commercial zone).

In terms of compatibility with other uses allowed on the same site and the surrounding area, it is important to note that the subject site is currently regulated by a Planned Development (Ordinance 4709, as amended by Ordinance 4863), but other land immediately adjacent to the subject site is regulated by another Planned Development. The area to the north is subject to a Planned Development (Ordinance No. 4667) that is similar to the subject site, in that there was a limitation of uses on the site to office and medical uses and other compatible uses. The approved development plan for that site is identified below:



Only portions of that approved development plan have been constructed, including the portions of the site closest to SE Norton Lane. The original Planned Development (Ordinance No. 4667) was amended to allow for the development of other compatible uses, including the existing hotel and restaurant located in the area of the originally approved development plan that was identified as "Future Development". The most recent Planned Development amendment (Ordinance No. 4937) actually amended the allowable uses on the site to be any use allowed in the C-3 zone, other than a few storage and auto-oriented uses that were still listed as being prohibited. The commercial Planned Development area to the north, and the buildings that have been constructed, can be seen below:



This commercial development to the north of the subject site will serve as a buffer from Highway 18, which is identified in the McMinnville Transportation System Plan as a major arterial roadway, for any future potential development of a multiple family residential use on the subject site. Also, the proposed amendment to allow multiple family residential use on the subject site will provide an opportunity for more of a variety and mix of uses in the Planned Development areas along SE Norton Lane.

**17.74.070(B).** *Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;*

**APPLICANT'S RESPONSE:** Adding apartments to the list of allowed uses will not be inconsistent with the Comprehensive Plan, including general Goals, Policies and Objectives. This decision will be consistent with the prior amendment, which allowed limited residential uses.

[Note – the applicant also provided findings and responses to Comprehensive Plan goals and policies, which are provided above.]

**FINDING: SATISFIED.** City concurs with the applicant's findings. The proposed Planned Development Amendment would not be inconsistent with the applicable goals and policies of the Comprehensive Plan, as described in more detail above in the specific findings for each Comprehensive Plan goal and policy.

**17.74.070(C).** *The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;*

**APPLICANT'S RESPONSE:** Access to the property is from Salmon River Highway 18 and south on SE Norton Lane. The property fronts on the west side of SE Norton Lane, just south of the intersection with the SE Stratus Avenue Loop. The property abuts the city limits and UGB to the south, but any site development, regardless of use, will extend the street improvements to the city limits, or as determined by the City.

The proposed amendment does not substantially alter the demand for transportation and public service improvement to any greater degree than the currently allowed uses.

The existing right-of-way for SE Norton Lane is 60 feet down to SE Stratus Avenue, which is the extent of current improvements. Site development of the subject site will complete improvements across the site frontage.

The full range of urban services is currently available to the property, thereby ensuring timely and efficient provision of services. Utilities will be extended as required to the adjoining property, which to the south is outside of the city limits.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards.

**17.74.070(D).** *The plan can be completed within a reasonable period of time;*

**APPLICANT'S RESPONSE:** The applicant intends to proceed with development review and site development as quickly as possible, assuming this PD amendment is approved. Full site development would be expected to be completed by 2020-2021.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

**17.74.070(E).** *The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;*

**APPLICANT'S RESPONSE:** The existing streets serving the property are designed and constructed to City standards, with adequate capacity, based on the C-3 zoning. The accompanying Traffic Impact Report, prepared by Lancaster Engineering compares the multi-family traffic impacts to those of the allowed commercial offices and senior housing.

[See Table 1 in Traffic Impact Report]

The proposed multi-family residential use will actually generate significantly fewer total trips and peak hour trips than originally anticipated medical office development. The proposed apartment will, however, generate slightly more trips than senior housing, which is currently allowed.

Therefore the requested amendment is not expected to result in any significant increase in new traffic demands, as compared to the current allowed uses.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

The applicant has also provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use. Therefore, the use allowed by the proposed amendment would not be more detrimental to the surrounding street network or transportation system than other uses currently allowed on the subject site by Ordinance No. 4709 and Ordinance No. 4863.

**17.74.070(F).** *Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;*

**APPLICANT'S RESPONSE:** The full range of urban services is currently available to the property, thereby ensuring timely and efficient provision of services. Utilities will be extended as required to the adjoining properties. However, land to the south is outside of the city limits and UGB.

Utilities requirements for the proposed residential development will be generally consistent with the demands created by the current allowed senior housing. Therefore this amendment will not substantially alter the demand for urban services.

**FINDING: SATISFIED.** Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services.

Specifically related to sanitary sewer service, the Engineering Department provided comments on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site. In addition, McMinnville Water and Light and other utility providers were provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

**17.74.070(G).** *The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.*

**APPLICANT'S RESPONSE:** The noise, air, and water pollutants caused by the multi-family development allowed by this amendment will not result in any substantial change as compared to the current list of allowed uses, in particular the senior housing, which is substantially similar in potential impacts.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

CD

# **TRAFFIC IMPACT ANALYSIS**

**7**





lancaster  
**moble**

# Norton Landing Transportation Impact Study McMinnville, Oregon

Date:  
August 9, 2023

Prepared for:  
Chad Juranek  
Kohl, Inc

Prepared by:  
Daniel Stumpf, PE  
Ken Kim, PE



RENEWS: 6/30/2024



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## Executive Summary

1. The proposed Norton Landing project will include the development of an apartment complex on a single property (tax lot R4427 00701) located in McMinnville, Oregon. The project will include the construction of 138 apartment units with a single access driveway located at the intersection of SE Stratus Avenue at SE Norton Lane.
2. The trip generation calculations show that the proposed project is projected to generate a net increase of 66 morning peak hour trips, 80 evening peak hour trips, and 960 average weekday trips.
3. Based on a review of available crash data, no significant trends or crash patterns were identified at any of the study intersections that are indicative of safety concerns. Accordingly, no crash-related mitigation is necessary or recommended as part of the Norton Landing application.
4. Provided any obstructing on-site foliage and foliage along the north property line is either properly maintained or removed, adequate intersection sight distances can be made available at the site access intersection to allow for safe and efficient operation along SE Norton Lane. No other sight distance related mitigation is necessary at the intersection.
5. Left-turn lane warrants are not projected to be met at the proposed intersection of SE Stratus Avenue at SE Norton Lane (South Intersection, Site Access). Accordingly, no new left-turn lanes are necessary or recommended at any of the study intersections as part of the proposed Norton Landing project.
6. Traffic signal warrants are not projected to be met at any of the applicable unsignalized study intersections under any of the analysis scenarios. Accordingly, no new traffic signals are necessary or recommended as part of the proposed Norton Landing application.
7. Based on the results of the operational analysis, all study intersections are currently operating acceptably per jurisdictional standards and are projected to continue operating acceptably through the 2025 site buildout year. Accordingly, no operational mitigation is necessary or recommended at the study intersections.



# Project Description

## Introduction

The proposed Norton Landing project will include the development of an apartment complex on a single property (tax lot R4427 00701) located in McMinnville, Oregon. The project will include the construction of 138 apartment units with a single access driveway located at the intersection of SE Stratus Avenue at SE Norton Lane.

Based on correspondence with the City of McMinnville and Oregon Department of Transportation (ODOT) staff, the report conducts safety and capacity/level of service analyses at the following intersections during the morning and evening peak hours:

1. OR-18 at SE Norton Lane
2. SE Stratus Avenue at SE Norton Lane (North Intersection)
3. SE Stratus Avenue at SE Norton Lane (South Intersection, Site Access)

The purpose of this study is to determine whether the transportation system within the vicinity of the site is capable of safely and efficiently supporting the existing and proposed uses, and to determine any mitigation that may be necessary to do so. Detailed information on traffic counts, trip generation calculations, safety analyses, and level of service calculations is included in the appendix to this report.

## Location Description

The project site is located south of SE Stratus Avenue, east of Evergreen Parkway E, and west of SE Norton Lane in McMinnville, Oregon. The subject site is located in a commercial area (zoned C-3) of the City, with the Willamette Valley Medical Center to the east, a medical care center to the north, residential single-family houses to the west, and agriculture land use to the south. The site includes a single property (tax lots R4427 00701) which encompasses an approximate total of ±4.93 acres. The property is currently undeveloped/utilized for agricultural purposes.

Figure 1 presents an aerial image of the nearby vicinity with the project site outlined in yellow.



Figure 1: Aerial Photo of Site Vicinity (Image from Google Earth)

**Vicinity Streets**

The study intersections are composed of three roadways. Table 1 provides a description of these vicinity roadways.

Table 1: Vicinity Roadway Descriptions

| Street Name       | Jurisdiction     | Functional Classification        | Speed (MPH) | On-Street Parking   | Curbs & Sidewalks  | Bicycle Lanes |
|-------------------|------------------|----------------------------------|-------------|---------------------|--------------------|---------------|
| OR-18             | ODOT             | Major Arterial/Statewide Highway | 45/55       | Not Permitted       | Partial North Side | None          |
| SE Stratus Avenue | McMinnville/ODOT | Minor Collector                  | 35          | Not Permitted       | Partial South Side | None          |
| SE Norton Lane    | McMinnville/ODOT | Minor Collector                  | 25          | Partially Permitted | Both Sides         | None          |

Table Notes: Functional classification based on City of McMinnville TSP and ODOT's Online TransGIS Map.

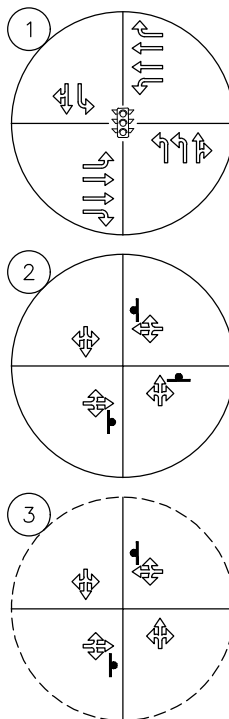
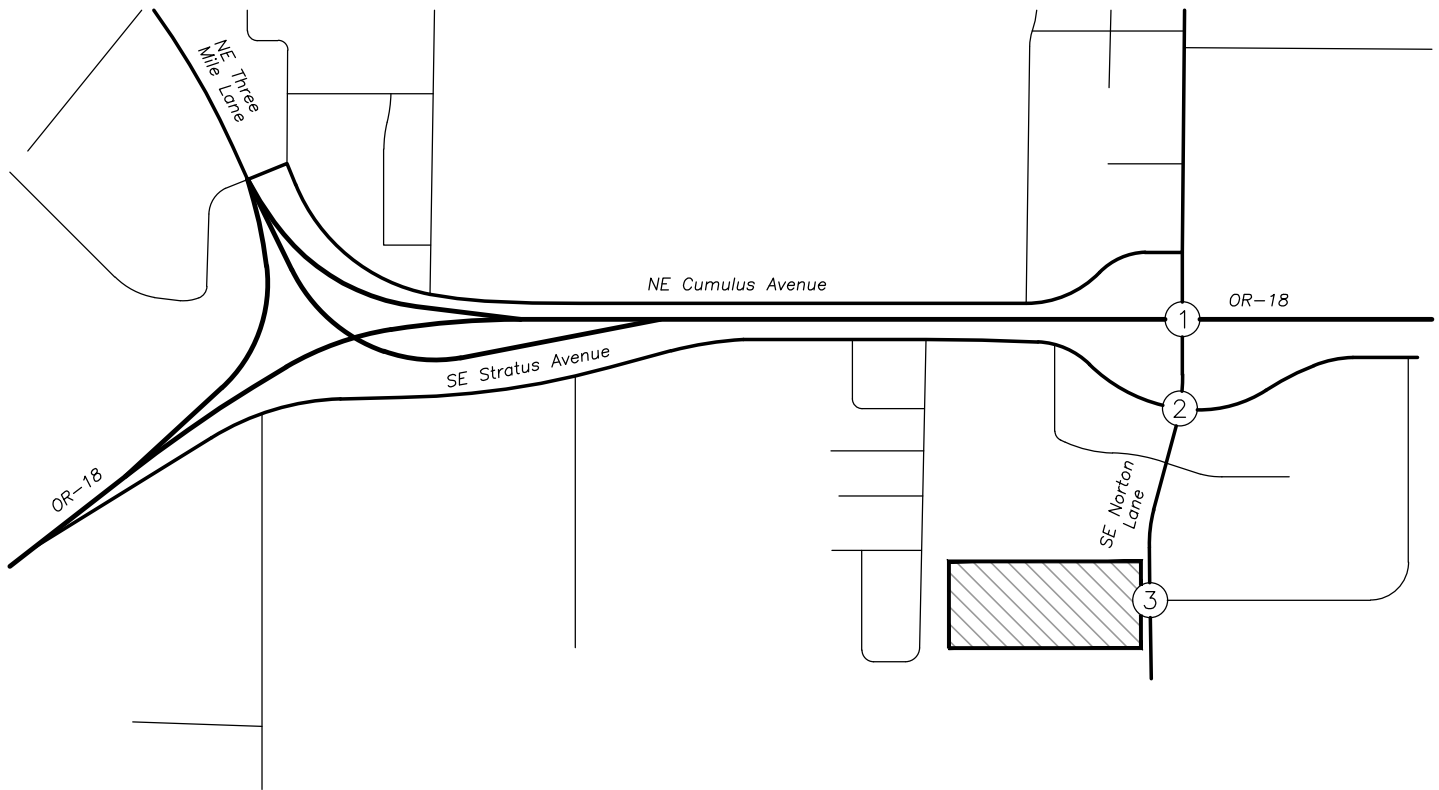
### Study Intersections

Based on coordination with agency staff, three intersections were identified for analysis. A summarized description of these study intersections is provided in Table 2.

**Table 2: Study Intersection Descriptions**

| Number | Intersection                        | Geometry                            | Traffic Control | Phasing/Stopped Approaches          |
|--------|-------------------------------------|-------------------------------------|-----------------|-------------------------------------|
| 1      | OR-18 at SE Norton Lane             | Four-Legged                         | Signalized      | Protected EB/WB/NB/SB Left-turns    |
| 2      | SE Stratus Avenue at SE Norton Lane | Four-Legged                         | Stop-Controlled | Stop-Controlled EB/WB/NB Approaches |
| 3      | SE Stratus Avenue at SE Norton Lane | Three-Legged (Proposed Four-Legged) | Stop-Controlled | Stop-Controlled (EB)/WB Approaches  |

A vicinity map showing the project site, vicinity streets, and study intersection configurations is shown in Figure 2.



**LEGEND**

- STUDY INTERSECTION (EXISTING)
- STUDY INTERSECTION (PROPOSED)
- STOP SIGN
- TRAFFIC SIGNAL
- PROJECT SITE
- ARTERIAL ROADWAY
- COLLECTOR ROADWAY
- LOCAL ROADWAY



no scale



## Site Trips

### Trip Generation

The proposed development will include the construction of 138 residential apartment units. To estimate the number of trips that will be generated by the proposed use, trip equations from the *Trip Generation Manual*<sup>1</sup> were used. Data from land use code 220, *Multifamily Housing (Low-Rise)*, was used to estimate site trip generation based on the number of dwelling units.

The trip generation calculations show that the proposed project is projected to generate an additional 66 morning peak hour trips, 80 evening peak hour trips, and 960 average weekday trips. The trip generation estimates are summarized in Table 3. Detailed trip generation calculations are included in the appendix.

**Table 3: Trip Generation Summary**

| ITE Code                       |     | Size/Rate | AM Peak Hour |      |       | PM Peak Hour |      |       | Weekday Total |
|--------------------------------|-----|-----------|--------------|------|-------|--------------|------|-------|---------------|
|                                |     |           | Enter        | Exit | Total | Enter        | Exit | Total |               |
| Multifamily Housing (Low-Rise) | 220 | 138 units | 16           | 50   | 66    | 50           | 30   | 80    | 960           |

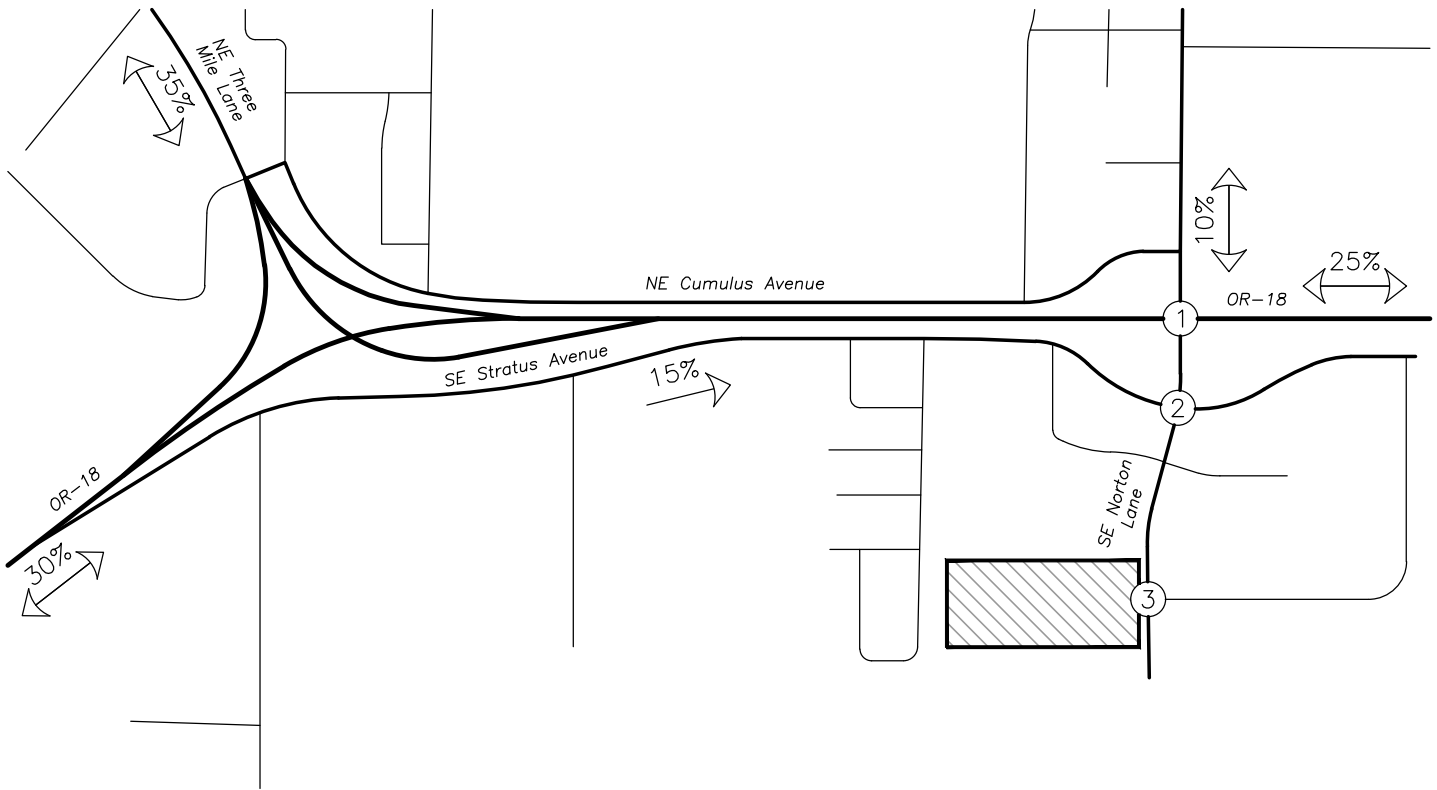
### Trip Distribution

The directional distribution of site trips was estimated based on the locations of likely trip destinations, locations of major transportation facilities in the site vicinity, and existing travel patterns at the study intersections. The following trip distribution was used for analysis:

- Approximately 35% of site trips will travel to/from the northwest along NE Three Mile Lane, north of OR-18.
- Approximately 30% of site trips will travel to/from the southwest along OR-18, west of NE Three Mile Lane.
  - Approximately half of site ingress trips will travel via SE Stratus Avenue.
  - Approximately half of site ingress trips will travel via OR-18.
  - All site egress trips will travel via OR-18.
- Approximately 25% of site trips will travel to/from the east along OR-18, east of SE Norton Lane.
- Approximately 10% of site trips will travel to/from the north along NE Norton Lane, north of OR-18.

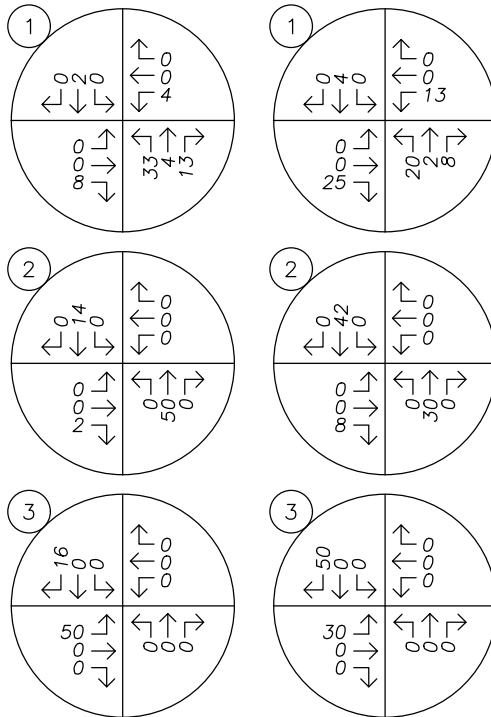
Figure 3 shows the trip distribution and assignment of the proposed development for the morning and evening peak hours.

<sup>1</sup> Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 11<sup>th</sup> Edition, 2021.



AM PEAK HOUR

PM PEAK HOUR



LEGEND

XX% PERCENT OF PROJECT TRIPS

| TRIP GENERATION |    |     |       |
|-----------------|----|-----|-------|
|                 | IN | OUT | TOTAL |
| AM              | 16 | 50  | 66    |
| PM              | 50 | 30  | 80    |



no scale

## Traffic Volumes

### Existing Conditions

Traffic counts were conducted at all the study intersections on Tuesday, August 1, 2023, from 6:00/7:00 AM to 9:00 AM and from 3:00/4:00 PM to 6:00 PM. Data was used from each intersection's respective morning and evening peak hours.

In accordance with the *Analysis Procedures Manual*<sup>2</sup> (APM), a seasonal adjustment factor was applied to the traffic volumes along OR-18 to estimate the 30<sup>th</sup> highest hour volumes on the ODOT facility. Utilizing the average of the Summer and Commuter trend data from ODOT's Seasonal Trend Table, a seasonal adjustment factor of 1.0227 was calculated. The seasonal adjustment factor was applied to the existing year morning and evening peak hour volumes along the OR-18 through movements.

Figure 4 shows the existing traffic volumes at the study intersections during the morning and evening peak hours.

### Background Conditions

To provide an analysis of the impact of the proposed development on the nearby transportation facilities, an estimate of future traffic volumes is required. It is expected that the proposed development will be constructed and in operation by year 2025. In order to approximate the future year 2025 traffic volumes at the study intersections, the following growth rates were applied to the study intersections:

- A linear growth rate of 1.40 percent per year, as calculated per ODOT's 2041 Future Volumes Table, was applied to the adjusted year 2023 east/west through volumes at the intersection of OR-18 at SE Norton Lane over a two-year period.
- A conservative compounded growth rate of two percent per year over a two-year period was applied to all other turning movements and study intersections.

In addition to the general growth rate, traffic from the proposed Stratus Village multifamily housing development located to the north of the project site was included in the traffic forecasts.

Figure 5 presents the year 2025 background volumes at the study intersections for the morning and evening peak hours. A figure depicting in-process trips is included in the appendix to this report.

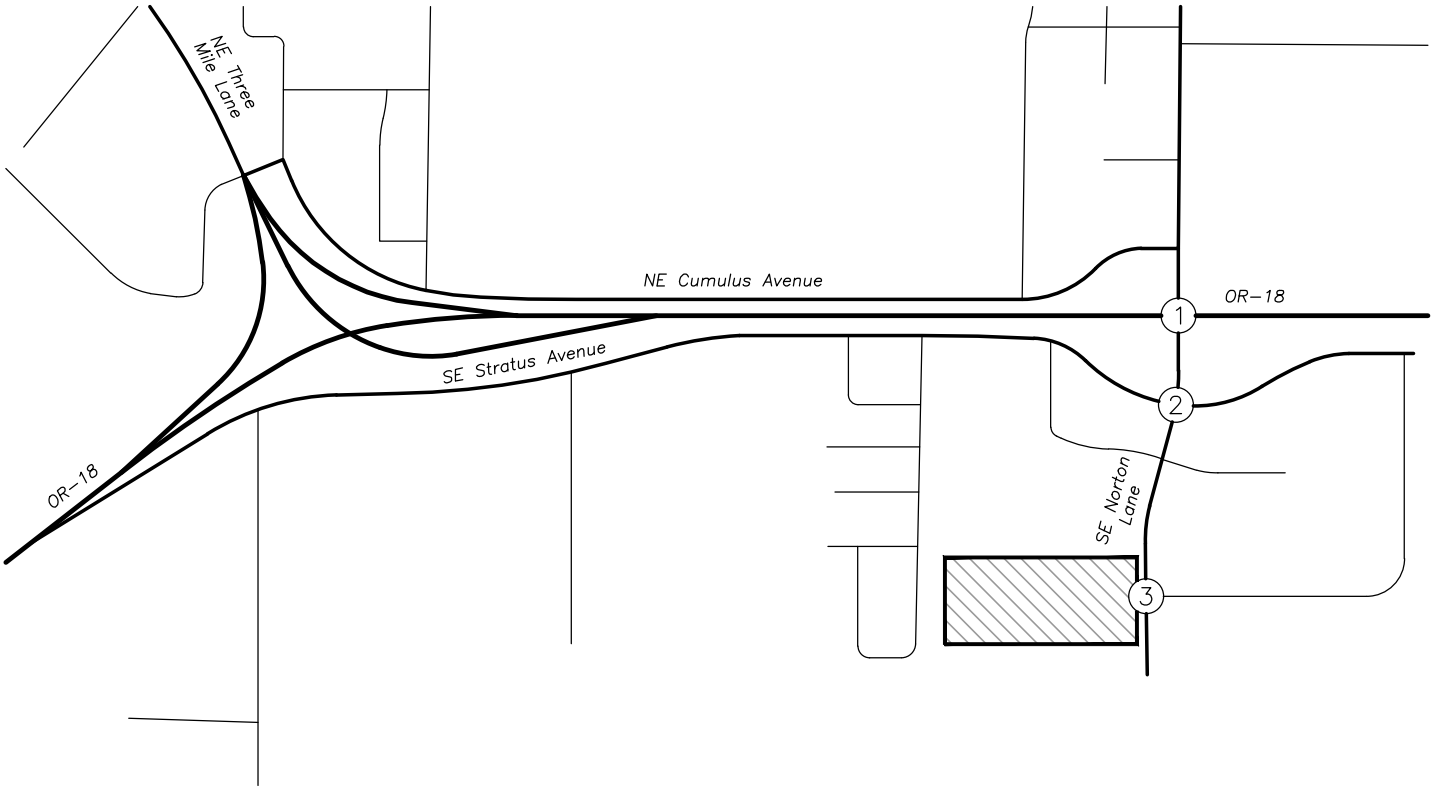
### Buildout Conditions

The peak hour trips calculated to be generated by the proposed development, as described earlier in the *Site Trips* section, were added to the projected year 2025 background traffic volumes to obtain the expected 2025 site buildout volumes.

Figure 6 shows year 2025 buildout traffic volumes at the study intersections during the morning and evening peak hours.

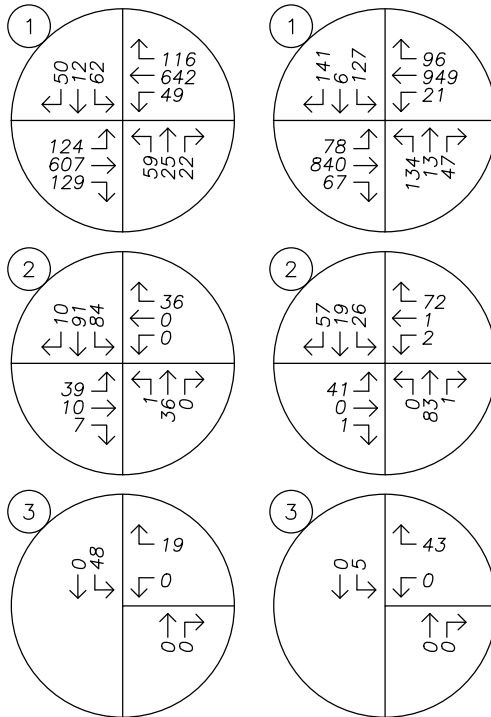
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<sup>2</sup> Oregon Department of Transportation, *Analysis Procedures Manual Version 2*. December 2019.

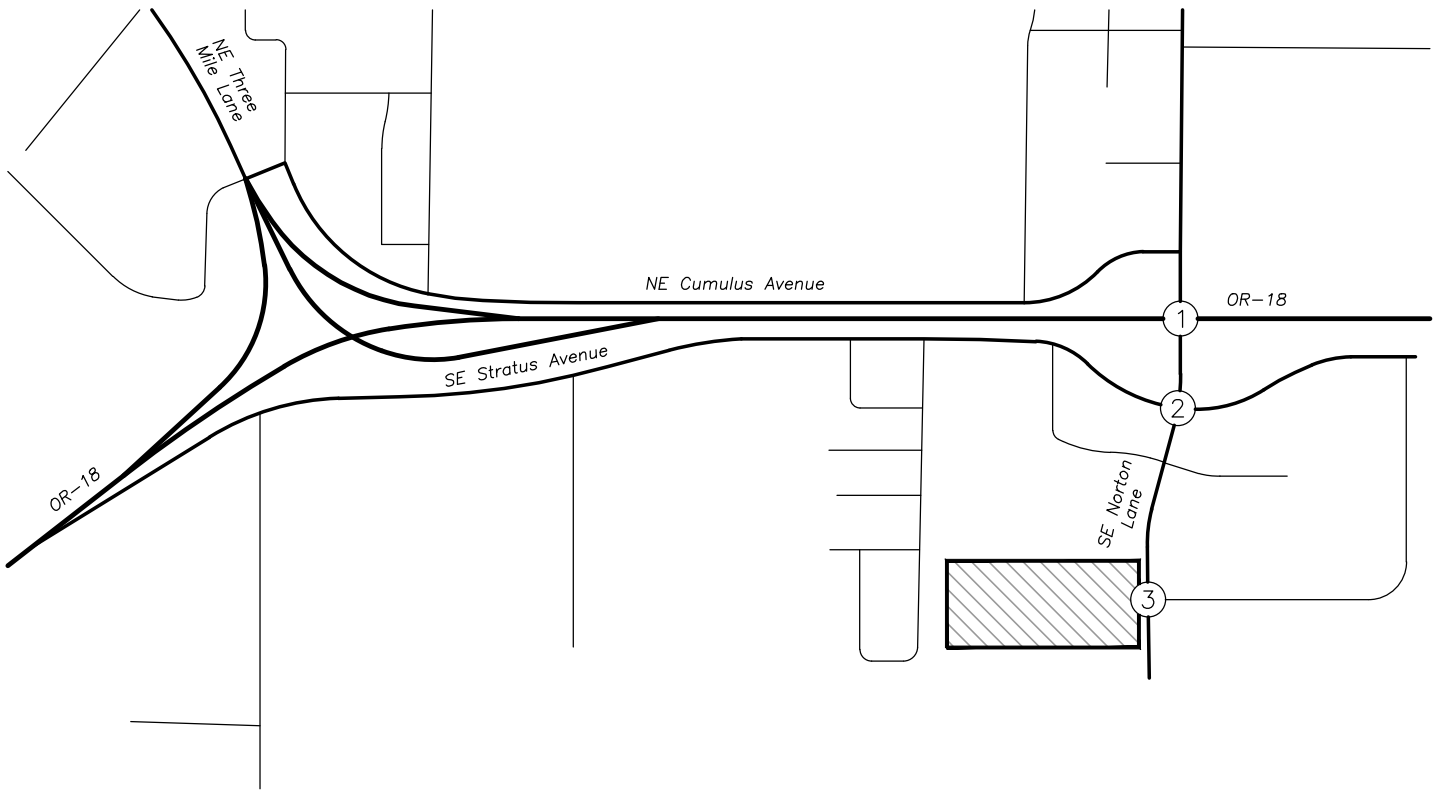


AM PEAK HOUR

PM PEAK HOUR

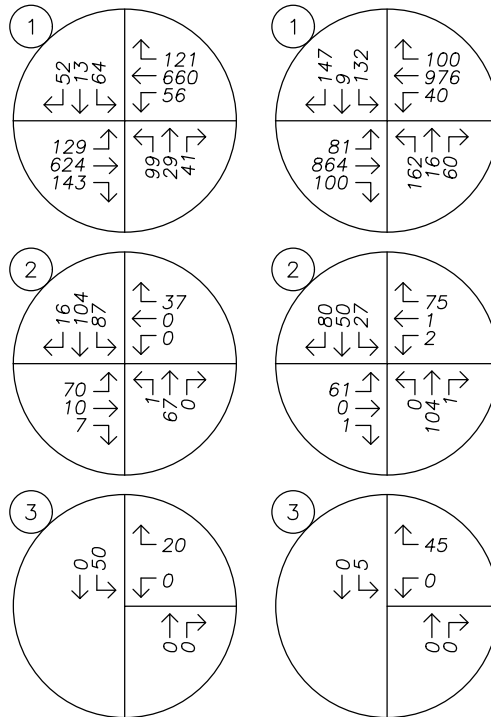


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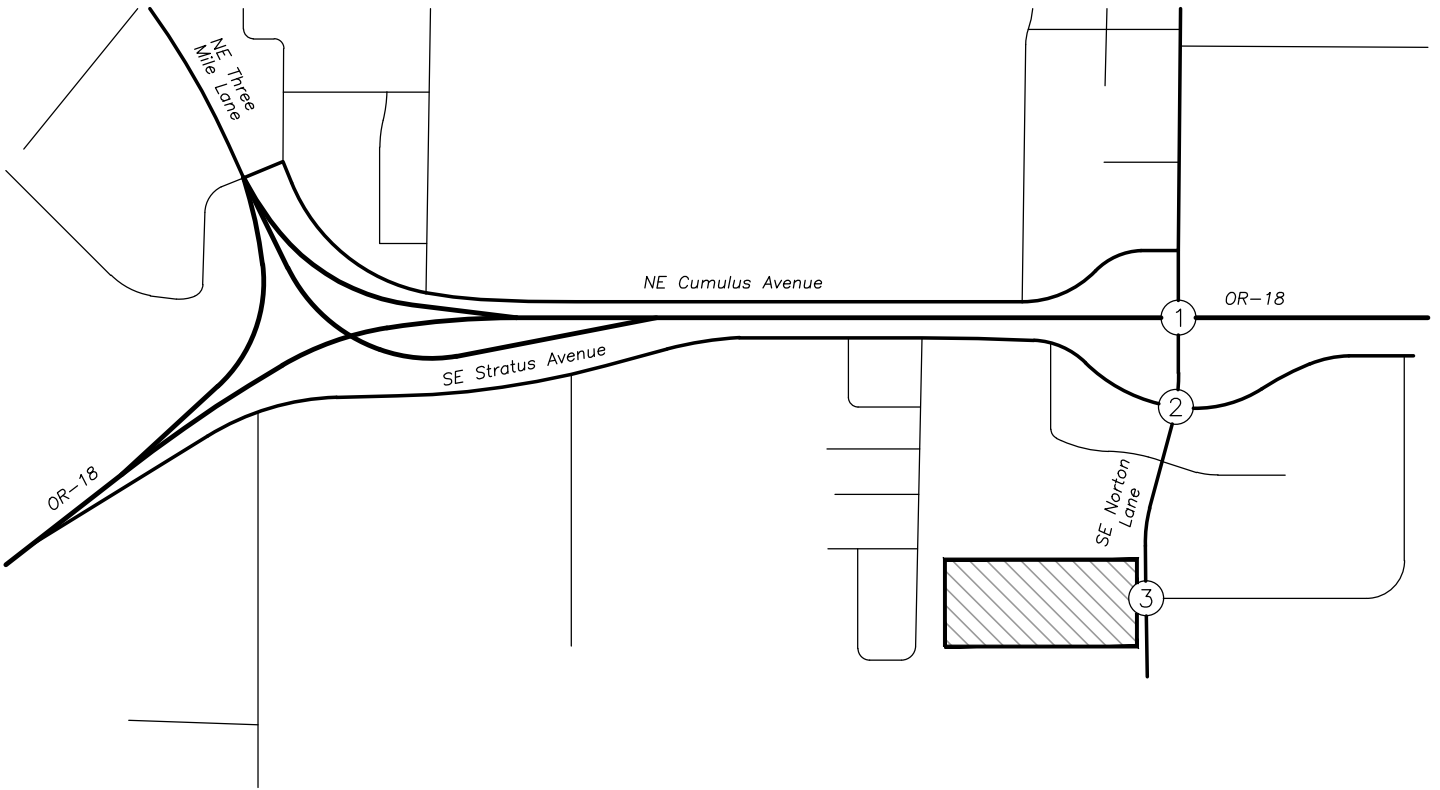


AM PEAK HOUR

PM PEAK HOUR

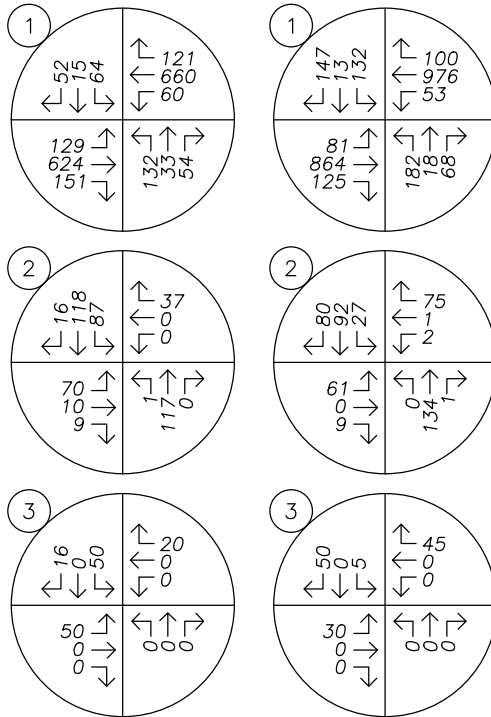


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AM PEAK HOUR

PM PEAK HOUR



no scale

# Safety Analysis

## Crash History Review

Using data obtained from ODOT's Crash Analysis & Reporting Unit and ODOT's online TransGIS website, a review was performed of the most recent five years of available crash data at the study intersections (January 2017 through December 2021). The crash data was evaluated based on the number of crashes, the type of collisions, the severity of the collisions, and the resulting crash rate for each intersection.

Crash rates provide the ability to compare safety risks at different intersections by accounting for both the number of crashes that have occurred during the study period and the number of vehicles that typically travel through the intersection. Crash rates were calculated under the common assumption that traffic counted during the evening peak hour represents approximately ten percent of annual average daily traffic (AADT) at each intersection. Crash rates in excess of 1.00 crashes per million entering vehicles (CMEV) may be indicative of design deficiencies and therefore require a need for further investigation and possible mitigation.

Excluding the site access intersection, all other study intersections are ODOT facilities which adhere to the crash analysis methodologies in ODOT's APM. According to *Exhibit 4-1: Intersection Crash Rates per MEV by Land Type and Traffic Control* of the APM, intersections which experience crash rates in excess of their respective 90<sup>th</sup> percentile crash rates should be "flagged for further analysis". For intersections in urban settings, the following average and 90<sup>th</sup> percentile rates are applicable to the study intersection:

- Signalized, Four-Legged Intersections:
  - Average rate of 0.477 CMEV.
  - 90<sup>th</sup> percentile rate of 0.860 CMEV.
- Stop-Controlled, Four-Legged Intersections:
  - Average rate of 0.198 CMEV.
  - 90<sup>th</sup> percentile rate of 0.408 CMEV.

With regard to crash severity, ODOT classifies crashes in the following categories:

- Property Damage Only (*PDO*);
- Possible Injury – Complaint of Pain (*Injury C*);
- Non-Incapacitating Injury (*Injury B*);
- Incapacitating Injury – Bleeding, Broken Bones (*Injury A*); and
- Fatality or Fatal Injury.

Table 4 provides a summary of crash types while Table 5 summarizes crash severities and rates for each of the study intersections. Detailed crash data reports are provided in the appendix.

Table 4: Crash Type Summary

| Number | Intersection   | Crash Type |                |                 |               |              |       | Total |
|--------|--|------------|----------------|-----------------|---------------|--------------|-------|-------|
|        |  | Rear End   | Turn/<br>Angle | Fixed<br>Object | Side<br>swipe | Ped/<br>Bike | Other |       |
| 1      | OR-18 at SE Norton Lane                                  | 17         | 5              | 0               | 3             | 0            | 0     | 25    |
| 2      | SE Stratus Avenue at SE Norton Lane (North Intersection) | 0          | 1              | 0               | 0             | 0            | 0     | 1     |
| 3      | SE Stratus Avenue at SE Norton Lane (South Intersection) | 0          | 0              | 0               | 0             | 0            | 0     | 0     |

Table 5: Crash Severity and Rate Summary

| Number | Intersection   | Crash Severity |    |   |   |       |         | Total Crashes | AADT   | Crash Rate |
|--------|--|----------------|----|---|---|-------|---------|---------------|--------|------------|
|        |  | PDO            | C  | B | A | Fatal | Unknown |               |        |            |
| 1      | OR-18 at SE Norton Lane                                  | 9              | 10 | 4 | 2 | 0     | 0       | 25            | 24,790 | 0.553      |
| 2      | SE Stratus Avenue at SE Norton Lane (North Intersection) | 1              | 0  | 0 | 0 | 0     | 0       | 1             | 3,030  | 0.181      |
| 3      | SE Stratus Avenue at SE Norton Lane (South Intersection) | 0              | 0  | 0 | 0 | 0     | 0       | 0             | 480    | 0.000      |

Table Notes: **BOLDED** text indicates a crash rate in excess of 1.00 CMEV or ODOT's 90th percentile rate.

There were two crashes at the intersection of OR-18 at SE Norton Lane that were classified as *Injury A*. Details of these crashes are as follows:

- The one of the crashes occurred when the driver of an eastbound through vehicle followed too closely and rear-ended another eastbound vehicle stopped at the intersection. A third vehicle was struck after the initial collision. The passenger of the of the rear-ended vehicle sustained injuries classified as *Injury A* while the driver of the vehicle at fault for causing the crash sustained injuries classified as *Injury B*. Two other people involved in the crash were uninjured.
- The second crash occurred when the driver of a westbound through vehicle disregarded the traffic signal and collided with an eastbound left turning vehicle. The driver of the westbound through vehicle sustained injuries consistent with *Injury C* classification while the driver of the eastbound left turning vehicle sustained injuries consistent with *Injury A* classification.





Based on a review of available crash data, no significant trends or crash patterns were identified at any of the study intersections that are indicative of safety concerns. In addition, none of the study intersections exhibit crash rates near or above the 1.00 CMEV threshold nor do any of the study intersections have a crash rate exceeding ODOT's 90<sup>th</sup> percentile rates. Accordingly, no crash-related mitigation is necessary or recommended as part of the Norton Landing application.

## Sight Distance Evaluation

Intersection sight distances were measured at the proposed site access intersection along SE Norton Lane, located opposite of SE Stratus Avenue. Sight distances along SE Norton Lane were measured and evaluated in accordance with standards established in *A Policy on Geometric Design of Highways and Streets*<sup>3</sup>.

According to AASHTO, the driver's eye is assumed to be approximately 14.5 feet from the near edge of the nearest travel lane, or traveled way, of the intersecting street and at a height of 3.5 feet above the minor-street approach pavement. The vehicle driver's eye height along the major-street approach is assumed to be 3.5 feet above the cross-street pavement. Based on a posted/statutory speed of 25 mph along the applicable segment of SE Norton Lane, the minimum recommended intersection sight distances include the following:

- 280 feet to the south for site egress left-turn vehicles.
- 240 feet to the north for site egress right-turn/through vehicles.

At the site access intersection, sight distance to the south was measured back to the end of the paved edge/gated portion of SE Norton Lane, approximately 50 feet away. Assuming in the future the paved portion of SE Norton Lane is extended further to the south, sight distances in excess of 300 feet can be obtained.

Due to a combination of overgrown foliage both on-site and along the north property line (i.e. overgrown grass and trees/bushes), sight distances were limited to approximately 135 feet. Provided all sight obstructing foliage is either properly maintained or removed from within the sight distance triangle to the north, sight distances of up to 240 feet to the north can be obtained.

Based on the above measurements and provided any obstructing on-site foliage and foliage along the north property line is either properly maintained or removed, adequate intersection sight distances can be made available at the site access intersection to allow for safe and efficient operation along SE Norton Lane. No other sight distance related mitigation is necessary at the intersection.

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<sup>3</sup> American Association of State Highway and Transportation Officials (AASHTO), *A Policy on Geometric Design of Highways and Streets*, 7<sup>th</sup> Edition, 2018.

## Warrant Analysis

Left-turn lane and preliminary traffic signal warrants were examined for the study intersections where such treatments would be applicable.

### **Left-Turn Lane Warrants**

A left-turn refuge lane is primarily a safety consideration for the major-street, removing left-turning vehicles from the through traffic stream. The left-turn lane warrants used were developed from the *National Cooperative Highway Research Project's (NCHRP) Report 457*. Turn lane warrants were evaluated based on the number of advancing and opposing vehicles as well as the number of turning vehicles, the travel speed, and the number of through lanes.

Left-turn lane warrants are not projected to be met at the proposed intersection of SE Stratus Avenue at SE Norton Lane (South Intersection, Site Access). Accordingly, no new left-turn lanes are necessary or recommended at any of the study intersections as part of the proposed Norton Landing project.

### **Preliminary Traffic Signal Warrants**

Preliminary traffic signal warrants were examined for the unsignalized study intersections to determine whether the installation of a new traffic signal will be warranted at the intersections by the 2025 site buildout year. Based on the preliminary analysis following a review of Warrant 1 in the MUTCD and considering methodologies presented in the APM for the ODOT study intersections, traffic signal warrants are not projected to be met at any of the applicable unsignalized study intersections under any of the analysis scenarios. Accordingly, no new traffic signals are necessary or recommended as part of the proposed Norton Landing application.

Detailed warrant analyses for each applicable study intersection are included in the technical appendix to this report.

# Operational Analysis

## Intersection Capacity Analysis

A capacity and delay analysis were conducted for each of the study intersections per the signalized and unsignalized intersection analysis methodologies in the *Highway Capacity Manual (HCM)*<sup>4</sup>, as well as the signalized intersection analysis methodology detailed in ODOT's APM Section 13. Intersections are generally evaluated based on the average control delay experienced by vehicles and are assigned a grade according to their operation. The level of service (LOS) of an intersection can range from LOS A, which indicates very little or no delay experienced by vehicles, to LOS F, which indicates a high degree of congestion and delay. The volume-to-capacity (v/c) ratio is a measure that compares the traffic volumes (demand) against the available capacity of an intersection.

### Performance Standards

#### *City of McMinnville*

According to the McMinnville Transportation System Plan (TSP), the Mobility Standard for all local (City) intersections and streets shall be a volume/capacity (v/c) ratio of 0.90.

#### *ODOT*

The study intersections of OR-18 at SE Norton Lane and SE Stratus Avenue at SE Norton Lane (North Intersection) operate under the jurisdiction of ODOT. The applicable minimum operation standards for these facilities are established under the *Oregon Highway Plan*<sup>5</sup> (OHP) and are based on the v/c ratio of the intersection. According to these documents, the following operation standards are applicable to the study intersections:

- The intersection of OR-18 at SE Norton Lane is to operate with a maximum allowable v/c ratio of 0.80.
- The intersection of SE Stratus Avenue at SE Norton Lane (North Intersection) is to operate with a maximum allowable v/c ratio of 0.95.

### Delay & Capacity Analysis

The TrafficWare Synchro software utilized for analysis does not report the overall v/c ratio of signalized intersections in the HCM 6<sup>th</sup> Edition capacity reports. Therefore, the signalized intersection v/c ratio was calculated utilizing methods detailed in ODOT's APM Sections 13.

For the intersection of SE Stratus Avenue at SE Norton Lane (North Intersection), due to the atypical traffic controls at the intersection (i.e. free-flow southbound approach and stop-controlled eastbound, westbound, and northbound approaches) the HCM 6<sup>th</sup> Edition does not provide intersection capacity outputs. In order to generate capacity outputs, the following was conducted:

- The southbound approach was modeled to operate as yield-control in lieu of free-flow.
- HCM 2000 results were used and the highest approach v/c ratio was reported.

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<sup>4</sup> Transportation Research Board, *Highway Capacity Manual 6<sup>th</sup> Edition*, 2016.

<sup>5</sup> Oregon Department of Transportation, *1999 Oregon Highway Plan*: Including amendments November 1999 through May 2015, 1999

During one of the applications neighborhood meetings, some of the community raised concerns regarding safety at the intersection of SE Stratus Avenue at SE Norton Lane (North Intersection) due to its atypical traffic controls. During the meeting it was suggested intersection controls could be revised to two-way stop-control, where the eastbound and westbound approaches would be stopped and the northbound and southbound approaches would operate under free-flow. To demonstrate such traffic controls would operate acceptably per agency standards, the intersection was modeled with this traffic control configuration under 2025 buildout conditions for informational purposes only.

The LOS, delay, and v/c results of the capacity analysis are shown in Table 6 for the morning and evening peak hours. Detailed calculations as well as tables showing the relationship between delay and LOS are included in the appendix to this report.

**Table 6: Capacity Analysis Summary**

|   |                                 | AM Peak Hour |           |      | PM Peak Hour |           |      |
|---|---------------------------------|--------------|-----------|------|--------------|-----------|------|
|   |                                 | LOS          | Delay (s) | v/c  | LOS          | Delay (s) | v/c  |
| <b>1. OR-18 at SE Norton Lane</b>   |                                 |              |           |      |              |           |      |
|   | 2023 Existing Conditions        | B            | 13        | 0.48 | B            | 16        | 0.62 |
|   | 2025 Background Conditions      | B            | 14        | 0.51 | B            | 17        | 0.64 |
|   | 2025 Buildout Conditions        | B            | 15        | 0.52 | B            | 17        | 0.65 |
| <b>2. SE Stratus Avenue at SE Norton Lane (North Intersection)</b>              |                                 |              |           |      |              |           |      |
|   | 2023 Existing Conditions        | A            | 9         | 0.28 | A            | 8         | 0.13 |
|   | 2025 Background Conditions      | A            | 9         | 0.32 | A            | 8         | 0.21 |
|   | 2025 Buildout Conditions        | A            | 10        | 0.35 | A            | 9         | 0.27 |
|   | 2025 Buildout Conditions (TWSC) | C            | 17        | 0.28 | B            | 13        | 0.15 |
| <b>3. SE Stratus Avenue at SE Norton Lane (South Intersection, Site Access)</b> |                                 |              |           |      |              |           |      |
|   | 2023 Existing Conditions        | A            | 9         | 0.04 | A            | 9         | 0.05 |
|   | 2025 Background Conditions      | A            | 9         | 0.04 | A            | 9         | 0.06 |
|   | 2025 Buildout Conditions        | B            | 10        | 0.10 | A            | 9         | 0.06 |

Table Notes: **BOLDED** text indicates intersection operation above jurisdictional standards.

Based on the results of the operational analysis, all study intersections are currently operating acceptably per jurisdictional standards and are projected to continue operating acceptably through the 2025 site buildout year. Accordingly, no operational mitigation is necessary or recommended at the study intersections.



## Conclusions

Based on a review of available crash data, no significant trends or crash patterns were identified at any of the study intersections that are indicative of safety concerns. Accordingly, no crash-related mitigation is necessary or recommended as part of the Norton Landing application.

Provided any obstructing on-site foliage and foliage along the north property line is either properly maintained or removed, adequate intersection sight distances can be made available at the site access intersection to allow for safe and efficient operation along SE Norton Lane. No other sight distance related mitigation is necessary at the intersection.

Left-turn lane warrants are not projected to be met at the proposed intersection of SE Stratus Avenue at SE Norton Lane (South Intersection, Site Access). Accordingly, no new left-turn lanes are necessary or recommended at any of the study intersections as part of the proposed Norton Landing project.

Traffic signal warrants are not projected to be met at any of the applicable unsignalized study intersections under any of the analysis scenarios. Accordingly, no new traffic signals are necessary or recommended as part of the proposed Norton Landing application.

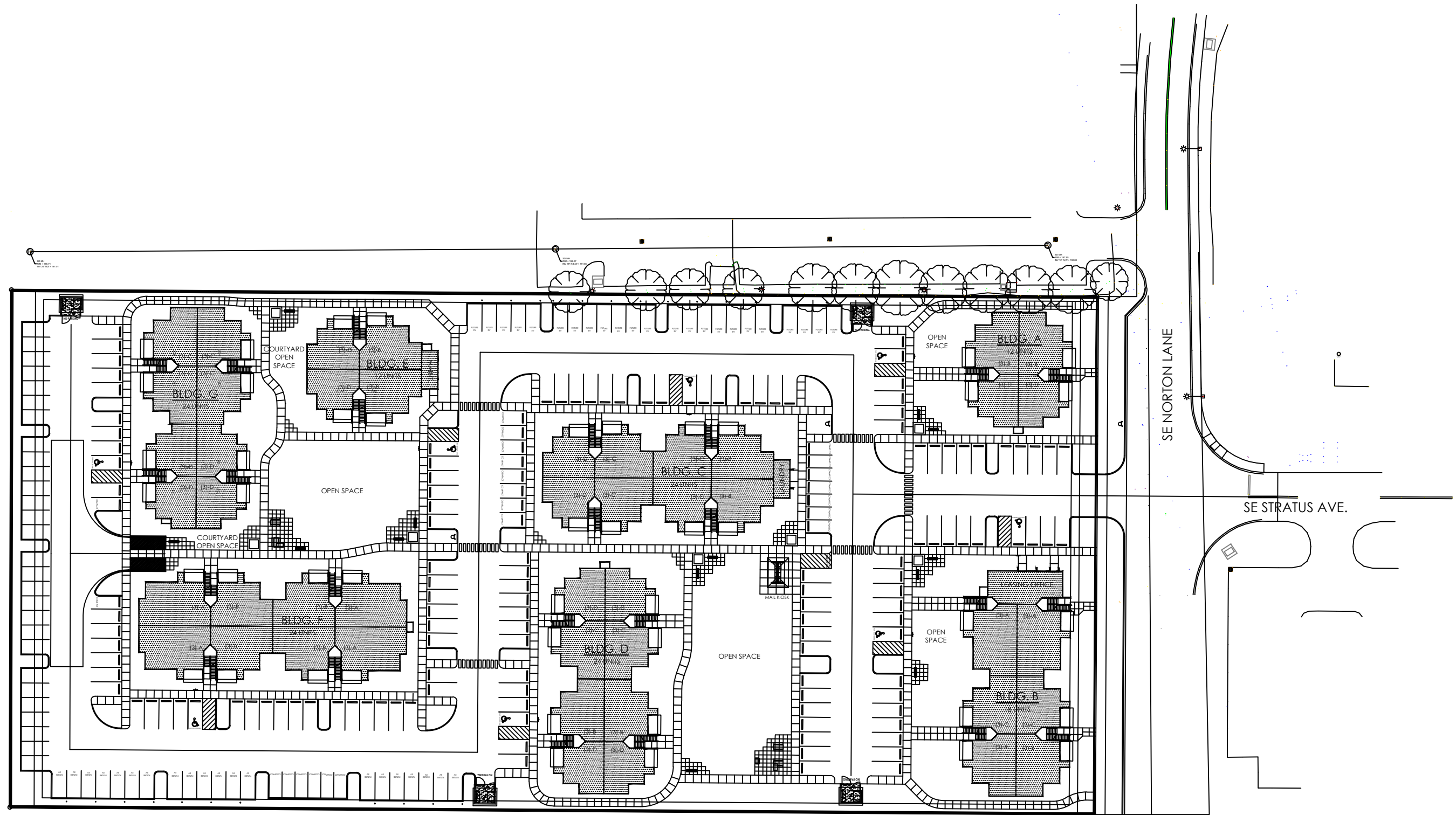
Based on the results of the operational analysis, all study intersections are currently operating acceptably per jurisdictional standards and are projected to continue operating acceptably through the 2025 site buildout year. Accordingly, no operational mitigation is necessary or recommended at the study intersections.



# Appendix A

## Site Plan





# Appendix B

## Trip Generation Calculations







TRIP GENERATION CALCULATIONS  
 Source: Trip Generation Manual, 11th Edition

Land Use: Multifamily Housing (Low-Rise)  
 Land Use Code: 220  
 Land Use Subcategory: Not Close to Rail Transit  
 Setting/Location: General Urban/Suburban  
 Variable: Dwelling Units  
 Trip Type: Vehicle  
 Formula Type: Equation  
 Variable Quantity: **138**

**AM PEAK HOUR**

*Trip Rate:* =0.31\*(\$X5)+22.85

|                   | Enter     | Exit      | Total     |
|-------------------|-----------|-----------|-----------|
| Directional Split | 24%       | 76%       |           |
| Trip Ends         | <b>16</b> | <b>50</b> | <b>66</b> |

**PM PEAK HOUR**

*Trip Rate:* =0.43\*(\$X5)+20.55

|                   | Enter     | Exit      | Total     |
|-------------------|-----------|-----------|-----------|
| Directional Split | 63%       | 37%       |           |
| Trip Ends         | <b>50</b> | <b>30</b> | <b>80</b> |

**WEEKDAY**

*Trip Rate:* =6.41\*(\$X5)+75.31

|                   | Enter      | Exit       | Total      |
|-------------------|------------|------------|------------|
| Directional Split | 50%        | 50%        |            |
| Trip Ends         | <b>480</b> | <b>480</b> | <b>960</b> |

**SATURDAY**

*Trip Rate:* =4.55\*(\$X5)

|                   | Enter      | Exit       | Total      |
|-------------------|------------|------------|------------|
| Directional Split | 50%        | 50%        |            |
| Trip Ends         | <b>314</b> | <b>314</b> | <b>628</b> |

*Caution: Small Sample Size*

Source: Trip Generation Manual, 11th Edition

## Appendix C

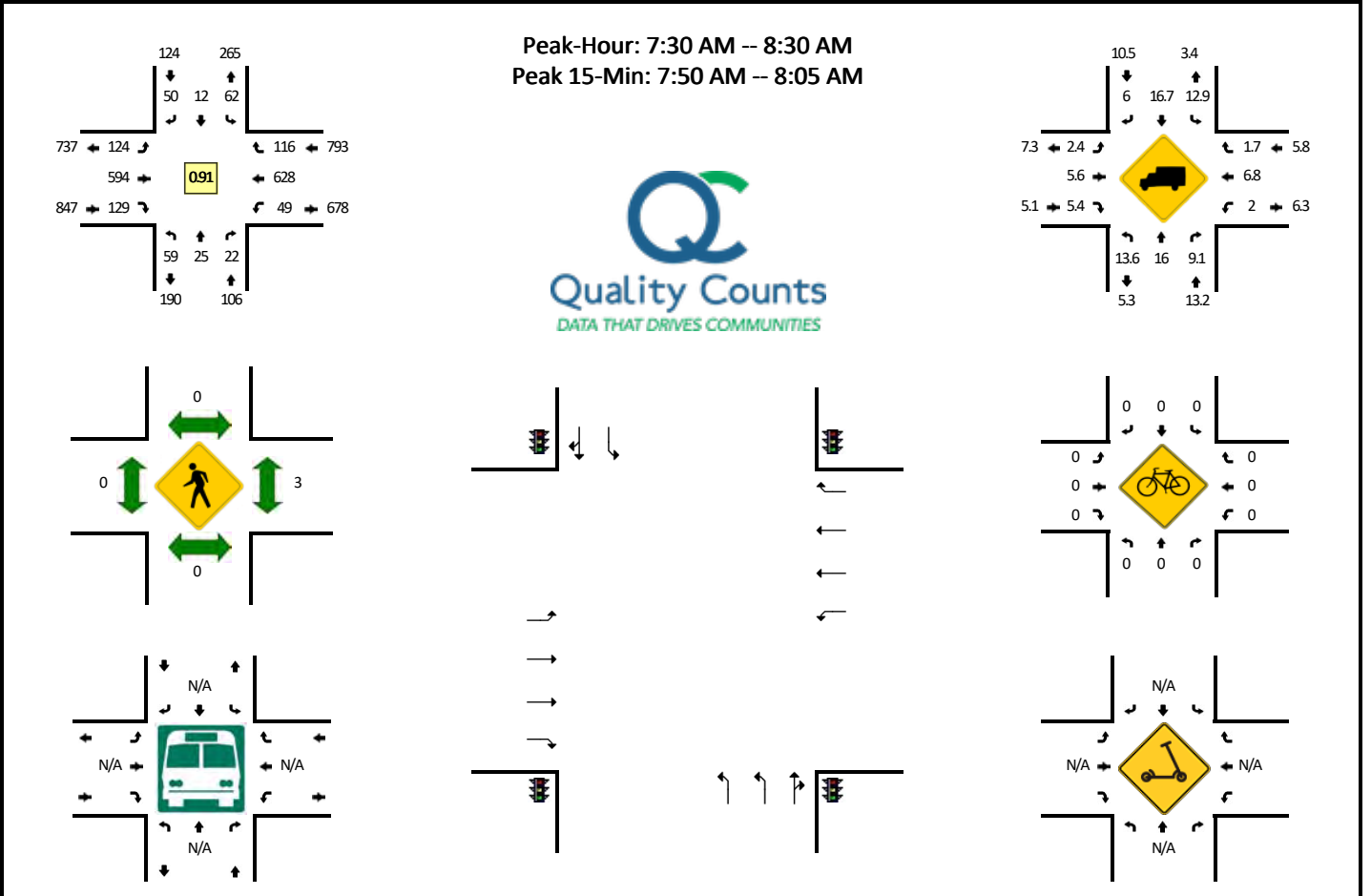
Traffic Counts

In-Process Data



**LOCATION:** Norton Ln -- OR-18  
**CITY/STATE:** McMinnville, OR

**QC JOB #:** 16274501  
**DATE:** Tue, Aug 1 2023



| 5-Min Count Period Beginning At | Norton Ln (Northbound) |      |       |   | Norton Ln (Southbound) |      |       |   | OR-18 (Eastbound) |      |       |   | OR-18 (Westbound) |      |       |   | Total | Hourly Totals |
|---------------------------------|------------------------|------|-------|---|------------------------|------|-------|---|-------------------|------|-------|---|-------------------|------|-------|---|-------|---------------|
|                                 | Left                   | Thru | Right | U | Left                   | Thru | Right | U | Left              | Thru | Right | U | Left              | Thru | Right | U |       |               |
| 6:00 AM                         | 1                      | 0    | 1     | 0 | 3                      | 0    | 2     | 0 | 0                 | 42   | 3     | 0 | 1                 | 14   | 2     | 0 | 69    |               |
| 6:05 AM                         | 4                      | 0    | 0     | 0 | 3                      | 1    | 2     | 0 | 1                 | 58   | 2     | 0 | 1                 | 20   | 3     | 0 | 95    |               |
| 6:10 AM                         | 3                      | 0    | 1     | 0 | 2                      | 0    | 4     | 0 | 0                 | 50   | 5     | 0 | 2                 | 27   | 4     | 0 | 98    |               |
| 6:15 AM                         | 3                      | 0    | 1     | 0 | 1                      | 0    | 2     | 0 | 5                 | 52   | 5     | 0 | 3                 | 21   | 4     | 0 | 97    |               |
| 6:20 AM                         | 5                      | 0    | 0     | 0 | 4                      | 0    | 1     | 0 | 1                 | 58   | 2     | 0 | 3                 | 16   | 2     | 0 | 92    |               |
| 6:25 AM                         | 3                      | 0    | 1     | 0 | 5                      | 1    | 1     | 0 | 5                 | 47   | 2     | 0 | 2                 | 23   | 2     | 0 | 92    |               |
| 6:30 AM                         | 2                      | 0    | 4     | 0 | 4                      | 1    | 1     | 0 | 2                 | 60   | 9     | 0 | 2                 | 25   | 3     | 0 | 113   |               |
| 6:35 AM                         | 9                      | 0    | 0     | 0 | 3                      | 1    | 4     | 0 | 5                 | 50   | 5     | 0 | 0                 | 43   | 2     | 0 | 122   |               |
| 6:40 AM                         | 3                      | 0    | 2     | 0 | 3                      | 1    | 2     | 0 | 3                 | 51   | 10    | 0 | 2                 | 29   | 3     | 0 | 109   |               |
| 6:45 AM                         | 3                      | 0    | 1     | 0 | 4                      | 0    | 3     | 0 | 5                 | 49   | 6     | 0 | 6                 | 42   | 5     | 0 | 124   |               |
| 6:50 AM                         | 3                      | 0    | 1     | 0 | 6                      | 0    | 3     | 0 | 7                 | 41   | 13    | 0 | 2                 | 33   | 4     | 0 | 113   |               |
| 6:55 AM                         | 2                      | 1    | 1     | 0 | 5                      | 0    | 1     | 0 | 6                 | 39   | 16    | 0 | 10                | 21   | 7     | 0 | 109   | 1233          |
| 7:00 AM                         | 5                      | 2    | 0     | 0 | 3                      | 1    | 1     | 0 | 7                 | 60   | 7     | 0 | 0                 | 44   | 6     | 0 | 136   | 1300          |
| 7:05 AM                         | 3                      | 1    | 1     | 0 | 3                      | 0    | 2     | 0 | 6                 | 50   | 4     | 0 | 2                 | 36   | 5     | 0 | 113   | 1318          |
| 7:10 AM                         | 5                      | 1    | 2     | 0 | 2                      | 1    | 2     | 0 | 3                 | 42   | 2     | 0 | 3                 | 18   | 8     | 0 | 89    | 1309          |
| 7:15 AM                         | 2                      | 1    | 2     | 0 | 3                      | 0    | 2     | 0 | 6                 | 68   | 17    | 0 | 4                 | 41   | 6     | 0 | 152   | 1364          |
| 7:20 AM                         | 4                      | 2    | 2     | 0 | 4                      | 0    | 3     | 0 | 10                | 39   | 6     | 0 | 3                 | 31   | 4     | 0 | 108   | 1380          |
| 7:25 AM                         | 11                     | 0    | 3     | 0 | 4                      | 0    | 1     | 0 | 6                 | 47   | 10    | 0 | 4                 | 36   | 9     | 0 | 131   | 1419          |
| 7:30 AM                         | 10                     | 1    | 2     | 0 | 3                      | 0    | 2     | 0 | 7                 | 54   | 11    | 0 | 6                 | 59   | 13    | 0 | 168   | 1474          |
| 7:35 AM                         | 3                      | 0    | 1     | 0 | 9                      | 1    | 4     | 0 | 11                | 54   | 11    | 0 | 5                 | 46   | 11    | 0 | 156   | 1508          |
| 7:40 AM                         | 6                      | 2    | 5     | 0 | 6                      | 1    | 1     | 0 | 6                 | 60   | 8     | 0 | 0                 | 66   | 8     | 0 | 169   | 1568          |
| 7:45 AM                         | 4                      | 1    | 2     | 0 | 8                      | 3    | 6     | 0 | 10                | 44   | 11    | 0 | 4                 | 51   | 9     | 0 | 153   | 1597          |
| 7:50 AM                         | 4                      | 2    | 0     | 0 | 4                      | 0    | 2     | 0 | 9                 | 34   | 16    | 0 | 7                 | 59   | 16    | 0 | 153   | 1637          |
| 7:55 AM                         | 1                      | 3    | 4     | 0 | 6                      | 2    | 8     | 0 | 15                | 52   | 17    | 0 | 7                 | 62   | 11    | 0 | 188   | 1716          |
| 8:00 AM                         | 9                      | 5    | 1     | 0 | 4                      | 2    | 8     | 0 | 14                | 48   | 13    | 0 | 3                 | 53   | 11    | 0 | 171   | 1751          |
| 8:05 AM                         | 6                      | 3    | 2     | 0 | 3                      | 0    | 1     | 0 | 9                 | 47   | 10    | 0 | 1                 | 41   | 7     | 0 | 130   | 1768          |
| 8:10 AM                         | 4                      | 1    | 0     | 0 | 1                      | 1    | 3     | 0 | 7                 | 51   | 2     | 0 | 3                 | 57   | 10    | 0 | 140   | 1819          |
| 8:15 AM                         | 4                      | 3    | 2     | 0 | 7                      | 1    | 3     | 0 | 12                | 43   | 9     | 0 | 5                 | 31   | 6     | 0 | 126   | 1793          |
| 8:20 AM                         | 3                      | 0    | 0     | 0 | 4                      | 1    | 3     | 0 | 11                | 64   | 12    | 0 | 6                 | 68   | 6     | 0 | 178   | 1863          |
| 8:25 AM                         | 5                      | 4    | 3     | 0 | 7                      | 0    | 9     | 0 | 13                | 43   | 9     | 0 | 2                 | 35   | 8     | 0 | 138   | 1870          |
| 8:30 AM                         | 8                      | 1    | 0     | 0 | 4                      | 1    | 6     | 0 | 10                | 49   | 7     | 0 | 6                 | 25   | 10    | 0 | 127   | 1829          |
| 8:35 AM                         | 10                     | 2    | 2     | 0 | 7                      | 2    | 4     | 0 | 5                 | 41   | 10    | 0 | 0                 | 52   | 9     | 0 | 144   | 1817          |
| 8:40 AM                         | 6                      | 3    | 0     | 0 | 3                      | 3    | 7     | 0 | 8                 | 55   | 11    | 0 | 2                 | 51   | 9     | 0 | 158   | 1806          |
| 8:45 AM                         | 9                      | 2    | 1     | 0 | 6                      | 1    | 8     | 0 | 5                 | 53   | 9     | 0 | 7                 | 48   | 4     | 0 | 153   | 1806          |
| 8:50 AM                         | 8                      | 4    | 4     | 0 | 1                      | 1    | 10    | 0 | 11                | 52   | 3     | 0 | 1                 | 45   | 8     | 0 | 148   | 1801          |
| 8:55 AM                         | 4                      | 0    | 0     | 0 | 2                      | 1    | 6     | 0 | 4                 | 32   | 9     | 0 | 3                 | 54   | 6     | 0 | 121   | 1734          |

| Peak 15-Min Flowrates | Northbound |      |       |   | Southbound |      |       |   | Eastbound |      |       |   | Westbound |      |       |   | Total |
|-----------------------|------------|------|-------|---|------------|------|-------|---|-----------|------|-------|---|-----------|------|-------|---|-------|
|                       | Left       | Thru | Right | U | Left       | Thru | Right | U | Left      | Thru | Right | U | Left      | Thru | Right | U |       |
| All Vehicles          | 56         | 40   | 20    | 0 | 56         | 16   | 72    | 0 | 152       | 536  | 184   | 0 | 68        | 696  | 152   | 0 | 2048  |
| Heavy Trucks          | 12         | 4    | 0     |   | 8          | 4    | 8     |   | 0         | 28   | 16    |   | 0         | 28   | 0     |   | 108   |
| Buses                 |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |
| Pedestrians           |            | 0    |       |   |            | 0    |       |   |           | 0    |       |   |           | 4    |       |   | 4     |
| Bicycles              | 0          | 0    | 0     |   | 0          | 0    | 0     |   | 0         | 0    | 0     |   | 0         | 0    | 0     |   | 0     |
| Scoters               |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |

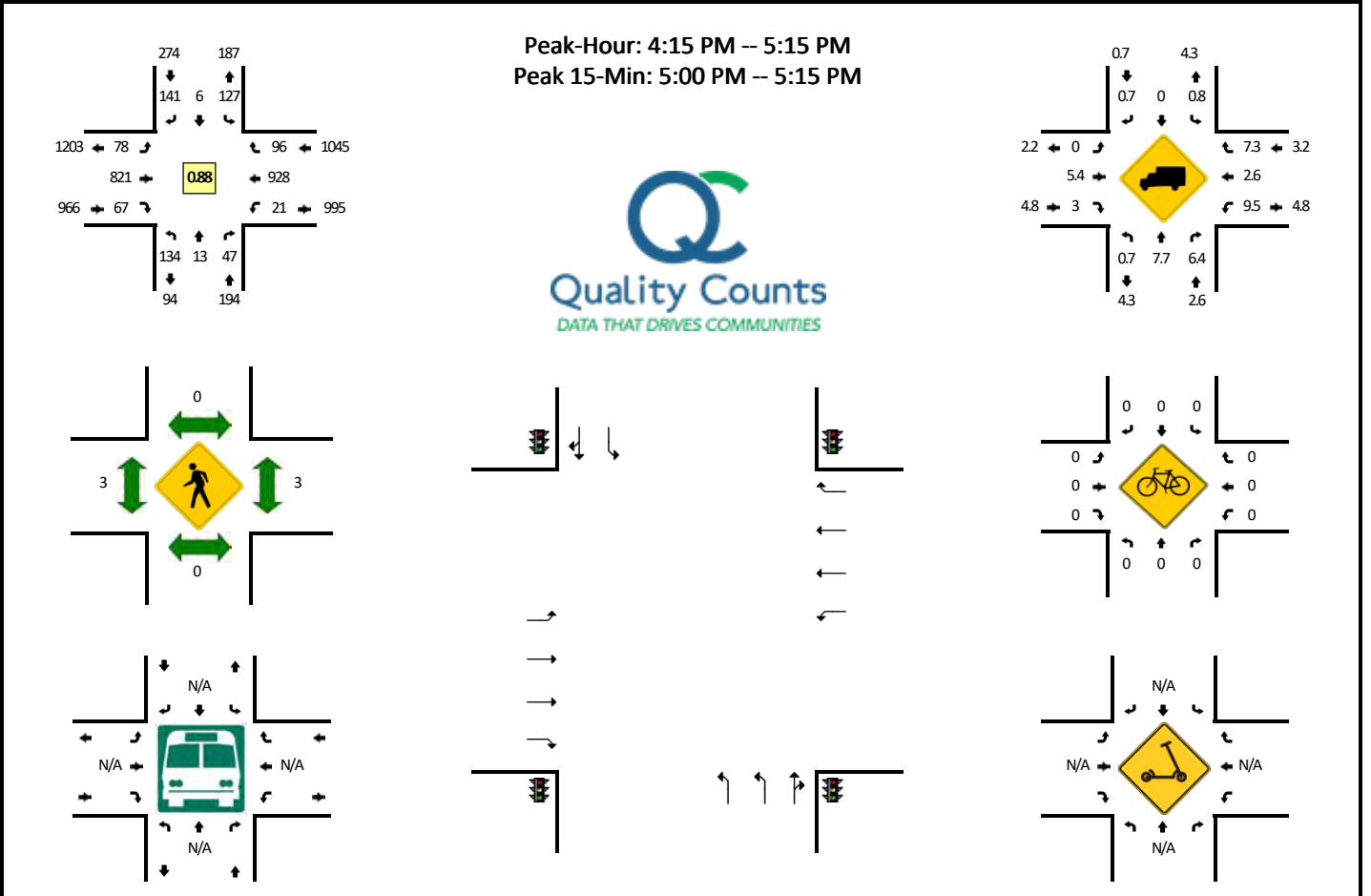
*Comments:*

Report generated on 8/3/2023 11:46 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

**LOCATION:** Norton Ln -- OR-18  
**CITY/STATE:** McMinnville, OR

**QC JOB #:** 16274502  
**DATE:** Tue, Aug 1 2023



| 5-Min Count Period Beginning At | Norton Ln (Northbound) |      |       |   | Norton Ln (Southbound) |      |       |   | OR-18 (Eastbound) |      |       |   | OR-18 (Westbound) |      |       |   | Total | Hourly Totals |
|---------------------------------|------------------------|------|-------|---|------------------------|------|-------|---|-------------------|------|-------|---|-------------------|------|-------|---|-------|---------------|
|                                 | Left                   | Thru | Right | U | Left                   | Thru | Right | U | Left              | Thru | Right | U | Left              | Thru | Right | U |       |               |
| 3:00 PM                         | 8                      | 2    | 5     | 0 | 7                      | 1    | 17    | 0 | 11                | 70   | 5     | 0 | 0                 | 63   | 9     | 0 | 198   |               |
| 3:05 PM                         | 6                      | 1    | 2     | 0 | 11                     | 0    | 15    | 0 | 3                 | 52   | 4     | 0 | 1                 | 50   | 12    | 0 | 157   |               |
| 3:10 PM                         | 7                      | 1    | 1     | 0 | 9                      | 1    | 15    | 0 | 18                | 76   | 2     | 0 | 4                 | 46   | 13    | 0 | 193   |               |
| 3:15 PM                         | 15                     | 3    | 2     | 0 | 8                      | 0    | 16    | 0 | 10                | 49   | 1     | 0 | 3                 | 72   | 15    | 0 | 194   |               |
| 3:20 PM                         | 10                     | 1    | 2     | 0 | 2                      | 1    | 14    | 0 | 12                | 65   | 7     | 0 | 0                 | 48   | 6     | 0 | 168   |               |
| 3:25 PM                         | 6                      | 1    | 6     | 0 | 10                     | 0    | 9     | 0 | 12                | 38   | 0     | 0 | 3                 | 68   | 10    | 0 | 163   |               |
| 3:30 PM                         | 18                     | 0    | 4     | 0 | 3                      | 1    | 10    | 0 | 11                | 54   | 9     | 0 | 1                 | 84   | 9     | 0 | 204   |               |
| 3:35 PM                         | 8                      | 0    | 5     | 0 | 9                      | 1    | 11    | 0 | 12                | 57   | 6     | 0 | 3                 | 61   | 8     | 0 | 181   |               |
| 3:40 PM                         | 13                     | 3    | 2     | 0 | 4                      | 0    | 14    | 0 | 8                 | 48   | 1     | 0 | 3                 | 67   | 9     | 0 | 172   |               |
| 3:45 PM                         | 11                     | 2    | 1     | 0 | 2                      | 0    | 12    | 0 | 7                 | 107  | 6     | 0 | 3                 | 69   | 10    | 0 | 230   |               |
| 3:50 PM                         | 17                     | 1    | 5     | 0 | 6                      | 2    | 10    | 0 | 10                | 57   | 1     | 0 | 2                 | 49   | 4     | 0 | 164   |               |
| 3:55 PM                         | 3                      | 4    | 2     | 0 | 4                      | 0    | 13    | 0 | 10                | 60   | 7     | 0 | 0                 | 79   | 8     | 0 | 190   | 2214          |
| 4:00 PM                         | 4                      | 2    | 5     | 0 | 11                     | 0    | 18    | 0 | 8                 | 50   | 4     | 0 | 4                 | 61   | 8     | 0 | 175   | 2191          |
| 4:05 PM                         | 13                     | 1    | 2     | 0 | 3                      | 1    | 10    | 0 | 5                 | 60   | 6     | 0 | 2                 | 93   | 7     | 0 | 203   | 2237          |
| 4:10 PM                         | 7                      | 0    | 5     | 0 | 5                      | 1    | 16    | 0 | 6                 | 66   | 3     | 0 | 2                 | 66   | 6     | 0 | 183   | 2227          |
| 4:15 PM                         | 8                      | 3    | 6     | 0 | 6                      | 0    | 5     | 0 | 5                 | 71   | 9     | 0 | 3                 | 95   | 11    | 0 | 222   | 2255          |
| 4:20 PM                         | 14                     | 1    | 4     | 0 | 8                      | 0    | 15    | 0 | 11                | 61   | 4     | 0 | 2                 | 63   | 10    | 0 | 193   | 2280          |
| 4:25 PM                         | 10                     | 0    | 2     | 0 | 6                      | 0    | 9     | 0 | 3                 | 55   | 8     | 0 | 2                 | 64   | 9     | 0 | 168   | 2285          |
| 4:30 PM                         | 9                      | 2    | 8     | 0 | 6                      | 0    | 15    | 0 | 7                 | 62   | 2     | 0 | 1                 | 87   | 6     | 0 | 205   | 2286          |
| 4:35 PM                         | 15                     | 1    | 7     | 0 | 14                     | 1    | 10    | 0 | 13                | 59   | 4     | 0 | 4                 | 66   | 5     | 0 | 199   | 2304          |
| 4:40 PM                         | 10                     | 1    | 2     | 0 | 5                      | 0    | 13    | 0 | 4                 | 79   | 14    | 0 | 2                 | 83   | 11    | 0 | 224   | 2356          |
| 4:45 PM                         | 9                      | 2    | 3     | 0 | 12                     | 0    | 10    | 0 | 4                 | 61   | 5     | 0 | 2                 | 72   | 7     | 0 | 187   | 2313          |
| 4:50 PM                         | 7                      | 0    | 2     | 0 | 7                      | 0    | 8     | 0 | 6                 | 70   | 4     | 0 | 1                 | 79   | 10    | 0 | 194   | 2343          |
| 4:55 PM                         | 11                     | 0    | 3     | 0 | 9                      | 0    | 5     | 0 | 7                 | 64   | 3     | 0 | 1                 | 80   | 3     | 0 | 186   | 2339          |
| 5:00 PM                         | 14                     | 0    | 4     | 0 | 14                     | 2    | 13    | 0 | 6                 | 83   | 2     | 0 | 0                 | 68   | 8     | 0 | 214   | 2378          |
| 5:05 PM                         | 14                     | 1    | 4     | 0 | 30                     | 2    | 29    | 0 | 4                 | 54   | 6     | 0 | 0                 | 70   | 7     | 0 | 221   | 2396          |
| 5:10 PM                         | 13                     | 2    | 2     | 0 | 10                     | 1    | 9     | 0 | 8                 | 102  | 6     | 0 | 3                 | 101  | 9     | 0 | 266   | 2479          |
| 5:15 PM                         | 14                     | 5    | 2     | 0 | 6                      | 0    | 12    | 0 | 4                 | 77   | 3     | 0 | 0                 | 75   | 9     | 0 | 207   | 2464          |
| 5:20 PM                         | 8                      | 1    | 4     | 0 | 6                      | 0    | 10    | 0 | 2                 | 58   | 5     | 0 | 2                 | 57   | 12    | 0 | 165   | 2436          |
| 5:25 PM                         | 6                      | 1    | 5     | 0 | 5                      | 1    | 4     | 0 | 4                 | 64   | 6     | 0 | 0                 | 55   | 13    | 0 | 164   | 2432          |
| 5:30 PM                         | 14                     | 4    | 2     | 0 | 6                      | 0    | 9     | 0 | 6                 | 57   | 6     | 0 | 4                 | 77   | 6     | 0 | 191   | 2418          |
| 5:35 PM                         | 6                      | 1    | 5     | 0 | 4                      | 0    | 6     | 0 | 8                 | 59   | 3     | 0 | 0                 | 66   | 7     | 0 | 165   | 2384          |
| 5:40 PM                         | 17                     | 0    | 2     | 0 | 4                      | 1    | 10    | 0 | 7                 | 52   | 2     | 0 | 3                 | 62   | 4     | 0 | 164   | 2324          |
| 5:45 PM                         | 9                      | 1    | 4     | 0 | 10                     | 1    | 10    | 0 | 5                 | 56   | 3     | 0 | 0                 | 71   | 4     | 0 | 174   | 2311          |
| 5:50 PM                         | 5                      | 0    | 2     | 0 | 1                      | 0    | 6     | 0 | 6                 | 60   | 3     | 0 | 0                 | 67   | 2     | 0 | 152   | 2269          |
| 5:55 PM                         | 7                      | 2    | 0     | 0 | 11                     | 1    | 13    | 0 | 3                 | 30   | 3     | 0 | 1                 | 50   | 4     | 0 | 125   | 2208          |

| Peak 15-Min Flowrates | Northbound |      |       |   | Southbound |      |       |   | Eastbound |      |       |   | Westbound |      |       |   | Total |
|-----------------------|------------|------|-------|---|------------|------|-------|---|-----------|------|-------|---|-----------|------|-------|---|-------|
|                       | Left       | Thru | Right | U | Left       | Thru | Right | U | Left      | Thru | Right | U | Left      | Thru | Right | U |       |
| All Vehicles          | 164        | 12   | 40    | 0 | 216        | 20   | 204   | 0 | 72        | 956  | 56    | 0 | 12        | 956  | 96    | 0 | 2804  |
| Heavy Trucks          | 0          | 0    | 0     |   | 4          | 0    | 0     |   | 0         | 36   | 0     |   | 0         | 8    | 4     |   | 52    |
| Buses                 |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |
| Pedestrians           |            | 0    |       |   |            | 0    |       |   |           | 0    |       |   |           | 4    |       |   | 4     |
| Bicycles              | 0          | 0    | 0     |   | 0          | 0    | 0     |   | 0         | 0    | 0     |   | 0         | 0    | 0     |   | 0     |
| Scoters               |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |

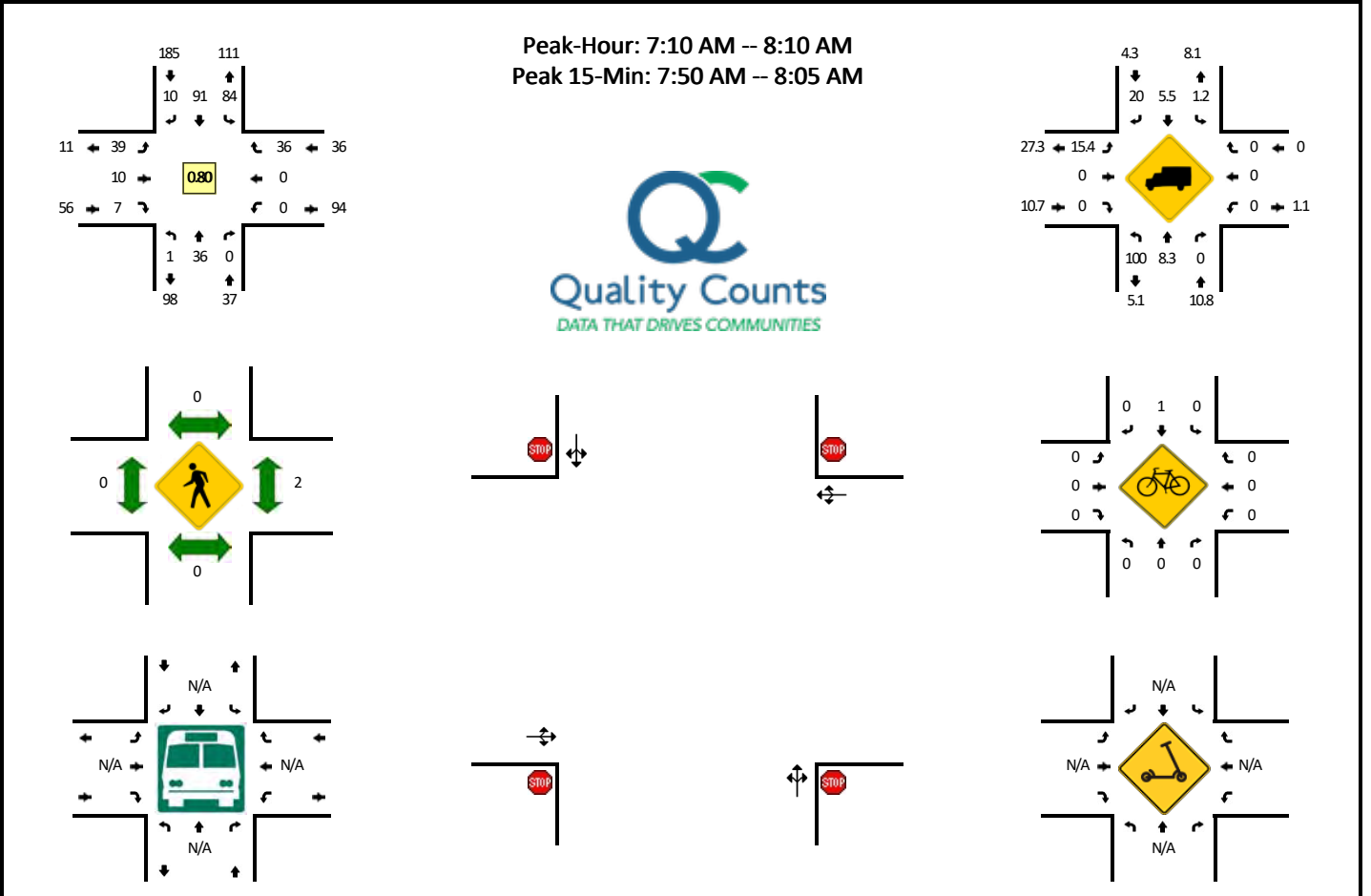
Comments:

Report generated on 8/3/2023 11:46 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

**LOCATION:** SE Norton Ln -- SE Stratus Ave (north)  
**CITY/STATE:** McMinnville, OR

**QC JOB #:** 16274503  
**DATE:** Tue, Aug 1 2023



| 5-Min Count Period Beginning At | SE Norton Ln (Northbound) |      |       |   | SE Norton Ln (Southbound) |      |       |   | SE Stratus Ave (north) (Eastbound) |      |       |   | SE Stratus Ave (north) (Westbound) |      |       |   | Total | Hourly Totals |     |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|------------------------------------|------|-------|---|------------------------------------|------|-------|---|-------|---------------|-----|
|                                 | Left                      | Thru | Right | U | Left                      | Thru | Right | U | Left                               | Thru | Right | U | Left                               | Thru | Right | U |       |               |     |
| 6:00 AM                         | 0                         | 0    | 0     | 0 | 1                         | 2    | 0     | 0 | 1                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 4     |               |     |
| 6:05 AM                         | 0                         | 1    | 0     | 0 | 0                         | 3    | 1     | 0 | 1                                  | 0    | 1     | 0 | 0                                  | 0    | 0     | 1 | 0     | 8             |     |
| 6:10 AM                         | 0                         | 1    | 0     | 0 | 2                         | 3    | 1     | 0 | 3                                  | 2    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     | 12            |     |
| 6:15 AM                         | 0                         | 1    | 0     | 0 | 4                         | 3    | 2     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     | 13            |     |
| 6:20 AM                         | 0                         | 1    | 0     | 0 | 2                         | 2    | 0     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 1 | 0     | 9             |     |
| 6:25 AM                         | 0                         | 0    | 0     | 0 | 1                         | 3    | 1     | 0 | 5                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     | 10            |     |
| 6:30 AM                         | 0                         | 0    | 0     | 0 | 4                         | 8    | 0     | 0 | 5                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 1 | 0     | 18            |     |
| 6:35 AM                         | 0                         | 4    | 0     | 0 | 4                         | 3    | 0     | 0 | 3                                  | 0    | 1     | 0 | 0                                  | 0    | 0     | 1 | 0     | 16            |     |
| 6:40 AM                         | 0                         | 1    | 0     | 0 | 4                         | 5    | 2     | 0 | 5                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     | 17            |     |
| 6:45 AM                         | 0                         | 0    | 0     | 0 | 6                         | 5    | 0     | 0 | 2                                  | 1    | 1     | 0 | 0                                  | 0    | 0     | 2 | 0     | 17            |     |
| 6:50 AM                         | 0                         | 0    | 0     | 0 | 3                         | 11   | 2     | 0 | 2                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 1 | 0     | 19            |     |
| 6:55 AM                         | 0                         | 0    | 0     | 0 | 15                        | 9    | 1     | 0 | 1                                  | 1    | 2     | 0 | 0                                  | 0    | 0     | 4 | 0     | 33            | 176 |
| 7:00 AM                         | 0                         | 0    | 0     | 0 | 4                         | 5    | 0     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 1 | 0     | 13            | 185 |
| 7:05 AM                         | 0                         | 3    | 0     | 0 | 2                         | 2    | 0     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 1 | 0     | 11            | 188 |
| 7:10 AM                         | 0                         | 2    | 0     | 0 | 3                         | 2    | 0     | 0 | 3                                  | 1    | 2     | 0 | 0                                  | 0    | 3     | 0 | 0     | 16            | 192 |
| 7:15 AM                         | 0                         | 2    | 0     | 0 | 8                         | 9    | 0     | 0 | 3                                  | 0    | 2     | 0 | 0                                  | 0    | 2     | 0 | 0     | 26            | 205 |
| 7:20 AM                         | 1                         | 2    | 0     | 0 | 4                         | 8    | 0     | 0 | 2                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 0     | 19            | 215 |
| 7:25 AM                         | 0                         | 6    | 0     | 0 | 7                         | 4    | 1     | 0 | 3                                  | 1    | 0     | 0 | 0                                  | 0    | 5     | 0 | 0     | 27            | 232 |
| 7:30 AM                         | 0                         | 6    | 0     | 0 | 10                        | 8    | 0     | 0 | 3                                  | 1    | 1     | 0 | 0                                  | 0    | 2     | 0 | 0     | 31            | 245 |
| 7:35 AM                         | 0                         | 2    | 0     | 0 | 9                         | 8    | 0     | 0 | 1                                  | 2    | 0     | 0 | 0                                  | 0    | 4     | 0 | 0     | 26            | 255 |
| 7:40 AM                         | 0                         | 2    | 0     | 0 | 3                         | 4    | 2     | 0 | 4                                  | 1    | 0     | 0 | 0                                  | 0    | 5     | 0 | 0     | 21            | 259 |
| 7:45 AM                         | 0                         | 2    | 0     | 0 | 8                         | 10   | 1     | 0 | 3                                  | 1    | 0     | 0 | 0                                  | 0    | 3     | 0 | 0     | 28            | 270 |
| 7:50 AM                         | 0                         | 1    | 0     | 0 | 10                        | 8    | 2     | 0 | 4                                  | 0    | 1     | 0 | 0                                  | 0    | 1     | 0 | 0     | 27            | 278 |
| 7:55 AM                         | 0                         | 2    | 0     | 0 | 11                        | 14   | 1     | 0 | 3                                  | 2    | 0     | 0 | 0                                  | 0    | 4     | 0 | 0     | 37            | 282 |
| 8:00 AM                         | 0                         | 5    | 0     | 0 | 7                         | 9    | 2     | 0 | 7                                  | 0    | 1     | 0 | 0                                  | 0    | 3     | 0 | 0     | 34            | 303 |
| 8:05 AM                         | 0                         | 4    | 0     | 0 | 4                         | 7    | 1     | 0 | 3                                  | 1    | 0     | 0 | 0                                  | 0    | 2     | 0 | 0     | 22            | 314 |
| 8:10 AM                         | 0                         | 2    | 0     | 0 | 3                         | 2    | 0     | 1 | 2                                  | 0    | 1     | 0 | 0                                  | 0    | 2     | 0 | 0     | 13            | 311 |
| 8:15 AM                         | 0                         | 0    | 0     | 0 | 4                         | 3    | 4     | 0 | 6                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 0     | 19            | 304 |
| 8:20 AM                         | 0                         | 2    | 0     | 0 | 12                        | 8    | 2     | 0 | 3                                  | 0    | 1     | 0 | 0                                  | 0    | 1     | 0 | 0     | 29            | 314 |
| 8:25 AM                         | 0                         | 1    | 0     | 0 | 5                         | 5    | 0     | 0 | 4                                  | 0    | 1     | 0 | 0                                  | 0    | 4     | 0 | 0     | 20            | 307 |
| 8:30 AM                         | 0                         | 6    | 0     | 0 | 6                         | 8    | 2     | 0 | 2                                  | 0    | 1     | 0 | 0                                  | 0    | 5     | 0 | 0     | 30            | 306 |
| 8:35 AM                         | 0                         | 1    | 0     | 0 | 7                         | 3    | 0     | 0 | 4                                  | 2    | 0     | 0 | 1                                  | 0    | 4     | 0 | 0     | 22            | 302 |
| 8:40 AM                         | 0                         | 7    | 1     | 0 | 6                         | 7    | 1     | 0 | 1                                  | 1    | 0     | 0 | 0                                  | 0    | 2     | 0 | 0     | 26            | 307 |
| 8:45 AM                         | 0                         | 3    | 0     | 0 | 10                        | 9    | 1     | 0 | 6                                  | 1    | 0     | 0 | 0                                  | 0    | 4     | 0 | 0     | 34            | 313 |
| 8:50 AM                         | 0                         | 3    | 0     | 0 | 1                         | 2    | 1     | 0 | 3                                  | 2    | 4     | 0 | 0                                  | 0    | 7     | 0 | 0     | 23            | 309 |
| 8:55 AM                         | 0                         | 0    | 0     | 0 | 7                         | 5    | 2     | 0 | 0                                  | 2    | 1     | 0 | 1                                  | 1    | 4     | 0 | 0     | 23            | 295 |

| Peak 15-Min Flowrates | Northbound |      |       |   | Southbound |      |       |   | Eastbound |      |       |   | Westbound |      |       |   | Total |
|-----------------------|------------|------|-------|---|------------|------|-------|---|-----------|------|-------|---|-----------|------|-------|---|-------|
|                       | Left       | Thru | Right | U | Left       | Thru | Right | U | Left      | Thru | Right | U | Left      | Thru | Right | U |       |
| All Vehicles          | 0          | 32   | 0     | 0 | 112        | 124  | 20    | 0 | 56        | 8    | 8     | 0 | 0         | 0    | 32    | 0 | 392   |
| Heavy Trucks          | 0          | 4    | 0     | 0 | 0          | 8    | 4     | 0 | 8         | 0    | 0     | 0 | 0         | 0    | 0     | 0 | 24    |
| Buses                 |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |
| Pedestrians           |            | 0    |       |   |            | 0    |       |   |           | 0    |       |   |           | 8    |       |   | 8     |
| Bicycles              | 0          | 0    | 0     |   | 0          | 0    | 0     |   | 0         | 0    | 0     |   | 0         | 0    | 0     |   | 0     |
| Scoters               |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |

*Comments: Stop signs at all approaches except for SB*

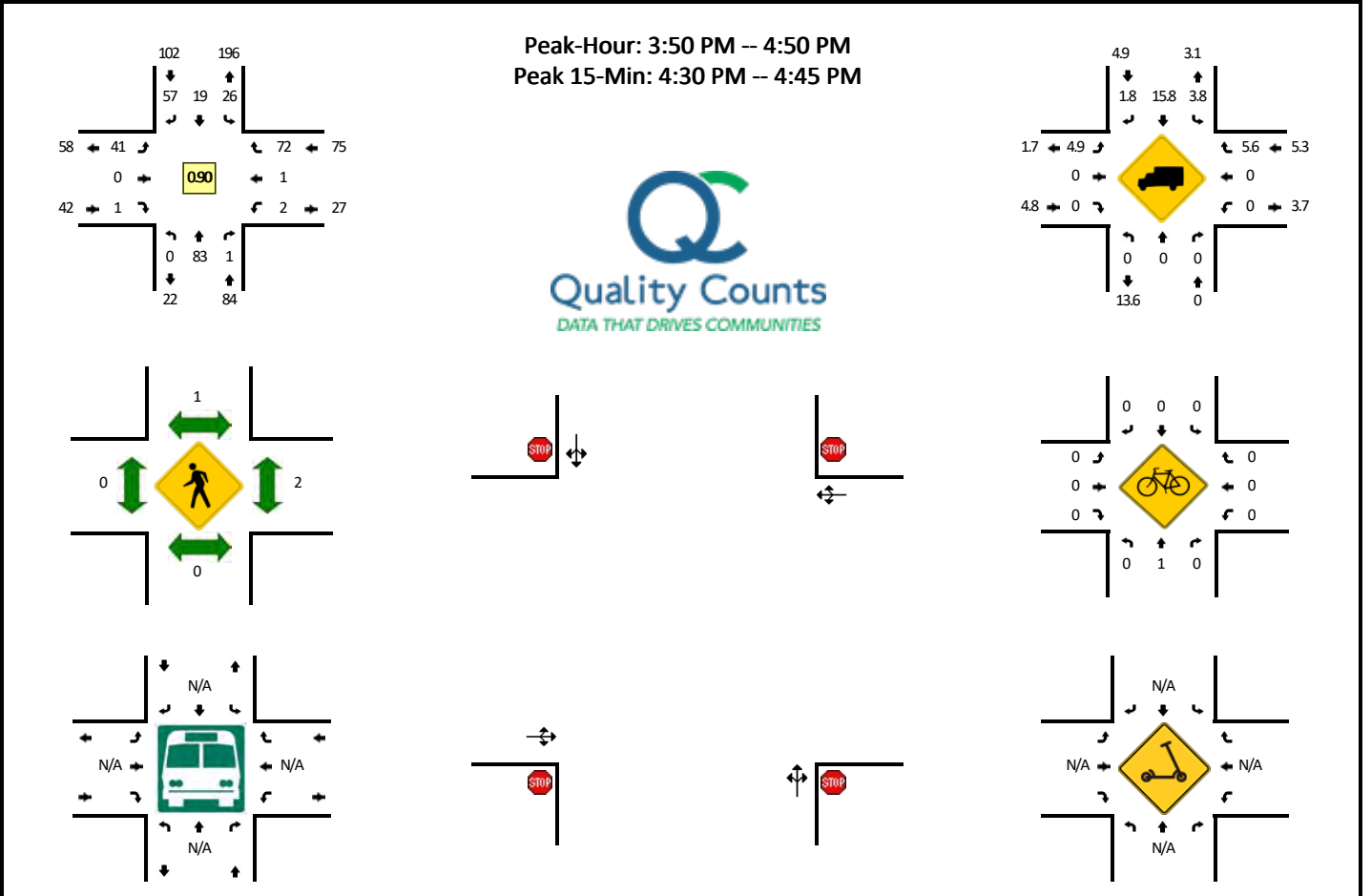
Report generated on 8/3/2023 11:46 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212



**LOCATION:** SE Norton Ln -- SE Stratus Ave (north)  
**CITY/STATE:** McMinnville, OR

**QC JOB #:** 16274504  
**DATE:** Tue, Aug 1 2023



| 5-Min Count Period Beginning At | SE Norton Ln (Northbound) |      |       |   | SE Norton Ln (Southbound) |      |       |   | SE Stratus Ave (north) (Eastbound) |      |       |   | SE Stratus Ave (north) (Westbound) |      |       |   | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|------------------------------------|------|-------|---|------------------------------------|------|-------|---|-------|---------------|
|                                 | Left                      | Thru | Right | U | Left                      | Thru | Right | U | Left                               | Thru | Right | U | Left                               | Thru | Right | U |       |               |
| 3:00 PM                         | 0                         | 4    | 0     | 0 | 1                         | 3    | 3     | 0 | 3                                  | 1    | 1     | 0 | 0                                  | 0    | 6     | 0 | 22    |               |
| 3:05 PM                         | 0                         | 1    | 0     | 0 | 1                         | 1    | 3     | 0 | 3                                  | 0    | 1     | 0 | 0                                  | 0    | 4     | 0 | 14    |               |
| 3:10 PM                         | 0                         | 5    | 0     | 0 | 1                         | 3    | 2     | 0 | 2                                  | 1    | 0     | 0 | 0                                  | 0    | 5     | 0 | 19    |               |
| 3:15 PM                         | 1                         | 8    | 0     | 0 | 2                         | 1    | 1     | 0 | 7                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 21    |               |
| 3:20 PM                         | 0                         | 6    | 0     | 0 | 1                         | 7    | 1     | 0 | 4                                  | 0    | 1     | 0 | 0                                  | 0    | 5     | 0 | 25    |               |
| 3:25 PM                         | 0                         | 7    | 0     | 0 | 3                         | 1    | 1     | 0 | 3                                  | 0    | 2     | 0 | 0                                  | 0    | 3     | 0 | 20    |               |
| 3:30 PM                         | 0                         | 7    | 1     | 0 | 6                         | 1    | 3     | 0 | 9                                  | 1    | 0     | 0 | 0                                  | 0    | 6     | 0 | 34    |               |
| 3:35 PM                         | 0                         | 3    | 0     | 0 | 0                         | 6    | 5     | 0 | 2                                  | 1    | 1     | 0 | 0                                  | 0    | 6     | 0 | 24    |               |
| 3:40 PM                         | 0                         | 6    | 0     | 0 | 1                         | 2    | 0     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 10    | 0 | 22    |               |
| 3:45 PM                         | 1                         | 3    | 0     | 0 | 0                         | 6    | 2     | 0 | 2                                  | 0    | 0     | 0 | 0                                  | 0    | 8     | 0 | 22    |               |
| 3:50 PM                         | 0                         | 10   | 0     | 0 | 3                         | 0    | 2     | 0 | 6                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 28    |               |
| 3:55 PM                         | 0                         | 4    | 0     | 0 | 2                         | 4    | 1     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 16    | 267           |
| 4:00 PM                         | 0                         | 6    | 0     | 0 | 3                         | 2    | 5     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 0    | 4     | 0 | 23    | 268           |
| 4:05 PM                         | 0                         | 7    | 0     | 0 | 1                         | 3    | 5     | 0 | 2                                  | 0    | 0     | 0 | 1                                  | 0    | 6     | 0 | 25    | 279           |
| 4:10 PM                         | 0                         | 9    | 0     | 0 | 2                         | 0    | 3     | 0 | 5                                  | 0    | 1     | 0 | 0                                  | 1    | 2     | 0 | 23    | 283           |
| 4:15 PM                         | 0                         | 4    | 0     | 0 | 1                         | 2    | 10    | 0 | 1                                  | 0    | 0     | 0 | 0                                  | 0    | 11    | 0 | 29    | 291           |
| 4:20 PM                         | 0                         | 7    | 0     | 0 | 1                         | 3    | 3     | 0 | 6                                  | 0    | 0     | 0 | 0                                  | 0    | 6     | 0 | 26    | 292           |
| 4:25 PM                         | 0                         | 8    | 1     | 0 | 4                         | 2    | 4     | 0 | 2                                  | 0    | 0     | 0 | 0                                  | 0    | 4     | 0 | 25    | 297           |
| 4:30 PM                         | 0                         | 11   | 0     | 0 | 0                         | 0    | 3     | 0 | 5                                  | 0    | 0     | 0 | 1                                  | 0    | 10    | 0 | 30    | 293           |
| 4:35 PM                         | 0                         | 8    | 0     | 0 | 4                         | 1    | 5     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 25    | 294           |
| 4:40 PM                         | 0                         | 2    | 0     | 0 | 4                         | 1    | 11    | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 29    | 301           |
| 4:45 PM                         | 0                         | 7    | 0     | 0 | 1                         | 1    | 5     | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 6     | 0 | 24    | 303           |
| 4:50 PM                         | 0                         | 4    | 0     | 0 | 0                         | 2    | 3     | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 13    | 288           |
| 4:55 PM                         | 0                         | 3    | 0     | 0 | 0                         | 1    | 2     | 0 | 5                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 18    | 290           |
| 5:00 PM                         | 0                         | 4    | 0     | 0 | 2                         | 1    | 2     | 0 | 2                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 18    | 285           |
| 5:05 PM                         | 0                         | 8    | 0     | 0 | 2                         | 1    | 5     | 0 | 6                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 29    | 289           |
| 5:10 PM                         | 0                         | 6    | 0     | 0 | 1                         | 3    | 6     | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 8     | 0 | 28    | 294           |
| 5:15 PM                         | 0                         | 8    | 0     | 0 | 0                         | 2    | 1     | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 6     | 0 | 21    | 286           |
| 5:20 PM                         | 0                         | 7    | 0     | 0 | 0                         | 1    | 6     | 0 | 4                                  | 1    | 1     | 0 | 0                                  | 0    | 2     | 0 | 22    | 282           |
| 5:25 PM                         | 0                         | 3    | 0     | 0 | 3                         | 2    | 3     | 0 | 2                                  | 0    | 0     | 0 | 0                                  | 0    | 10    | 0 | 23    | 280           |
| 5:30 PM                         | 0                         | 6    | 0     | 0 | 1                         | 3    | 6     | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 6     | 0 | 26    | 276           |
| 5:35 PM                         | 0                         | 5    | 0     | 0 | 0                         | 0    | 1     | 0 | 4                                  | 0    | 0     | 0 | 0                                  | 0    | 7     | 0 | 17    | 268           |
| 5:40 PM                         | 0                         | 2    | 0     | 0 | 1                         | 1    | 5     | 0 | 8                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 18    | 257           |
| 5:45 PM                         | 0                         | 2    | 0     | 0 | 1                         | 1    | 2     | 0 | 6                                  | 0    | 0     | 0 | 0                                  | 0    | 4     | 0 | 16    | 249           |
| 5:50 PM                         | 0                         | 4    | 0     | 0 | 0                         | 1    | 2     | 0 | 3                                  | 0    | 0     | 0 | 0                                  | 1    | 1     | 0 | 12    | 248           |
| 5:55 PM                         | 0                         | 3    | 0     | 0 | 1                         | 1    | 2     | 0 | 4                                  | 0    | 1     | 0 | 0                                  | 0    | 1     | 0 | 13    | 243           |

| Peak 15-Min Flowrates | Northbound |      |       |   | Southbound |      |       |   | Eastbound |      |       |   | Westbound |      |       |   | Total |
|-----------------------|------------|------|-------|---|------------|------|-------|---|-----------|------|-------|---|-----------|------|-------|---|-------|
|                       | Left       | Thru | Right | U | Left       | Thru | Right | U | Left      | Thru | Right | U | Left      | Thru | Right | U |       |
| All Vehicles          | 0          | 84   | 0     | 0 | 32         | 8    | 76    | 0 | 36        | 0    | 0     | 0 | 4         | 0    | 96    | 0 | 336   |
| Heavy Trucks          | 0          | 0    | 0     | 0 | 0          | 0    | 0     | 0 | 0         | 0    | 0     | 0 | 0         | 0    | 4     | 0 | 4     |
| Buses                 |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |
| Pedestrians           |            | 0    |       |   |            | 0    |       |   |           | 0    |       |   |           | 4    |       |   | 4     |
| Bicycles              | 0          | 0    | 0     |   | 0          | 0    | 0     |   | 0         | 0    | 0     |   | 0         | 0    | 0     |   | 0     |
| Scoters               |            |      |       |   |            |      |       |   |           |      |       |   |           |      |       |   |       |

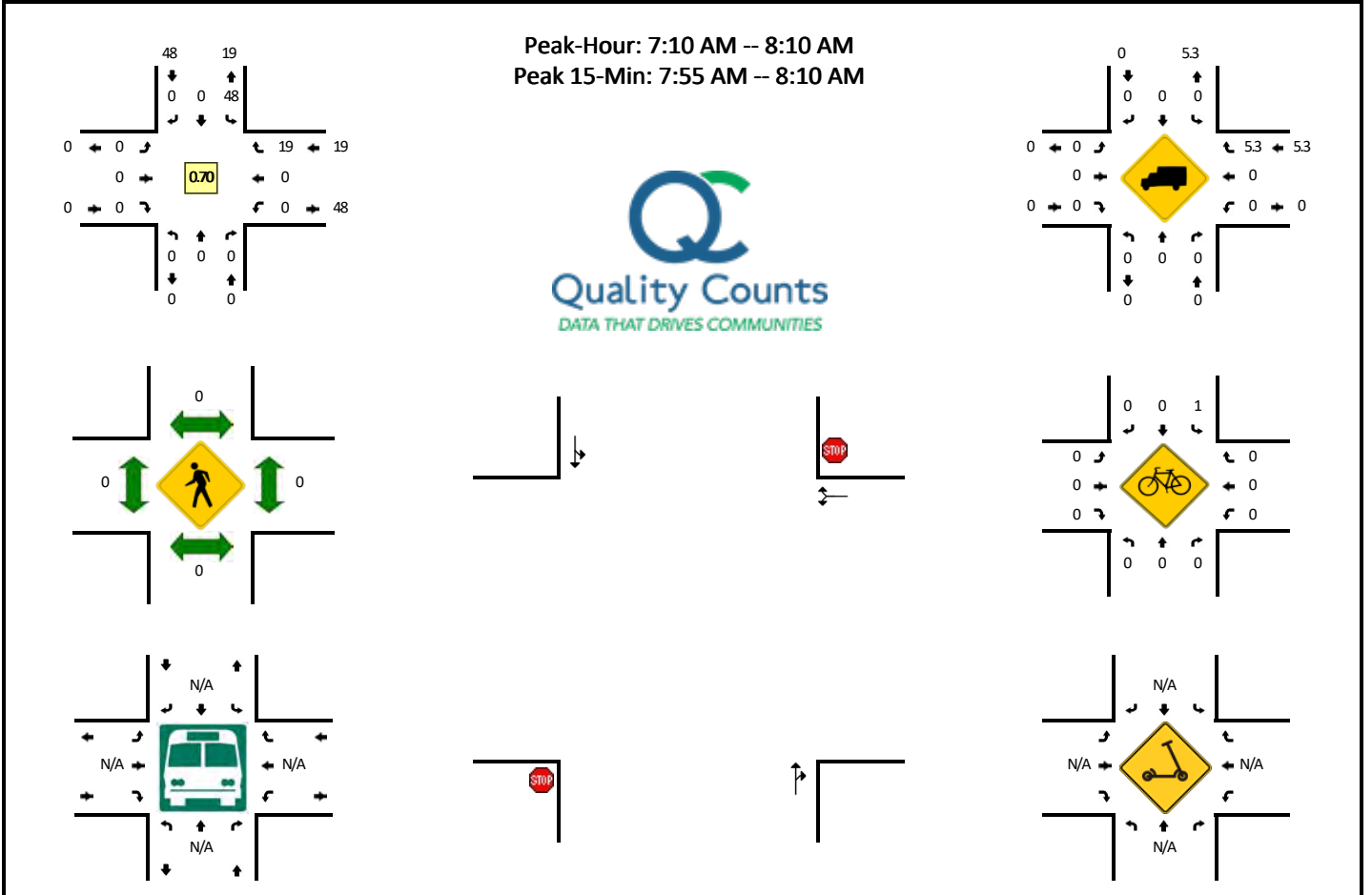
*Comments: Stop signs at all approaches except for SB*

Report generated on 8/3/2023 11:46 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

**LOCATION:** SE Norton Ln -- SE Stratus Ave (south)  
**CITY/STATE:** McMinnville, OR

**QC JOB #:** 16274505  
**DATE:** Tue, Aug 1 2023

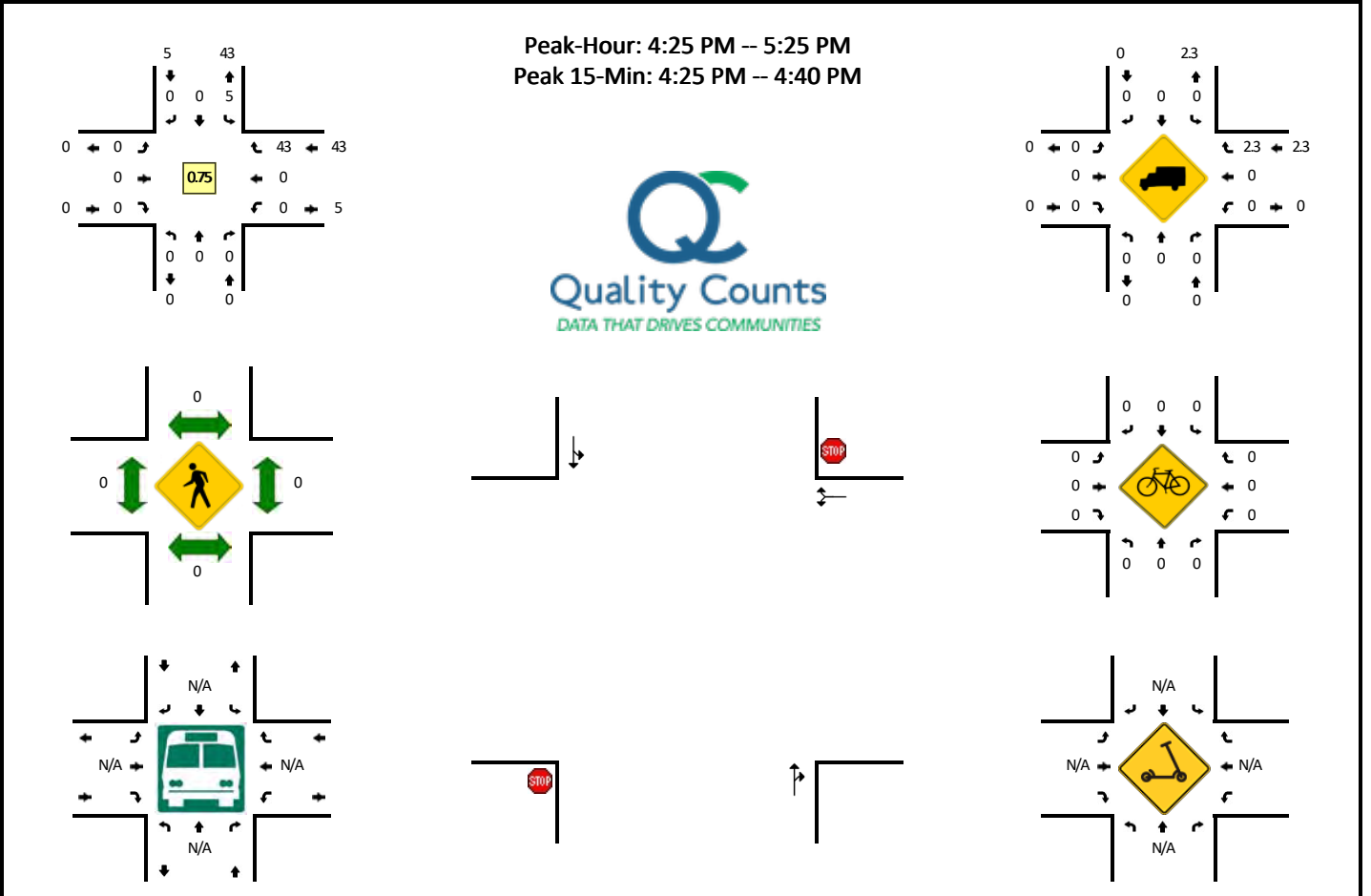


| 5-Min Count Period Beginning At | SE Norton Ln (Northbound) |      |       |   | SE Norton Ln (Southbound) |      |       |   | SE Stratus Ave (south) (Eastbound) |      |       |   | SE Stratus Ave (south) (Westbound) |      |       |   | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|------------------------------------|------|-------|---|------------------------------------|------|-------|---|-------|---------------|
|                                 | Left                      | Thru | Right | U | Left                      | Thru | Right | U | Left                               | Thru | Right | U | Left                               | Thru | Right | U |       |               |
| 7:00 AM                         | 0                         | 0    | 0     | 0 | 7                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 7     |               |
| 7:05 AM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 2     |               |
| 7:10 AM                         | 0                         | 0    | 0     | 0 | 2                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 3     |               |
| 7:15 AM                         | 0                         | 0    | 0     | 0 | 5                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 5     |               |
| 7:20 AM                         | 0                         | 0    | 0     | 0 | 2                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 4     |               |
| 7:25 AM                         | 0                         | 0    | 0     | 0 | 2                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 3     | 0 | 5     |               |
| 7:30 AM                         | 0                         | 0    | 0     | 0 | 5                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 7     |               |
| 7:35 AM                         | 0                         | 0    | 0     | 0 | 5                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 6     |               |
| 7:40 AM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 2     |               |
| 7:45 AM                         | 0                         | 0    | 0     | 0 | 5                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 7     |               |
| 7:50 AM                         | 0                         | 0    | 0     | 0 | 4                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 4     |               |
| 7:55 AM                         | 0                         | 0    | 0     | 0 | 7                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 7     | 59            |
| 8:00 AM                         | 0                         | 0    | 0     | 0 | 7                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 3    | 0     | 0 | 10    | 62            |
| 8:05 AM                         | 0                         | 0    | 0     | 0 | 3                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 4    | 0     | 0 | 7     | 67            |
| 8:10 AM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 1     | 65            |
| 8:15 AM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 1     | 61            |
| 8:20 AM                         | 0                         | 0    | 0     | 0 | 2                         | 2    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 5     | 62            |
| 8:25 AM                         | 0                         | 0    | 0     | 0 | 3                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 3     | 60            |
| 8:30 AM                         | 0                         | 0    | 0     | 0 | 3                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 5     | 58            |
| 8:35 AM                         | 0                         | 0    | 0     | 0 | 3                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 4     | 56            |
| 8:40 AM                         | 0                         | 2    | 0     | 0 | 2                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 4     | 58            |
| 8:45 AM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 1     | 52            |
| 8:50 AM                         | 0                         | 0    | 0     | 0 | 2                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 3     | 51            |
| 8:55 AM                         | 0                         | 0    | 0     | 0 | 3                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 3     | 47            |
| Peak 15-Min Flowrates           | Northbound                |      |       |   | Southbound                |      |       |   | Eastbound                          |      |       |   | Westbound                          |      |       |   | Total |               |
|                                 | Left                      | Thru | Right | U | Left                      | Thru | Right | U | Left                               | Thru | Right | U | Left                               | Thru | Right | U |       |               |
| All Vehicles                    | 0                         | 0    | 0     | 0 | 68                        | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 28    | 0 | 96    |               |
| Heavy Trucks                    | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     |               |
| Buses                           |                           |      |       |   |                           |      |       |   |                                    |      |       |   |                                    |      |       |   |       |               |
| Pedestrians                     |                           | 0    |       |   |                           | 0    |       |   |                                    | 0    |       |   |                                    | 0    |       |   | 0     |               |
| Bicycles                        | 0                         | 0    | 0     |   | 0                         | 0    | 0     |   | 0                                  | 0    | 0     |   | 0                                  | 0    | 0     |   | 0     |               |
| Scoters                         |                           |      |       |   |                           |      |       |   |                                    |      |       |   |                                    |      |       |   |       |               |

Comments:

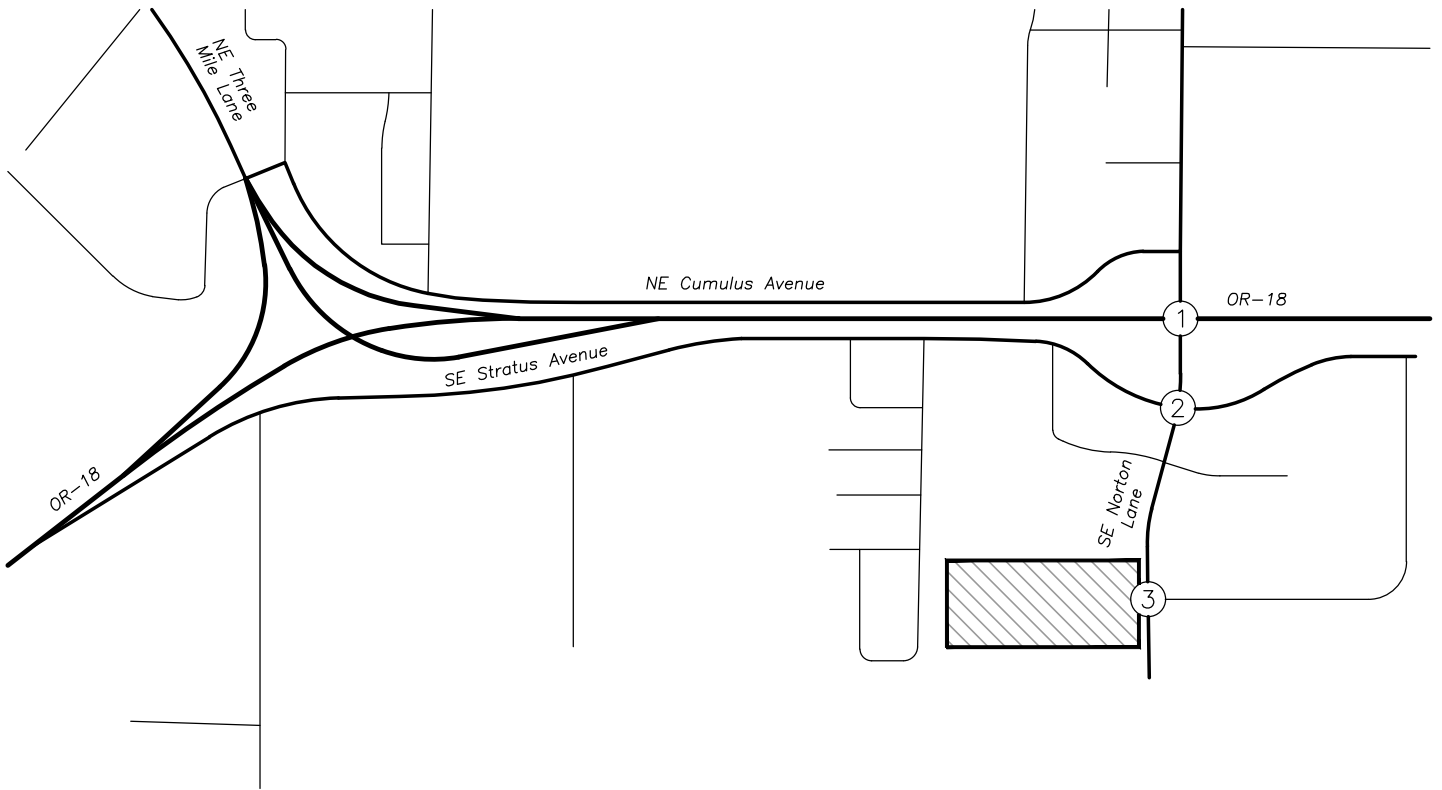
**LOCATION:** SE Norton Ln -- SE Stratus Ave (south)  
**CITY/STATE:** McMinnville, OR

**QC JOB #:** 16274506  
**DATE:** Tue, Aug 1 2023



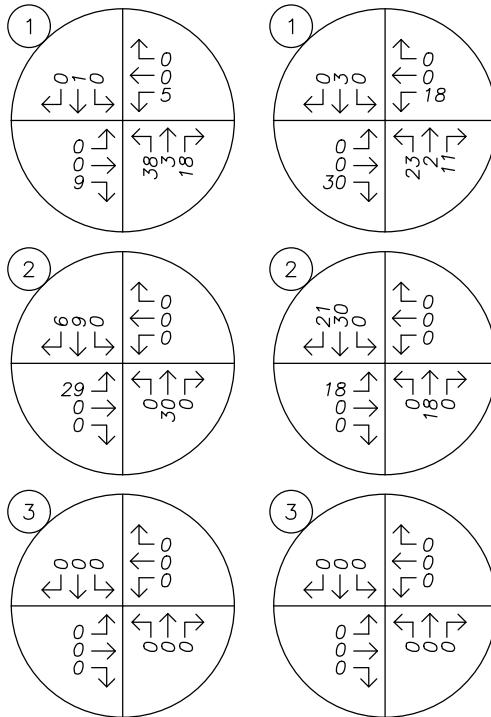
| 5-Min Count Period Beginning At | SE Norton Ln (Northbound) |      |       |   | SE Norton Ln (Southbound) |      |       |   | SE Stratus Ave (south) (Eastbound) |      |       |   | SE Stratus Ave (south) (Westbound) |      |       |   | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|------------------------------------|------|-------|---|------------------------------------|------|-------|---|-------|---------------|
|                                 | Left                      | Thru | Right | U | Left                      | Thru | Right | U | Left                               | Thru | Right | U | Left                               | Thru | Right | U |       |               |
| 4:00 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 3     | 0 | 3     |               |
| 4:05 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 1     |               |
| 4:10 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 5     | 0 | 5     |               |
| 4:15 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 1     |               |
| 4:20 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 3     | 0 | 3     |               |
| 4:25 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 4     | 0 | 4     |               |
| 4:30 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 6     | 0 | 6     |               |
| 4:35 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 5     | 0 | 6     |               |
| 4:40 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     |               |
| 4:45 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 4     | 0 | 4     |               |
| 4:50 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 4     | 0 | 4     |               |
| 4:55 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 3     | 0 | 3     | 40            |
| 5:00 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 3     | 40            |
| 5:05 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 3     | 0 | 3     | 42            |
| 5:10 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 3     | 40            |
| 5:15 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 5     | 0 | 6     | 45            |
| 5:20 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 5     | 0 | 6     | 48            |
| 5:25 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 2     | 46            |
| 5:30 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 3     | 43            |
| 5:35 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 2     | 39            |
| 5:40 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 1     | 0 | 1     | 40            |
| 5:45 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 2     | 38            |
| 5:50 PM                         | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 2     | 36            |
| 5:55 PM                         | 0                         | 0    | 0     | 0 | 1                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 2     | 0 | 3     | 36            |
| Peak 15-Min Flowrates           | Northbound                |      |       |   | Southbound                |      |       |   | Eastbound                          |      |       |   | Westbound                          |      |       |   | Total |               |
|                                 | Left                      | Thru | Right | U | Left                      | Thru | Right | U | Left                               | Thru | Right | U | Left                               | Thru | Right | U |       |               |
| All Vehicles                    | 0                         | 0    | 0     | 0 | 4                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 60    | 0 | 64    |               |
| Heavy Trucks                    | 0                         | 0    | 0     | 0 | 0                         | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0                                  | 0    | 0     | 0 | 0     |               |
| Buses                           |                           |      |       |   |                           |      |       |   |                                    |      |       |   |                                    |      |       |   |       |               |
| Pedestrians                     |                           | 0    |       |   |                           | 0    |       |   |                                    | 0    |       |   |                                    | 0    |       |   | 0     |               |
| Bicycles                        | 0                         | 0    | 0     |   | 0                         | 0    | 0     |   | 0                                  | 0    | 0     |   | 0                                  | 0    | 0     |   | 0     |               |
| Scooters                        |                           |      |       |   |                           |      |       |   |                                    |      |       |   |                                    |      |       |   |       |               |

*Comments:*



AM PEAK HOUR

PM PEAK HOUR



no scale

## Appendix D

Crash History Data

Left-turn Lane Warrants

Preliminary Traffic Signal Warrants





039: SALMON RIVER

Highway 039 ALL ROAD TYPES, MP 46.56 to 46.83 01/01/2017 to 12/31/2021, Both Add and Non-Add mileage

1 - 26 of 26 Crash records shown.

OR<25

|       |             |            |              |       |    |                 |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       |       |
|-------|-------------|------------|--------------|-------|----|-----------------|-------|--------|------------|-----|----------|-----------|-----------|---|--------|------|------|----|-----|-------|---------|-----|-----|-----|-------|-------|
| 00713 | N N N N     | 07/18/2018 | YAMHILL      | 1     | 14 | STRGHT          |       | N      | N          | CLR | S-STRGHT | 01        | NONE      | 9 | STRGHT |      |      |    |     | 29    |         |     |     |     |       |       |
| NONE  |             | WE         | MCMINNVILLE  | MN    | 0  | SALMON RIVER HY | W     | (NONE) | UNKNOWN    | N   | DRY      | REAR      | N/A       |   | W -E   |      |      |    | 000 | 00    |         |     |     |     |       |       |
| N     |             | 4P         | MCMINVL UA   | 46.67 |    | NE NORTON LN    | 03    |        |            | N   | DAY      | PDO       | PSNGR CAR |   | 01     | DRVR | NONE | 00 | Unk | UNK   | 000     | 000 | 00  |     |       |       |
| N     |             | 45 12 3.3  | -123 10 1.25 |       |    | 003900100S00    |       | (04)   |            |     |          |           |           |   |        |      |      |    |     |       |         |     | UNK |     |       |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | 02        | NONE      | 9 | STRGHT |      |      |    |     |       |         |     |     |     |       |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | N/A       |           |   | W -E   |      |      |    |     |       |         |     |     | 000 | 00    |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | PSNGR CAR |           |   | 01     | DRVR | NONE | 00 | Unk | UNK   | 000     | 000 |     |     | 00    |       |
|       |             |            |              |       |    |                 |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     | UNK   |       |
| 00535 | N N N N     | 07/24/2020 | YAMHILL      | 1     | 14 | STRGHT          |       | Y      | N          | CLR | S-1STOP  | 01        | NONE      | 9 | STRGHT |      |      |    |     |       |         |     |     |     | 07    |       |
| NONE  |             | FR         | MCMINNVILLE  | MN    | 0  | SALMON RIVER HY | W     | (NONE) | TRF SIGNAL | N   | DRY      | REAR      | N/A       |   | W -E   |      |      |    |     |       |         |     |     |     | 088   | 00    |
| N     |             | 6P         | MCMINVL UA   | 46.67 |    | NE NORTON LN    | 03    |        |            | N   | DAY      | PDO       | PSNGR CAR |   | 01     | DRVR | NONE | 00 | Unk | UNK   | 000     | 000 |     |     | 00    |       |
| N     |             | 45 12 3.35 | -123 10 1.24 |       |    | 003900100S00    |       | (04)   |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       | UNK   |
|       |             |            |              |       |    |                 |       |        |            |     |          | 02        | NONE      | 9 | STOP   |      |      |    |     |       |         |     |     |     |       |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | N/A       |           |   | W -E   |      |      |    |     |       |         |     |     |     | 011   | 00    |
|       |             |            |              |       |    |                 |       |        |            |     |          | PSNGR CAR |           |   | 01     | DRVR | NONE | 00 | Unk | UNK   | 000     | 000 |     |     | 00    |       |
|       |             |            |              |       |    |                 |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       | UNK   |
| 00569 | N N N N     | 06/16/2019 | YAMHILL      | 1     | 14 | INTER           | CROSS | N      | N          | CLR | S-1STOP  | 01        | NONE      |   | STRGHT |      |      |    |     |       |         |     |     |     | 07,29 |       |
| STATE |             | SU         | MCMINNVILLE  | MN    | 0  | NE NORTON LN    | W     |        | TRF SIGNAL | N   | DRY      | REAR      | PRVTE     |   | W -E   |      |      |    |     |       |         |     |     |     | 000   | 00    |
| N     |             | 6P         | MCMINVL UA   | 46.67 |    | SALMON RIVER HY | 04    | 1      |            | N   | DAY      | INJ       | PSNGR CAR |   | 01     | DRVR | INJC | 41 | F   | OTH-Y | 026,043 | 000 |     |     | 07    |       |
| N     |             | 45 12 3.37 | -123 10 1.27 |       |    | 003900100S00    |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       | N-RES |
|       |             |            |              |       |    |                 |       |        |            |     |          | 01        | NONE      |   | STRGHT |      |      |    |     |       |         |     |     |     |       |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | PRVTE     |           |   | W -E   |      |      |    |     |       |         |     |     |     | 000   | 00    |
|       |             |            |              |       |    |                 |       |        |            |     |          | PSNGR CAR |           |   | 02     | PSNG | INJC | 13 | F   |       | 000     | 000 |     |     | 00    |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | 02        | NONE      | 0 | STOP   |      |      |    |     |       |         |     |     |     |       |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | PRVTE     |           |   | W -E   |      |      |    |     |       |         |     |     |     | 011   | 00    |
|       |             |            |              |       |    |                 |       |        |            |     |          | PSNGR CAR |           |   | 01     | DRVR | INJC | 58 | M   | OTH-Y | 000     | 000 |     |     | 00    |       |
|       |             |            |              |       |    |                 |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       | N-RES |
| 00522 | N N N N N N | 06/17/2021 | YAMHILL      | 1     | 14 | INTER           | CROSS | N      | N          | CLR | S-1STOP  | 01        | NONE      | 0 | STRGHT |      |      |    |     |       |         |     |     |     | 27,29 |       |
| CITY  |             | TH         | MCMINNVILLE  | MN    | 0  | NE NORTON LN    | N     |        | TRF SIGNAL | N   | DRY      | REAR      | PRVTE     |   | N -S   |      |      |    |     |       |         |     |     |     | 000   | 00    |
| N     |             | 1P         | MCMINVL UA   | 46.69 |    | SALMON RIVER HY | 06    | 0      |            | N   | DAY      | INJ       | PSNGR CAR |   | 01     | DRVR | NONE | 23 | M   | OR-Y  | 016,043 | 038 |     |     | 27,29 |       |
| N     |             | 45 12 3.32 | -123 9 59.72 |       |    | 003900100S00    |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       | OR<25 |
|       |             |            |              |       |    |                 |       |        |            |     |          | 02        | NONE      | 0 | STOP   |      |      |    |     |       |         |     |     |     |       |       |
|       |             |            |              |       |    |                 |       |        |            |     |          | PRVTE     |           |   | N -S   |      |      |    |     |       |         |     |     |     | 011   | 00    |
|       |             |            |              |       |    |                 |       |        |            |     |          | PSNGR CAR |           |   | 01     | DRVR | INJB | 61 | M   | OR-Y  | 000     | 000 |     |     | 00    |       |
|       |             |            |              |       |    |                 |       |        |            |     |          |           |           |   |        |      |      |    |     |       |         |     |     |     |       | OR<25 |



039: SALMON RIVER

Highway 039 ALL ROAD TYPES, MP 46.56 to 46.83 01/01/2017 to 12/31/2021, Both Add and Non-Add mileage

1 - 26 of 26 Crash records shown.

| ID    | Sequence    | Date       | Location     | MP           | Side  | Direction       | Crash Type | Weather    | Time | Severity | Vehicle  | Other     | Count | Notes        | Count     | Notes                |
|-------|-------------|------------|--------------|--------------|-------|-----------------|------------|------------|------|----------|----------|-----------|-------|--------------|-----------|----------------------|
| 00364 | N N Y Y N N | 05/05/2021 | YAMHILL      | 1 14         |       | INTER           | CROSS      | N          | N    | CLR      | S-1STOP  | 01 NONE   | 0     | STRGHT       |           | 32,29,16             |
|       |             |            | CITY WE      | MCMINNVILLE  | MN 0  | NE NORTON LN    | E          | TRF SIGNAL | N    | DRY      | REAR     | PRVTE     |       | E -W         |           | 000 00               |
|       |             |            | N 9A         | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06         | 0          | N    | DAY      | INJ      | PSNGR CAR |       | 01 DRVR NONE | 44 M OR-Y | 052,026 025 32,29,16 |
|       |             |            | N 45 12 3.32 | -123 9 59.72 |       | 003900100S00    |            |            |      |          |          |           |       |              |           | OR>25                |
|       |             |            |              |              |       |                 |            |            |      |          |          | 02 NONE   | 0     | STOP         |           |                      |
|       |             |            |              |              |       |                 |            |            |      |          |          | PRVTE     |       | E -W         |           | 011 00               |
|       |             |            |              |              |       |                 |            |            |      |          |          | PSNGR CAR |       | 01 DRVR INJC | 68 F OR-Y | 000 000 00           |
|       |             |            |              |              |       |                 |            |            |      |          |          |           |       |              |           | OR<25                |
| 00415 | N N N N N N | 04/22/2018 | YAMHILL      | 1 14         |       | INTER           | CROSS      | N          | N    | CLR      | S-OTHER  | 01 NONE   | 0     | TURN-L       |           | 13                   |
|       |             |            | CITY SU      | MCMINNVILLE  | MN 0  | NE NORTON LN    | W          | TRF SIGNAL | N    | DRY      | TURN     | PRVTE     |       | S -W         |           | 000 00               |
|       |             |            | N 7P         | MCMINVL UA   | 46.69 | SALMON RIVER HY | 05         | 0          | N    | DUSK     | INJ      | PSNGR CAR |       | 01 DRVR INJC | 30 F OR-Y | 000 000 00           |
|       |             |            | N 45 12 3.32 | -123 9 59.72 |       | 003900100S00    |            |            |      |          |          |           |       |              |           | OR>25                |
|       |             |            |              |              |       |                 |            |            |      |          |          | 02 NONE   | 0     | TURN-L       |           |                      |
|       |             |            |              |              |       |                 |            |            |      |          |          | PRVTE     |       | S -W         |           | 000 00               |
|       |             |            |              |              |       |                 |            |            |      |          |          | PSNGR CAR |       | 01 DRVR NONE | 61 F OR-Y | 045 000 13           |
|       |             |            |              |              |       |                 |            |            |      |          |          |           |       |              |           | OR<25                |
| 00076 | N N N N N N | 01/18/2018 | YAMHILL      | 1 14         |       | INTER           | CROSS      | N          | N    | RAIN     | S-STRGHT | 01 NONE   | 0     | STRGHT       |           | 10                   |
|       |             |            | STATE TH     | MCMINNVILLE  | MN 0  | NE NORTON LN    | W          | UNKNOWN    | N    | WET      | SS-O     | PRVTE     |       | W -E         |           | 006 00               |
|       |             |            | N 7A         | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06         | 0          | N    | DAY      | INJ      | PSNGR CAR |       | 01 DRVR INJC | 39 F NONE | 080 017 10           |
|       |             |            | N 45 12 3.32 | -123 9 59.72 |       | 003900100S00    |            |            |      |          |          |           |       |              |           | OR<25                |
|       |             |            |              |              |       |                 |            |            |      |          |          | 02 NONE   | 1     | STRGHT       |           |                      |
|       |             |            |              |              |       |                 |            |            |      |          |          | PRVTE     |       | W -E         |           | 000 00               |
|       |             |            |              |              |       |                 |            |            |      |          |          | SEMI TOW  |       | 01 DRVR NONE | 68 M OR-Y | 000 000 00           |
|       |             |            |              |              |       |                 |            |            |      |          |          |           |       |              |           | OR<25                |
| 00517 | N N N N N N | 07/16/2020 | YAMHILL      | 1 14         |       | INTER           | CROSS      | N          | N    | CLR      | S-1STOP  | 01 NONE   | 0     | STRGHT       |           | 27,07                |
|       |             |            | CITY TH      | MCMINNVILLE  | MN 0  | NE NORTON LN    | W          | TRF SIGNAL | N    | DRY      | REAR     | PRVTE     |       | W -E         |           | 000 00               |
|       |             |            | N 11A        | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06         | 0          | N    | DAY      | INJ      | PSNGR CAR |       | 01 DRVR INJB | 57 M OR-Y | 016,043 038 27,07    |
|       |             |            | N 45 12 3.32 | -123 9 59.72 |       | 003900100S00    |            |            |      |          |          |           |       |              |           | OR<25                |
|       |             |            |              |              |       |                 |            |            |      |          |          | 02 NONE   | 0     | STOP         |           |                      |
|       |             |            |              |              |       |                 |            |            |      |          |          | PRVTE     |       | W -E         |           | 011 00               |
|       |             |            |              |              |       |                 |            |            |      |          |          | PSNGR CAR |       | 01 DRVR NONE | 68 F OR-Y | 000 000 00           |
|       |             |            |              |              |       |                 |            |            |      |          |          |           |       |              |           | OR>25                |
|       |             |            |              |              |       |                 |            |            |      |          |          | 02 NONE   | 0     | STOP         |           |                      |
|       |             |            |              |              |       |                 |            |            |      |          |          | PRVTE     |       | W -E         |           | 011 00               |
|       |             |            |              |              |       |                 |            |            |      |          |          | PSNGR CAR |       | 02 PSNG INJA | 68 F      | 000 000 00           |
|       |             |            |              |              |       |                 |            |            |      |          |          |           |       |              |           | OR<25                |
|       |             |            |              |              |       |                 |            |            |      |          |          | 03 NONE   | 0     | STRGHT       |           |                      |

039: SALMON RIVER

Highway 039 ALL ROAD TYPES, MP 46.56 to 46.83 01/01/2017 to 12/31/2021, Both Add and Non-Add mileage

1 - 26 of 26 Crash records shown.

| Case No | Y | N | N | N | N | N | Date       | Locality | MN | 0          | NE           | W     | CROSS           | TRF | SIGNAL    | N   | WET      | REAR | PSNGR  | CAR | 01     | DRVR | NONE     | 58   | M  | OR-Y | 000  | 022 | 00  |     |     |     |             |     |          |            |  |
|---------|---|---|---|---|---|---|------------|----------|----|------------|--------------|-------|-----------------|-----|-----------|-----|----------|------|--------|-----|--------|------|----------|------|----|------|------|-----|-----|-----|-----|-----|-------------|-----|----------|------------|--|
| 00984   | Y | N | N | N | N | N | 12/15/2020 | YAMHILL  | 1  | 14         |              | INTER | CROSS           | N   | N         | CLD | S-1STOP  | 01   | NONE   | 9   | STRGHT |      |          |      |    |      |      |     | 00  |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            | STATE    |    | TU         | MCMINNVILLE  | MN    | 0               | NE  | NORTON LN | W   |          | TRF  | SIGNAL | N   | WET    | REAR | N/A      |      |    |      |      |     | 088 | 00  |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            | N        |    | 1P         | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06  | 0         |     |          | DAY  | PDO    | N   | DAY    | PDO  | PSNGR    | CAR  | 01 | DRVR | NONE | 00  | Unk | UNK | 000 | 000 | 00          |     |          |            |  |
|         |   |   |   |   |   |   |            | N        |    | 45 12 3.33 | -123 9 59.73 |       | 003900100S00    |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
| 00888   | N | N | N | N | N | N | 09/14/2021 | YAMHILL  | 1  | 14         |              | INTER | CROSS           | N   | N         | CLR | S-1STOP  | 01   | UNKN   | 0   | STRGHT |      |          |      |    |      |      |     |     |     |     |     |             |     | 29       |            |  |
|         |   |   |   |   |   |   |            | NONE     |    | TU         | MCMINNVILLE  | MN    | 0               | NE  | NORTON LN | W   |          | TRF  | SIGNAL | N   | DRY    | REAR | UNKN     |      |    |      |      |     |     |     |     |     |             | 000 | 00       |            |  |
|         |   |   |   |   |   |   |            | N        |    | 10A        | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06  | 0         |     |          | DAY  | INJ    | N   | DAY    | INJ  | UNKN     | UNKN | 01 | DRVR | NONE | 00  | F   | UNK | 026 | 000 | 29          |     |          |            |  |
|         |   |   |   |   |   |   |            | N        |    | 45 12 3.32 | -123 9 59.72 |       | 003900100S00    |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
| 01254   | N | N | N | N | N | N | 12/21/2021 | YAMHILL  | 1  | 14         |              | INTER | CROSS           | N   | N         | CLD | S-1STOP  | 01   | NONE   | 0   | STRGHT |      |          |      |    |      |      |     |     |     |     |     |             |     | 06,05,02 |            |  |
|         |   |   |   |   |   |   |            | CITY     |    | TU         | MCMINNVILLE  | MN    | 0               | NE  | NORTON LN | W   |          | TRF  | SIGNAL | N   | WET    | SS-O | PRVTE    |      |    |      |      |     |     |     |     |     |             | 000 | 00       |            |  |
|         |   |   |   |   |   |   |            | N        |    | 1P         | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06  | 0         |     |          | DAY  | INJ    | N   | DAY    | INJ  | MOTRHOME |      |    |      |      |     |     |     |     |     | 034,044,028 | 000 | 06,05,02 |            |  |
|         |   |   |   |   |   |   |            | N        |    | 45 12 3.32 | -123 9 59.72 |       | 003900100S00    |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
| 00642   | N | N | N | N | N | N | 07/17/2021 | YAMHILL  | 1  | 14         |              | INTER | CROSS           | N   | N         | CLR | S-1STOP  | 01   | NONE   | 9   | STRGHT |      |          |      |    |      |      |     |     |     |     |     |             |     |          | 07,29      |  |
|         |   |   |   |   |   |   |            | STATE    |    | SA         | MCMINNVILLE  | MN    | 0               | NE  | NORTON LN | W   |          | TRF  | SIGNAL | N   | DRY    | REAR | N/A      |      |    |      |      |     |     |     |     |     |             |     | 000      | 00         |  |
|         |   |   |   |   |   |   |            | N        |    | 8P         | MCMINVL UA   | 46.69 | SALMON RIVER HY | 06  | 0         |     |          | DUSK | PDO    | N   | DUSK   | PDO  | PSNGR    | CAR  | 01 | DRVR | NONE | 00  | Unk | UNK | 000 | 000 | 00          |     |          |            |  |
|         |   |   |   |   |   |   |            | N        |    | 45 12 3.32 | -123 9 59.71 |       | 003900100S00    |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
|         |   |   |   |   |   |   |            |          |    |            |              |       |                 |     |           |     |          |      |        |     |        |      |          |      |    |      |      |     |     |     |     |     |             |     |          |            |  |
| 01163   | N | N | N | N | N | N | 10/23/2017 | YAMHILL  | 1  | 14         |              | INTER | CROSS           | N   | N         | CLR | ANGL-OTH | 01   | NONE   | 0   | STRGHT |      |          |      |    |      |      |     |     |     |     |     |             |     |          | 003,013 04 |  |

039: SALMON RIVER

Highway 039 ALL ROAD TYPES, MP 46.56 to 46.83 01/01/2017 to 12/31/2021, Both Add and Non-Add mileage

1 - 26 of 26 Crash records shown.

| CITY  | MO                     | MCMINNVILLE  | MN    | 0               | NE NORTON LN    | CN           |        | TRF SIGNAL | N   | DRY  | TURN      | PRVTE      | E -W    |         |         |      |     | 000  | 00         |            |            |
|-------|------------------------|--------------|-------|-----------------|-----------------|--------------|--------|------------|-----|------|-----------|------------|---------|---------|---------|------|-----|------|------------|------------|------------|
| N     | 10A                    | MCMINVL UA   | 46.69 | SALMON RIVER HY | 01              | 0            |        | N          | DAY | INJ  | PSNGR CAR |            |         | 01 DRVR | INJB    | 17   | M   | OR-Y | 020        | 000 003 04 |            |
| N     | 45 12 3.32             | -123 9 59.72 |       |                 |                 | 003900100S00 |        |            |     |      |           |            |         |         |         |      |     |      | OR<25      |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | 02 NONE   | 0          | TURN-L  |         |         |      |     |      |            |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PRVTE     |            | N -E    |         |         |      |     |      | 000 013 00 |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PSNGR CAR |            |         | 01 DRVR | NONE    | 86   | M   | OR-Y | 000        | 000 000 00 |            |
|       |                        |              |       |                 |                 |              |        |            |     |      |           |            |         |         |         |      |     |      | OR<25      |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | 03 NONE   | 0          | STOP    |         |         |      |     |      |            |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PRVTE     |            | W -E    |         |         |      |     |      | 022 00     |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PSNGR CAR |            |         | 01 DRVR | NONE    | 49   | M   | OR-Y | 000        | 000 000 00 |            |
|       |                        |              |       |                 |                 |              |        |            |     |      |           |            |         |         |         |      |     |      | OR<25      |            |            |
| 01420 | N N N N N N 12/23/2017 | YAMHILL      | 1     | 14              |                 |              | INTER  | CROSS      | N   | N    | CLD       | O-1 L-TURN | 01 NONE | 0       | STRGHT  |      |     |      |            | 04         |            |
| STATE | SA                     | MCMINNVILLE  | MN    | 0               | NE NORTON LN    | CN           |        | UNKNOWN    | N   | DRY  | TURN      | PRVTE      | E -W    |         |         |      |     |      |            | 000 00     |            |
| N     | 6P                     | MCMINVL UA   | 46.69 | SALMON RIVER HY | 02              | 0            |        |            | N   | DLIT | INJ       | PSNGR CAR  |         |         | 01 DRVR | INJC | 69  | F    | OR-Y       | 020        | 000 000 04 |
| N     | 45 12 3.32             | -123 9 59.72 |       |                 |                 | 003900100S00 |        |            |     |      |           |            |         |         |         |      |     |      |            | OR<25      |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | 02 NONE   | 0          | TURN-L  |         |         |      |     |      |            |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PRVTE     |            | W -N    |         |         |      |     |      |            | 000 00     |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PSNGR CAR |            |         | 01 DRVR | INJA    | 57   | M   | OR-Y | 000        | 000 000 00 |            |
|       |                        |              |       |                 |                 |              |        |            |     |      |           |            |         |         |         |      |     |      |            | OR<25      |            |
| 00110 | N N N N N 02/07/2020   | YAMHILL      | 1     | 14              |                 |              | INTER  | CROSS      | N   | N    | CLR       | O-1 L-TURN | 01 NONE | 9       | STRGHT  |      |     |      |            |            | 02         |
| NONE  | FR                     | MCMINNVILLE  | MN    | 0               | NE NORTON LN    | CN           |        | TRF SIGNAL | N   | DRY  | TURN      | N/A        | S -N    |         |         |      |     |      |            |            | 000 00     |
| N     | 2P                     | MCMINVL UA   | 46.69 | SALMON RIVER HY | 04              | 0            |        |            | N   | DAY  | PDO       | PSNGR CAR  |         |         | 01 DRVR | NONE | 00  | Unk  | UNK        | 000        | 000 000 00 |
| N     | 45 12 3.33             | -123 9 59.74 |       |                 |                 | 003900100S00 |        |            |     |      |           |            |         |         |         |      |     |      |            | UNK        |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | 02 NONE   | 9          | TURN-L  |         |         |      |     |      |            |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | N/A       |            | N -E    |         |         |      |     |      |            | 088 00     |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PSNGR CAR |            |         | 01 DRVR | NONE    | 00   | Unk | UNK  | 000        | 000 000 00 |            |
|       |                        |              |       |                 |                 |              |        |            |     |      |           |            |         |         |         |      |     |      |            | UNK        |            |
| 00236 | N N Y N N N 03/26/2021 | YAMHILL      | 1     | 14              |                 |              | STRGHT |            | Y   | N    | CLR       | S-1TURN    | 01 NONE | 9       | TURN-L  |      |     |      |            |            | 08,13      |
| STATE | FR                     | MCMINNVILLE  | MN    | 0               | SALMON RIVER HY | E            | (NONE) | TRF SIGNAL | N   | DRY  | TURN      | N/A        | E -S    |         |         |      |     |      |            |            | 000 00     |
| N     | 6P                     | MCMINVL UA   | 46.70 | NE NORTON LN    | 06              |              |        |            | N   | DAY  | PDO       | PSNGR CAR  |         |         | 01 DRVR | NONE | 00  | Unk  | UNK        | 000        | 000 000 00 |
| N     | 45 12 3.32             | -123 9 58.98 |       |                 |                 | 003900100S00 |        | (04)       |     |      |           |            |         |         |         |      |     |      |            | UNK        |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | 02 NONE   | 9          | STRGHT  |         |         |      |     |      |            |            |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | N/A       |            | E -W    |         |         |      |     |      |            | 000 00     |            |
|       |                        |              |       |                 |                 |              |        |            |     |      | PSNGR CAR |            |         | 01 DRVR | NONE    | 00   | Unk | UNK  | 000        | 000 000 00 |            |
|       |                        |              |       |                 |                 |              |        |            |     |      |           |            |         |         |         |      |     |      |            | UNK        |            |
| 00232 | N N N N N N 03/18/2020 | YAMHILL      | 1     | 14              |                 |              | STRGHT |            | Y   | N    | CLR       | S-1STOP    | 01 NONE | 9       | STRGHT  |      |     |      |            |            | 07         |
| CITY  | WE                     | MCMINNVILLE  | MN    | 0               | SALMON RIVER HY | E            | (NONE) | TRF SIGNAL | N   | DRY  | REAR      | N/A        | E -W    |         |         |      |     |      |            |            | 088 00     |

039: SALMON RIVER

Highway 039 ALL ROAD TYPES, MP 46.56 to 46.83 01/01/2017 to 12/31/2021, Both Add and Non-Add mileage

1 - 26 of 26 Crash records shown.

|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      |         |       |     |          |             |     |          |
|-------|-------------|--------------|-------------|--------------|-----------------|--------|--------|------------|-----|-----------|------|-----------|--------|------|---------|-------|-----|----------|-------------|-----|----------|
| N     | 4P          | MCMINVL UA   | 46.71       | NE NORTON LN | 06              |        | N      | DAY        | PDO | PSNGR CAR | 01   | DRVR      | NONE   | 00   | Unk UNK | 000   | 000 | 00       |             |     |          |
| N     | 45 12 3.35  | -123 9 58.29 |             | 003900100S00 | (04)            |        |        |            |     |           |      |           |        |      | UNK     |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | 02        | NONE | 9         | STOP   |      |         |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | N/A       |      |           | E -W   |      |         |       | 011 | 00       |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PSNGR CAR | 01   | DRVR      | NONE   | 00   | Unk UNK | 000   | 000 | 00       |             |     |          |
|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      | UNK     |       |     |          |             |     |          |
| 00546 | Y Y Y N N N | 07/29/2020   | YAMHILL     | 1 14         |                 | STRGHT | Y      | N          | CLR | S-1STOP   | 01   | NONE      |        |      | STRGHT  |       | 013 | 01,29,10 |             |     |          |
| CITY  | WE          |              | MCMINNVILLE | MN 0         | SALMON RIVER HY | E      | (NONE) | TRF SIGNAL | N   | DRY       | REAR | PRVTE     |        |      | E -W    |       | 000 | 00       |             |     |          |
| N     | 5P          |              | MCMINVL UA  | 46.72        | NE NORTON LN    | 05     |        |            | N   | DAY       | INJ  | PSNGR CAR | 01     | DRVR | NONE    | 33    | M   | OR-Y     | 026         | 000 | 01,29,10 |
| N     | 45 12 3.39  | -123 9 57.57 |             | 003900100S00 | (04)            |        |        |            |     |           |      |           |        |      | OR>25   |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | 02        | NONE |           | STOP   |      |         |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PRVTE     |      |           | E -W   |      |         |       | 011 | 013      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     | PSNGR CAR | 01   | DRVR      | NONE   | 60   | M       | OTH-Y | 000 | 000      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      | N-RES   |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | 03        | NONE |           | STOP   |      |         |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PRVTE     |      |           | E -W   |      |         |       | 011 | 013      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     | PSNGR CAR | 01   | DRVR      | INJC   | 21   | M       | OR-Y  | 000 | 000      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      | OR<25   |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | 04        | NONE |           | STOP   |      |         |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PRVTE     |      |           | E -W   |      |         |       | 011 | 00       |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PSNGR CAR | 01   | DRVR      | NONE   | 26   | F       | OR-Y  | 000 | 000      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      | OR<25   |       |     |          |             |     |          |
| 00372 | N N N N     | 04/24/2019   | YAMHILL     | 1 14         |                 | STRGHT | N      | N          | CLR | O-STRGHT  | 01   | NONE      |        |      | STRGHT  |       |     | 05,15,33 |             |     |          |
| CITY  | WE          |              | MCMINNVILLE | MN 0         | NE NORTON LN    | E      | (NONE) | UNKNOWN    | N   | DRY       | SS-M | PRVTE     |        |      | E -W    |       | 000 | 00       |             |     |          |
| Y     | 6A          |              | MCMINVL UA  | 46.73        | SALMON RIVER HY | 04     |        |            | N   | DAY       | INJ  | PSNGR CAR | 01     | DRVR | NONE    | 44    | M   | SUSP     | 044,051,080 | 017 | 05,15,33 |
| N     | 45 12 3.34  | -123 9 56.78 |             | 003900100S00 | (04)            |        |        |            |     |           |      |           |        |      | OR<25   |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | 02        | NONE |           | STRGHT |      |         |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PRVTE     |      |           | W -E   |      |         |       | 000 | 00       |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PSNGR CAR | 01   | DRVR      | INJB   | 44   | M       | OR-Y  | 000 | 000      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      | OR<25   |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | 03        | NONE |           | STRGHT |      |         |       |     |          |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PRVTE     |      |           | W -E   |      |         |       | 000 | 00       |             |     |          |
|       |             |              |             |              |                 |        |        |            |     | PSNGR CAR | 01   | DRVR      | INJC   | 29   | M       | OR-Y  | 000 | 000      | 00          |     |          |
|       |             |              |             |              |                 |        |        |            |     |           |      |           |        |      | OR<25   |       |     |          |             |     |          |
| 00546 | N N N N N N | 06/22/2021   | YAMHILL     | 1 14         |                 | STRGHT | N      | N          | CLR | S-1STOP   | 01   | NONE      | 0      |      | STRGHT  |       | 013 | 27,29    |             |     |          |
| CITY  | TU          |              | MCMINNVILLE | MN 0         | SALMON RIVER HY | E      | (NONE) | UNKNOWN    | N   | DRY       | REAR | PRVTE     |        |      | E -W    |       | 000 | 00       |             |     |          |
| N     | 6P          |              | MCMINVL UA  | 46.79        | NE NORTON LN    | 06     |        |            | N   | DAY       | INJ  | PSNGR CAR | 01     | DRVR | INJC    | 48    | M   | OR-Y     | 016,026     | 038 | 27,29    |
| N     | 45 12 3.34  | -123 9 52.38 |             | 003900100S00 | (04)            |        |        |            |     |           |      |           |        |      | OR<25   |       |     |          |             |     |          |

039: SALMON RIVER

Highway 039 ALL ROAD TYPES, MP 46.56 to 46.83 01/01/2017 to 12/31/2021, Both Add and Non-Add mileage

1 - 26 of 26 Crash records shown.

|    |       |     |  |  |  |         |      |    |   |      |         |        |
|----|-------|-----|--|--|--|---------|------|----|---|------|---------|--------|
| 02 | NONE  | 0   |  |  |  |         | STOP |    |   |      |         |        |
|    | PRVTE |     |  |  |  |         | E -W |    |   |      | 011 013 | 00     |
|    | PSNGR | CAR |  |  |  | 01 DRVR | INJC | 43 | M | OR-Y | 000     | 000 00 |
|    |       |     |  |  |  |         |      |    |   |      |         |        |
|    |       |     |  |  |  |         |      |    |   |      | OR<25   |        |
| 03 | NONE  | 0   |  |  |  |         | STOP |    |   |      |         |        |
|    | PRVTE |     |  |  |  |         | E -W |    |   |      | 022     | 00     |
|    | PSNGR | CAR |  |  |  | 01 DRVR | INJB | 43 | F | OR-Y | 000     | 022 00 |
|    |       |     |  |  |  |         |      |    |   |      |         |        |
|    |       |     |  |  |  |         |      |    |   |      | OR<25   |        |

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CITY OF MCMINNVILLE, YAMHILL COUNTY

**NORTON LN and Intersectional Crashes at NORTON LN, City of McMinnville, Yamhill County, 01/01/2017 to 12/31/2021**

1 - 22 of 22 Crash records shown.

| SER#  | S<br>P | D<br>R | M<br>J | W<br>S | DATE       | CLASS            | CITY STREET     | RD CHAR | INT-TYPE | SPCL USE | INVEST   | RD DPT   | UNLOC? | E<br>D    | A<br>C | U<br>S | I<br>V    | C<br>L | O<br>K | DAY  | TIME  | FROM   | SECOND STREET | DIRECT | LEGS  | TRAF- | RNDBT | SURF  | COLL | OWNER | FROM | PRTC | INJ | G<br>E | LICNS | PED | CAUSE |
|-------|--------|--------|--------|--------|------------|------------------|-----------------|---------|----------|----------|----------|----------|--------|-----------|--------|--------|-----------|--------|--------|------|-------|--------|---------------|--------|-------|-------|-------|-------|------|-------|------|------|-----|--------|-------|-----|-------|
|       | D      | C      | S      | V      | L          | K                | LAT             | LONG    | LRS      | LOCTN    | (#LANES) | CONTL    | DRVWY  | LIGHT     | SVRTY  | V#     | TYPE      | TO     | P#     | TYPE | SVRTY | E<br>X | RES           | LOC    | ERROR | ACT   | EVENT | CAUSE |      |       |      |      |     |        |       |     |       |
| 00635 | N      | N      | N      | N      | 06/15/2018 | 14               | NE NORTON LN    | STRGHT  | N        | N        | CLR      | S-STRGHT | 01     | NONE      | 9      | STRGHT |           |        |        |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       |     | 07,29 |
| NONE  |        |        |        |        | FR         |                  | SALMON RIVER HY | W       | (NONE)   | UNKNOWN  | N        | DRY      | REAR   | N/A       | W -E   |        |           |        |        |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       | 000 | 00    |
| N     |        |        |        |        | 3P         |                  |                 | 04      |          |          | N        | DAY      | PDO    | PSNGR CAR |        |        |           |        | 01     | DRVR | NONE  | 00     | Unk           | UNK    | 000   |       |       |       |      |       |      |      |     |        |       | 000 | 00    |
| N     |        |        |        |        | 45 12 3.3  | -123 10<br>15.55 | 003900100800    |         | (04)     |          |          |          |        |           |        |        |           |        |        |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       | UNK |       |
|       |        |        |        |        |            |                  |                 |         |          |          |          |          |        |           |        | 02     | NONE      | 9      | STRGHT |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       |     |       |
|       |        |        |        |        |            |                  |                 |         |          |          |          |          |        |           |        |        | N/A       | W -E   |        |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       | 006 | 00    |
|       |        |        |        |        |            |                  |                 |         |          |          |          |          |        |           |        |        | PSNGR CAR |        |        |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       | 000 | 000   |
|       |        |        |        |        |            |                  |                 |         |          |          |          |          |        |           |        |        |           |        |        |      |       |        |               |        |       |       |       |       |      |       |      |      |     |        |       | UNK |       |

CITY OF MCMINNVILLE, YAMHILL COUNTY

**STRATUS RD and NORTON LN, City of McMinnville, Yamhill County, 01/01/2017 to 12/31/2021**

1 - 1 of 1 Crash records shown.

| SER#   | S P R J S W | D M   | DATE       | CLASS         | CITY STREET  | INT-TYPE | SPCL USE |       |       |         |          |       |      |        |      |       |     |     |     |       |     |       |       |       |
|--------|-------------|-------|------------|---------------|--------------|----------|----------|-------|-------|---------|----------|-------|------|--------|------|-------|-----|-----|-----|-------|-----|-------|-------|-------|
| INVEST | E A U I C O | DAY   | DIST       | FIRST STREET  | RD CHAR      | (MEDIAN) | INT-REL  | OFFRD | WTHR  | CRASH   | TRLR QTY | MOVE  | A S  |        |      |       |     |     |     |       |     |       |       |       |
| RD DPT | E L G N H R | TIME  | FROM       | SECOND STREET | DIRECT       | LEGS     | TRAF-    | RNDBT | SURF  | COLL    | OWNER    | FROM  | PRTC | INJ    | G E  | LICNS | PED |     |     |       |     |       |       |       |
| UNLOC? | D C S V L K | LAT   | LONG       | LRS           | LOCTN        | (#LANES) | CONTL    | DRVWY | LIGHT | SVRTY   | V#       | TYPE  | TO   | P#     | TYPE | SVRTY | E X | RES | LOC | ERROR | ACT | EVENT | CAUSE |       |
| 00171  | N N N N N   |       | 02/21/2019 | 19            | SE NORTON LN | ALLEY    | N        | N     | CLR   | PRKD MV | 01       | NONE  | 9    | TURN-R |      |       |     |     |     |       |     |       |       | 08,10 |
| NONE   |             | TH    | 170        | SE STRATUS RD | S            | (NONE)   | UNKNOWN  | N     | DRY   | SS-M    | N/A      | W     | -S   |        |      |       |     |     |     |       |     |       | 018   | 00    |
| Y      |             | 9P    |            |               | 08           |          |          | N     | DLIT  | PDO     |          | PSNGR | CAR  | 01     | DRVR | NONE  | 00  | Unk | UNK |       | 000 |       | 000   | 00    |
| N      |             | 45 11 | 58.75      | -123 10       | .48          |          | (02)     |       |       |         |          |       |      |        |      |       |     |     |     |       |     |       |       | UNK   |

## Left-Turn Lane Warrant Analysis



Project: 23087 - Norton Lane Apt  
 Intersection: SE Stratus Avenue at SE Norton Lane (South Intersection)  
 Date: 8/9/2023  
 Scenario: 2025 Buildout Conditions - AM Peak Hour (NB)

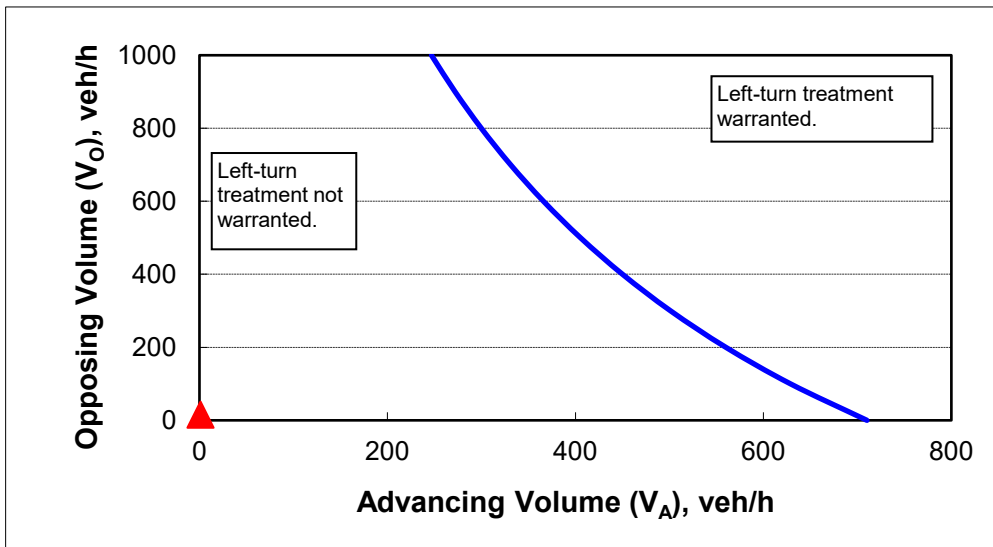
### 2-lane roadway (English)

#### INPUT

| Variable  | Value |
|---|-------|
| 85 <sup>th</sup> percentile speed, mph:                 | 25    |
| Percent of left-turns in advancing volume ( $V_A$ ), %: | 91%   |
| Advancing volume ( $V_A$ ), veh/h:                      | 1     |
| Opposing volume ( $V_O$ ), veh/h:                       | 16    |

#### OUTPUT

| Variable   | Value |
|--|-------|
| Limiting advancing volume ( $V_A$ ), veh/h:                              | 696   |
| <b>Guidance for determining the need for a major-road left-turn bay:</b> |       |
| <b>Left-turn treatment NOT warranted.</b>                                |       |



#### CALIBRATION CONSTANTS

| Variable   | Value |
|--|-------|
| Average time for making left-turn, s:                              | 3.0   |
| Critical headway, s:   | 5.0   |
| Average time for left-turn vehicle to clear the advancing lane, s: | 1.9   |



## Left-Turn Lane Warrant Analysis



Project: 23087 - Norton Lane Apt  
 Intersection: SE Stratus Avenue at SE Norton Lane (South Intersection)  
 Date: 8/9/2023  
 Scenario: 2025 Buildout Conditions - AM Peak Hour (NB)

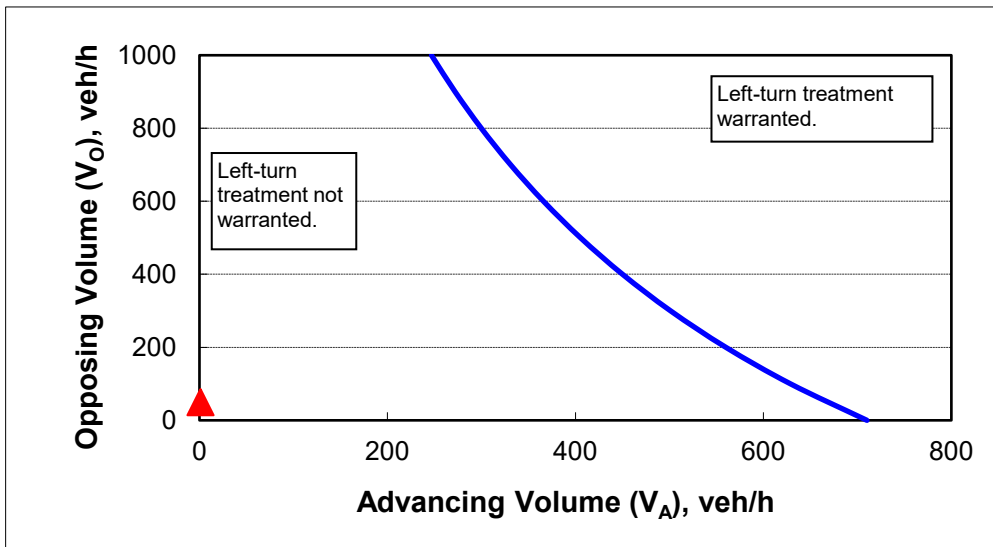
### 2-lane roadway (English)

#### INPUT

| Variable  | Value |
|---|-------|
| 85 <sup>th</sup> percentile speed, mph:                 | 25    |
| Percent of left-turns in advancing volume ( $V_A$ ), %: | 91%   |
| Advancing volume ( $V_A$ ), veh/h:                      | 1     |
| Opposing volume ( $V_O$ ), veh/h:                       | 50    |

#### OUTPUT

| Variable   | Value |
|--|-------|
| Limiting advancing volume ( $V_A$ ), veh/h:                              | 668   |
| <b>Guidance for determining the need for a major-road left-turn bay:</b> |       |
| <b>Left-turn treatment NOT warranted.</b>                                |       |



#### CALIBRATION CONSTANTS

| Variable   | Value |
|--|-------|
| Average time for making left-turn, s:                              | 3.0   |
| Critical headway, s:   | 5.0   |
| Average time for left-turn vehicle to clear the advancing lane, s: | 1.9   |

## Left-Turn Lane Warrant Analysis



Project: 23087 - Norton Lane Apt  
 Intersection: SE Stratus Avenue at SE Norton Lane (South Intersection)  
 Date: 8/9/2023  
 Scenario: 2025 Buildout Conditions - AM Peak Hour (SB)

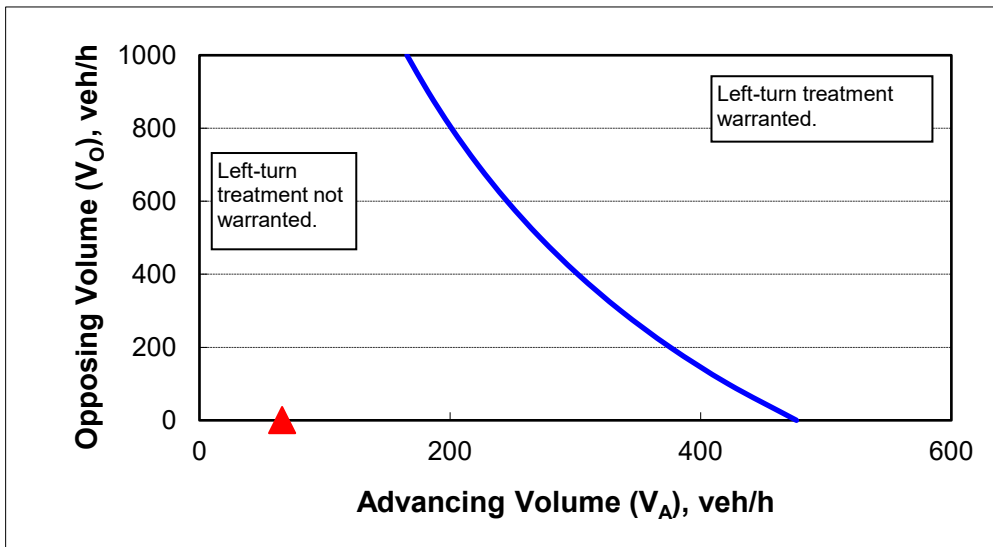
### 2-lane roadway (English)

#### INPUT

| Variable  | Value |
|---|-------|
| 85 <sup>th</sup> percentile speed, mph:                 | 25    |
| Percent of left-turns in advancing volume ( $V_A$ ), %: | 76%   |
| Advancing volume ( $V_A$ ), veh/h:                      | 66    |
| Opposing volume ( $V_O$ ), veh/h:                       | 1     |

#### OUTPUT

| Variable   | Value |
|--|-------|
| Limiting advancing volume ( $V_A$ ), veh/h:                              | 476   |
| <b>Guidance for determining the need for a major-road left-turn bay:</b> |       |
| <b>Left-turn treatment NOT warranted.</b>                                |       |



#### CALIBRATION CONSTANTS

| Variable   | Value |
|--|-------|
| Average time for making left-turn, s:                              | 3.0   |
| Critical headway, s:   | 5.0   |
| Average time for left-turn vehicle to clear the advancing lane, s: | 1.9   |

## Left-Turn Lane Warrant Analysis



Project: 23087 - Norton Lane Apt  
 Intersection: SE Stratus Avenue at SE Norton Lane (South Intersection)  
 Date: 8/9/2023  
 Scenario: 2025 Buildout Conditions - AM Peak Hour (SB)

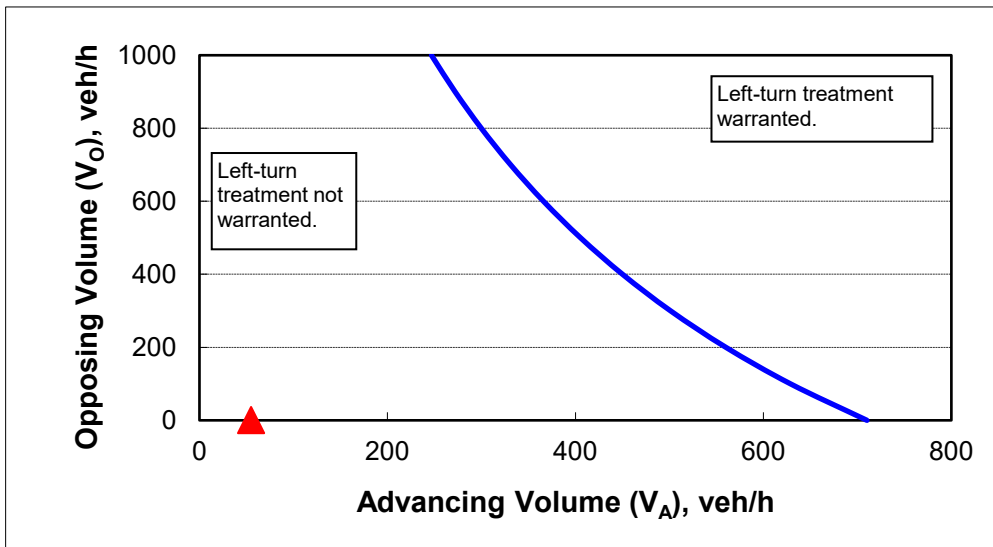
### 2-lane roadway (English)

#### INPUT

| Variable  | Value |
|---|-------|
| 85 <sup>th</sup> percentile speed, mph:                 | 25    |
| Percent of left-turns in advancing volume ( $V_A$ ), %: | 9%    |
| Advancing volume ( $V_A$ ), veh/h:                      | 55    |
| Opposing volume ( $V_O$ ), veh/h:                       | 1     |

#### OUTPUT

| Variable   | Value |
|--|-------|
| Limiting advancing volume ( $V_A$ ), veh/h:                              | 709   |
| <b>Guidance for determining the need for a major-road left-turn bay:</b> |       |
| <b>Left-turn treatment NOT warranted.</b>                                |       |



#### CALIBRATION CONSTANTS

| Variable   | Value |
|--|-------|
| Average time for making left-turn, s:                              | 3.0   |
| Critical headway, s:   | 5.0   |
| Average time for left-turn vehicle to clear the advancing lane, s: | 1.9   |

# Traffic Signal Warrant Analysis



Project: 23087 - Norton Lane Apartments  
 Date: 8/8/2023  
 Scenario: Year 2025 Buildout Conditions

|                       |                |                       |                            |
|-----------------------|----------------|-----------------------|----------------------------|
| Major Street:         | SE Norton Lane | Minor Street:         | SE Stratus Ave (North Int) |
| Number of Lanes:      | 1              | Number of Lanes:      | 1                          |
| PM Peak Hour Volumes: | 334            | PM Peak Hour Volumes: | 61                         |

**Warrant Used:**

- 100 percent of standard warrants used
- 70 percent of standard warrants used due to 85th percentile speed in excess of 40 mph or isolated community with population less than 10,000.

| Number of Lanes for Moving Traffic on Each Approach: |                  | ADT on Major St. (total of both approaches) |                 | ADT on Minor St. (higher-volume approach) |                 |
|--|------------------|---|-----------------|---|-----------------|
| <u>Major St.</u>                                     | <u>Minor St.</u> | <u>Warrants</u>                             | <u>Warrants</u> | <u>Warrants</u>                           | <u>Warrants</u> |
| <b>WARRANT 1, CONDITION A</b>                        |                  |   |                 |   |                 |
| 1  | 1                | 8,850                                       | 6,200           | 2,650                                     | 1,850           |
| 2 or more  | 1                | 10,600                                      | 7,400           | 2,650                                     | 1,850           |
| 2 or more  | 2 or more        | 10,600                                      | 7,400           | 3,550                                     | 2,500           |
| 1  | 2 or more        | 8,850                                       | 6,200           | 3,550                                     | 2,500           |
| <b>WARRANT 1, CONDITION B</b>                        |                  |   |                 |   |                 |
| 1  | 1                | 13,300                                      | 9,300           | 1,350                                     | 950             |
| 2 or more  | 1                | 15,900                                      | 11,100          | 1,350                                     | 950             |
| 2 or more  | 2 or more        | 15,900                                      | 11,100          | 1,750                                     | 1,250           |
| 1  | 2 or more        | 13,300                                      | 9,300           | 1,750                                     | 1,250           |

Note: ADT volumes assume 8th highest hour is 5.65% of the daily volume

|  | Approach Volumes | Minimum Volumes | Is Signal Warrant Met? |
|--|------------------|-----------------|------------------------|
| <b>Warrant 1</b>                                       |                  |                 |                        |
| <i>Condition A: Minimum Vehicular Volume</i>           |                  |                 |                        |
| Major Street   | 3,340            | 8,850           |                        |
| Minor Street*  | 610              | 2,650           | <b>No</b>              |
| <i>Condition B: Interruption of Continuous Traffic</i> |                  |                 |                        |
| Major Street   | 3,340            | 13,300          |                        |
| Minor Street*  | 610              | 1,350           | <b>No</b>              |
| <i>Combination Warrant</i>                             |                  |                 |                        |
| Major Street   | 3,340            | 10,640          |                        |
| Minor Street*  | 610              | 2,120           | <b>No</b>              |

\* Minor street right-turning traffic volumes reduced by 85% of the turn capacity.

# Traffic Signal Warrant Analysis



Project: 23087 - Norton Lane Apartments  
 Date: 8/8/2023  
 Scenario: Year 2025 Buildout Conditions

|                       |                |                       |                            |
|-----------------------|----------------|-----------------------|----------------------------|
| Major Street:         | SE Norton Lane | Minor Street:         | SE Stratus Ave (South Int) |
| Number of Lanes:      | 1              | Number of Lanes:      | 1                          |
| PM Peak Hour Volumes: | 55             | PM Peak Hour Volumes: | 34                         |

### Warrant Used:

    X     100 percent of standard warrants used  
           70 percent of standard warrants used due to 85th percentile speed in excess of 40 mph or isolated community with population less than 10,000.

| Number of Lanes for Moving Traffic on Each Approach: |                  | ADT on Major St. (total of both approaches) |                 | ADT on Minor St. (higher-volume approach) |                 |
|--|------------------|---|-----------------|---|-----------------|
| <u>Major St.</u>                                     | <u>Minor St.</u> | <u>Warrants</u>                             | <u>Warrants</u> | <u>Warrants</u>                           | <u>Warrants</u> |
| <b>WARRANT 1, CONDITION A</b>                        |                  |   |                 |   |                 |
|  |                  | 100%  | 70%             | 100%                                      | 70%             |
| 1  | 1                | 8,850                                       | 6,200           | 2,650                                     | 1,850           |
| 2 or more  | 1                | 10,600                                      | 7,400           | 2,650                                     | 1,850           |
| 2 or more  | 2 or more        | 10,600                                      | 7,400           | 3,550                                     | 2,500           |
| 1  | 2 or more        | 8,850                                       | 6,200           | 3,550                                     | 2,500           |
| <b>WARRANT 1, CONDITION B</b>                        |                  |   |                 |   |                 |
| 1  | 1                | 13,300                                      | 9,300           | 1,350                                     | 950             |
| 2 or more  | 1                | 15,900                                      | 11,100          | 1,350                                     | 950             |
| 2 or more  | 2 or more        | 15,900                                      | 11,100          | 1,750                                     | 1,250           |
| 1  | 2 or more        | 13,300                                      | 9,300           | 1,750                                     | 1,250           |

Note: ADT volumes assume 8th highest hour is 5.65% of the daily volume

|  | Approach Volumes | Minimum Volumes | Is Signal Warrant Met? |
|--|------------------|-----------------|------------------------|
| <b>Warrant 1</b>                                       |                  |                 |                        |
| <i>Condition A: Minimum Vehicular Volume</i>           |                  |                 |                        |
| Major Street   | 550              | 8,850           |                        |
| Minor Street*  | 340              | 2,650           | <b>No</b>              |
| <i>Condition B: Interruption of Continuous Traffic</i> |                  |                 |                        |
| Major Street   | 550              | 13,300          |                        |
| Minor Street*  | 340              | 1,350           | <b>No</b>              |
| <i>Combination Warrant</i>                             |                  |                 |                        |
| Major Street   | 550              | 10,640          |                        |
| Minor Street*  | 340              | 2,120           | <b>No</b>              |

\* Minor street right-turning traffic volumes reduced by 25%

## Appendix E

Level of Service Descriptions

Capacity Reports





## Level of Service Definitions

Level of service is used to describe the quality of traffic flow. Levels of service A to C are considered good, and rural roads are usually designed for level of service C. Urban streets and signalized intersections are typically designed for level of service D. Level of service E is considered to be the limit of acceptable delay. For unsignalized intersections, level of service E is generally considered acceptable. Here is a more complete description of levels of service:

- *Level of service A:* Very low delay at intersections, with all traffic signal cycles clearing and no vehicles waiting through more than one signal cycle. On highways, low volume and high speeds, with speeds not restricted by other vehicles.
- *Level of service B:* Operating speeds beginning to be affected by other traffic; short traffic delays at intersections. Higher average intersection delay than for level of service A resulting from more vehicles stopping.
- *Level of service C:* Operating speeds and maneuverability closely controlled by other traffic; higher delays at intersections than for level of service B due to a significant number of vehicles stopping. Not all signal cycles clear the waiting vehicles. This is the recommended design standard for rural highways.
- *Level of service D:* Tolerable operating speeds; long traffic delays occur at intersections. The influence of congestion is noticeable. At traffic signals many vehicles stop, and the proportion of vehicles not stopping declines. The number of signal cycle failures, for which vehicles must wait through more than one signal cycle, are noticeable. This is typically the design level for urban signalized intersections.
- *Level of service E:* Restricted speeds, very long traffic delays at traffic signals, and traffic volumes near capacity. Flow is unstable so that any interruption, no matter how minor, will cause queues to form and service to deteriorate to level of service F. Traffic signal cycle failures are frequent occurrences. For unsignalized intersections, level of service E or better is generally considered acceptable.
- *Level of service F:* Extreme delays, resulting in long queues which may interfere with other traffic movements. There may be stoppages of long duration, and speeds may drop to zero. There may be frequent signal cycle failures. Level of service F will typically result when vehicle arrival rates are greater than capacity. It is considered unacceptable by most drivers.



Level of Service Criteria  
For Signalized Intersections

| Level of Service (LOS) | Control Delay per Vehicle (Seconds) |
|------------------------|-------------------------------------|
| A                      | <10                                 |
| B                      | 10-20                               |
| C                      | 20-35                               |
| D                      | 35-55                               |
| E                      | 55-80                               |
| F                      | >80                                 |























Level of Service Criteria  
For Unsignalized Intersections

| Level of Service (LOS) | Control Delay per Vehicle (Seconds) |
|------------------------|-------------------------------------|
| A                      | <10                                 |
| B                      | 10-15                               |
| C                      | 15-25                               |
| D                      | 25-35                               |
| E                      | 35-50                               |
| F                      | >50                                 |



















HCM 6th Signalized Intersection Summary  
 1: SE Norton Lane & NE Three Mile Lane (OR 18)

08/08/2023

|                              |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                     | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations          |  |  |  |  |  |  |  |  |   |  |  |   |
| Traffic Volume (veh/h)       | 124   | 607   | 129   | 49  | 642   | 116   | 59   | 25  | 22  | 62  | 12  | 50  |
| Future Volume (veh/h)        | 124   | 607   | 129   | 49  | 642   | 116   | 59   | 25  | 22  | 62  | 12  | 50  |
| Initial Q (Qb), veh          | 0   | 0   | 0   | 0   | 0   | 0   | 0  | 0   | 0   | 0   | 0   | 0   |
| Ped-Bike Adj(A_pbT)          | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.99  | 1.00  |   | 0.99  |
| Parking Bus, Adj             | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Work Zone On Approach        |   | No  |   |   | No  |   |  | No  |   |   | No  |   |
| Adj Sat Flow, veh/h/ln       | 1682  | 1682  | 1682  | 1668  | 1668  | 1668  | 1573   | 1573  | 1573  | 1614  | 1614  | 1614  |
| Adj Flow Rate, veh/h         | 136   | 667   | 35  | 54  | 705   | 22  | 65   | 27  | 6   | 68  | 13  | 13  |
| Peak Hour Factor             | 0.91  | 0.91  | 0.91  | 0.91  | 0.91  | 0.91  | 0.91   | 0.91  | 0.91  | 0.91  | 0.91  | 0.91  |
| Percent Heavy Veh, %         | 5   | 5   | 5   | 6   | 6   | 6   | 13   | 13  | 13  | 10  | 10  | 10  |
| Cap, veh/h                   | 168   | 1190  | 531   | 73  | 994   | 443   | 153  | 118   | 26  | 84  | 71  | 71  |
| Arrive On Green              | 0.10  | 0.37  | 0.37  | 0.05  | 0.31  | 0.31  | 0.05   | 0.09  | 0.09  | 0.05  | 0.10  | 0.10  |
| Sat Flow, veh/h              | 1602  | 3195  | 1425  | 1589  | 3169  | 1414  | 2905   | 1243  | 276   | 1537  | 736   | 736   |
| Grp Volume(v), veh/h         | 136   | 667   | 35  | 54  | 705   | 22  | 65   | 0   | 33  | 68  | 0   | 26  |
| Grp Sat Flow(s),veh/h/ln     | 1602  | 1598  | 1425  | 1589  | 1585  | 1414  | 1453   | 0   | 1519  | 1537  | 0   | 1471  |
| Q Serve(g_s), s              | 3.1   | 6.1   | 0.6   | 1.2   | 7.3   | 0.4   | 0.8  | 0.0   | 0.7   | 1.6   | 0.0   | 0.6   |
| Cycle Q Clear(g_c), s        | 3.1   | 6.1   | 0.6   | 1.2   | 7.3   | 0.4   | 0.8  | 0.0   | 0.7   | 1.6   | 0.0   | 0.6   |
| Prop In Lane                 | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.18  | 1.00  |   | 0.50  |
| Lane Grp Cap(c), veh/h       | 168   | 1190  | 531   | 73  | 994   | 443   | 153  | 0   | 144   | 84  | 0   | 142   |
| V/C Ratio(X)                 | 0.81  | 0.56  | 0.07  | 0.74  | 0.71  | 0.05  | 0.42   | 0.00  | 0.23  | 0.81  | 0.00  | 0.18  |
| Avail Cap(c_a), veh/h        | 390   | 1555  | 694   | 387   | 1542  | 688   | 707  | 0   | 739   | 374   | 0   | 716   |
| HCM Platoon Ratio            | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Upstream Filter(I)           | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 0.00  | 1.00  | 1.00  | 0.00  | 1.00  |
| Uniform Delay (d), s/veh     | 16.2  | 9.2   | 7.5   | 17.4  | 11.2  | 8.9   | 17.0   | 0.0   | 15.5  | 17.3  | 0.0   | 15.4  |
| Incr Delay (d2), s/veh       | 6.8   | 0.3   | 0.0   | 10.2  | 0.7   | 0.0   | 1.4  | 0.0   | 0.6   | 13.1  | 0.0   | 0.5   |
| Initial Q Delay(d3),s/veh    | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0  | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   |
| %ile BackOfQ(50%),veh/ln     | 1.2   | 1.3   | 0.1   | 0.5   | 1.5   | 0.1   | 0.3  | 0.0   | 0.2   | 0.8   | 0.0   | 0.2   |
| Unsig. Movement Delay, s/veh |   |   |   |   |   |   |  |   |   |   |   |   |
| LnGrp Delay(d),s/veh         | 23.0  | 9.5   | 7.5   | 27.7  | 11.9  | 8.9   | 18.4   | 0.0   | 16.1  | 30.4  | 0.0   | 15.8  |
| LnGrp LOS                    | C   | A   | A   | C   | B   | A   | B  | A   | B   | C   | A   | B   |
| Approach Vol, veh/h          |   | 838   |   |   | 781   |   |  | 98  |   |   |   | 94  |
| Approach Delay, s/veh        |   | 11.6  |   |   | 12.9  |   |  | 17.6  |   |   |   | 26.4  |
| Approach LOS                 |   | B   |   |   | B   |   |  | B   |   |   |   | C   |
| Timer - Assigned Phs         | 1   | 2   | 3   | 4   | 5   | 6   | 7  | 8   |   |   |   |   |
| Phs Duration (G+Y+Rc), s     | 7.9   | 15.6  | 5.9   | 7.6   | 5.7   | 17.8  | 6.0  | 7.5   |   |   |   |   |
| Change Period (Y+Rc), s      | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0  | 4.0   |   |   |   |   |
| Max Green Setting (Gmax), s  | 9.0   | 18.0  | 9.0   | 18.0  | 9.0   | 18.0  | 9.0  | 18.0  |   |   |   |   |
| Max Q Clear Time (g_c+I1), s | 5.1   | 9.3   | 2.8   | 2.6   | 3.2   | 8.1   | 3.6  | 2.7   |   |   |   |   |
| Green Ext Time (p_c), s      | 0.1   | 2.3   | 0.1   | 0.0   | 0.0   | 2.5   | 0.0  | 0.1   |   |   |   |   |
| <b>Intersection Summary</b>  |   |   |   |   |   |   |  |   |   |   |   |   |
| HCM 6th Ctrl Delay           |   |   |   | 13.3  |   |   |  |   |   |   |   |   |
| HCM 6th LOS                  |   |   |   | B   |   |   |  |   |   |   |   |   |

## HCM Unsignalized Intersection Capacity Analysis 2: SE Norton Lane & SE Stratus Avenue

08/08/2023

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |   |  |   |   |  |   |  |  |   |   |  |   |
| Sign Control                      |   | Stop  |   |   | Stop  |   |  | Stop  |   |   | Yield   |   |
| Traffic Volume (vph)              | 39  | 10  | 7   | 1   | 1   | 36  | 1  | 36  | 1   | 84  | 91  | 10  |
| Future Volume (vph)               | 39  | 10  | 7   | 1   | 1   | 36  | 1  | 36  | 1   | 84  | 91  | 10  |
| Peak Hour Factor                  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80   | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  |
| Hourly flow rate (vph)            | 49  | 12  | 9   | 1   | 1   | 45  | 1  | 45  | 1   | 105   | 114   | 12  |
| Direction, Lane #                 | EB 1  | WB 1  | NB 1  | SB 1  |   |   |  |   |   |   |   |   |
| Volume Total (vph)                | 70  | 47  | 47  | 231   |   |   |  |   |   |   |   |   |
| Volume Left (vph)                 | 49  | 1   | 1   | 105   |   |   |  |   |   |   |   |   |
| Volume Right (vph)                | 9   | 45  | 1   | 12  |   |   |  |   |   |   |   |   |
| Hadj (s)                          | 0.25  | -0.57   | 0.18  | 0.13  |   |   |  |   |   |   |   |   |
| Departure Headway (s)             | 4.8   | 4.1   | 4.6   | 4.4   |   |   |  |   |   |   |   |   |
| Degree Utilization, x             | 0.09  | 0.05  | 0.06  | 0.28  |   |   |  |   |   |   |   |   |
| Capacity (veh/h)                  | 692   | 813   | 745   | 795   |   |   |  |   |   |   |   |   |
| Control Delay (s)                 | 8.4   | 7.3   | 7.9   | 9.1   |   |   |  |   |   |   |   |   |
| Approach Delay (s)                | 8.4   | 7.3   | 7.9   | 9.1   |   |   |  |   |   |   |   |   |
| Approach LOS                      | A   | A   | A   | A   |   |   |  |   |   |   |   |   |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |
| Delay                             |   |   | 8.6   |   |   |   |  |   |   |   |   |   |
| Level of Service                  |   |   | A   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 34.3%   | ICU Level of Service  | A   |   |  |   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

HCM 6th TWSC  
3: SE Norton Lane & SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 7.4  |      |      |      |      |      |
| Movement                 | WBL  | WBR  | NBT  | NBR  | SBL  | SBT  |
| Lane Configurations      |      |      |      |      |      |      |
| Traffic Vol, veh/h       | 1    | 19   | 1    | 1    | 48   | 1    |
| Future Vol, veh/h        | 1    | 19   | 1    | 1    | 48   | 1    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | 0    | -    | -    | 0    |
| Grade, %                 | 0    | -    | 0    | -    | -    | 0    |
| Peak Hour Factor         | 70   | 70   | 70   | 70   | 70   | 70   |
| Heavy Vehicles, %        | 5    | 5    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 1    | 27   | 1    | 1    | 69   | 1    |

























| Major/Minor          | Minor1 | Major1 | Major2 |   |      |   |
|----------------------|--------|--------|--------|---|------|---|
| Conflicting Flow All | 141    | 2      | 0      | 0 | 2    | 0 |
| Stage 1              | 2      | -      | -      | - | -    | - |
| Stage 2              | 139    | -      | -      | - | -    | - |
| Critical Hdwy        | 6.45   | 6.25   | -      | - | 4.1  | - |
| Critical Hdwy Stg 1  | 5.45   | -      | -      | - | -    | - |
| Critical Hdwy Stg 2  | 5.45   | -      | -      | - | -    | - |
| Follow-up Hdwy       | 3.545  | 3.345  | -      | - | 2.2  | - |
| Pot Cap-1 Maneuver   | 845    | 1073   | -      | - | 1634 | - |
| Stage 1              | 1013   | -      | -      | - | -    | - |
| Stage 2              | 880    | -      | -      | - | -    | - |
| Platoon blocked, %   |        |        | -      | - |      | - |
| Mov Cap-1 Maneuver   | 810    | 1073   | -      | - | 1634 | - |
| Mov Cap-2 Maneuver   | 810    | -      | -      | - | -    | - |
| Stage 1              | 1013   | -      | -      | - | -    | - |
| Stage 2              | 843    | -      | -      | - | -    | - |

| Approach             | WB  | NB | SB  |
|----------------------|-----|----|-----|
| HCM Control Delay, s | 8.5 | 0  | 7.2 |
| HCM LOS              | A   |    |     |

| Minor Lane/Major Mvmt | NBT | NBRWBLn1 | SBL   | SBT   |
|-----------------------|-----|----------|-------|-------|
| Capacity (veh/h)      | -   | -        | 1056  | 1634  |
| HCM Lane V/C Ratio    | -   | -        | 0.027 | 0.042 |
| HCM Control Delay (s) | -   | -        | 8.5   | 7.3   |
| HCM Lane LOS          | -   | -        | A     | A     |
| HCM 95th %tile Q(veh) | -   | -        | 0.1   | 0.1   |

HCM 6th Signalized Intersection Summary  
 1: SE Norton Lane & NE Three Mile Lane (OR 18)

















08/08/2023

|                              |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                     | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations          |  |  |  |  |  |  |  |  |  |  |  |  |
| Traffic Volume (veh/h)       | 78  | 840   | 67  | 21  | 949   | 96  | 134  | 13  | 47  | 127   | 6   | 141   |
| Future Volume (veh/h)        | 78  | 840   | 67  | 21  | 949   | 96  | 134  | 13  | 47  | 127   | 6   | 141   |
| Initial Q (Qb), veh          | 0   | 0   | 0   | 0   | 0   | 0   | 0  | 0   | 0   | 0   | 0   | 0   |
| Ped-Bike Adj(A_pbT)          | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.99  | 1.00  |   | 0.99  |
| Parking Bus, Adj             | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Work Zone On Approach        |   | No  |   |   | No  |   |  | No  |   |   | No  |   |
| Adj Sat Flow, veh/h/ln       | 1682  | 1682  | 1682  | 1709  | 1709  | 1709  | 1709   | 1709  | 1709  | 1736  | 1736  | 1736  |
| Adj Flow Rate, veh/h         | 89  | 955   | 0   | 24  | 1078  | 25  | 152  | 15  | 10  | 144   | 7   | 28  |
| Peak Hour Factor             | 0.88  | 0.88  | 0.88  | 0.88  | 0.88  | 0.88  | 0.88   | 0.88  | 0.88  | 0.88  | 0.88  | 0.88  |
| Percent Heavy Veh, %         | 5   | 5   | 5   | 3   | 3   | 3   | 3  | 3   | 3   | 1   | 1   | 1   |
| Cap, veh/h                   | 109   | 1472  | 656   | 37  | 1349  | 602   | 248  | 86  | 58  | 181   | 37  | 146   |
| Arrive On Green              | 0.07  | 0.46  | 0.00  | 0.02  | 0.42  | 0.42  | 0.08   | 0.09  | 0.09  | 0.11  | 0.12  | 0.12  |
| Sat Flow, veh/h              | 1602  | 3195  | 1425  | 1628  | 3247  | 1448  | 3158   | 951   | 634   | 1654  | 301   | 1204  |
| Grp Volume(v), veh/h         | 89  | 955   | 0   | 24  | 1078  | 25  | 152  | 0   | 25  | 144   | 0   | 35  |
| Grp Sat Flow(s),veh/h/ln     | 1602  | 1598  | 1425  | 1628  | 1624  | 1448  | 1579   | 0   | 1585  | 1654  | 0   | 1505  |
| Q Serve(g_s), s              | 2.8   | 11.6  | 0.0   | 0.7   | 14.7  | 0.5   | 2.4  | 0.0   | 0.7   | 4.3   | 0.0   | 1.1   |
| Cycle Q Clear(g_c), s        | 2.8   | 11.6  | 0.0   | 0.7   | 14.7  | 0.5   | 2.4  | 0.0   | 0.7   | 4.3   | 0.0   | 1.1   |
| Prop In Lane                 | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.40  | 1.00  |   | 0.80  |
| Lane Grp Cap(c), veh/h       | 109   | 1472  | 656   | 37  | 1349  | 602   | 248  | 0   | 144   | 181   | 0   | 183   |
| V/C Ratio(X)                 | 0.82  | 0.65  | 0.00  | 0.65  | 0.80  | 0.04  | 0.61   | 0.00  | 0.17  | 0.80  | 0.00  | 0.19  |
| Avail Cap(c_a), veh/h        | 285   | 1771  | 790   | 290   | 1800  | 803   | 563  | 0   | 565   | 295   | 0   | 536   |
| HCM Platoon Ratio            | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Upstream Filter(I)           | 1.00  | 1.00  | 0.00  | 1.00  | 1.00  | 1.00  | 1.00   | 0.00  | 1.00  | 1.00  | 0.00  | 1.00  |
| Uniform Delay (d), s/veh     | 23.2  | 10.5  | 0.0   | 24.5  | 12.9  | 8.8   | 22.5   | 0.0   | 21.2  | 22.0  | 0.0   | 20.0  |
| Incr Delay (d2), s/veh       | 10.5  | 0.5   | 0.0   | 13.5  | 1.7   | 0.0   | 1.8  | 0.0   | 0.4   | 5.9   | 0.0   | 0.4   |
| Initial Q Delay(d3),s/veh    | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0  | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   |
| %ile BackOfQ(50%),veh/ln     | 1.2   | 2.8   | 0.0   | 0.4   | 3.7   | 0.1   | 0.9  | 0.0   | 0.3   | 1.8   | 0.0   | 0.4   |
| Unsig. Movement Delay, s/veh |   |   |   |   |   |   |  |   |   |   |   |   |
| LnGrp Delay(d),s/veh         | 33.8  | 11.0  | 0.0   | 38.0  | 14.6  | 8.8   | 24.4   | 0.0   | 21.6  | 27.9  | 0.0   | 20.3  |
| LnGrp LOS                    | C   | B   | A   | D   | B   | A   | C  | A   | C   | C   | A   | C   |
| Approach Vol, veh/h          |   | 1044  |   |   | 1127  |   |  | 177   |   |   |   | 179   |
| Approach Delay, s/veh        |   | 12.9  |   |   | 15.0  |   |  | 24.0  |   |   |   | 26.4  |
| Approach LOS                 |   | B   |   |   | B   |   |  | C   |   |   |   | C   |
| Timer - Assigned Phs         | 1   | 2   | 3   | 4   | 5   | 6   | 7  | 8   |   |   |   |   |
| Phs Duration (G+Y+Rc), s     | 7.4   | 25.0  | 8.0   | 10.1  | 5.1   | 27.3  | 9.5  | 8.6   |   |   |   |   |
| Change Period (Y+Rc), s      | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0  | 4.0   |   |   |   |   |
| Max Green Setting (Gmax), s  | 9.0   | 28.0  | 9.0   | 18.0  | 9.0   | 28.0  | 9.0  | 18.0  |   |   |   |   |
| Max Q Clear Time (g_c+I1), s | 4.8   | 16.7  | 4.4   | 3.1   | 2.7   | 13.6  | 6.3  | 2.7   |   |   |   |   |
| Green Ext Time (p_c), s      | 0.0   | 4.3   | 0.1   | 0.1   | 0.0   | 4.5   | 0.1  | 0.0   |   |   |   |   |
| <b>Intersection Summary</b>  |   |   |   |   |   |   |  |   |   |   |   |   |
| HCM 6th Ctrl Delay           |   |   |   | 15.6  |   |   |  |   |   |   |   |   |
| HCM 6th LOS                  |   |   |   | B   |   |   |  |   |   |   |   |   |

# HCM Unsignalized Intersection Capacity Analysis

## 2: SE Norton Lane & SE Stratus Avenue

08/08/2023

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |   |  |   |   |  |   |  |  |   |   |  |   |
| Sign Control                      |   | Stop  |   |   | Stop  |   |  | Stop  |   |   | Yield   |   |
| Traffic Volume (vph)              | 41  | 1   | 1   | 2   | 1   | 72  | 1  | 83  | 1   | 26  | 19  | 57  |
| Future Volume (vph)               | 41  | 1   | 1   | 2   | 1   | 72  | 1  | 83  | 1   | 26  | 19  | 57  |
| Peak Hour Factor                  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90   | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  |
| Hourly flow rate (vph)            | 46  | 1   | 1   | 2   | 1   | 80  | 1  | 92  | 1   | 29  | 21  | 63  |
| Direction, Lane #                 | EB 1  | WB 1  | NB 1  | SB 1  |   |   |  |   |   |   |   |   |
| Volume Total (vph)                | 48  | 83  | 94  | 113   |   |   |  |   |   |   |   |   |
| Volume Left (vph)                 | 46  | 2   | 1   | 29  |   |   |  |   |   |   |   |   |
| Volume Right (vph)                | 1   | 80  | 1   | 63  |   |   |  |   |   |   |   |   |
| Hadj (s)                          | 0.26  | -0.49   | 0.00  | -0.20   |   |   |  |   |   |   |   |   |
| Departure Headway (s)             | 4.7   | 3.9   | 4.3   | 4.1   |   |   |  |   |   |   |   |   |
| Degree Utilization, x             | 0.06  | 0.09  | 0.11  | 0.13  |   |   |  |   |   |   |   |   |
| Capacity (veh/h)                  | 726   | 867   | 801   | 847   |   |   |  |   |   |   |   |   |
| Control Delay (s)                 | 8.0   | 7.3   | 7.8   | 7.7   |   |   |  |   |   |   |   |   |
| Approach Delay (s)                | 8.0   | 7.3   | 7.8   | 7.7   |   |   |  |   |   |   |   |   |
| Approach LOS                      | A   | A   | A   | A   |   |   |  |   |   |   |   |   |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |
| Delay                             |   |   | 7.7   |   |   |   |  |   |   |   |   |   |
| Level of Service                  |   |   | A   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 29.3%   | ICU Level of Service  | A   |   |  |   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

HCM 6th TWSC  
 3: SE Norton Lane & SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 7.9  |      |      |      |      |      |
| Movement                 | WBL  | WBR  | NBT  | NBR  | SBL  | SBT  |
| Lane Configurations      |      |      |      |      |      |      |
| Traffic Vol, veh/h       | 1    | 43   | 1    | 1    | 5    | 1    |
| Future Vol, veh/h        | 1    | 43   | 1    | 1    | 5    | 1    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | 0    | -    | -    | 0    |
| Grade, %                 | 0    | -    | 0    | -    | -    | 0    |
| Peak Hour Factor         | 75   | 75   | 75   | 75   | 75   | 75   |
| Heavy Vehicles, %        | 2    | 2    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 1    | 57   | 1    | 1    | 7    | 1    |























| Major/Minor          | Minor1 | Major1 | Major2 |   |      |   |
|----------------------|--------|--------|--------|---|------|---|
| Conflicting Flow All | 17     | 2      | 0      | 0 | 2    | 0 |
| Stage 1              | 2      | -      | -      | - | -    | - |
| Stage 2              | 15     | -      | -      | - | -    | - |
| Critical Hdwy        | 6.42   | 6.22   | -      | - | 4.1  | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -      | - | -    | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -      | - | -    | - |
| Follow-up Hdwy       | 3.518  | 3.318  | -      | - | 2.2  | - |
| Pot Cap-1 Maneuver   | 1001   | 1082   | -      | - | 1634 | - |
| Stage 1              | 1021   | -      | -      | - | -    | - |
| Stage 2              | 1008   | -      | -      | - | -    | - |
| Platoon blocked, %   |        |        | -      | - |      | - |
| Mov Cap-1 Maneuver   | 997    | 1082   | -      | - | 1634 | - |
| Mov Cap-2 Maneuver   | 997    | -      | -      | - | -    | - |
| Stage 1              | 1021   | -      | -      | - | -    | - |
| Stage 2              | 1004   | -      | -      | - | -    | - |

| Approach             | WB  | NB | SB |
|----------------------|-----|----|----|
| HCM Control Delay, s | 8.5 | 0  | 6  |
| HCM LOS              | A   |    |    |

| Minor Lane/Major Mvmt | NBT | NBRWBLn1 | SBL   | SBT   |
|-----------------------|-----|----------|-------|-------|
| Capacity (veh/h)      | -   | -        | 1080  | 1634  |
| HCM Lane V/C Ratio    | -   | -        | 0.054 | 0.004 |
| HCM Control Delay (s) | -   | -        | 8.5   | 7.2   |
| HCM Lane LOS          | -   | -        | A     | A     |
| HCM 95th %tile Q(veh) | -   | -        | 0.2   | 0     |

















HCM 6th Signalized Intersection Summary  
 1: SE Norton Lane & NE Three Mile Lane (OR 18)

08/08/2023

|                              |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                     | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations          |  |  |  |  |  |  |  |  |   |  |  |   |
| Traffic Volume (veh/h)       | 129   | 624   | 143   | 56  | 660   | 121   | 99   | 29  | 41  | 64  | 13  | 52  |
| Future Volume (veh/h)        | 129   | 624   | 143   | 56  | 660   | 121   | 99   | 29  | 41  | 64  | 13  | 52  |
| Initial Q (Qb), veh          | 0   | 0   | 0   | 0   | 0   | 0   | 0  | 0   | 0   | 0   | 0   | 0   |
| Ped-Bike Adj(A_pbT)          | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.99  | 1.00  |   | 0.99  |
| Parking Bus, Adj             | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Work Zone On Approach        |   | No  |   |   | No  |   |  | No  |   |   | No  |   |
| Adj Sat Flow, veh/h/ln       | 1682  | 1682  | 1682  | 1668  | 1668  | 1668  | 1573   | 1573  | 1573  | 1614  | 1614  | 1614  |
| Adj Flow Rate, veh/h         | 142   | 686   | 38  | 62  | 725   | 28  | 109  | 32  | 11  | 70  | 14  | 14  |
| Peak Hour Factor             | 0.91  | 0.91  | 0.91  | 0.91  | 0.91  | 0.91  | 0.91   | 0.91  | 0.91  | 0.91  | 0.91  | 0.91  |
| Percent Heavy Veh, %         | 5   | 5   | 5   | 6   | 6   | 6   | 13   | 13  | 13  | 10  | 10  | 10  |
| Cap, veh/h                   | 176   | 1193  | 532   | 80  | 994   | 443   | 206  | 126   | 43  | 84  | 71  | 71  |
| Arrive On Green              | 0.11  | 0.37  | 0.37  | 0.05  | 0.31  | 0.31  | 0.07   | 0.11  | 0.11  | 0.05  | 0.10  | 0.10  |
| Sat Flow, veh/h              | 1602  | 3195  | 1425  | 1589  | 3169  | 1414  | 2905   | 1115  | 383   | 1537  | 735   | 735   |
| Grp Volume(v), veh/h         | 142   | 686   | 38  | 62  | 725   | 28  | 109  | 0   | 43  | 70  | 0   | 28  |
| Grp Sat Flow(s),veh/h/ln     | 1602  | 1598  | 1425  | 1589  | 1585  | 1414  | 1453   | 0   | 1499  | 1537  | 0   | 1471  |
| Q Serve(g_s), s              | 3.4   | 6.7   | 0.7   | 1.5   | 8.0   | 0.5   | 1.4  | 0.0   | 1.0   | 1.8   | 0.0   | 0.7   |
| Cycle Q Clear(g_c), s        | 3.4   | 6.7   | 0.7   | 1.5   | 8.0   | 0.5   | 1.4  | 0.0   | 1.0   | 1.8   | 0.0   | 0.7   |
| Prop In Lane                 | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.26  | 1.00  |   | 0.50  |
| Lane Grp Cap(c), veh/h       | 176   | 1193  | 532   | 80  | 994   | 443   | 206  | 0   | 170   | 84  | 0   | 142   |
| V/C Ratio(X)                 | 0.81  | 0.58  | 0.07  | 0.78  | 0.73  | 0.06  | 0.53   | 0.00  | 0.25  | 0.84  | 0.00  | 0.20  |
| Avail Cap(c_a), veh/h        | 368   | 1470  | 656   | 365   | 1458  | 650   | 668  | 0   | 690   | 354   | 0   | 677   |
| HCM Platoon Ratio            | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Upstream Filter(I)           | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 0.00  | 1.00  | 1.00  | 0.00  | 1.00  |
| Uniform Delay (d), s/veh     | 17.0  | 9.8   | 7.9   | 18.4  | 12.0  | 9.4   | 17.5   | 0.0   | 15.8  | 18.3  | 0.0   | 16.3  |
| Incr Delay (d2), s/veh       | 6.4   | 0.3   | 0.0   | 11.4  | 0.8   | 0.0   | 1.6  | 0.0   | 0.6   | 14.7  | 0.0   | 0.5   |
| Initial Q Delay(d3),s/veh    | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0  | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   |
| %ile BackOfQ(50%),veh/ln     | 1.3   | 1.5   | 0.2   | 0.7   | 1.7   | 0.1   | 0.5  | 0.0   | 0.3   | 0.9   | 0.0   | 0.2   |
| Unsig. Movement Delay, s/veh |   |   |   |   |   |   |  |   |   |   |   |   |
| LnGrp Delay(d),s/veh         | 23.4  | 10.1  | 7.9   | 29.8  | 12.7  | 9.4   | 19.1   | 0.0   | 16.4  | 33.0  | 0.0   | 16.8  |
| LnGrp LOS                    | C   | B   | A   | C   | B   | A   | B  | A   | B   | C   | A   | B   |
| Approach Vol, veh/h          |   | 866   |   |   | 815   |   |  | 152   |   |   |   | 98  |
| Approach Delay, s/veh        |   | 12.2  |   |   | 13.9  |   |  | 18.3  |   |   |   | 28.4  |
| Approach LOS                 |   | B   |   |   | B   |   |  | B   |   |   |   | C   |
| Timer - Assigned Phs         | 1   | 2   | 3   | 4   | 5   | 6   | 7  | 8   |   |   |   |   |
| Phs Duration (G+Y+Rc), s     | 8.3   | 16.3  | 6.8   | 7.8   | 6.0   | 18.6  | 6.1  | 8.4   |   |   |   |   |
| Change Period (Y+Rc), s      | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0  | 4.0   |   |   |   |   |
| Max Green Setting (Gmax), s  | 9.0   | 18.0  | 9.0   | 18.0  | 9.0   | 18.0  | 9.0  | 18.0  |   |   |   |   |
| Max Q Clear Time (g_c+I1), s | 5.4   | 10.0  | 3.4   | 2.7   | 3.5   | 8.7   | 3.8  | 3.0   |   |   |   |   |
| Green Ext Time (p_c), s      | 0.1   | 2.3   | 0.1   | 0.1   | 0.0   | 2.5   | 0.0  | 0.1   |   |   |   |   |
| <b>Intersection Summary</b>  |   |   |   |   |   |   |  |   |   |   |   |   |
| HCM 6th Ctrl Delay           |   |   | 14.2  |   |   |   |  |   |   |   |   |   |
| HCM 6th LOS                  |   |   | B   |   |   |   |  |   |   |   |   |   |

HCM Unsignalized Intersection Capacity Analysis  
 2: SE Norton Lane & SE Stratus Avenue

08/08/2023

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |   |  |   |   |  |   |  |  |   |   |  |   |
| Sign Control                      |   | Stop  |   |   | Stop  |   |  | Stop  |   |   | Yield   |   |
| Traffic Volume (vph)              | 70  | 10  | 7   | 1   | 1   | 37  | 1  | 67  | 1   | 87  | 104   | 16  |
| Future Volume (vph)               | 70  | 10  | 7   | 1   | 1   | 37  | 1  | 67  | 1   | 87  | 104   | 16  |
| Peak Hour Factor                  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80   | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  |
| Hourly flow rate (vph)            | 88  | 12  | 9   | 1   | 1   | 46  | 1  | 84  | 1   | 109   | 130   | 20  |
| Direction, Lane #                 | EB 1  | WB 1  | NB 1  | SB 1  |   |   |  |   |   |   |   |   |
| Volume Total (vph)                | 109   | 48  | 86  | 259   |   |   |  |   |   |   |   |   |
| Volume Left (vph)                 | 88  | 1   | 1   | 109   |   |   |  |   |   |   |   |   |
| Volume Right (vph)                | 9   | 46  | 1   | 20  |   |   |  |   |   |   |   |   |
| Hadj (s)                          | 0.30  | -0.57   | 0.18  | 0.11  |   |   |  |   |   |   |   |   |
| Departure Headway (s)             | 5.1   | 4.3   | 4.8   | 4.5   |   |   |  |   |   |   |   |   |
| Degree Utilization, x             | 0.15  | 0.06  | 0.11  | 0.32  |   |   |  |   |   |   |   |   |
| Capacity (veh/h)                  | 657   | 757   | 713   | 765   |   |   |  |   |   |   |   |   |
| Control Delay (s)                 | 9.0   | 7.6   | 8.4   | 9.7   |   |   |  |   |   |   |   |   |
| Approach Delay (s)                | 9.0   | 7.6   | 8.4   | 9.7   |   |   |  |   |   |   |   |   |
| Approach LOS                      | A   | A   | A   | A   |   |   |  |   |   |   |   |   |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |
| Delay                             |   |   | 9.1   |   |   |   |  |   |   |   |   |   |
| Level of Service                  |   |   | A   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 37.5%   | ICU Level of Service  | A   |   |  |   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |



HCM 6th TWSC  
 3: SE Norton Lane & SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 7.4  |      |      |      |      |      |
| Movement                 | WBL  | WBR  | NBT  | NBR  | SBL  | SBT  |
| Lane Configurations      |      |      |      |      |      |      |
| Traffic Vol, veh/h       | 1    | 20   | 1    | 1    | 50   | 1    |
| Future Vol, veh/h        | 1    | 20   | 1    | 1    | 50   | 1    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | 0    | -    | -    | 0    |
| Grade, %                 | 0    | -    | 0    | -    | -    | 0    |
| Peak Hour Factor         | 70   | 70   | 70   | 70   | 70   | 70   |
| Heavy Vehicles, %        | 5    | 5    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 1    | 29   | 1    | 1    | 71   | 1    |























| Major/Minor          | Minor1 | Major1 | Major2 |   |      |   |
|----------------------|--------|--------|--------|---|------|---|
| Conflicting Flow All | 145    | 2      | 0      | 0 | 2    | 0 |
| Stage 1              | 2      | -      | -      | - | -    | - |
| Stage 2              | 143    | -      | -      | - | -    | - |
| Critical Hdwy        | 6.45   | 6.25   | -      | - | 4.1  | - |
| Critical Hdwy Stg 1  | 5.45   | -      | -      | - | -    | - |
| Critical Hdwy Stg 2  | 5.45   | -      | -      | - | -    | - |
| Follow-up Hdwy       | 3.545  | 3.345  | -      | - | 2.2  | - |
| Pot Cap-1 Maneuver   | 840    | 1073   | -      | - | 1634 | - |
| Stage 1              | 1013   | -      | -      | - | -    | - |
| Stage 2              | 877    | -      | -      | - | -    | - |
| Platoon blocked, %   |        |        | -      | - |      | - |
| Mov Cap-1 Maneuver   | 804    | 1073   | -      | - | 1634 | - |
| Mov Cap-2 Maneuver   | 804    | -      | -      | - | -    | - |
| Stage 1              | 1013   | -      | -      | - | -    | - |
| Stage 2              | 839    | -      | -      | - | -    | - |

| Approach             | WB  | NB | SB  |
|----------------------|-----|----|-----|
| HCM Control Delay, s | 8.5 | 0  | 7.2 |
| HCM LOS              | A   |    |     |

| Minor Lane/Major Mvmt | NBT | NBRWBLn1 | SBL   | SBT   |
|-----------------------|-----|----------|-------|-------|
| Capacity (veh/h)      | -   | -        | 1056  | 1634  |
| HCM Lane V/C Ratio    | -   | -        | 0.028 | 0.044 |
| HCM Control Delay (s) | -   | -        | 8.5   | 7.3   |
| HCM Lane LOS          | -   | -        | A     | A     |
| HCM 95th %tile Q(veh) | -   | -        | 0.1   | 0.1   |

















HCM 6th Signalized Intersection Summary  
 1: SE Norton Lane & NE Three Mile Lane (OR 18)

08/08/2023

|                              |  |  |  |  |  |  |  |  |  |  |  |  |
|------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                     | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations          |  |  |  |  |  |  |  |  |   |  |  |   |
| Traffic Volume (veh/h)       | 81  | 864   | 100   | 40  | 976   | 100   | 162  | 16  | 60  | 132   | 9   | 147   |
| Future Volume (veh/h)        | 81  | 864   | 100   | 40  | 976   | 100   | 162  | 16  | 60  | 132   | 9   | 147   |
| Initial Q (Qb), veh          | 0   | 0   | 0   | 0   | 0   | 0   | 0  | 0   | 0   | 0   | 0   | 0   |
| Ped-Bike Adj(A_pbT)          | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.99  | 1.00  |   | 0.99  |
| Parking Bus, Adj             | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Work Zone On Approach        |   | No  |   |   | No  |   |  | No  |   |   | No  |   |
| Adj Sat Flow, veh/h/ln       | 1682  | 1682  | 1682  | 1709  | 1709  | 1709  | 1709   | 1709  | 1709  | 1736  | 1736  | 1736  |
| Adj Flow Rate, veh/h         | 92  | 982   | 50  | 45  | 1109  | 47  | 184  | 18  | 6   | 150   | 10  | 22  |
| Peak Hour Factor             | 0.88  | 0.88  | 0.88  | 0.88  | 0.88  | 0.88  | 0.88   | 0.88  | 0.88  | 0.88  | 0.88  | 0.88  |
| Percent Heavy Veh, %         | 5   | 5   | 5   | 3   | 3   | 3   | 3  | 3   | 3   | 1   | 1   | 1   |
| Cap, veh/h                   | 113   | 1451  | 647   | 60  | 1364  | 608   | 287  | 110   | 37  | 188   | 54  | 118   |
| Arrive On Green              | 0.07  | 0.45  | 0.45  | 0.04  | 0.42  | 0.42  | 0.09   | 0.09  | 0.09  | 0.11  | 0.11  | 0.11  |
| Sat Flow, veh/h              | 1602  | 3195  | 1425  | 1628  | 3247  | 1448  | 3158   | 1222  | 407   | 1654  | 479   | 1054  |
| Grp Volume(v), veh/h         | 92  | 982   | 50  | 45  | 1109  | 47  | 184  | 0   | 24  | 150   | 0   | 32  |
| Grp Sat Flow(s),veh/h/ln     | 1602  | 1598  | 1425  | 1628  | 1624  | 1448  | 1579   | 0   | 1629  | 1654  | 0   | 1533  |
| Q Serve(g_s), s              | 3.0   | 12.7  | 1.0   | 1.4   | 15.7  | 1.0   | 2.9  | 0.0   | 0.7   | 4.6   | 0.0   | 1.0   |
| Cycle Q Clear(g_c), s        | 3.0   | 12.7  | 1.0   | 1.4   | 15.7  | 1.0   | 2.9  | 0.0   | 0.7   | 4.6   | 0.0   | 1.0   |
| Prop In Lane                 | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00   |   | 0.25  | 1.00  |   | 0.69  |
| Lane Grp Cap(c), veh/h       | 113   | 1451  | 647   | 60  | 1364  | 608   | 287  | 0   | 146   | 188   | 0   | 172   |
| V/C Ratio(X)                 | 0.81  | 0.68  | 0.08  | 0.75  | 0.81  | 0.08  | 0.64   | 0.00  | 0.16  | 0.80  | 0.00  | 0.19  |
| Avail Cap(c_a), veh/h        | 276   | 1712  | 764   | 280   | 1740  | 776   | 544  | 0   | 561   | 285   | 0   | 528   |
| HCM Platoon Ratio            | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Upstream Filter(I)           | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00   | 0.00  | 1.00  | 1.00  | 0.00  | 1.00  |
| Uniform Delay (d), s/veh     | 23.9  | 11.2  | 8.1   | 24.9  | 13.3  | 9.1   | 22.9   | 0.0   | 22.0  | 22.6  | 0.0   | 21.0  |
| Incr Delay (d2), s/veh       | 10.0  | 0.7   | 0.0   | 13.2  | 2.2   | 0.0   | 1.8  | 0.0   | 0.4   | 7.3   | 0.0   | 0.4   |
| Initial Q Delay(d3),s/veh    | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0  | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   |
| %ile BackOfQ(50%),veh/ln     | 1.3   | 3.2   | 0.3   | 0.7   | 4.1   | 0.3   | 1.1  | 0.0   | 0.3   | 2.1   | 0.0   | 0.4   |
| Unsig. Movement Delay, s/veh |   |   |   |   |   |   |  |   |   |   |   |   |
| LnGrp Delay(d),s/veh         | 33.9  | 12.0  | 8.1   | 38.1  | 15.5  | 9.1   | 24.7   | 0.0   | 22.4  | 29.9  | 0.0   | 21.4  |
| LnGrp LOS                    | C   | B   | A   | D   | B   | A   | C  | A   | C   | C   | A   | C   |
| Approach Vol, veh/h          |   | 1124  |   |   | 1201  |   |  | 208   |   |   |   | 182   |
| Approach Delay, s/veh        |   | 13.6  |   |   | 16.1  |   |  | 24.4  |   |   |   | 28.4  |
| Approach LOS                 |   | B   |   |   | B   |   |  | C   |   |   |   | C   |
| Timer - Assigned Phs         | 1   | 2   | 3   | 4   | 5   | 6   | 7  | 8   |   |   |   |   |
| Phs Duration (G+Y+Rc), s     | 7.7   | 26.0  | 8.8   | 9.9   | 5.9   | 27.7  | 9.9  | 8.7   |   |   |   |   |
| Change Period (Y+Rc), s      | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0   | 4.0  | 4.0   |   |   |   |   |
| Max Green Setting (Gmax), s  | 9.0   | 28.0  | 9.0   | 18.0  | 9.0   | 28.0  | 9.0  | 18.0  |   |   |   |   |
| Max Q Clear Time (g_c+I1), s | 5.0   | 17.7  | 4.9   | 3.0   | 3.4   | 14.7  | 6.6  | 2.7   |   |   |   |   |
| Green Ext Time (p_c), s      | 0.0   | 4.2   | 0.2   | 0.1   | 0.0   | 4.6   | 0.1  | 0.0   |   |   |   |   |
| <b>Intersection Summary</b>  |   |   |   |   |   |   |  |   |   |   |   |   |
| HCM 6th Ctrl Delay           |   |   |   | 16.5  |   |   |  |   |   |   |   |   |
| HCM 6th LOS                  |   |   |   | B   |   |   |  |   |   |   |   |   |

HCM Unsignalized Intersection Capacity Analysis  
 2: SE Norton Lane & SE Stratus Avenue

08/08/2023

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |   |  |   |   |  |   |  |  |   |   |  |   |
| Sign Control                      |   | Stop  |   |   | Stop  |   |  | Stop  |   |   | Yield   |   |
| Traffic Volume (vph)              | 61  | 1   | 1   | 2   | 1   | 75  | 1  | 104   | 1   | 27  | 50  | 80  |
| Future Volume (vph)               | 61  | 1   | 1   | 2   | 1   | 75  | 1  | 104   | 1   | 27  | 50  | 80  |
| Peak Hour Factor                  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90   | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  |
| Hourly flow rate (vph)            | 68  | 1   | 1   | 2   | 1   | 83  | 1  | 116   | 1   | 30  | 56  | 89  |
| Direction, Lane #                 | EB 1  | WB 1  | NB 1  | SB 1  |   |   |  |   |   |   |   |   |
| Volume Total (vph)                | 70  | 86  | 118   | 175   |   |   |  |   |   |   |   |   |
| Volume Left (vph)                 | 68  | 2   | 1   | 30  |   |   |  |   |   |   |   |   |
| Volume Right (vph)                | 1   | 83  | 1   | 89  |   |   |  |   |   |   |   |   |
| Hadj (s)                          | 0.27  | -0.49   | 0.00  | -0.19   |   |   |  |   |   |   |   |   |
| Departure Headway (s)             | 4.9   | 4.2   | 4.5   | 4.2   |   |   |  |   |   |   |   |   |
| Degree Utilization, x             | 0.10  | 0.10  | 0.15  | 0.21  |   |   |  |   |   |   |   |   |
| Capacity (veh/h)                  | 676   | 795   | 769   | 811   |   |   |  |   |   |   |   |   |
| Control Delay (s)                 | 8.5   | 7.6   | 8.2   | 8.3   |   |   |  |   |   |   |   |   |
| Approach Delay (s)                | 8.5   | 7.6   | 8.2   | 8.3   |   |   |  |   |   |   |   |   |
| Approach LOS                      | A   | A   | A   | A   |   |   |  |   |   |   |   |   |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |
| Delay                             |   |   | 8.2   |   |   |   |  |   |   |   |   |   |
| Level of Service                  |   |   | A   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 33.8%   | ICU Level of Service  | A   |   |  |   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

HCM 6th TWSC  
 3: SE Norton Lane & SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh         | 7.9  |      |      |      |      |      |
| Movement                 | WBL  | WBR  | NBT  | NBR  | SBL  | SBT  |
| Lane Configurations      |      |      |      |      |      |      |
| Traffic Vol, veh/h       | 1    | 45   | 1    | 1    | 5    | 1    |
| Future Vol, veh/h        | 1    | 45   | 1    | 1    | 5    | 1    |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Free | Free | Free | Free |
| RT Channelized           | -    | None | -    | None | -    | None |
| Storage Length           | 0    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | 0    | -    | 0    | -    | -    | 0    |
| Grade, %                 | 0    | -    | 0    | -    | -    | 0    |
| Peak Hour Factor         | 75   | 75   | 75   | 75   | 75   | 75   |
| Heavy Vehicles, %        | 2    | 2    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 1    | 60   | 1    | 1    | 7    | 1    |

| Major/Minor          | Minor1 | Major1 | Major2 |   |      |   |
|----------------------|--------|--------|--------|---|------|---|
| Conflicting Flow All | 17     | 2      | 0      | 0 | 2    | 0 |
| Stage 1              | 2      | -      | -      | - | -    | - |
| Stage 2              | 15     | -      | -      | - | -    | - |
| Critical Hdwy        | 6.42   | 6.22   | -      | - | 4.1  | - |
| Critical Hdwy Stg 1  | 5.42   | -      | -      | - | -    | - |
| Critical Hdwy Stg 2  | 5.42   | -      | -      | - | -    | - |
| Follow-up Hdwy       | 3.518  | 3.318  | -      | - | 2.2  | - |
| Pot Cap-1 Maneuver   | 1001   | 1082   | -      | - | 1634 | - |
| Stage 1              | 1021   | -      | -      | - | -    | - |
| Stage 2              | 1008   | -      | -      | - | -    | - |
| Platoon blocked, %   |        |        | -      | - |      | - |
| Mov Cap-1 Maneuver   | 997    | 1082   | -      | - | 1634 | - |
| Mov Cap-2 Maneuver   | 997    | -      | -      | - | -    | - |
| Stage 1              | 1021   | -      | -      | - | -    | - |
| Stage 2              | 1004   | -      | -      | - | -    | - |

| Approach             | WB  | NB | SB |
|----------------------|-----|----|----|
| HCM Control Delay, s | 8.5 | 0  | 6  |
| HCM LOS              | A   |    |    |

| Minor Lane/Major Mvmt | NBT | NBRWBLn1 | SBL   | SBT   |
|-----------------------|-----|----------|-------|-------|
| Capacity (veh/h)      | -   | -        | 1080  | 1634  |
| HCM Lane V/C Ratio    | -   | -        | 0.057 | 0.004 |
| HCM Control Delay (s) | -   | -        | 8.5   | 7.2   |
| HCM Lane LOS          | -   | -        | A     | A     |
| HCM 95th %tile Q(veh) | -   | -        | 0.2   | 0     |

















HCM 6th Signalized Intersection Summary  
 1: SE Norton Lane & NE Three Mile Lane (OR 18)

08/08/2023

| Movement                     | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations          |      |      |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h)       | 129  | 624  | 151  | 60   | 660  | 121  | 132  | 33   | 54   | 64   | 15   | 52   |
| Future Volume (veh/h)        | 129  | 624  | 151  | 60   | 660  | 121  | 132  | 33   | 54   | 64   | 15   | 52   |
| Initial Q (Qb), veh          | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    |
| Ped-Bike Adj(A_pbT)          | 1.00 |      | 1.00 | 1.00 |      | 1.00 | 1.00 |      | 0.99 | 1.00 |      | 0.99 |
| Parking Bus, Adj             | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach        |      | No   |      |      | No   |      |      | No   |      |      | No   |      |
| Adj Sat Flow, veh/h/ln       | 1682 | 1682 | 1682 | 1668 | 1668 | 1668 | 1573 | 1573 | 1573 | 1614 | 1614 | 1614 |
| Adj Flow Rate, veh/h         | 142  | 686  | 41   | 66   | 725  | 28   | 145  | 36   | 15   | 70   | 16   | 14   |
| Peak Hour Factor             | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 | 0.91 |
| Percent Heavy Veh, %         | 5    | 5    | 5    | 6    | 6    | 6    | 13   | 13   | 13   | 10   | 10   | 10   |
| Cap, veh/h                   | 176  | 1179 | 526  | 83   | 986  | 440  | 237  | 131  | 55   | 83   | 77   | 67   |
| Arrive On Green              | 0.11 | 0.37 | 0.37 | 0.05 | 0.31 | 0.31 | 0.08 | 0.13 | 0.13 | 0.05 | 0.10 | 0.10 |
| Sat Flow, veh/h              | 1602 | 3195 | 1425 | 1589 | 3169 | 1414 | 2905 | 1051 | 438  | 1537 | 789  | 690  |
| Grp Volume(v), veh/h         | 142  | 686  | 41   | 66   | 725  | 28   | 145  | 0    | 51   | 70   | 0    | 30   |
| Grp Sat Flow(s),veh/h/ln     | 1602 | 1598 | 1425 | 1589 | 1585 | 1414 | 1453 | 0    | 1489 | 1537 | 0    | 1479 |
| Q Serve(g_s), s              | 3.5  | 6.9  | 0.7  | 1.6  | 8.2  | 0.6  | 1.9  | 0.0  | 1.2  | 1.8  | 0.0  | 0.7  |
| Cycle Q Clear(g_c), s        | 3.5  | 6.9  | 0.7  | 1.6  | 8.2  | 0.6  | 1.9  | 0.0  | 1.2  | 1.8  | 0.0  | 0.7  |
| Prop In Lane                 | 1.00 |      | 1.00 | 1.00 |      | 1.00 | 1.00 |      | 0.29 | 1.00 |      | 0.47 |
| Lane Grp Cap(c), veh/h       | 176  | 1179 | 526  | 83   | 986  | 440  | 237  | 0    | 186  | 83   | 0    | 144  |
| V/C Ratio(X)                 | 0.81 | 0.58 | 0.08 | 0.80 | 0.74 | 0.06 | 0.61 | 0.00 | 0.27 | 0.84 | 0.00 | 0.21 |
| Avail Cap(c_a), veh/h        | 360  | 1438 | 641  | 357  | 1426 | 636  | 654  | 0    | 670  | 346  | 0    | 666  |
| HCM Platoon Ratio            | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I)           | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.00 | 1.00 | 1.00 | 0.00 | 1.00 |
| Uniform Delay (d), s/veh     | 17.4 | 10.1 | 8.2  | 18.8 | 12.3 | 9.7  | 17.8 | 0.0  | 15.9 | 18.8 | 0.0  | 16.6 |
| Incr Delay (d2), s/veh       | 6.4  | 0.3  | 0.0  | 12.3 | 0.9  | 0.0  | 1.9  | 0.0  | 0.6  | 15.3 | 0.0  | 0.5  |
| Initial Q Delay(d3),s/veh    | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  |
| %ile BackOfQ(50%),veh/ln     | 1.3  | 1.6  | 0.2  | 0.7  | 1.8  | 0.1  | 0.6  | 0.0  | 0.4  | 0.9  | 0.0  | 0.2  |
| Unsig. Movement Delay, s/veh |      |      |      |      |      |      |      |      |      |      |      |      |
| LnGrp Delay(d),s/veh         | 23.8 | 10.5 | 8.2  | 31.0 | 13.2 | 9.7  | 19.7 | 0.0  | 16.4 | 34.1 | 0.0  | 17.1 |
| LnGrp LOS                    | C    | B    | A    | C    | B    | A    | B    | A    | B    | C    | A    | B    |
| Approach Vol, veh/h          |      | 869  |      |      | 819  |      |      | 196  |      |      |      | 100  |
| Approach Delay, s/veh        |      | 12.5 |      |      | 14.5 |      |      | 18.8 |      |      |      | 29.0 |
| Approach LOS                 |      | B    |      |      | B    |      |      | B    |      |      |      | C    |
| Timer - Assigned Phs         | 1    | 2    | 3    | 4    | 5    | 6    | 7    | 8    |      |      |      |      |
| Phs Duration (G+Y+Rc), s     | 8.4  | 16.4 | 7.3  | 7.9  | 6.1  | 18.8 | 6.2  | 9.0  |      |      |      |      |
| Change Period (Y+Rc), s      | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  |      |      |      |      |
| Max Green Setting (Gmax), s  | 9.0  | 18.0 | 9.0  | 18.0 | 9.0  | 18.0 | 9.0  | 18.0 |      |      |      |      |
| Max Q Clear Time (g_c+I1), s | 5.5  | 10.2 | 3.9  | 2.7  | 3.6  | 8.9  | 3.8  | 3.2  |      |      |      |      |
| Green Ext Time (p_c), s      | 0.1  | 2.3  | 0.1  | 0.1  | 0.0  | 2.5  | 0.0  | 0.1  |      |      |      |      |
| <b>Intersection Summary</b>  |      |      |      |      |      |      |      |      |      |      |      |      |
| HCM 6th Ctrl Delay           |      |      | 14.8 |      |      |      |      |      |      |      |      |      |
| HCM 6th LOS                  |      |      | B    |      |      |      |      |      |      |      |      |      |

## HCM Unsignalized Intersection Capacity Analysis 2: SE Norton Lane & SE Stratus Avenue

08/08/2023

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |   |  |   |   |  |   |  |  |   |   |  |   |
| Sign Control                      |   | Stop  |   |   | Stop  |   |  | Stop  |   |   | Yield   |   |
| Traffic Volume (vph)              | 70  | 10  | 9   | 1   | 1   | 37  | 1  | 117   | 1   | 87  | 118   | 16  |
| Future Volume (vph)               | 70  | 10  | 9   | 1   | 1   | 37  | 1  | 117   | 1   | 87  | 118   | 16  |
| Peak Hour Factor                  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  | 0.80   | 0.80  | 0.80  | 0.80  | 0.80  | 0.80  |
| Hourly flow rate (vph)            | 88  | 12  | 11  | 1   | 1   | 46  | 1  | 146   | 1   | 109   | 148   | 20  |
| Direction, Lane #                 | EB 1  | WB 1  | NB 1  | SB 1  |   |   |  |   |   |   |   |   |
| Volume Total (vph)                | 111   | 48  | 148   | 277   |   |   |  |   |   |   |   |   |
| Volume Left (vph)                 | 88  | 1   | 1   | 109   |   |   |  |   |   |   |   |   |
| Volume Right (vph)                | 11  | 46  | 1   | 20  |   |   |  |   |   |   |   |   |
| Hadj (s)                          | 0.29  | -0.57   | 0.18  | 0.10  |   |   |  |   |   |   |   |   |
| Departure Headway (s)             | 5.3   | 4.5   | 4.8   | 4.6   |   |   |  |   |   |   |   |   |
| Degree Utilization, x             | 0.16  | 0.06  | 0.20  | 0.35  |   |   |  |   |   |   |   |   |
| Capacity (veh/h)                  | 626   | 707   | 708   | 748   |   |   |  |   |   |   |   |   |
| Control Delay (s)                 | 9.3   | 7.8   | 9.0   | 10.1  |   |   |  |   |   |   |   |   |
| Approach Delay (s)                | 9.3   | 7.8   | 9.0   | 10.1  |   |   |  |   |   |   |   |   |
| Approach LOS                      | A   | A   | A   | B   |   |   |  |   |   |   |   |   |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |
| Delay                             |   |   | 9.5   |   |   |   |  |   |   |   |   |   |
| Level of Service                  |   |   | A   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 38.5%   | ICU Level of Service  | A   |   |  |   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

HCM 6th TWSC  
 3: SE Norton Lane & Site Access/SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 7.6  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 50   | 1    | 1    | 1    | 1    | 20   | 1    | 1    | 1    | 50   | 1    | 16   |
| Future Vol, veh/h        | 50   | 1    | 1    | 1    | 1    | 20   | 1    | 1    | 1    | 50   | 1    | 16   |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 70   | 70   | 70   | 70   | 70   | 70   | 70   | 70   | 70   | 70   | 70   | 70   |
| Heavy Vehicles, %        | 0    | 0    | 0    | 5    | 5    | 5    | 0    | 0    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 71   | 1    | 1    | 1    | 1    | 29   | 1    | 1    | 1    | 71   | 1    | 23   |

| Major/Minor          | Minor2 |     | Minor1 |       |       | Major1 |      |   | Major2 |      |   |   |
|----------------------|--------|-----|--------|-------|-------|--------|------|---|--------|------|---|---|
| Conflicting Flow All | 174    | 159 | 13     | 160   | 170   | 2      | 24   | 0 | 0      | 2    | 0 | 0 |
| Stage 1              | 155    | 155 | -      | 4     | 4     | -      | -    | - | -      | -    | - | - |
| Stage 2              | 19     | 4   | -      | 156   | 166   | -      | -    | - | -      | -    | - | - |
| Critical Hdwy        | 7.1    | 6.5 | 6.2    | 7.15  | 6.55  | 6.25   | 4.1  | - | -      | 4.1  | - | - |
| Critical Hdwy Stg 1  | 6.1    | 5.5 | -      | 6.15  | 5.55  | -      | -    | - | -      | -    | - | - |
| Critical Hdwy Stg 2  | 6.1    | 5.5 | -      | 6.15  | 5.55  | -      | -    | - | -      | -    | - | - |
| Follow-up Hdwy       | 3.5    | 4   | 3.3    | 3.545 | 4.045 | 3.345  | 2.2  | - | -      | 2.2  | - | - |
| Pot Cap-1 Maneuver   | 793    | 737 | 1073   | 799   | 718   | 1073   | 1604 | - | -      | 1634 | - | - |
| Stage 1              | 852    | 773 | -      | 1011  | 887   | -      | -    | - | -      | -    | - | - |
| Stage 2              | 1005   | 897 | -      | 839   | 755   | -      | -    | - | -      | -    | - | - |
| Platoon blocked, %   |        |     |        |       |       |        |      | - | -      | -    | - | - |
| Mov Cap-1 Maneuver   | 744    | 704 | 1073   | 769   | 686   | 1073   | 1604 | - | -      | 1634 | - | - |
| Mov Cap-2 Maneuver   | 744    | 704 | -      | 769   | 686   | -      | -    | - | -      | -    | - | - |
| Stage 1              | 851    | 739 | -      | 1010  | 886   | -      | -    | - | -      | -    | - | - |
| Stage 2              | 976    | 896 | -      | 799   | 722   | -      | -    | - | -      | -    | - | - |

| Approach             | EB   |  | WB  |  |  | NB  |  |  | SB  |  |  |
|----------------------|------|--|-----|--|--|-----|--|--|-----|--|--|
| HCM Control Delay, s | 10.3 |  | 8.6 |  |  | 2.4 |  |  | 5.5 |  |  |
| HCM LOS              | B    |  | A   |  |  |     |  |  |     |  |  |

| Minor Lane/Major Mvmt | NBL   | NBT | NBR | EBLn1 | WBLn1 | SBL   | SBT | SBR |
|-----------------------|-------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h)      | 1604  | -   | -   | 748   | 1028  | 1634  | -   | -   |
| HCM Lane V/C Ratio    | 0.001 | -   | -   | 0.099 | 0.031 | 0.044 | -   | -   |
| HCM Control Delay (s) | 7.2   | 0   | -   | 10.3  | 8.6   | 7.3   | 0   | -   |
| HCM Lane LOS          | A     | A   | -   | B     | A     | A     | A   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | -   | 0.3   | 0.1   | 0.1   | -   | -   |

HCM 6th TWSC  
2: SE Norton Lane & SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 5.5  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 70   | 10   | 9    | 1    | 1    | 37   | 1    | 117  | 1    | 87   | 118  | 16   |
| Future Vol, veh/h        | 70   | 10   | 9    | 1    | 1    | 37   | 1    | 117  | 1    | 87   | 118  | 16   |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    | 8    | 0    | 0    | 0    | 0    | 8    |
| Sign Control             | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 80   | 80   | 80   | 80   | 80   | 80   | 80   | 80   | 80   | 80   | 80   | 80   |
| Heavy Vehicles, %        | 11   | 11   | 11   | 0    | 0    | 0    | 11   | 11   | 11   | 4    | 4    | 4    |
| Mvmt Flow                | 88   | 13   | 11   | 1    | 1    | 46   | 1    | 146  | 1    | 109  | 148  | 20   |

| Major/Minor          | Minor2 |       | Minor1 |     | Major1 |     | Major2 |   |   |       |   |   |
|----------------------|--------|-------|--------|-----|--------|-----|--------|---|---|-------|---|---|
| Conflicting Flow All | 556    | 533   | 166    | 537 | 543    | 147 | 176    | 0 | 0 | 147   | 0 | 0 |
| Stage 1              | 384    | 384   | -      | 149 | 149    | -   | -      | - | - | -     | - | - |
| Stage 2              | 172    | 149   | -      | 388 | 394    | -   | -      | - | - | -     | - | - |
| Critical Hdwy        | 7.21   | 6.61  | 6.31   | 7.1 | 6.5    | 6.2 | 4.21   | - | - | 4.14  | - | - |
| Critical Hdwy Stg 1  | 6.21   | 5.61  | -      | 6.1 | 5.5    | -   | -      | - | - | -     | - | - |
| Critical Hdwy Stg 2  | 6.21   | 5.61  | -      | 6.1 | 5.5    | -   | -      | - | - | -     | - | - |
| Follow-up Hdwy       | 3.599  | 4.099 | 3.399  | 3.5 | 4      | 3.3 | 2.299  | - | - | 2.236 | - | - |
| Pot Cap-1 Maneuver   | 428    | 440   | 855    | 458 | 450    | 905 | 1348   | - | - | 1423  | - | - |
| Stage 1              | 621    | 596   | -      | 858 | 778    | -   | -      | - | - | -     | - | - |
| Stage 2              | 809    | 757   | -      | 640 | 609    | -   | -      | - | - | -     | - | - |
| Platoon blocked, %   |        |       |        |     |        |     |        | - | - | -     | - | - |
| Mov Cap-1 Maneuver   | 376    | 399   | 848    | 412 | 408    | 905 | 1338   | - | - | 1423  | - | - |
| Mov Cap-2 Maneuver   | 376    | 399   | -      | 412 | 408    | -   | -      | - | - | -     | - | - |
| Stage 1              | 615    | 541   | -      | 857 | 777    | -   | -      | - | - | -     | - | - |
| Stage 2              | 766    | 756   | -      | 564 | 553    | -   | -      | - | - | -     | - | - |

| Approach             | EB   | WB  | NB  | SB |
|----------------------|------|-----|-----|----|
| HCM Control Delay, s | 17.4 | 9.5 | 0.1 | 3  |
| HCM LOS              | C    | A   |     |    |

| Minor Lane/Major Mvmt | NBL   | NBT | NBR | EBLn1 | WBLn1 | SBL   | SBT | SBR |
|-----------------------|-------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h)      | 1338  | -   | -   | 401   | 852   | 1423  | -   | -   |
| HCM Lane V/C Ratio    | 0.001 | -   | -   | 0.277 | 0.057 | 0.076 | -   | -   |
| HCM Control Delay (s) | 7.7   | 0   | -   | 17.4  | 9.5   | 7.7   | 0   | -   |
| HCM Lane LOS          | A     | A   | -   | C     | A     | A     | A   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | -   | 1.1   | 0.2   | 0.2   | -   | -   |



HCM 6th Signalized Intersection Summary  
 1: SE Norton Lane & NE Three Mile Lane (OR 18)

















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| Movement                     | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations          |      |      |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h)       | 81   | 864  | 125  | 53   | 976  | 100  | 182  | 18   | 68   | 132  | 13   | 147  |
| Future Volume (veh/h)        | 81   | 864  | 125  | 53   | 976  | 100  | 182  | 18   | 68   | 132  | 13   | 147  |
| Initial Q (Qb), veh          | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    |
| Ped-Bike Adj(A_pbT)          | 1.00 |      | 1.00 | 1.00 |      | 1.00 | 1.00 |      | 0.99 | 1.00 |      | 0.99 |
| Parking Bus, Adj             | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Work Zone On Approach        |      | No   |      |      | No   |      |      | No   |      |      | No   |      |
| Adj Sat Flow, veh/h/ln       | 1682 | 1682 | 1682 | 1709 | 1709 | 1709 | 1709 | 1709 | 1709 | 1736 | 1736 | 1736 |
| Adj Flow Rate, veh/h         | 92   | 982  | 58   | 60   | 1109 | 30   | 207  | 20   | 13   | 150  | 15   | 30   |
| Peak Hour Factor             | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Percent Heavy Veh, %         | 5    | 5    | 5    | 3    | 3    | 3    | 3    | 3    | 3    | 1    | 1    | 1    |
| Cap, veh/h                   | 113  | 1424 | 635  | 72   | 1362 | 608  | 315  | 87   | 57   | 188  | 54   | 107  |
| Arrive On Green              | 0.07 | 0.45 | 0.45 | 0.04 | 0.42 | 0.42 | 0.10 | 0.09 | 0.09 | 0.11 | 0.10 | 0.10 |
| Sat Flow, veh/h              | 1602 | 3195 | 1425 | 1628 | 3247 | 1448 | 3158 | 962  | 625  | 1654 | 513  | 1025 |
| Grp Volume(v), veh/h         | 92   | 982  | 58   | 60   | 1109 | 30   | 207  | 0    | 33   | 150  | 0    | 45   |
| Grp Sat Flow(s),veh/h/ln     | 1602 | 1598 | 1425 | 1628 | 1624 | 1448 | 1579 | 0    | 1587 | 1654 | 0    | 1538 |
| Q Serve(g_s), s              | 3.0  | 12.9 | 1.2  | 1.9  | 15.8 | 0.6  | 3.3  | 0.0  | 1.0  | 4.6  | 0.0  | 1.4  |
| Cycle Q Clear(g_c), s        | 3.0  | 12.9 | 1.2  | 1.9  | 15.8 | 0.6  | 3.3  | 0.0  | 1.0  | 4.6  | 0.0  | 1.4  |
| Prop In Lane                 | 1.00 |      | 1.00 | 1.00 |      | 1.00 | 1.00 |      | 0.39 | 1.00 |      | 0.67 |
| Lane Grp Cap(c), veh/h       | 113  | 1424 | 635  | 72   | 1362 | 608  | 315  | 0    | 144  | 188  | 0    | 161  |
| V/C Ratio(X)                 | 0.81 | 0.69 | 0.09 | 0.83 | 0.81 | 0.05 | 0.66 | 0.00 | 0.23 | 0.80 | 0.00 | 0.28 |
| Avail Cap(c_a), veh/h        | 275  | 1710 | 763  | 280  | 1738 | 775  | 543  | 0    | 546  | 284  | 0    | 529  |
| HCM Platoon Ratio            | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 |
| Upstream Filter(I)           | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 1.00 | 0.00 | 1.00 | 1.00 | 0.00 | 1.00 |
| Uniform Delay (d), s/veh     | 24.0 | 11.6 | 8.4  | 24.8 | 13.4 | 9.0  | 22.7 | 0.0  | 22.1 | 22.6 | 0.0  | 21.6 |
| Incr Delay (d2), s/veh       | 10.0 | 0.8  | 0.0  | 16.0 | 2.2  | 0.0  | 1.7  | 0.0  | 0.6  | 7.3  | 0.0  | 0.7  |
| Initial Q Delay(d3),s/veh    | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  |
| %ile BackOfQ(50%),veh/ln     | 1.3  | 3.3  | 0.3  | 0.9  | 4.1  | 0.2  | 1.2  | 0.0  | 0.4  | 2.1  | 0.0  | 0.5  |
| Unsig. Movement Delay, s/veh |      |      |      |      |      |      |      |      |      |      |      |      |
| LnGrp Delay(d),s/veh         | 34.0 | 12.4 | 8.4  | 40.8 | 15.6 | 9.0  | 24.4 | 0.0  | 22.7 | 29.9 | 0.0  | 22.3 |
| LnGrp LOS                    | C    | B    | A    | D    | B    | A    | C    | A    | C    | C    | A    | C    |
| Approach Vol, veh/h          |      | 1132 |      |      | 1199 |      |      | 240  |      |      |      | 195  |
| Approach Delay, s/veh        |      | 14.0 |      |      | 16.7 |      |      | 24.2 |      |      |      | 28.2 |
| Approach LOS                 |      | B    |      |      | B    |      |      | C    |      |      |      | C    |
| Timer - Assigned Phs         | 1    | 2    | 3    | 4    | 5    | 6    | 7    | 8    |      |      |      |      |
| Phs Duration (G+Y+Rc), s     | 7.7  | 26.0 | 9.2  | 9.5  | 6.3  | 27.3 | 9.9  | 8.7  |      |      |      |      |
| Change Period (Y+Rc), s      | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  | 4.0  |      |      |      |      |
| Max Green Setting (Gmax), s  | 9.0  | 28.0 | 9.0  | 18.0 | 9.0  | 28.0 | 9.0  | 18.0 |      |      |      |      |
| Max Q Clear Time (g_c+I1), s | 5.0  | 17.8 | 5.3  | 3.4  | 3.9  | 14.9 | 6.6  | 3.0  |      |      |      |      |
| Green Ext Time (p_c), s      | 0.0  | 4.2  | 0.2  | 0.1  | 0.0  | 4.5  | 0.1  | 0.1  |      |      |      |      |
| <b>Intersection Summary</b>  |      |      |      |      |      |      |      |      |      |      |      |      |
| HCM 6th Ctrl Delay           |      |      |      | 17.0 |      |      |      |      |      |      |      |      |
| HCM 6th LOS                  |      |      |      | B    |      |      |      |      |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 2: SE Norton Lane & SE Stratus Avenue

08/08/2023

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |   |  |   |   |  |   |  |  |   |   |  |   |
| Sign Control                      |   | Stop  |   |   | Stop  |   |  | Stop  |   |   | Yield   |   |
| Traffic Volume (vph)              | 61  | 1   | 9   | 2   | 1   | 75  | 1  | 134   | 1   | 27  | 92  | 80  |
| Future Volume (vph)               | 61  | 1   | 9   | 2   | 1   | 75  | 1  | 134   | 1   | 27  | 92  | 80  |
| Peak Hour Factor                  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  | 0.90   | 0.90  | 0.90  | 0.90  | 0.90  | 0.90  |
| Hourly flow rate (vph)            | 68  | 1   | 10  | 2   | 1   | 83  | 1  | 149   | 1   | 30  | 102   | 89  |
| Direction, Lane #                 | EB 1  | WB 1  | NB 1  | SB 1  |   |   |  |   |   |   |   |   |
| Volume Total (vph)                | 79  | 86  | 151   | 221   |   |   |  |   |   |   |   |   |
| Volume Left (vph)                 | 68  | 2   | 1   | 30  |   |   |  |   |   |   |   |   |
| Volume Right (vph)                | 10  | 83  | 1   | 89  |   |   |  |   |   |   |   |   |
| Hadj (s)                          | 0.18  | -0.49   | 0.00  | -0.13   |   |   |  |   |   |   |   |   |
| Departure Headway (s)             | 5.0   | 4.4   | 4.6   | 4.4   |   |   |  |   |   |   |   |   |
| Degree Utilization, x             | 0.11  | 0.10  | 0.19  | 0.27  |   |   |  |   |   |   |   |   |
| Capacity (veh/h)                  | 654   | 747   | 752   | 786   |   |   |  |   |   |   |   |   |
| Control Delay (s)                 | 8.7   | 7.9   | 8.6   | 8.9   |   |   |  |   |   |   |   |   |
| Approach Delay (s)                | 8.7   | 7.9   | 8.6   | 8.9   |   |   |  |   |   |   |   |   |
| Approach LOS                      | A   | A   | A   | A   |   |   |  |   |   |   |   |   |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |
| Delay                             |   |   | 8.6   |   |   |   |  |   |   |   |   |   |
| Level of Service                  |   |   | A   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 41.3%   | ICU Level of Service  | A   |   |  |   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

HCM 6th TWSC  
 3: SE Norton Lane & Site Access/SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 5.4  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 30   | 1    | 1    | 1    | 1    | 45   | 1    | 1    | 1    | 5    | 1    | 50   |
| Future Vol, veh/h        | 30   | 1    | 1    | 1    | 1    | 45   | 1    | 1    | 1    | 5    | 1    | 50   |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 0    |
| Sign Control             | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 75   | 75   | 75   | 75   | 75   | 75   | 75   | 75   | 75   | 75   | 75   | 75   |
| Heavy Vehicles, %        | 0    | 0    | 0    | 2    | 2    | 2    | 0    | 0    | 0    | 0    | 0    | 0    |
| Mvmt Flow                | 40   | 1    | 1    | 1    | 1    | 60   | 1    | 1    | 1    | 7    | 1    | 67   |

| Major/Minor          | Minor2 |     | Minor1 |       |       | Major1 |      |   | Major2 |      |   |   |
|----------------------|--------|-----|--------|-------|-------|--------|------|---|--------|------|---|---|
| Conflicting Flow All | 83     | 53  | 35     | 54    | 86    | 2      | 68   | 0 | 0      | 2    | 0 | 0 |
| Stage 1              | 49     | 49  | -      | 4     | 4     | -      | -    | - | -      | -    | - | - |
| Stage 2              | 34     | 4   | -      | 50    | 82    | -      | -    | - | -      | -    | - | - |
| Critical Hdwy        | 7.1    | 6.5 | 6.2    | 7.12  | 6.52  | 6.22   | 4.1  | - | -      | 4.1  | - | - |
| Critical Hdwy Stg 1  | 6.1    | 5.5 | -      | 6.12  | 5.52  | -      | -    | - | -      | -    | - | - |
| Critical Hdwy Stg 2  | 6.1    | 5.5 | -      | 6.12  | 5.52  | -      | -    | - | -      | -    | - | - |
| Follow-up Hdwy       | 3.5    | 4   | 3.3    | 3.518 | 4.018 | 3.318  | 2.2  | - | -      | 2.2  | - | - |
| Pot Cap-1 Maneuver   | 909    | 842 | 1044   | 944   | 804   | 1082   | 1546 | - | -      | 1634 | - | - |
| Stage 1              | 969    | 858 | -      | 1018  | 892   | -      | -    | - | -      | -    | - | - |
| Stage 2              | 987    | 897 | -      | 963   | 827   | -      | -    | - | -      | -    | - | - |
| Platoon blocked, %   |        |     |        |       |       |        |      | - | -      | -    | - | - |
| Mov Cap-1 Maneuver   | 854    | 838 | 1044   | 938   | 800   | 1082   | 1546 | - | -      | 1634 | - | - |
| Mov Cap-2 Maneuver   | 854    | 838 | -      | 938   | 800   | -      | -    | - | -      | -    | - | - |
| Stage 1              | 968    | 855 | -      | 1017  | 891   | -      | -    | - | -      | -    | - | - |
| Stage 2              | 930    | 896 | -      | 956   | 824   | -      | -    | - | -      | -    | - | - |

| Approach             | EB  |  | WB  |  |  | NB  |  |  | SB  |  |  |
|----------------------|-----|--|-----|--|--|-----|--|--|-----|--|--|
| HCM Control Delay, s | 9.4 |  | 8.6 |  |  | 2.4 |  |  | 0.6 |  |  |
| HCM LOS              | A   |  | A   |  |  |     |  |  |     |  |  |

| Minor Lane/Major Mvmt | NBL   | NBT | NBR | EBLn1 | WBLn1 | SBL   | SBT | SBR |
|-----------------------|-------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h)      | 1546  | -   | -   | 858   | 1070  | 1634  | -   | -   |
| HCM Lane V/C Ratio    | 0.001 | -   | -   | 0.05  | 0.059 | 0.004 | -   | -   |
| HCM Control Delay (s) | 7.3   | 0   | -   | 9.4   | 8.6   | 7.2   | 0   | -   |
| HCM Lane LOS          | A     | A   | -   | A     | A     | A     | A   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | -   | 0.2   | 0.2   | 0     | -   | -   |

HCM 6th TWSC  
2: SE Norton Lane & SE Stratus Avenue

08/08/2023

| Intersection             |      |      |      |      |      |      |      |      |      |      |      |      |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh         | 3.9  |      |      |      |      |      |      |      |      |      |      |      |
| Movement                 | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
| Lane Configurations      |      | ↕    |      |      | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Vol, veh/h       | 61   | 1    | 9    | 2    | 1    | 75   | 1    | 134  | 1    | 27   | 92   | 80   |
| Future Vol, veh/h        | 61   | 1    | 9    | 2    | 1    | 75   | 1    | 134  | 1    | 27   | 92   | 80   |
| Conflicting Peds, #/hr   | 0    | 0    | 0    | 0    | 0    | 0    | 4    | 0    | 0    | 0    | 0    | 4    |
| Sign Control             | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized           | -    | -    | None | -    | -    | None | -    | -    | None | -    | -    | None |
| Storage Length           | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    | -    |
| Veh in Median Storage, # | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Grade, %                 | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    | -    | 0    | -    |
| Peak Hour Factor         | 90   | 90   | 90   | 90   | 90   | 90   | 90   | 90   | 90   | 90   | 90   | 90   |
| Heavy Vehicles, %        | 5    | 5    | 5    | 5    | 5    | 5    | 0    | 0    | 0    | 5    | 5    | 5    |
| Mvmt Flow                | 68   | 1    | 10   | 2    | 1    | 83   | 1    | 149  | 1    | 30   | 102  | 89   |

| Major/Minor          | Minor2 |       | Minor1 |       | Major1 |       | Major2 |   |   |       |   |   |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 405    | 363   | 151    | 364   | 407    | 150   | 195    | 0 | 0 | 150   | 0 | 0 |
| Stage 1              | 211    | 211   | -      | 152   | 152    | -     | -      | - | - | -     | - | - |
| Stage 2              | 194    | 152   | -      | 212   | 255    | -     | -      | - | - | -     | - | - |
| Critical Hdwy        | 7.15   | 6.55  | 6.25   | 7.15  | 6.55   | 6.25  | 4.1    | - | - | 4.15  | - | - |
| Critical Hdwy Stg 1  | 6.15   | 5.55  | -      | 6.15  | 5.55   | -     | -      | - | - | -     | - | - |
| Critical Hdwy Stg 2  | 6.15   | 5.55  | -      | 6.15  | 5.55   | -     | -      | - | - | -     | - | - |
| Follow-up Hdwy       | 3.545  | 4.045 | 3.345  | 3.545 | 4.045  | 3.345 | 2.2    | - | - | 2.245 | - | - |
| Pot Cap-1 Maneuver   | 551    | 560   | 887    | 586   | 529    | 889   | 1390   | - | - | 1413  | - | - |
| Stage 1              | 784    | 722   | -      | 843   | 766    | -     | -      | - | - | -     | - | - |
| Stage 2              | 801    | 766   | -      | 783   | 691    | -     | -      | - | - | -     | - | - |
| Platoon blocked, %   |        |       |        |       |        |       |        | - | - | -     | - | - |
| Mov Cap-1 Maneuver   | 487    | 544   | 884    | 567   | 514    | 889   | 1385   | - | - | 1413  | - | - |
| Mov Cap-2 Maneuver   | 487    | 544   | -      | 567   | 514    | -     | -      | - | - | -     | - | - |
| Stage 1              | 780    | 702   | -      | 842   | 765    | -     | -      | - | - | -     | - | - |
| Stage 2              | 724    | 765   | -      | 754   | 672    | -     | -      | - | - | -     | - | - |

| Approach             | EB   |  | WB  |  | NB  |  | SB |  |
|----------------------|------|--|-----|--|-----|--|----|--|
| HCM Control Delay, s | 13.2 |  | 9.6 |  | 0.1 |  | 1  |  |
| HCM LOS              | B    |  | A   |  |     |  |    |  |

| Minor Lane/Major Mvmt | NBL   | NBT | NBR | EBLn1 | WBLn1 | SBL   | SBT | SBR |
|-----------------------|-------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h)      | 1385  | -   | -   | 517   | 868   | 1413  | -   | -   |
| HCM Lane V/C Ratio    | 0.001 | -   | -   | 0.153 | 0.1   | 0.021 | -   | -   |
| HCM Control Delay (s) | 7.6   | 0   | -   | 13.2  | 9.6   | 7.6   | 0   | -   |
| HCM Lane LOS          | A     | A   | -   | B     | A     | A     | A   | -   |
| HCM 95th %tile Q(veh) | 0     | -   | -   | 0.5   | 0.3   | 0.1   | -   | -   |

**1. OR-18 & Norton Lane**

Right Turns on Red  
 APM Section 13.4.2: RTOR  
 Equation:  $vRTOR = sRTOR * (r/C)$

| AM Peak Hour    |       |     |     |     |     |     |     |     |    |       |     |     |     |
|-----------------|-------|-----|-----|-----|-----|-----|-----|-----|----|-------|-----|-----|-----|
|                 | sRTOR |     |     |     | r   |     |     |     | C  | vRTOR |     |     |     |
|                 | EBR   | WBR | NBR | SBR | EBR | WBR | NBR | SBR |    | EBR   | WBR | NBR | SBR |
| 2023 Existing   | 142   | 140 | 24  | 55  | 48  | 48  | 48  | 48  | 70 | 97    | 96  | 16  | 38  |
| 2025 Background | 157   | 140 | 45  | 57  | 48  | 48  | 48  | 48  | 70 | 108   | 96  | 31  | 39  |
| 2025 Buildout   | 166   | 140 | 59  | 57  | 48  | 48  | 48  | 48  | 70 | 114   | 96  | 40  | 39  |

| PM Peak Hour    |       |     |     |     |     |     |     |     |    |       |     |     |     |
|-----------------|-------|-----|-----|-----|-----|-----|-----|-----|----|-------|-----|-----|-----|
|                 | sRTOR |     |     |     | r   |     |     |     | C  | vRTOR |     |     |     |
|                 | EBR   | WBR | NBR | SBR | EBR | WBR | NBR | SBR |    | EBR   | WBR | NBR | SBR |
| 2023 Existing   | 123   | 123 | 53  | 160 | 48  | 48  | 58  | 58  | 80 | 74    | 74  | 38  | 116 |
| 2025 Background | 123   | 123 | 68  | 167 | 48  | 48  | 58  | 58  | 80 | 74    | 74  | 49  | 121 |
| 2025 Buildout   | 123   | 123 | 77  | 167 | 48  | 48  | 58  | 58  | 80 | 74    | 74  | 56  | 121 |

Intersection v/c  
 APM Section 13.4.4: Critical Intersection v/c ratio  
 Method: Determine Critical Movements in HCM 2000 reports  
 HCM 6th reports, determine adjusted and sat flow rates  
 Adjust Flow/Sat Flow  
 Sum up Crit Movement Flow Rates  
 $X_c \text{ of intersection} = \text{sum}(\text{crit.move. Flow rates} * (C / (C-L)))$

| AM Peak Hour    |                    |             |     |        |     |                |      |        |      |               |          |          |          |          |    |    |       |  |  |
|-----------------|--------------------|-------------|-----|--------|-----|----------------|------|--------|------|---------------|----------|----------|----------|----------|----|----|-------|--|--|
|                 | Critical Movement  | Adjust Flow |     |        |     | Saturated Flow |      |        |      | Adj/Sat Flows |          |          |          | Sum      | C  | L  | Xc    |  |  |
|                 |                    | EBL         | WBT | NBTh+R | SBL | EBL            | WBT  | NBTh+R | SBL  | EBL           | WBT      | NBTh+R   | SBL      |          |    |    |       |  |  |
| 2023 Existing   |                    | 136         | 705 | 33     | 68  | 1602           | 3169 | 1519   | 1537 | 0.084894      | 0.222468 | 0.021725 | 0.044242 | 0.373328 | 70 | 16 | 0.484 |  |  |
| 2025 Background | EBL WBT NBTh+R SBL | 142         | 725 | 43     | 70  | 1602           | 3169 | 1498   | 1537 | 0.088639      | 0.228779 | 0.028705 | 0.045543 | 0.391666 | 70 | 16 | 0.508 |  |  |
| 2025 Buildout   |                    | 142         | 725 | 51     | 70  | 1602           | 3169 | 1489   | 1537 | 0.088639      | 0.228779 | 0.034251 | 0.045543 | 0.397212 | 70 | 16 | 0.515 |  |  |

| PM Peak Hour    |                    |             |      |     |        |                |      |      |        |               |          |          |          |          |    |    |       |  |  |
|-----------------|--------------------|-------------|------|-----|--------|----------------|------|------|--------|---------------|----------|----------|----------|----------|----|----|-------|--|--|
|                 | Critical Movement  | Adjust Flow |      |     |        | Saturated Flow |      |      |        | Adj/Sat Flows |          |          |          | Sum      | C  | L  | Xc    |  |  |
|                 |                    | EBL         | WBT  | SBL | SBTh+R | EBL            | WBT  | SBL  | SBTh+R | EBL           | WBT      | SBL      | SBTh+R   |          |    |    |       |  |  |
| 2023 Existing   |                    | 89          | 1078 | 144 | 35     | 1602           | 3247 | 1654 | 1505   | 0.055556      | 0.331999 | 0.087062 | 0.023256 | 0.497872 | 80 | 16 | 0.622 |  |  |
| 2025 Background | EBL WBT SBL SBTh+R | 92          | 1109 | 150 | 32     | 1602           | 3247 | 1654 | 1533   | 0.057428      | 0.341546 | 0.090689 | 0.020874 | 0.510538 | 80 | 16 | 0.638 |  |  |
| 2025 Buildout   |                    | 92          | 1109 | 150 | 45     | 1602           | 3247 | 1654 | 1538   | 0.057428      | 0.341546 | 0.090689 | 0.029259 | 0.518922 | 80 | 16 | 0.649 |  |  |

# **NEIGHBORHOOD MEETING MATERIALS**

**8**



**CIVIL LAND USE PLANNING SURVEY**  
 P 503.643.8286 F 844.715.4743 www.pd-grp.com  
 9020 SW Washington Square Rd Suite 170  
 Portland, Oregon 97223

June 7, 2023

**RE: NOTICE OF NEIGHBORHOOD REVIEW MEETING – PROPOSED APARTMENT COMPLEX ON SE NORTON LANE, WITHIN THREE MILE LANE PLANNED DEVELOPMENT.**

Dear Resident or Property Owner:

Pioneer Design Group, Inc. is representing the owner and developer of property located at the south end of SE Norton Lane, just south of Altimus Plaza Medical Offices. The site is more specifically identified as Tax Lot 701 of Tax Map T4S R4W 27, as shown on the attached map. The developer is proposing a 144-Unit Apartment Complex in eight 3-Story Buildings. It is anticipated that Master Plan Approval; Planned Development Approval; and Three-Mile Lane Design Review Approval will be required for the development.

Prior to applying to the City of McMinnville we would like to take the opportunity to discuss the proposal in more detail with you.

The purpose of this neighborhood meeting is to provide an informal forum for the applicant and surrounding property owners/residents to review the proposal and to identify issues so that such issues may be considered before the formal application is turned in to the City. This meeting gives you the opportunity to share with us any special information you know about the property involved. We will attempt to answer questions which may be relevant to meeting development standards consistent with City of McMinnville’s Community Development Code and Comprehensive Plan.

Pursuant to Section 17.72.095 of the McMinnville Development Code, you are invited to attend a Neighborhood meeting on:

**Wednesday, June 28, 2023  
 at 6:00 pm  
 McMinnville Library – Carnegie Room  
 225 NW Adams Street  
 McMinnville, OR 97128**

Please note this meeting will be an informational meeting to discuss the proposed development plans. Following this meeting a formal Land Use Application will be submitted to the City of McMinnville. After submittal you may receive a future official notice from the City for you to either participate with written comments and/or an opportunity to attend a public hearing.

I look forward to more specifically discussing the proposal with you. Please feel free to call me at 541-993-9015 or email [baltman@pd-grp.com](mailto:baltman@pd-grp.com) if you have questions.

Sincerely,

**Ben Altman  
 Senior Planner**

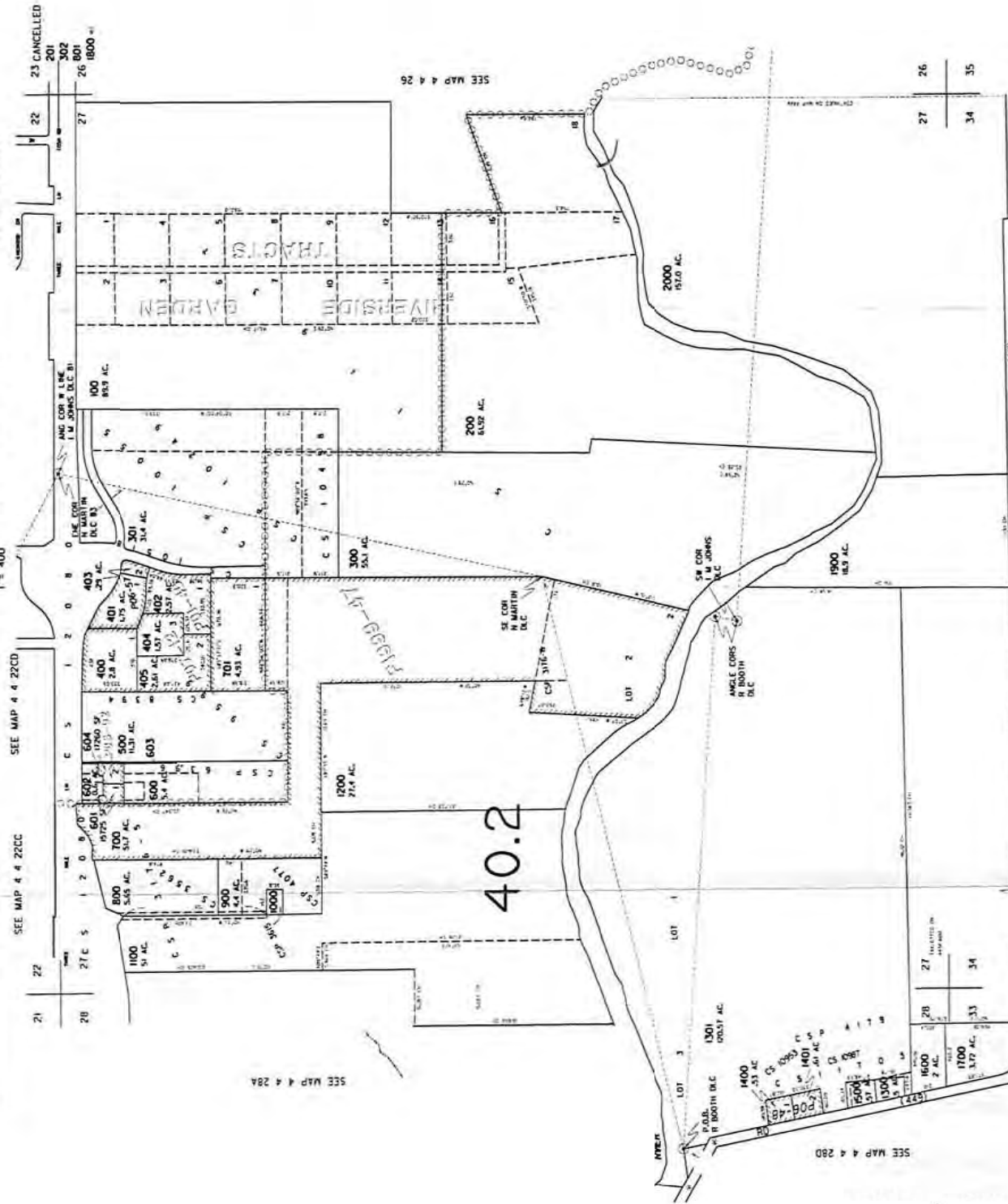
THIS MAP WAS PREPARED FOR  
ASSESSMENT PURPOSE ONLY

SECTION 27 T4S R4W W.M.  
YAMHILL COUNTY

4 4 27

SEE MAP 4 4 2200

SEE MAP 4 4 22



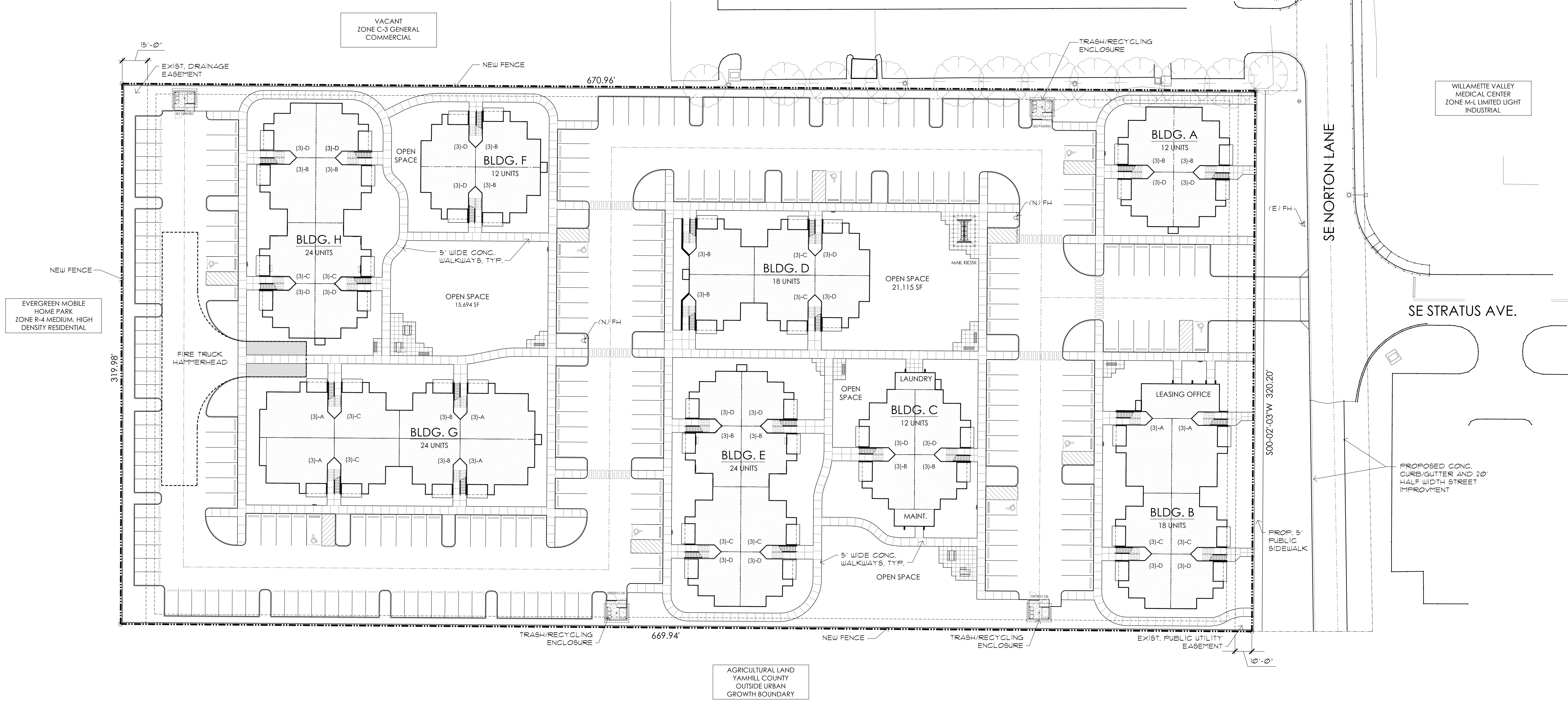
4 4 27

SEE MAP 4 4 34



**PROJECT DATA**

|                          |                              |     |
|--------------------------|------------------------------|-----|
| <b>LIVING UNITS</b>      |                              |     |
| ONE BEDROOM/ 1 BATH:     | 48 UNITS                     | 33% |
| TWO BEDROOM/ 1 BATH:     | 36 UNITS                     | 25% |
| TWO BEDROOM/ 2 BATH:     | 42 UNITS                     | 29% |
| THREE BEDROOM/ 2 BATH:   | 18 UNITS                     | 16% |
| <b>TOTAL:</b>            | <b>144 UNITS</b>             |     |
| <b>PARKING REQUIRED</b>  |                              |     |
| 1BR AND 2BR UNITS:       | 1.5 SPACES/UNIT = 189 SPACES |     |
| 3BR UNITS:               | 2.0 SPACES/UNIT = 36 SPACES  |     |
| <b>TOTAL REQUIRED</b>    | <b>225 SPACES</b>            |     |
| <b>PARKING PROVIDED:</b> | <b>226 SPACES</b>            |     |



FRESENIUS KIDNEY CARE PNRS  
MCMINNVILLE KIDNEY  
CARE CENTER  
ZONE C-3 GENERAL  
COMMERCIAL

VACANT  
ZONE C-3 GENERAL  
COMMERCIAL

WILLAMETTE VALLEY  
MEDICAL CENTER  
ZONE M-L LIMITED LIGHT  
INDUSTRIAL

EVERGREEN MOBILE  
HOME PARK  
ZONE R-4 MEDIUM, HIGH  
DENSITY RESIDENTIAL

AGRICULTURAL LAND  
YAMHILL COUNTY  
OUTSIDE URBAN  
GROWTH BOUNDARY

1 CONCEPTUAL SITE PLAN  
1" = 30'-0"

PRELIMINARY  
NOT FOR  
CONSTRUCTION

**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED

6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANE APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

CONCEPTUAL SITE PLAN

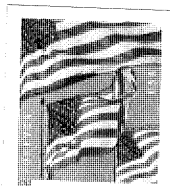
|          |              |
|----------|--------------|
| date:    | JUNE 2, 2023 |
| scale:   | AS NOTED     |
| drawn:   |              |
| job no.: | 2301         |

**A**  
**1.0**

| PIN                      | SITUS                  | OWNER                                | OWNER                              | MAILING ADD                    | MAILING ADD | MAILING ADD | MAILING ADD |
|--------------------------|------------------------|--------------------------------------|------------------------------------|--------------------------------|-------------|-------------|-------------|
| R4427 00701              |                        | KWDS LLC                             |                                    | PO BOX 145                     | WILSONVILLE | OR          | 97070       |
| R4427 00402              | 375 SE NORTON LN       | ROBERTI INVESTMENT COMPANY LLC       |                                    | 10333 MAIN ST STE 263          | BELLEVUE    | WA          | 98004       |
| R4427 00405, R4427 00404 |                        | HOUSING AUTHORITY OF YAMHILL COUNTY  |                                    | 135 NE DUNN PL                 | MCMINNVILLE | OR          | 97128       |
| R4427 00500              | 2400 SE STRATUS AVE 38 | STRATUS AVENUE HOLDINGS LLC          |                                    | 18150 SW BOONES FERRY RD       | PORTLAND    | OR          | 97224       |
| R4427 00700              | 2270 SE THREE MILE LN  | MOYER PHYLLIS TRUSTEE FOR            | MCMULLIN GREGORY K TRUSTEE         | 6738 E HUBBELL                 | SCOTTSDALE  | AZ          | 85257       |
| R4427 00301              | 2700 SE THREE MILE LN  | MPT OF MCMINNVILLE-CAPELLA LLC       | ATTN: MEDICAL PROPERTIES TRUST INC | 1000 URBAN CENTER DR SUITE 501 | BIRMINGHAM  | AL          | 35242       |
|                          |                        | CITY OF MCMINNVILLE                  | ATTN: PLANNING DIVISION            | 231 NE FIFTH STREET            | MCMINNVILLE | OR          | 97128       |
|                          |                        | REITER DESIGN ARCHITECT INCORPORATED |                                    | 6107 SW MURRAY BLVD, # 480     | BEAVERTON   | OR          | 97008       |



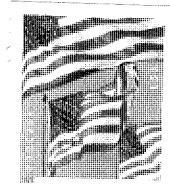
CIVIL • PLANNING • SURVEY • LANDSCAPE  
P 503.643.8286 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
Portland, Oregon 97223



KWDS LLC  
PO BOX 145  
WILSONVILLE, OR 97070



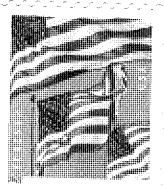
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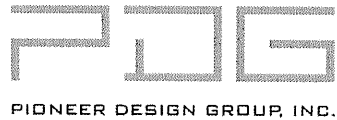
ROBERTI INVESTMENT COMPANY LLC  
10333 MAIN ST STE 263  
BELLEVUE, WA 98004



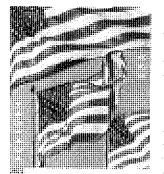
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P 503.643.8286 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
Portland, Oregon 97223



HOUSING AUTHORITY OF YAMHILL COUNTY  
135 NE DUNN PL  
MCMINNVILLE, OR 97128



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P 503.643.8286 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
Portland, Oregon 97223



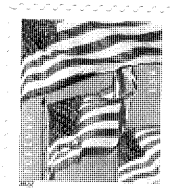
STRATUS AVENUE HOLDINGS LLC  
18150 SW BOONES FERRY RD  
PORTLAND, OR 97224



PIONEER DESIGN GROUP, INC.

CIVIL • PLANNING • SURVEY • LANDSCAPE

P 503.643.8286 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
Portland, Oregon 97223



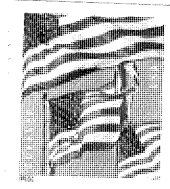
MOYER PHYLLIS TRUSTEE FOR MCMULLIN GREGORY  
K TRUSTEE  
6738 E HUBBELL  
SCOTTSDALE, AZ 85257



PIONEER DESIGN GROUP, INC.

CIVIL • PLANNING • SURVEY • LANDSCAPE

P 503.643.8286 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
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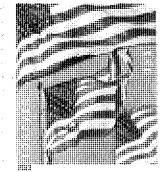


MPT OF MCMINNVILLE-CAPELLA LLC ATTN:  
MEDICAL PROPERTIES TRUST INC  
1000 URBAN CENTER DR SUITE 501  
BIRMINGHAM, AL 35242



PIONEER DESIGN GROUP, INC.

CIVIL • PLANNING • SURVEY • LANDSCAPE  
P 503.643.8288 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
Portland, Oregon 97223

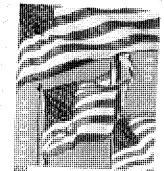


CITY OF MCMINNVILLE ATTN: PLANNING DIVISION  
231 NE FIFTH STREET  
MCMINNVILLE, OR 97128



PIONEER DESIGN GROUP, INC.

CIVIL • PLANNING • SURVEY • LANDSCAPE  
P 503.643.8286 www.pd-grp.com  
9020 SW Washington Square Rd Suite 170  
Portland, Oregon 97223



REITER DESIGN ARCHITECT INCORPORATED  
6107 SW MURRAY BLVD, # 480  
BEAVERTON, OR 97008



# NEIGHBORHOOD MEETING

## Future Land Use Application:

Proposed 144-Unit Apartment Complex  
in Eight 3-Story Buildings on SE Norton  
Lane, within the Three Mile Lane  
Planned Development

**DATE:** Wednesday June 28, 2023

**TIME:** 6:00 PM

## Meeting Location:

McMinnville Library - Carnegie Room  
225 NW Adams Street  
McMinnville, OR 97128

**Contact Name:** Ben Altman

**Contact Number:** 541-993-9015

Per Request of Section 17.72.095 of  
McMinnville City Code Requirements

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Per Request of Section 17.72.095 of  
McMinnville City Code Requirements

06/07/2023 4:31 PM







### McMinnville Public Library Hours

|           |             |
|-----------|-------------|
| Tuesday   | 10am to 7pm |
| Wednesday | 10am to 7pm |
| Thursday  | 10am to 6pm |
| Friday    | 10am to 6pm |
| Saturday  | 12pm to 5pm |
| Sunday    | 12pm to 5pm |
| Monday    | Closed      |

503-435-5562  
visit us online [mcp.library.org](http://mcp.library.org)

### Horarios de la Biblioteca Pública de McMinnville

|           |             |
|-----------|-------------|
| martes    | 10am to 7pm |
| miércoles | 10am to 7pm |
| jueves    | 10am to 6pm |
| viernes   | 10am to 6pm |
| sábado    | 12pm to 5pm |
| domingo   | 12pm to 5pm |
| lunes     | Cerrado     |

503-435-5562  
[mcp.library.org](http://mcp.library.org)

**Neighborhood Meeting Tonight**  
6:00 PM - 8:00 PM  
Community Room  
PUBLIC WELCOME TO ATTEND  
  
The City of McMinnville sponsors an informational Neighborhood Meeting to discuss a proposed development application located at 2000 West Street, adjacent to the City Library. The proposed development will consist of approximately 100,000 sq ft of retail space.  
Meeting location:  
City Library Building

NO SE LE DEBE REGAR A NINGUNA PERSONA LA DECISION DE USAR LA BIBLIOTECA, POR MOTIVOS DE EDAD, RAZA, ANTECEDENTES PERSONALES O PUNTO DE VISTA.  
  
Todos son bienvenidos.  
Todos son iguales.  
Apoyamos a las diferencias.

**NOTICE / AVISO**  
**Service Animal Policy**  
The City of McMinnville Library is committed to providing a safe and accessible environment for all patrons. Service animals are permitted in the library building and outdoor areas. Emotional support animals are not permitted in the library building or outdoor areas.  
Service animals are defined as animals that are trained to perform specific tasks for individuals with disabilities. Emotional support animals are not trained to perform specific tasks and are not considered service animals.  
The City of McMinnville Library is not responsible for the care or supervision of service animals. Patrons are responsible for the care and supervision of their service animals at all times.  
If you have a service animal, please contact the library at 503-435-5562 for more information.

**2013 MEMBER**

**WRITE YOUR NOVEL HERE!**



## **Neighborhood Meeting Tonight**

JUNE 28<sup>TH</sup> AT 6:00 PM

McMINNVILLE PUBLIC LIBRARY

Carnegie Room

### **PUBLIC WELCOME TO ATTEND**

You are cordially invited to attend an informational Neighborhood Meeting to discuss a proposed apartment development located on SE Norton Lane just South of Altimus Plaza Medical Offices. The proposed development will consist of approximately 144 living units in eight three-story buildings.







# NEIGHBORHOOD MEETING ATTENDANCE ROSTER

PROJECT: NORTON LANE APTS. MEETING DATE: 6/28/23

**PUBLIC RECORDS LAW DISCLOSURE:** This sign-in sheet is a public record of ~~Washington~~ <sup>Yamhill</sup> County and it is subject to public disclosure under Oregon Public Records Law.

**PLEASE PRINT LEGIBLY!**

| PRINTED NAME    | FULL MAILING ADDRESS & E-MAIL ADDRESS | CITY, STATE    | ZIP CODE | PHONE #      |
|-----------------|---------------------------------------|----------------|----------|--------------|
| Chad Waddington | wchw13@gmail.com                      | Mcminville, OR | 97128    | 253-329-4515 |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |
|                 |                                       |                |          |              |

(Attachment I)

# NORTON LANE APARTMENTS

## NEIGHBORHOOD MEETING

### OUTLINE

**SITE INFORMATION:**

- Site Area: 4.93 Acres (214,759 SF)
- Land-Use Designation: C-3, General Commercial
- Three-Mile Lane Planned Development Overlay

**PARKING:**

- Parking Required: 216 Spaces
- Parking Provided: 220 Spaces

**BUILDING INFORMATION:**

- Construction Type: Type V-B, Non-Rated, NFPA 13-R Fire Sprinklers
- Wood-Framed Construction
- Three-Stories
- Occupancy Group: R-2 Residential, Apartments

**LIVING UNITS:**

- One-Bedroom, 1 Bath: 703 SF 42 Units
- Two-Bedroom, 1 Bath: 837 SF 36 Units
- Two-Bedroom, 2 Bath: 903 SF 42 Units
- Three-Bedroom, 2 Bath: 1,100 SF 18 Units

Total Living Units: 138 Units

**LAND-USE APPLICATIONS:**

- Three-Mile Lane Planned Development Approval
- Three-Mile Lane Design Review
- Planned Development Master Plan Approval
- Landscape Review
- Minor Administrative Variance
  - Building Height Increase: 35 feet to 38 feet
  - Parking Island Spacing: 60 foot maximum to 64 foot maximum
  - Parking Setback at Side and Rear Property Lines: 10 Foot min. reduced to 6 Foot Min.

# NEIGHBORHOOD MEETING MINUTES

## Norton Lane Apartments

6/28/23

6:00PM – 6:45PM

The meeting began at 6:00pm. The meeting was held at the McMinnville Public Library, Carnegie Room. We waited until approximately 6:05pm to formally start the meeting to allow ample time for interested parties to arrive for the meeting.

The following is the order of topics and issues discussed.

- Welcome to The Norton Lane Apartments Neighborhood Meeting
  - The subject Property is located on SE Norton Lane at the intersection of SE Stratus Avenue.
  - Current zoning is C-3 General Commercial with a Planned Development Overlay, which allows for multifamily apartments.
  - Proposed development is for 144 apartment units on a vacant 4.93 acre site.
- Introductions
  - Scott and Tammi Reiter from Reiter Design Architect Incorporated, Architect and Planner
  - Owner's Representative, Kim McAvoy
  - Daniel Stumpf, Transportation Engineer with Lancaster-Mobley
- Project Description
  - The proposed project is for the development of 144 apartment units on a 4.93 acre vacant site. There will be eight three story apartment buildings.
  - There is a mix of one, two and three bedroom units. The proposed unit mix was presented.
  - The proposed improvements will include street frontage improvements of SE Norton Lane to the end of the property.
  - The project is located within the Three Mile Lane Planned Development Overlay. The proposed development application will go before the planning commission.
  - There are existing water and sewer utility lines in the SE Norton Lane right-of-way that will serve the site.
  - We presented a photo board with pictures of similar projects built and owned by the same developer and advised that this project would look very similar.
- Project Time Line
  - Land-Use Application and Processing: This is likely a 3-4 month process to obtain land-use approvals. Construction Permits will take approximately 3-4 months. The earliest anticipated construction start will be spring 2023.
- Neighbor questions received: There was only one person from the neighborhood who attended the meeting. The following are the questions he asked and the answers provided:
  - What are the plans for improving the intersection of SE Norton Lane and SE Stratus Avenue just south of the highway 18 intersection? The current 3-way stop is confusing to motorists.



Daniel Stumpf, transportation engineer advised that the intersection in question will be analyzed, however what not sure what improvement could be made due to the proximity with the highway intersection.

- He was concerned about any potential impacts to the hospital. Scott Reiter responded as to his concerns over any impacts and assured that the development on-site would not have any adverse impacts.
- He commented that he thought the apartment development would be welcomed by the hospital employees and that many would likely want to rent that close to the hospital.
- He also asked the owner's representative about a possibility of the Willamette Valley Cancer Foundation leasing units for family members of the hospital oncology patients to stay in during their family member's treatment.

Kim McAvoy responded that they were certainly interested in seeing if there was a possibly for the cancer foundation to lease units for this purpose. They exchanged information for future contact.

- The attending neighbor left the meeting at 6:30. We waited another 15 minutes to 6:45 and nobody else showed up.

The meeting concluded 6:45pm.









NE Cumulus Ave

18

NE Thre

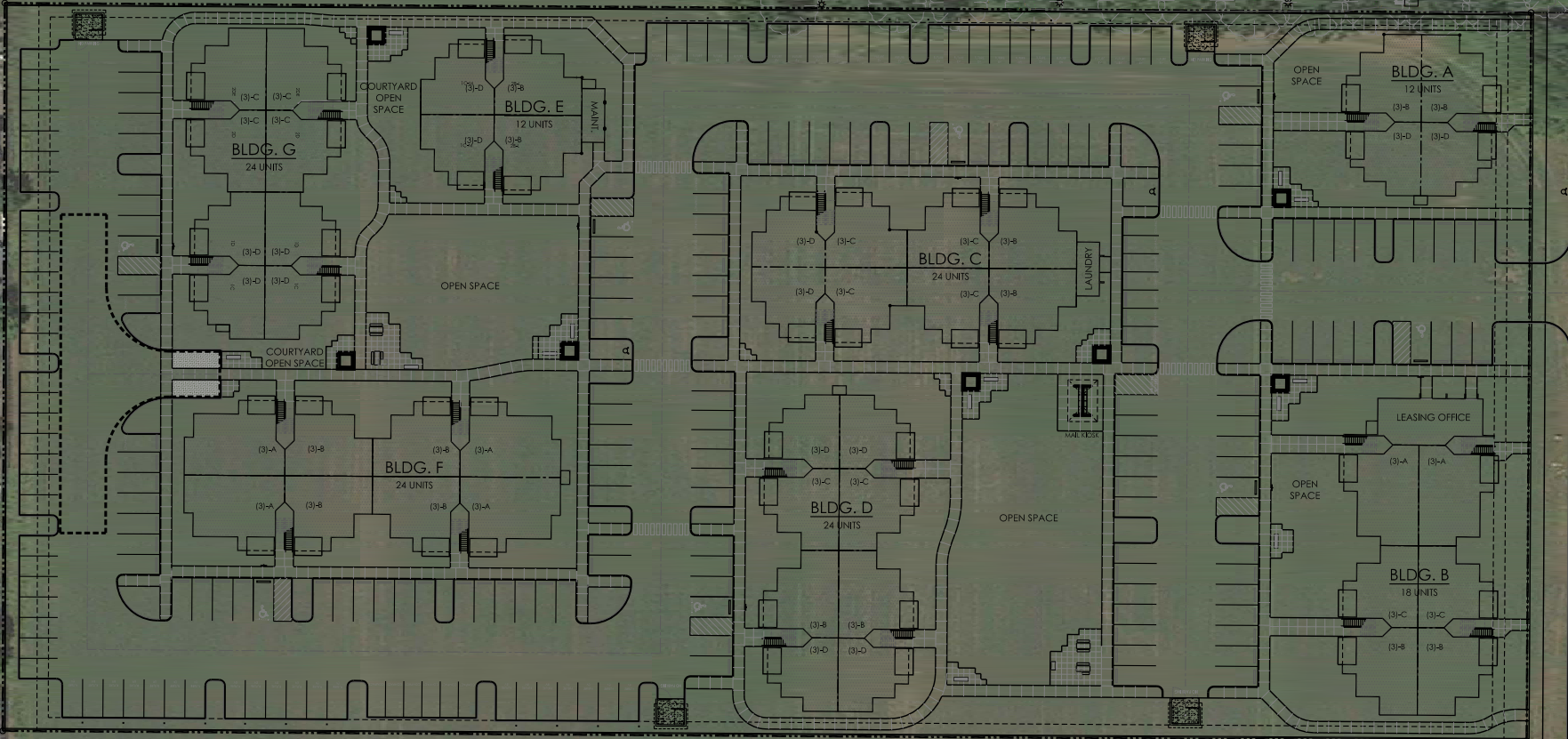
SE Stratus Ave

SE Norton Ln

Evergreen Pkwy E

SE Stratus Ave

SE Stratus Ave





**APPLICATION DRAWINGS**  
ARCHITECTURAL,  
CIVIL AND LANDSCAPE

**9**



**PROJECT DATA**

**SITE INFORMATION:**  
 ZONING: C3 - GENERAL COMMERCIAL, THREE-MILE LANE PLANNED DEVELOPMENT OVERLAY  
 MAP NO: T4SR4W-27, TAX LOT 701  
 SITE AREA: 214,759 SF, 4.93 ACRES

**BUILDING INFORMATION**  
 CONSTRUCTION TYPE: TYPE V-B  
 BUILDING HEIGHT: THREE-STORIES  
 OCCUPANCY GROUPS: R-2 RESIDENTIAL, APARTMENTS

**LIVING UNITS:**  
 ONE-BEDROOM, 1 BATH: 42 UNITS  
 TWO-BEDROOM, 1 BATH: 36 UNITS  
 TWO-BEDROOM, 2 BATH: 42 UNITS  
 THREE-BEDROOM, 2 BATH: 18 UNITS  
 TOTAL UNITS: 138 UNITS

**PARKING:**  
 REQUIRED:  
 1BR AND 2BR UNITS, 1.5 SPACES/UNIT= 180 SPACES  
 3BR UNITS, 2 SPACES/UNIT= 36 SPACES  
 TOTAL REQUIRED: 216 SPACES

**PROVIDED:**  
 STANDARD SPACES: 188 SPACES  
 COMPACT SPACES: 22 SPACES  
 ACCESSIBLE SPACES: 8 SPACES  
 TOTAL PARKING PROVIDED: 218 SPACES

**PROJECT TEAM**

**OWNER/DEVELOPER:**  
 KWDS, LLC  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**GENERAL CONTRACTOR:**  
 KOHL INC.  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**ARCHITECT/PLANNER:**  
 REITER DESIGN ARCHITECT, INC.  
 7965 SW CIRRUS DRIVE  
 BEAVERTON, OREGON 97008  
 (503) 574-3036  
 PROJECT ARCHITECT: SCOTT REITER

**CIVIL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 250 NW FRANKLIN AVE., SUITE 404  
 BEND, OREGON 97703  
 (541) 318-1161  
 PROJECT ENG.: JENNIFER VAN CAMP, PE

**STRUCTURAL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT ENGINEER: JEFF SCHWINDT, PE

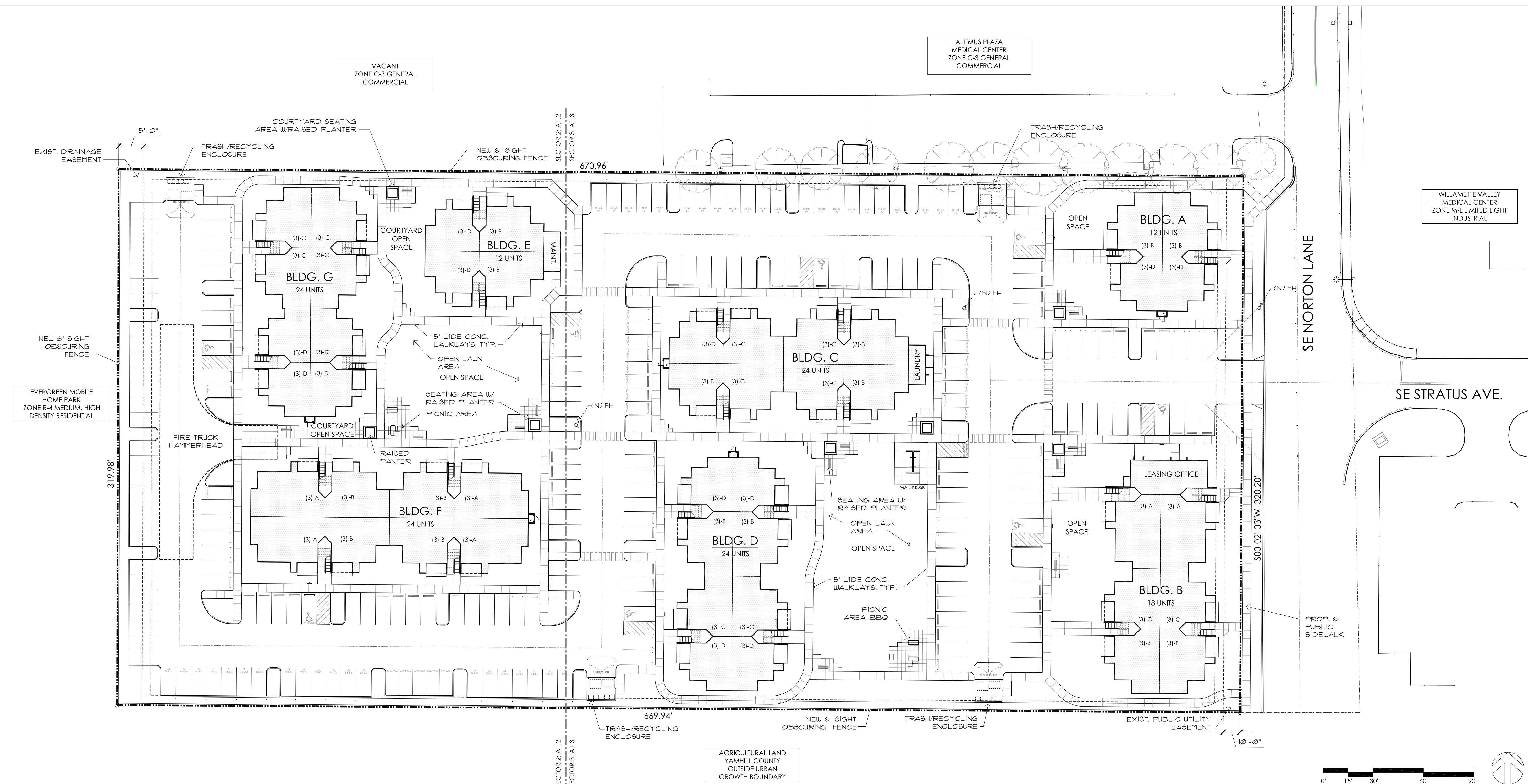
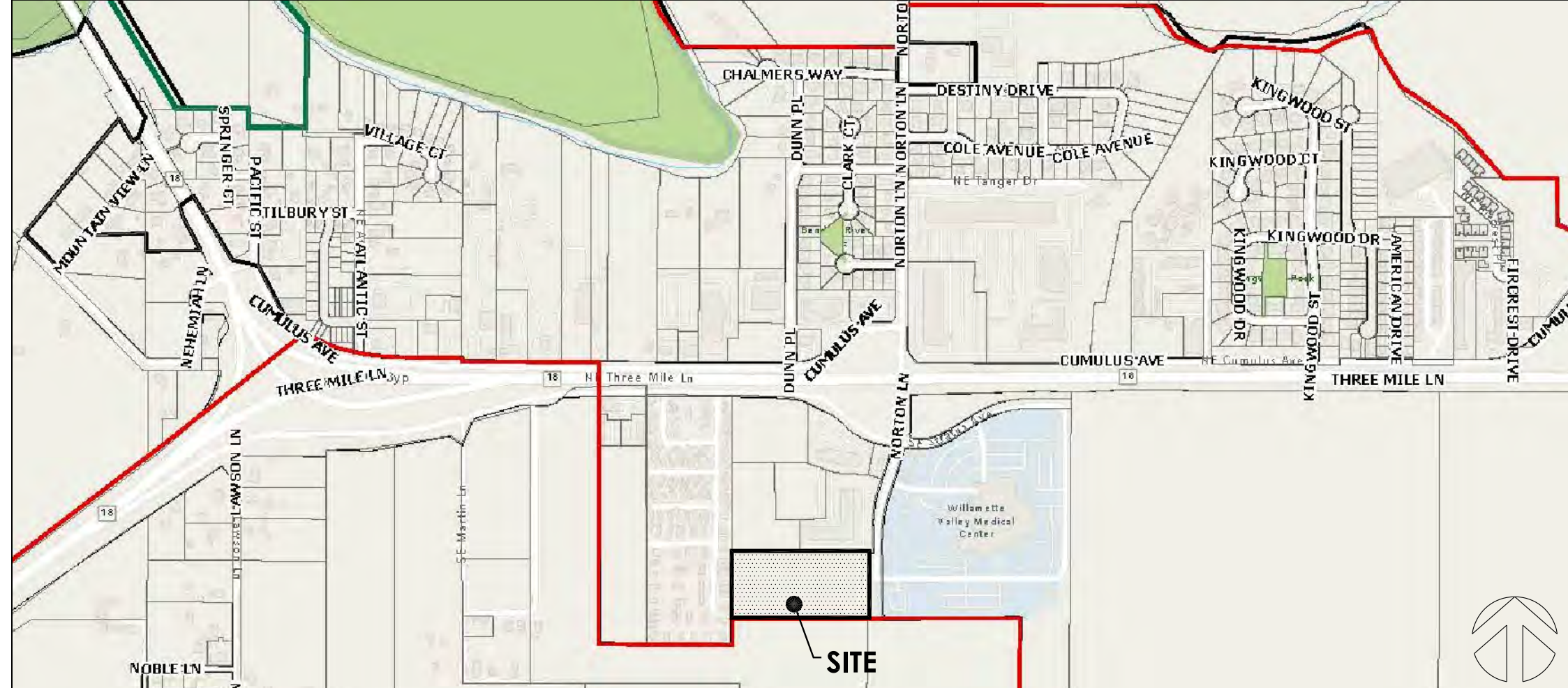
**TRANSPORTATION ENGINEER:**  
 LANCASTER-MOBY  
 321 SW 4TH, AVE, SUITE 400  
 PORTLAND, OREGON 97204  
 (503) 248-0313  
 CONTACT: DANIEL STUMPF

**LANDSCAPE ARCHITECT:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT LANDSCAPE ARCH.: JEFFERY CREEL

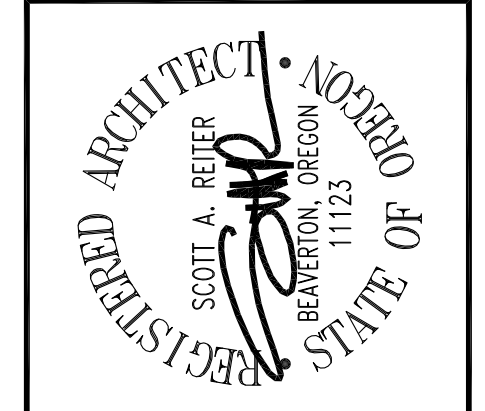
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**VICINITY MAP**



**CONCEPTUAL SITE PLAN** 1  
 Scale: 1" = 30'-0" A1.0



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 BEAVERTON, OREGON 97008 (503) 574-3036

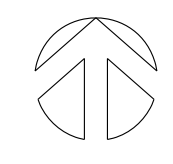
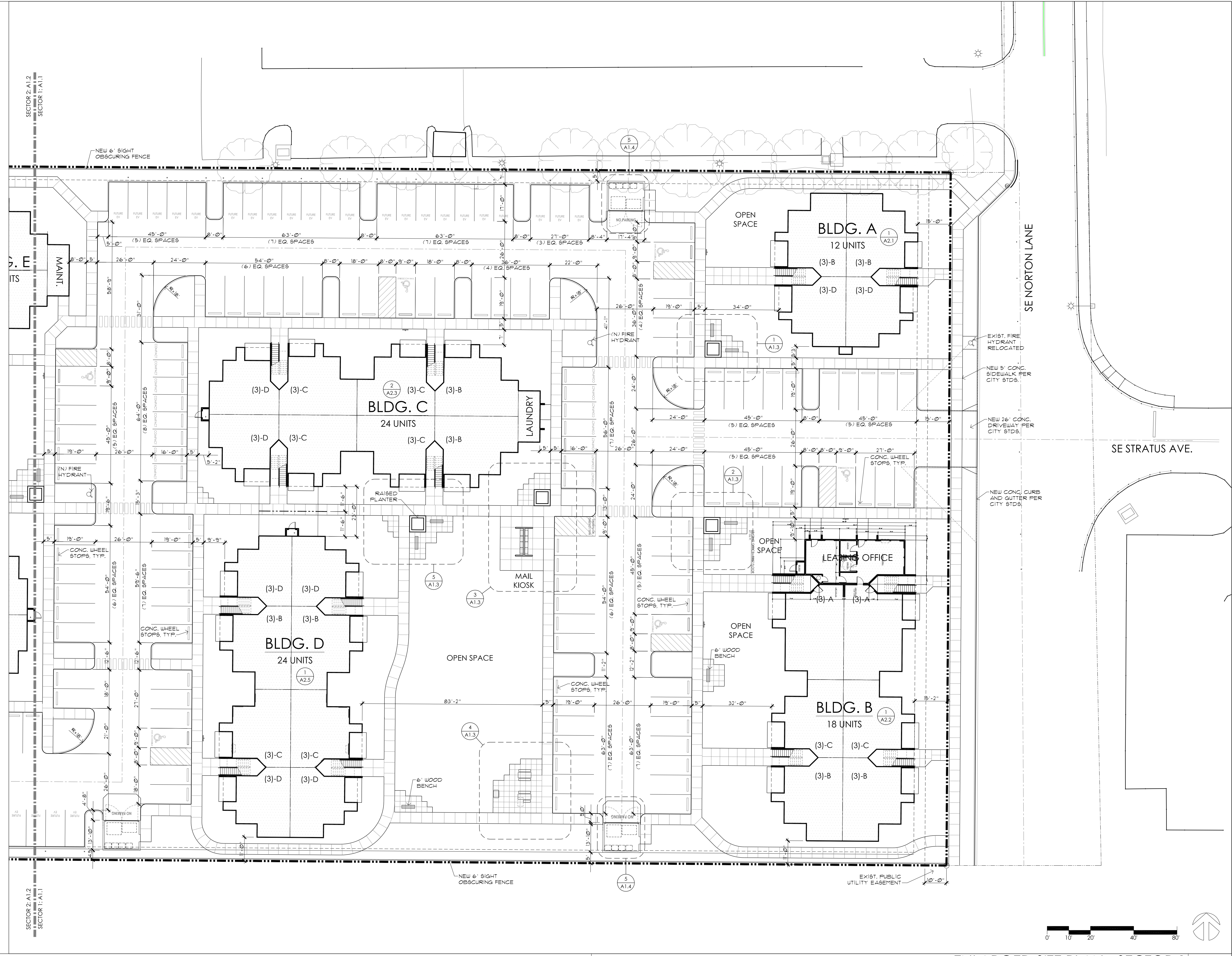
**NORTON LANDING APARTMENTS**  
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 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

CONCEPTUAL SITE PLAN

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   |               |            |  |
| job no.: | 2301          |            |  |

**A**  
**1.0**





ENLARGED SITE PLAN - SECTOR 1 1  
 Scale: 1" = 20'-0" A1.1

LAND-USE REVIEW

|                               |               |
|-------------------------------|---------------|
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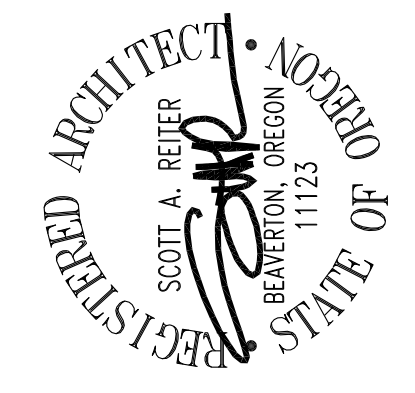
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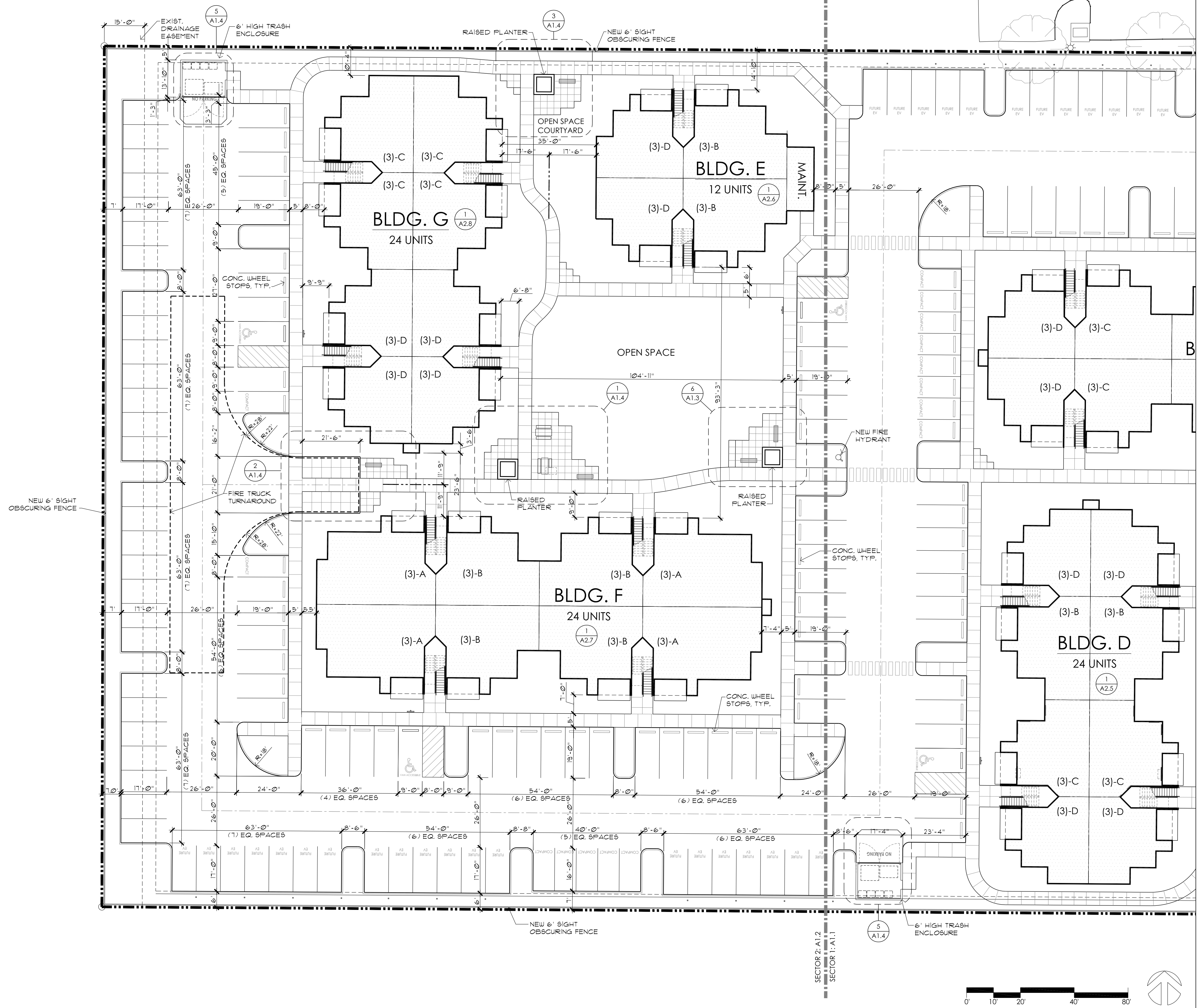
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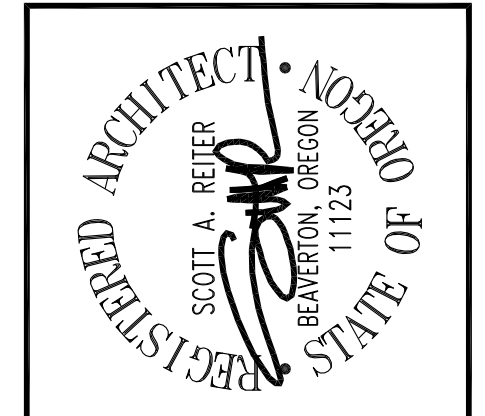
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ENLARGED SITE PLAN - SECTOR 2 1  
 Scale: 1" = 20'-0" A1.2



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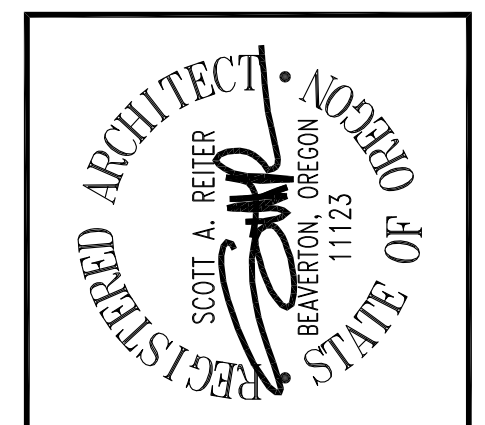
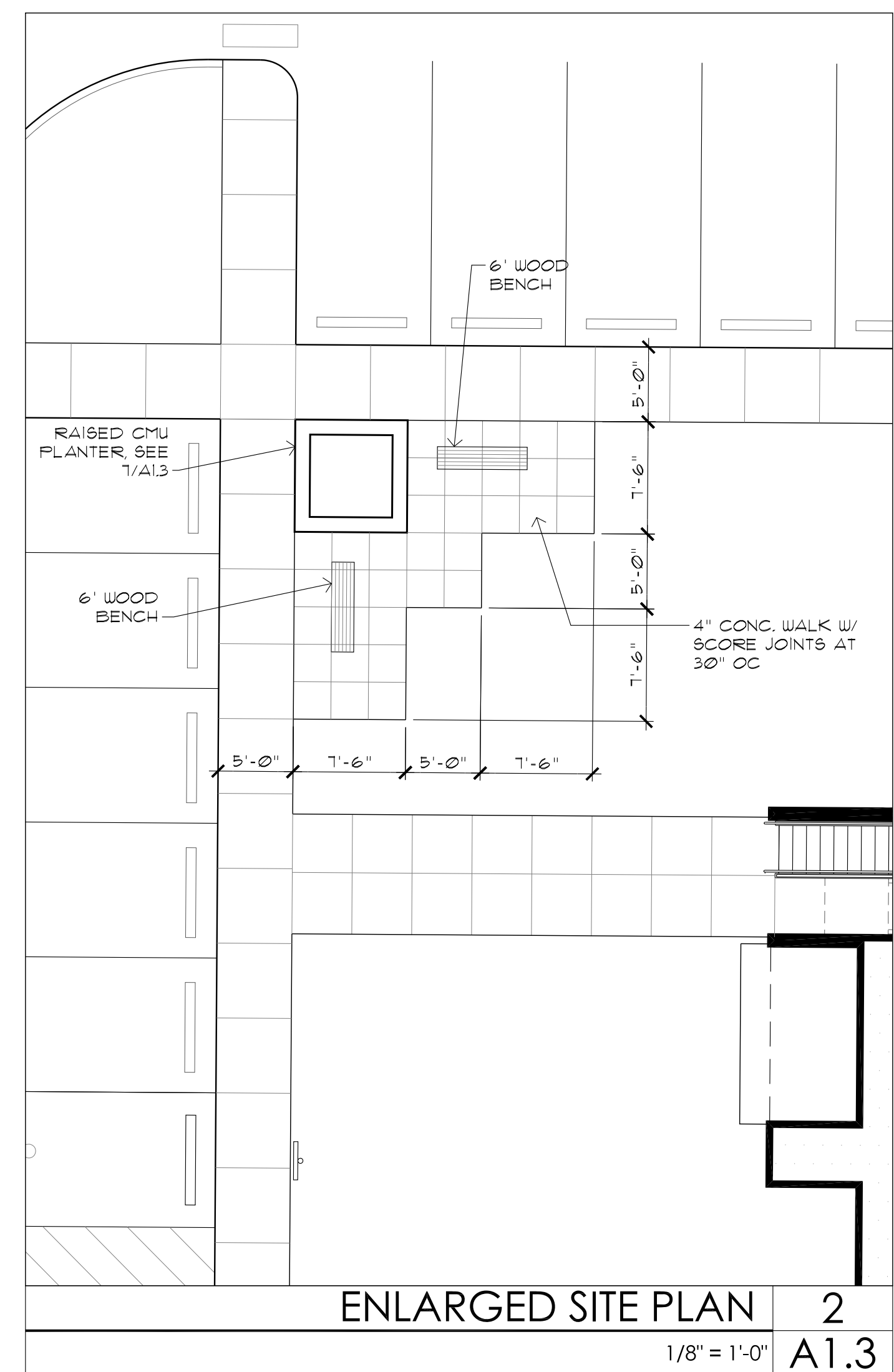
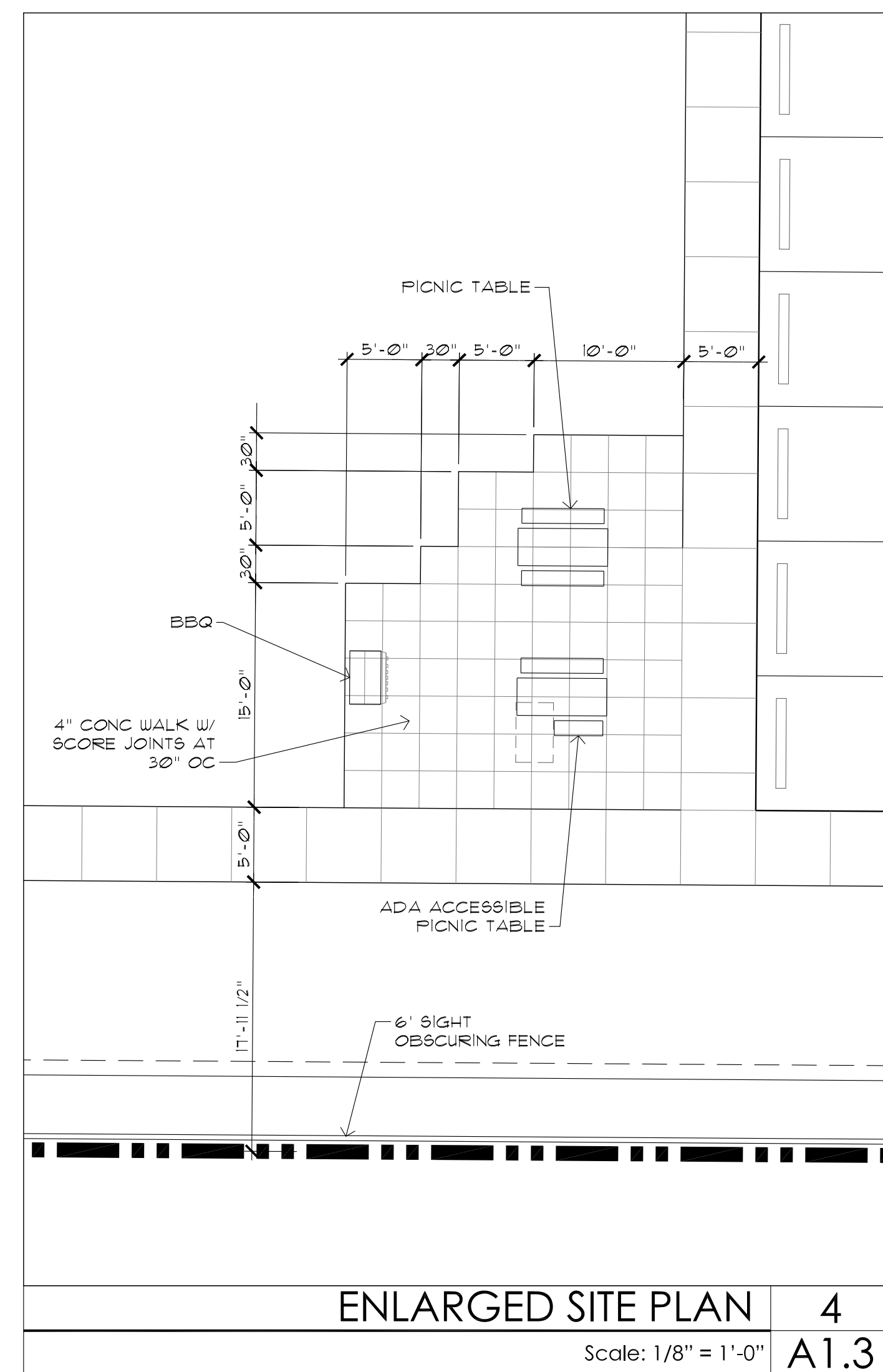
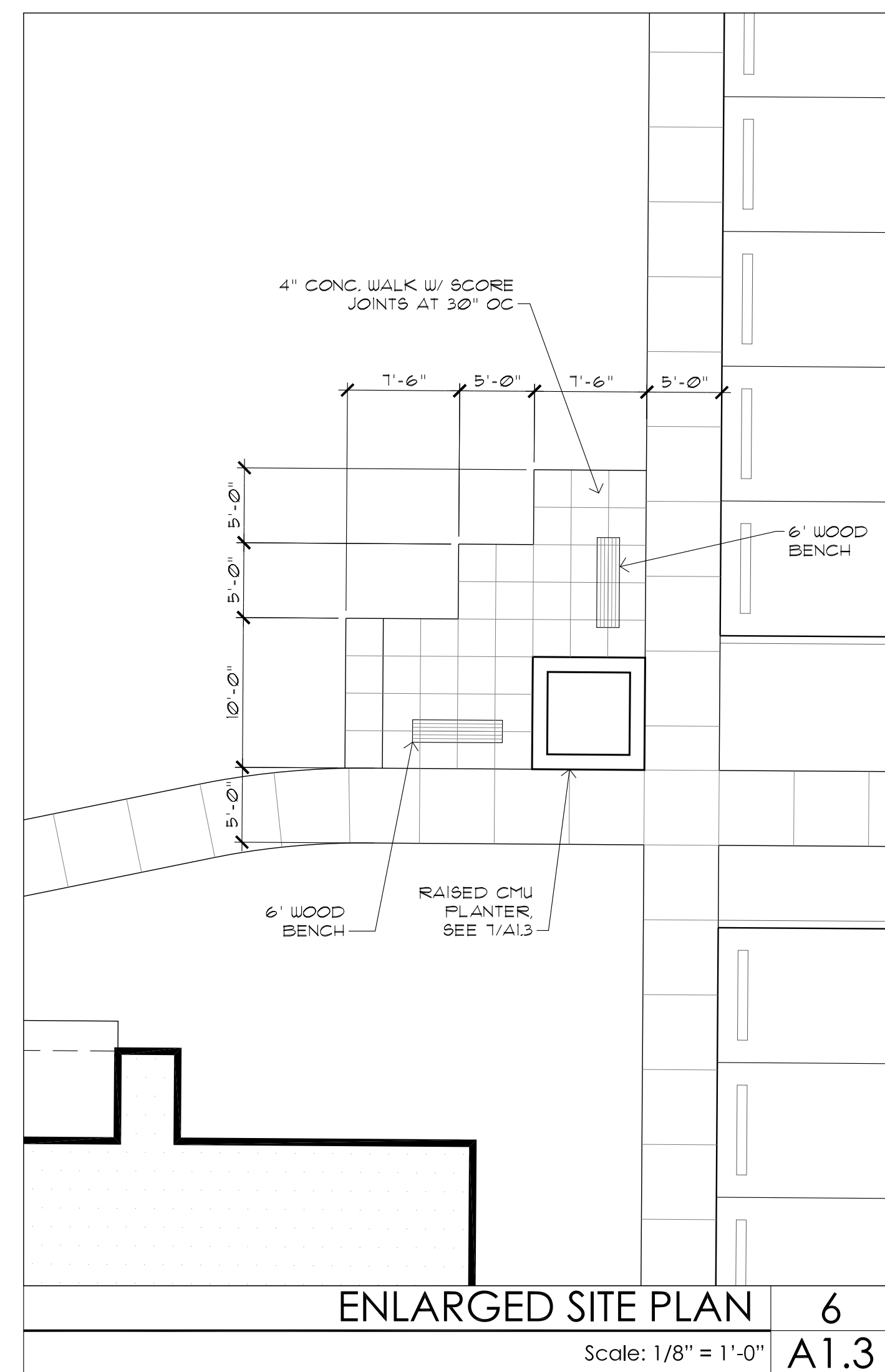
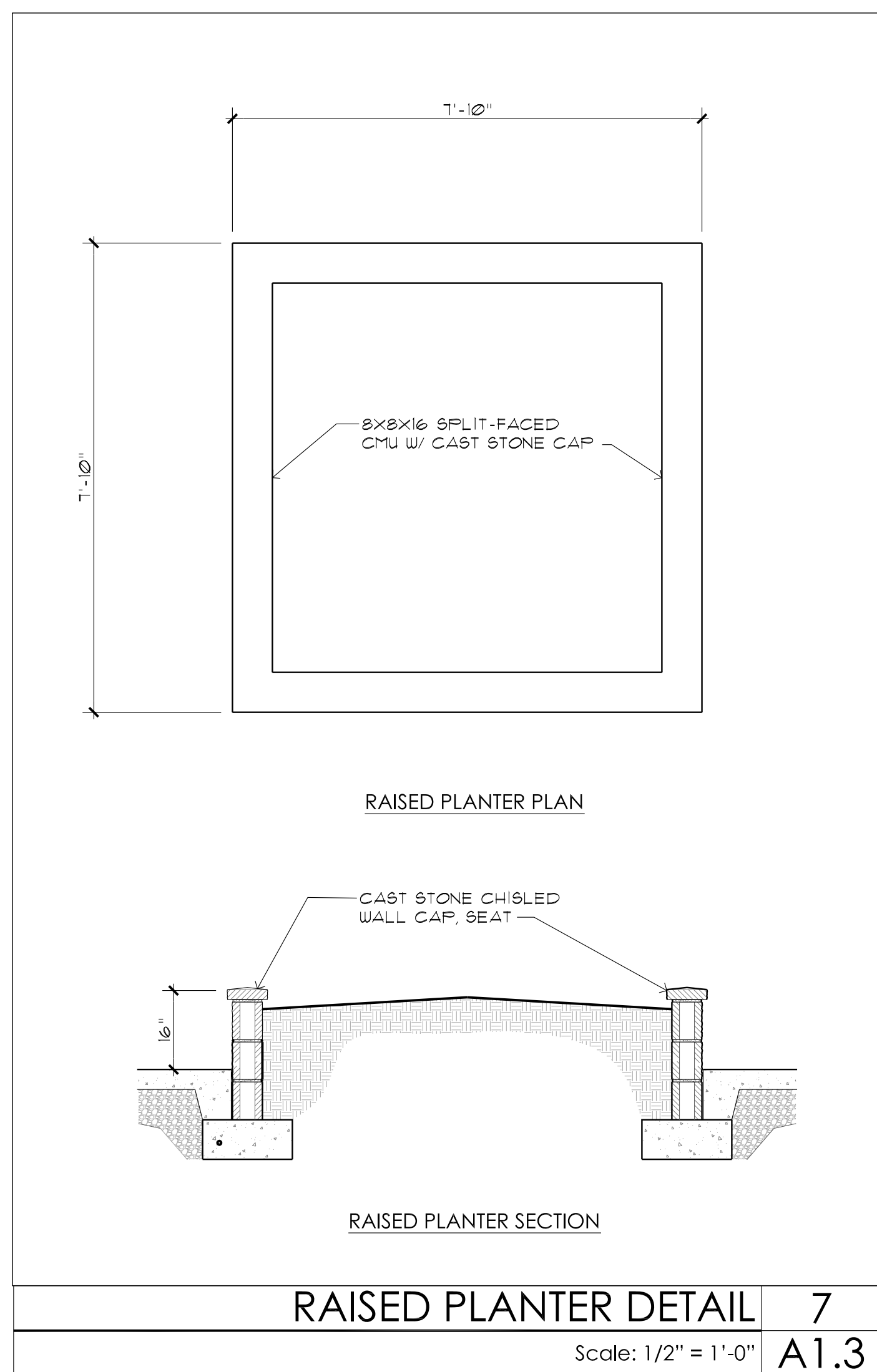
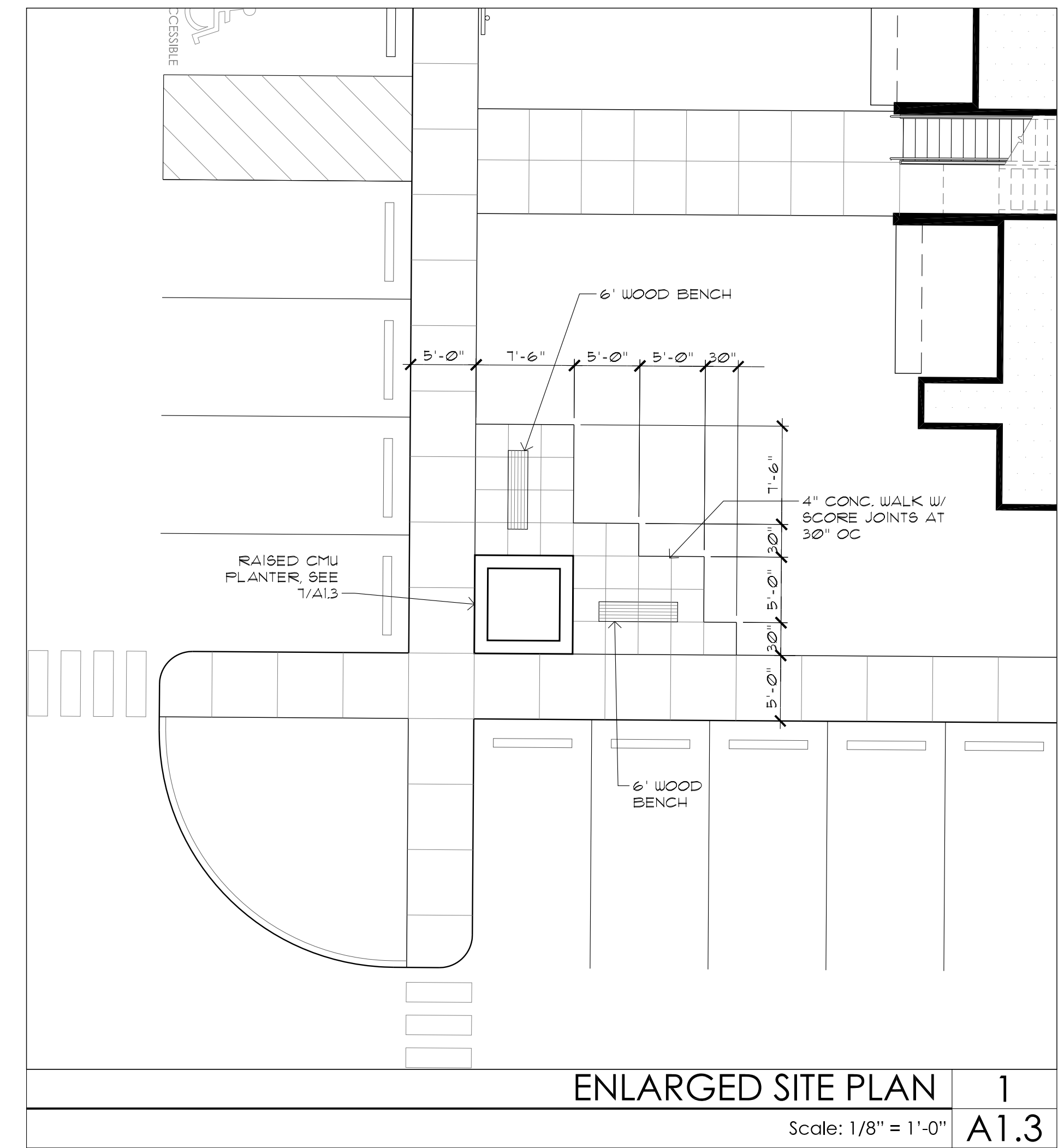
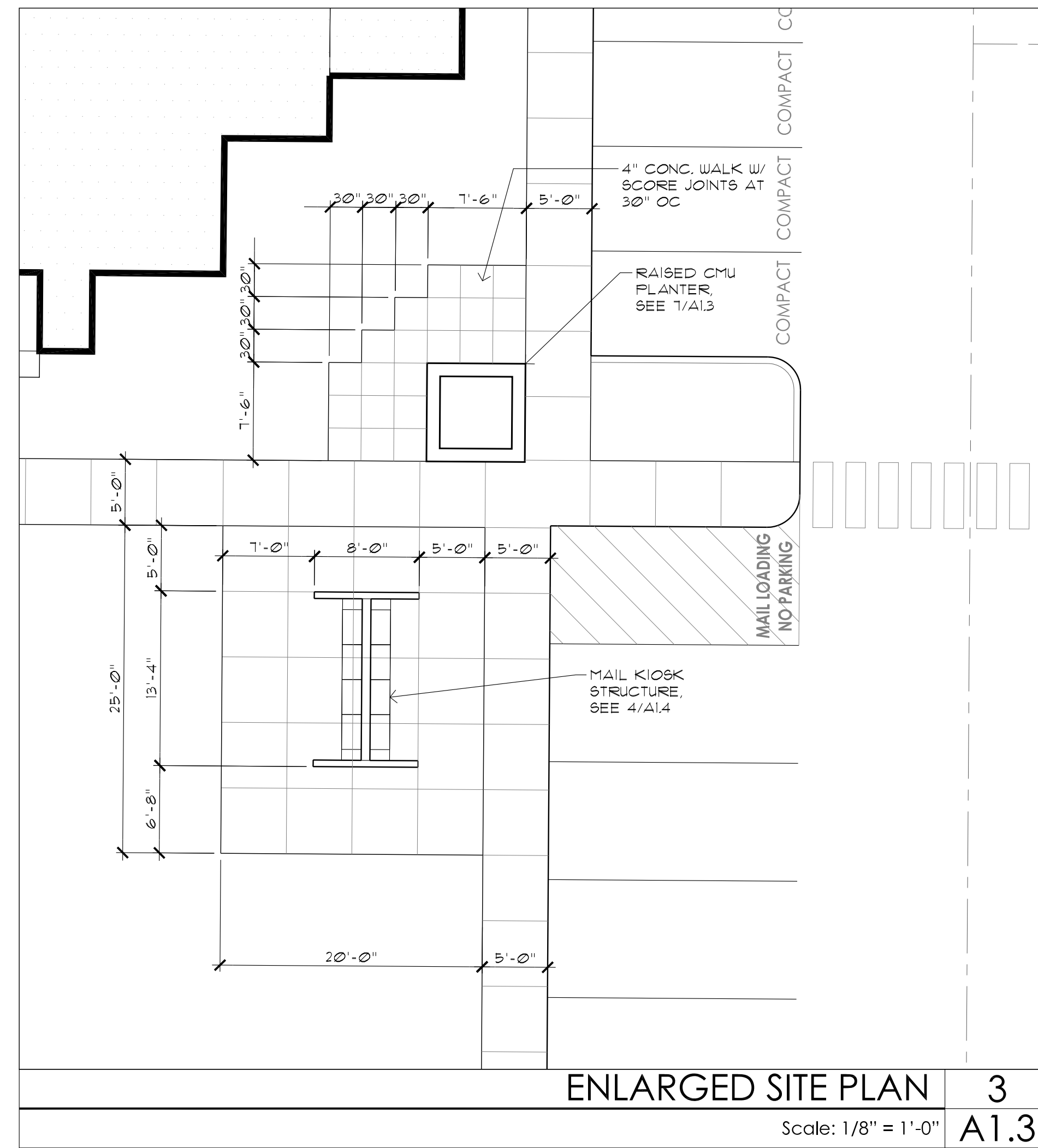
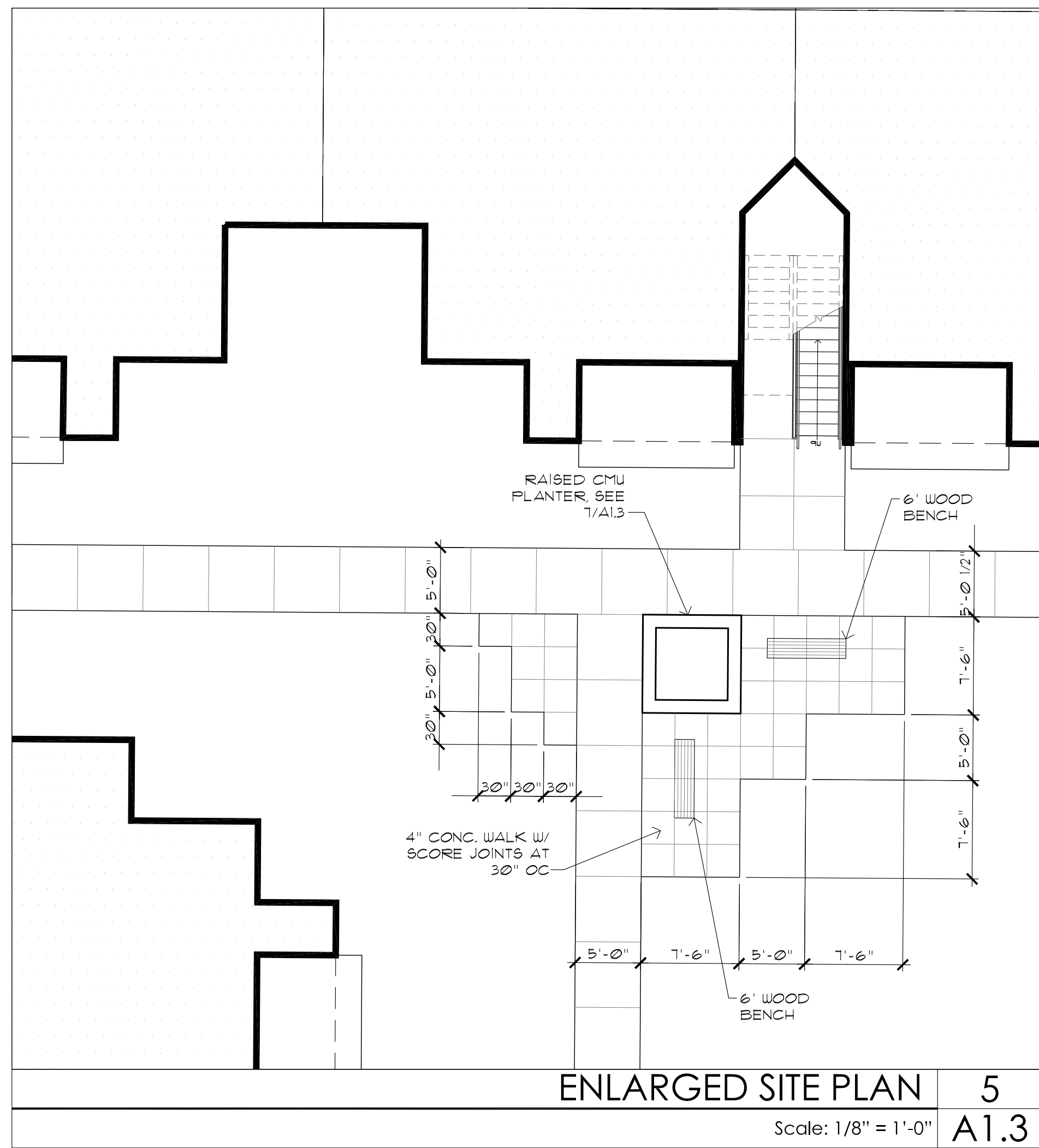
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 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLAN - SECTOR 2

| DATE          | REVISIONS |
|---------------|-----------|
| JULY 19, 2023 | AS NOTED  |
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 job no: 2301

**A**  
**1.2**



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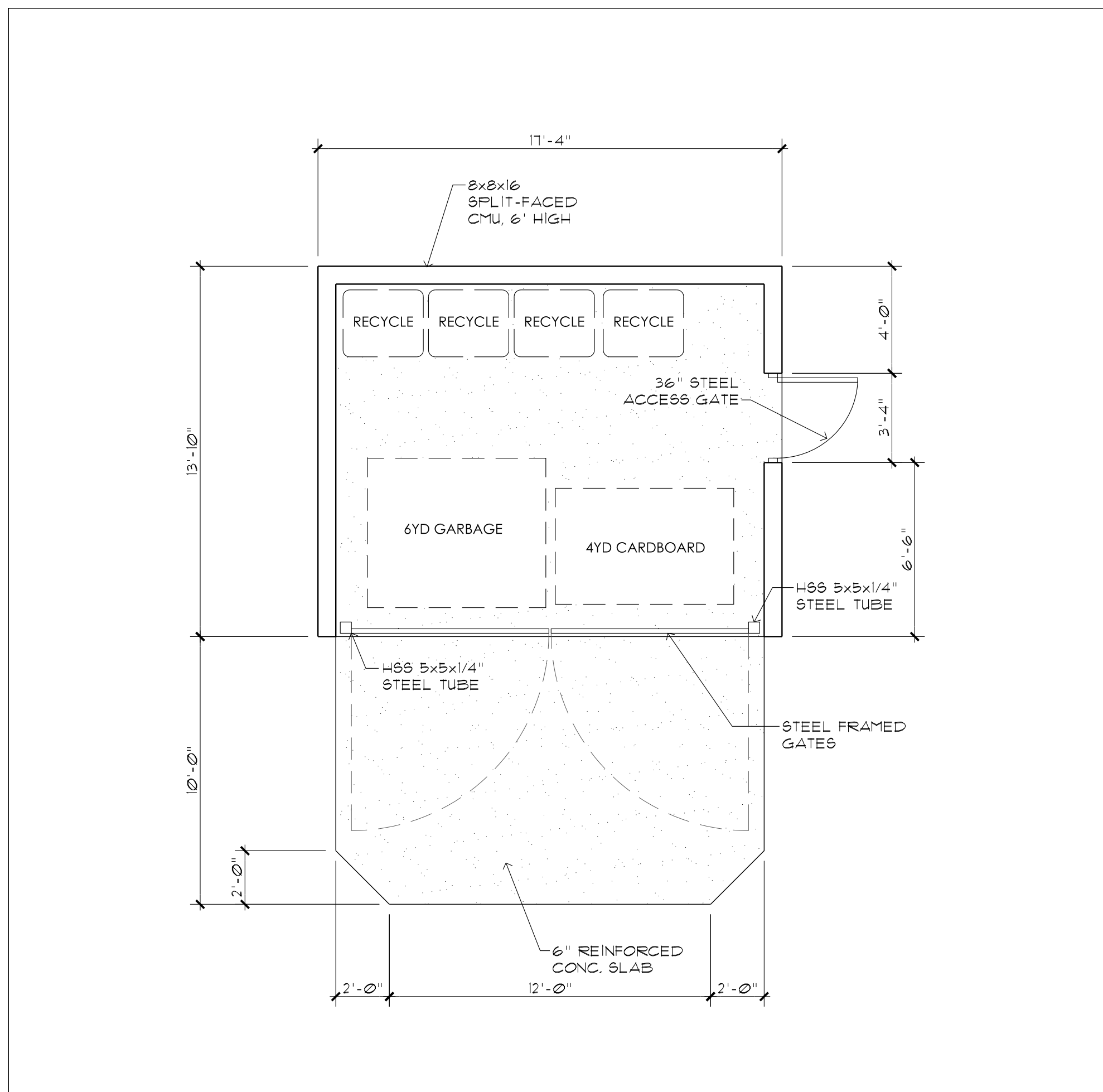
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ENLARGED SITE PLANS

|          |               |
|----------|---------------|
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| scale:   | AS NOTED      |
| drawn:   |               |
| job no.: | 2301          |

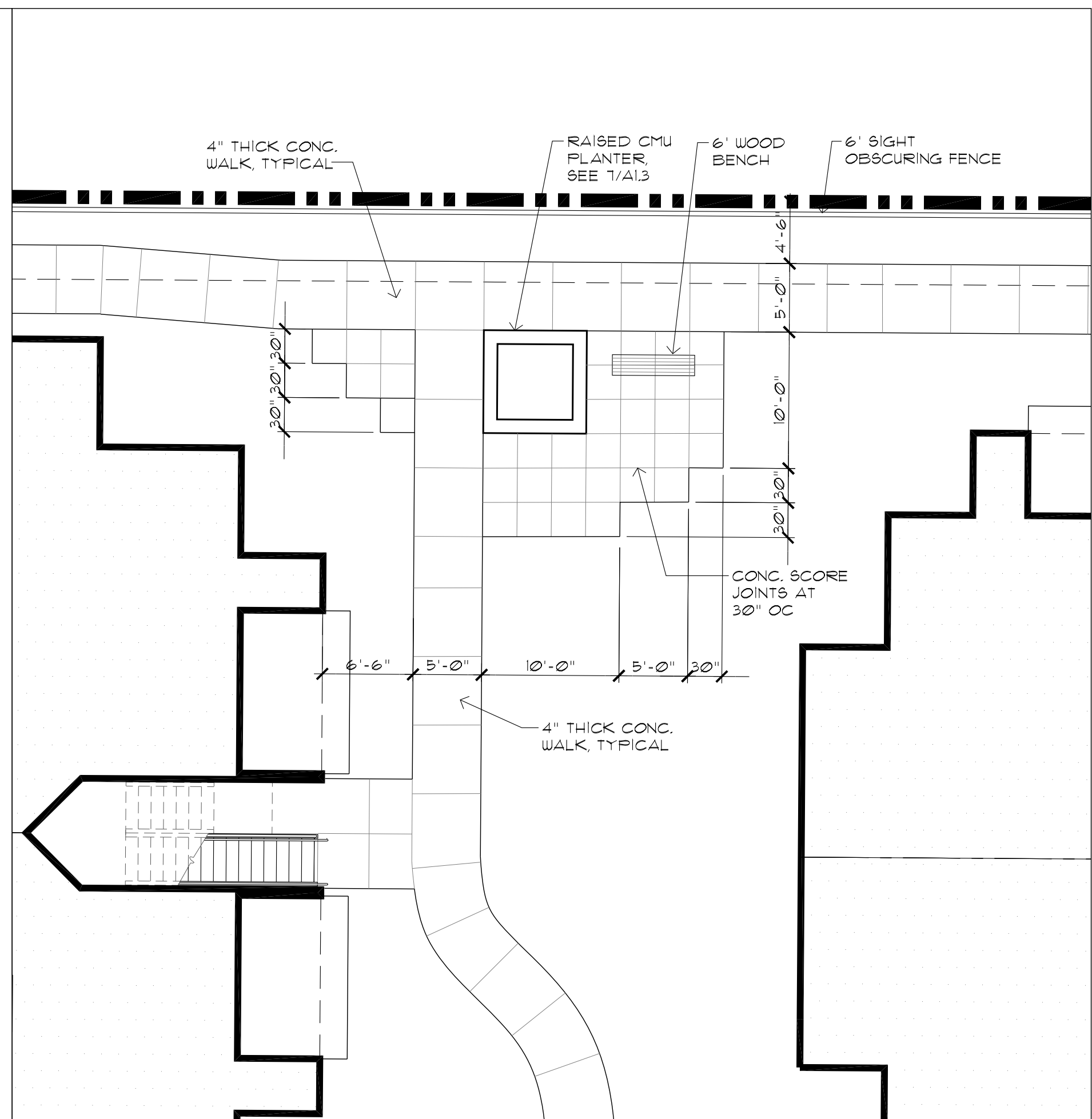
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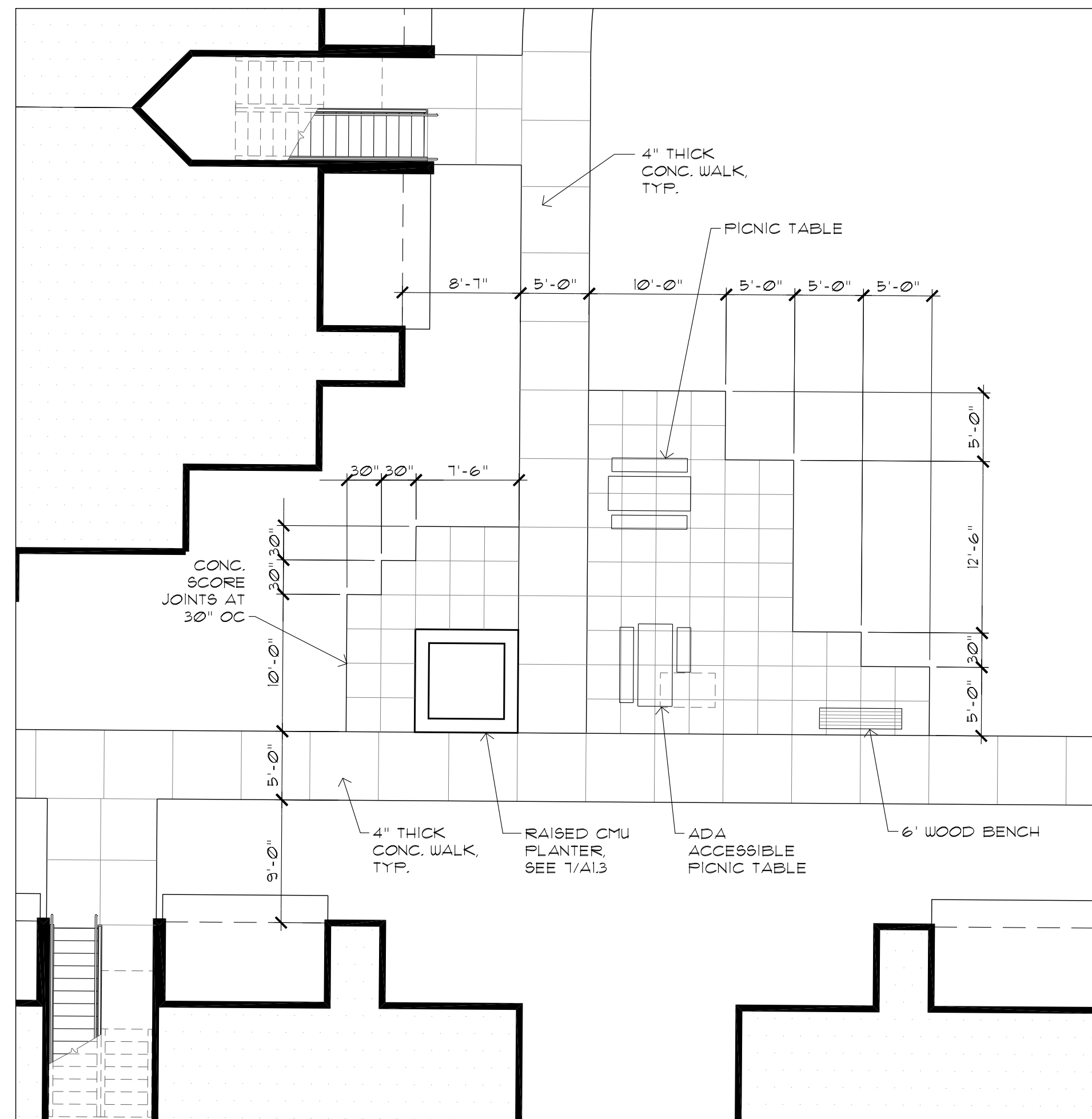
TRASH ENCLOSURE PLAN 5

Scale: 1/4" = 1'-0" A1.4



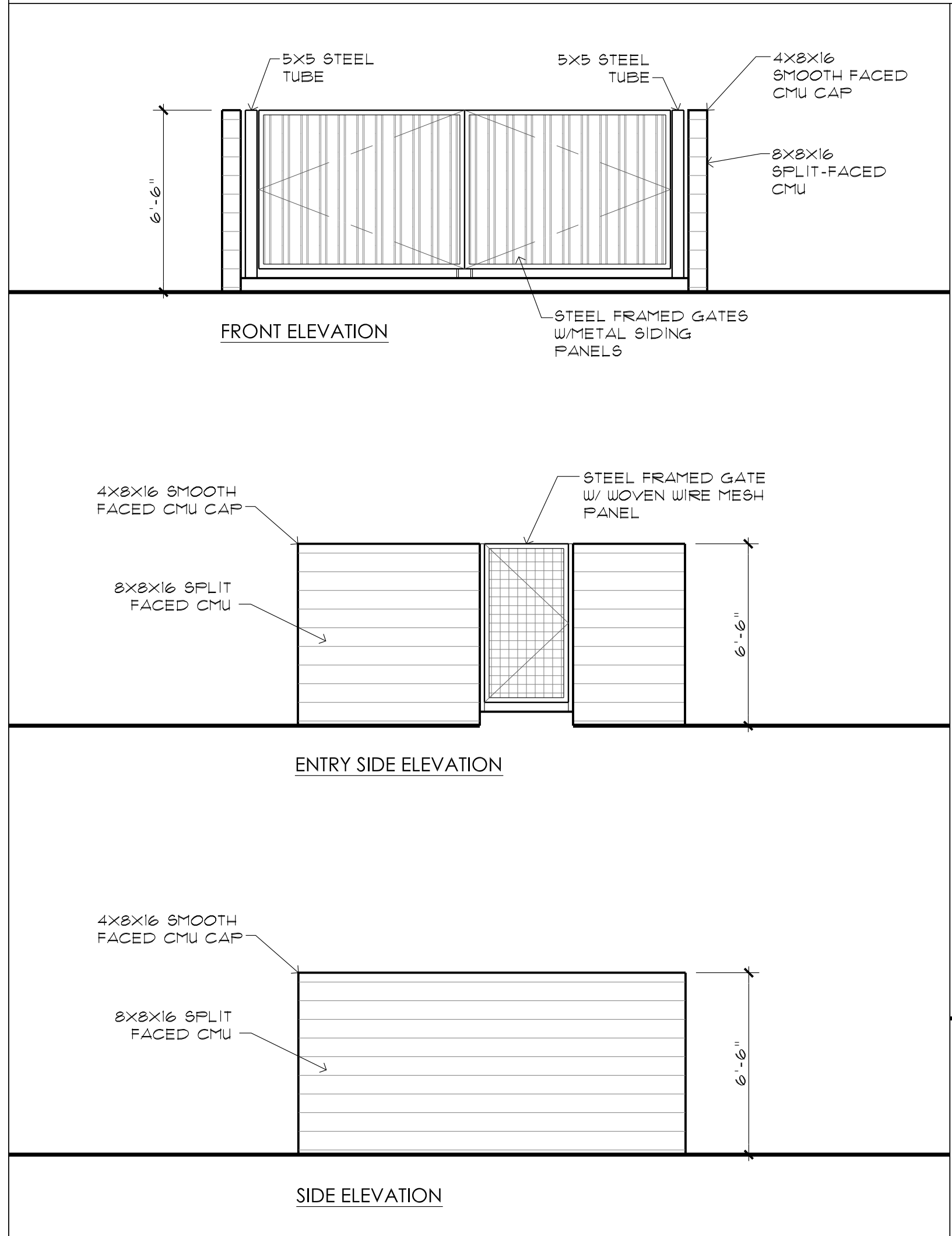
ENLARGED SITE PLAN 3

Scale: 1/8" = 1'-0" A1.4



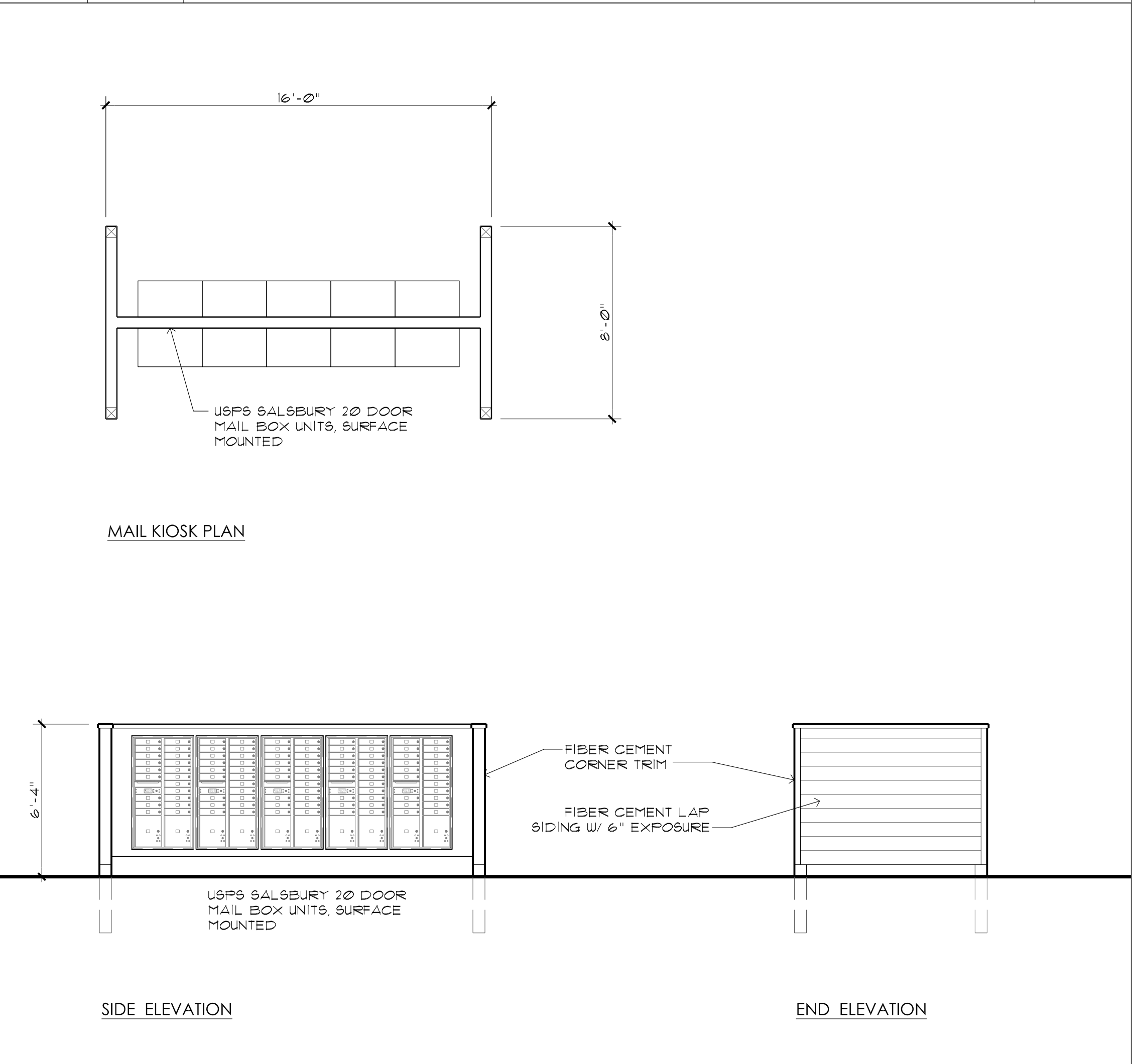
ENLARGED SITE PLAN 1

Scale: 1/8" = 1'-0" A1.4



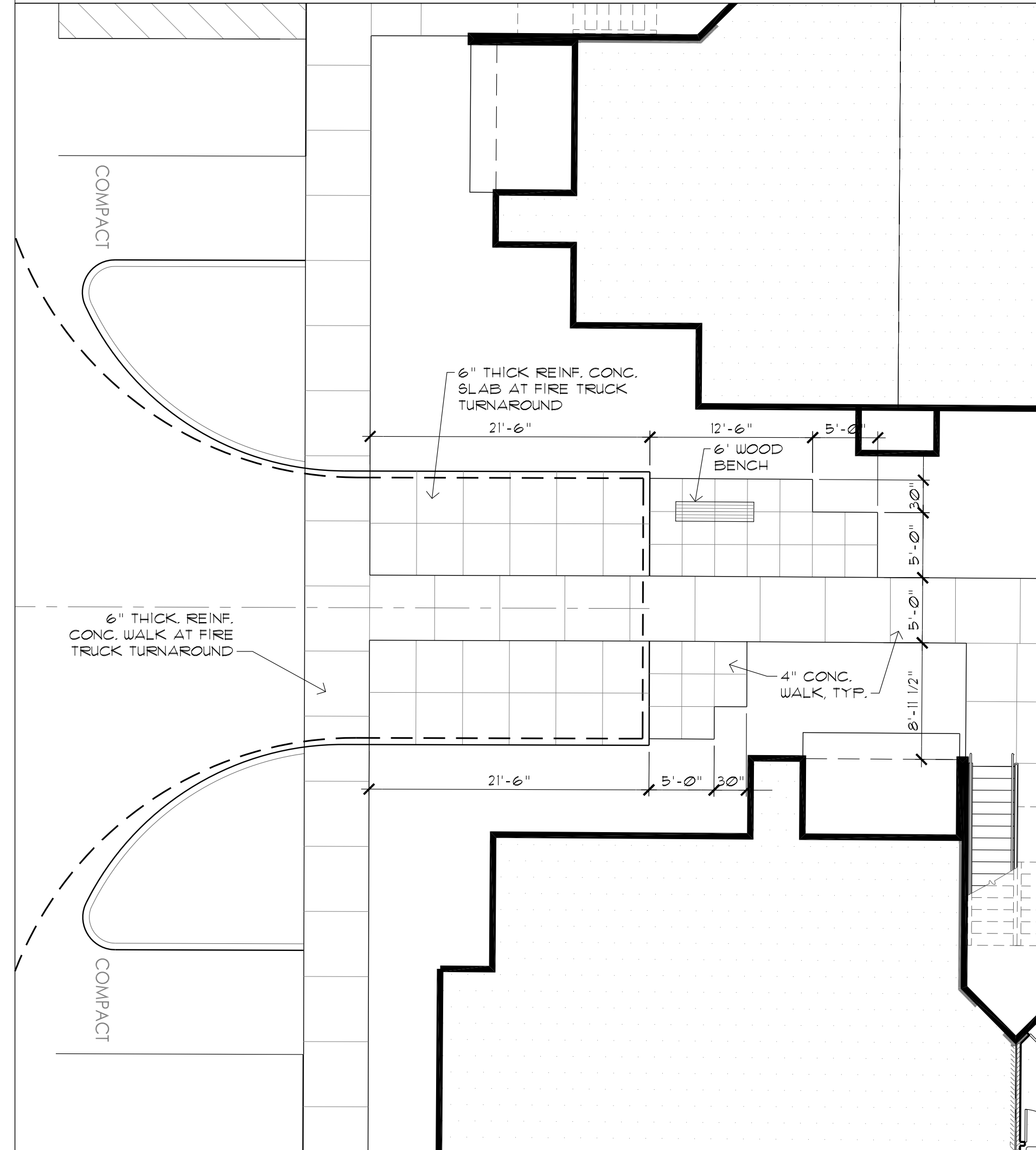
TRASH ENCLOSURE ELEVATIONS 6

Scale: 1/4" = 1'-0" A1.4



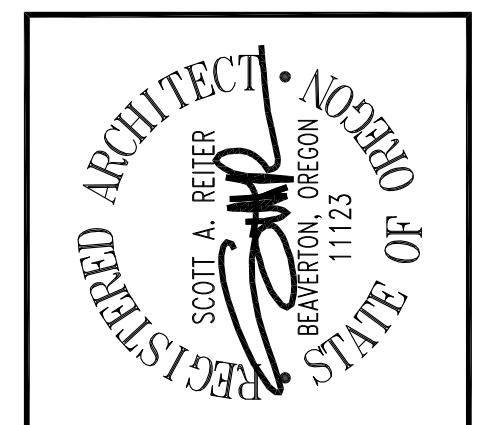
MAIL KIOSK PLAN, ELEVATIONS 4

Scale: 1/4" = 1'-0" A1.4



ENLARGED SITE PLAN 2

Scale: 1/8" = 1'-0" A1.4



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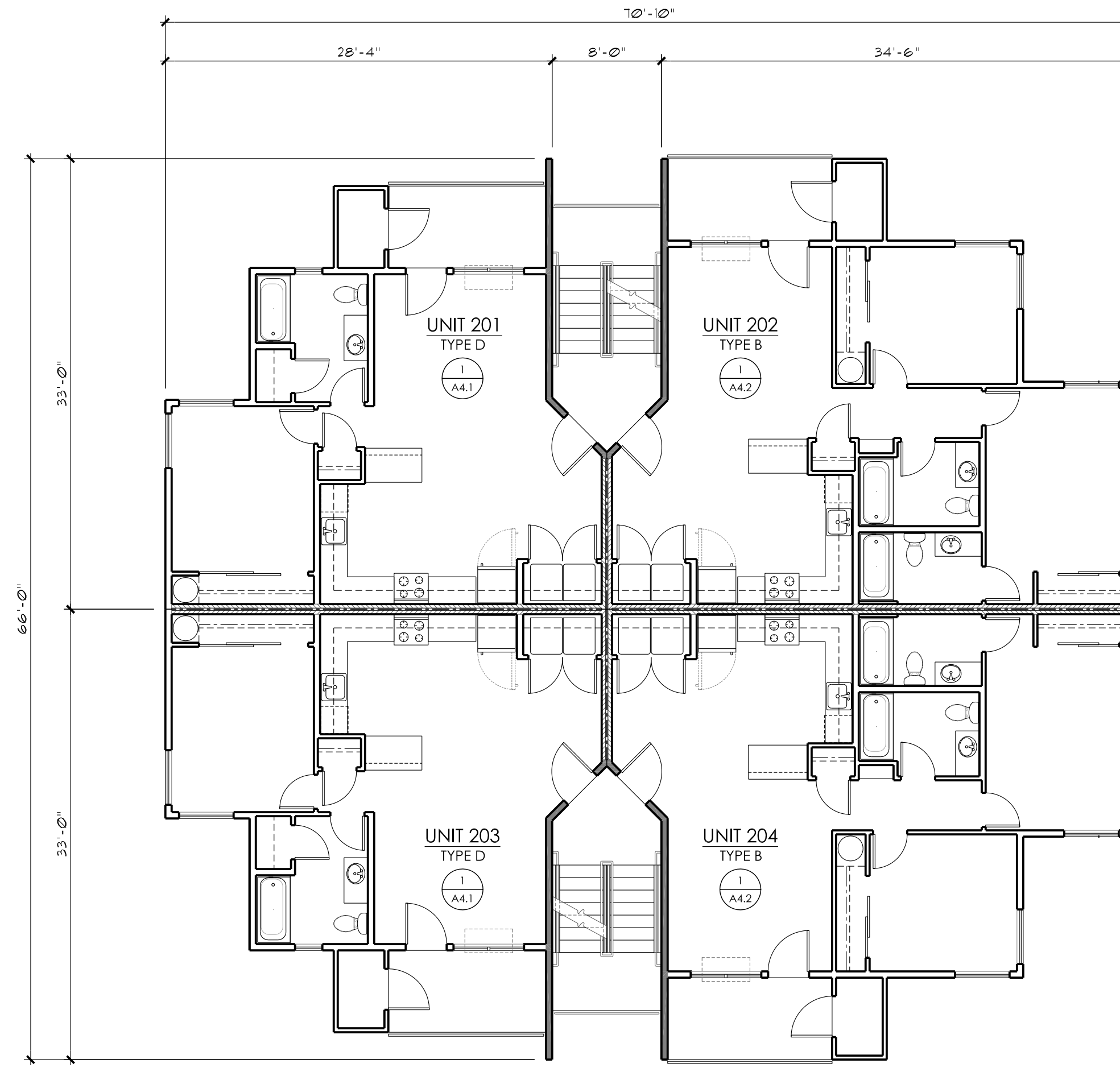
**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLANS  
MAIL KIOSK PLANS, ELEVATIONS  
TRASH ENCLOSURE PLAN, ELEVATIONS

date: JULY 19, 2023  
scale: AS NOTED  
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job no.: 2301

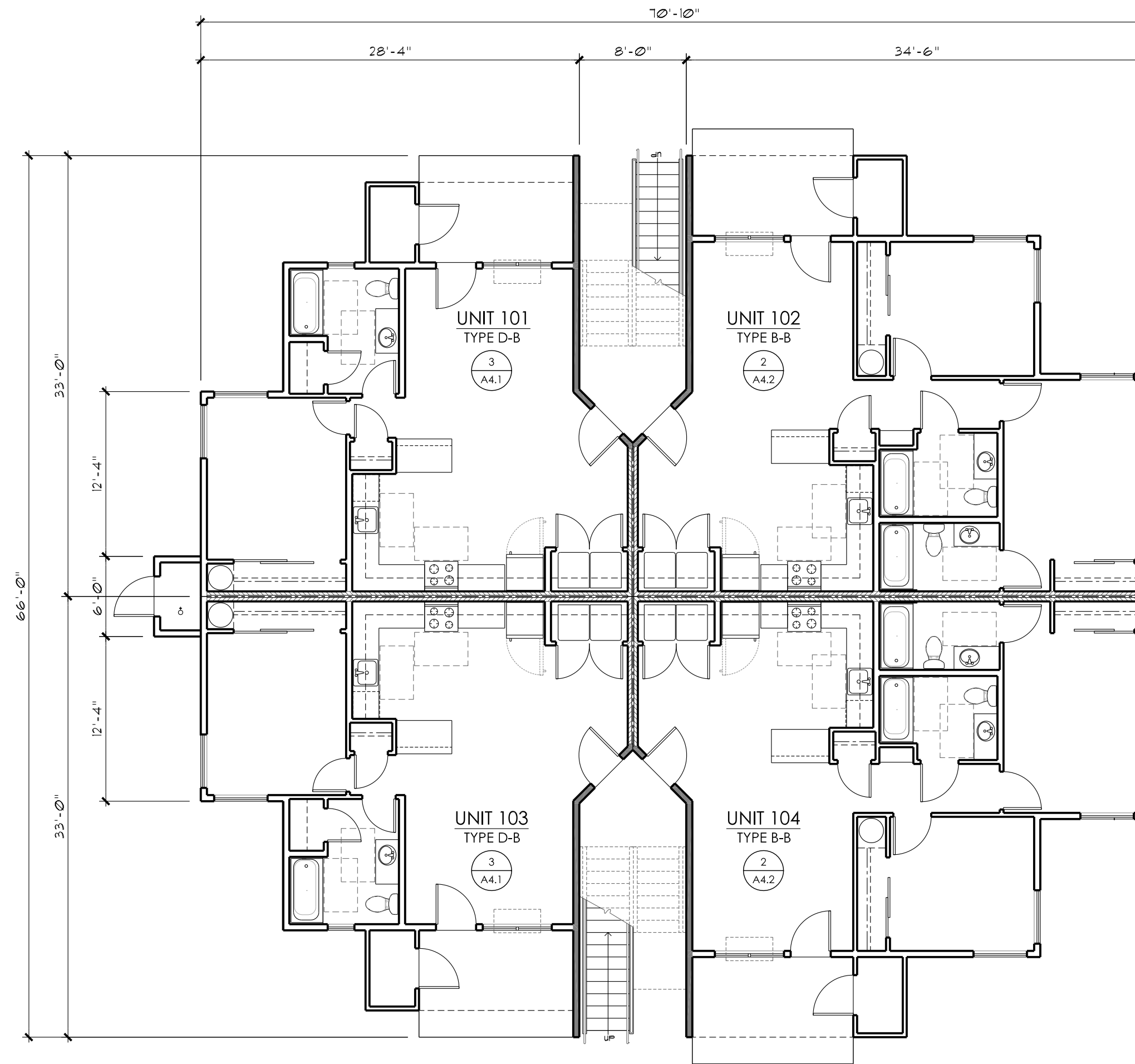
LAND-USE REVIEW  
**A**  
**1.4**



BUILDING 'A' - SECOND/THIRD FLOOR PLAN 2

Scale: 1/8" = 1'-0"

A2.1



BUILDING 'A' - FIRST FLOOR PLAN 1

Scale: 1/8" = 1'-0"

A2.1

LAND-USE REVIEW

BUILDING PLANS  
BUILDING 'A'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
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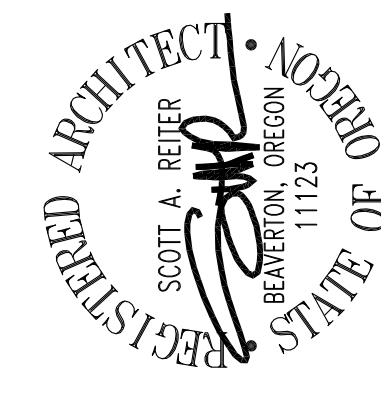
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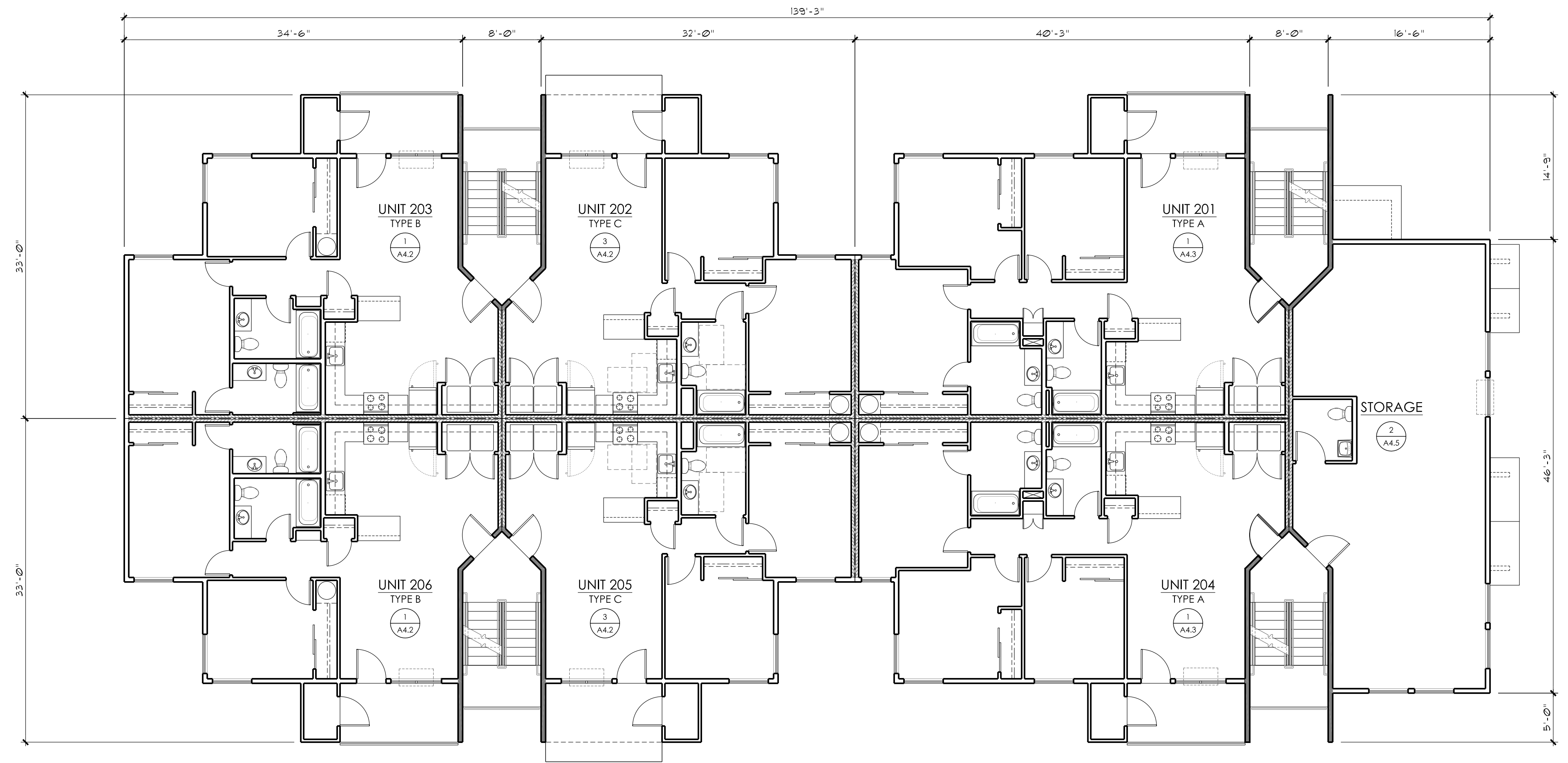
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INCORPORATED

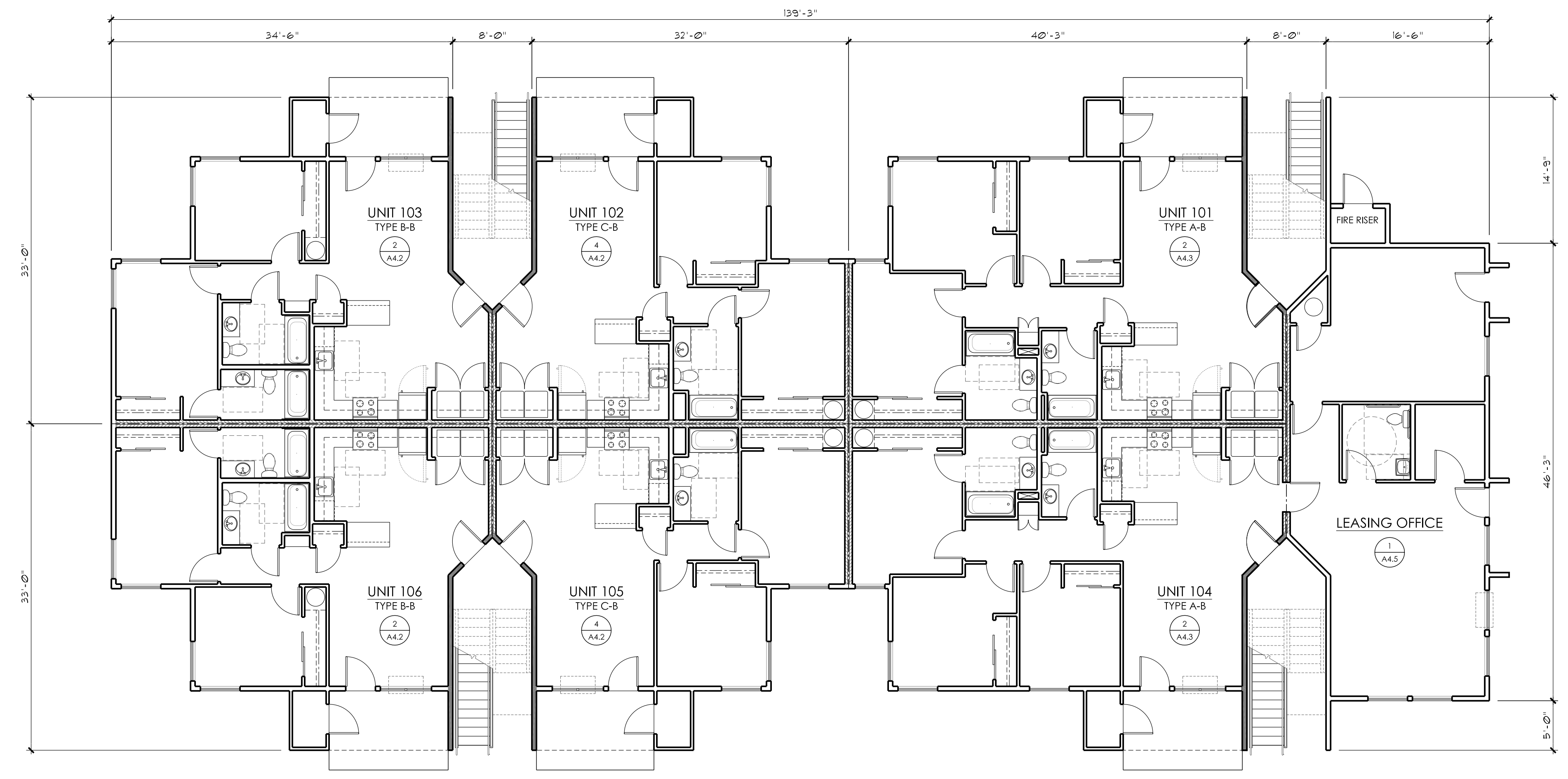
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BUILDING 'B' SECOND FLOOR PLAN 2

Scale: 1/8" = 1'-0" A2.2



BUILDING 'B' FIRST FLOOR PLAN 1

Scale: 1/8" = 1'-0" A2.2

LAND-USE REVIEW

BUILDING PLANS  
BUILDING 'B'

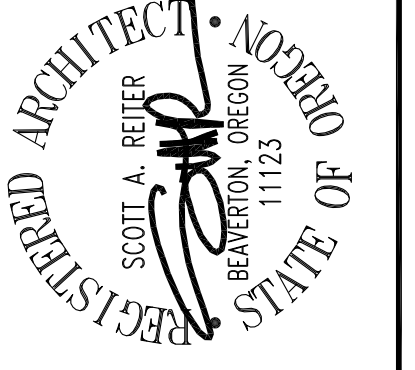
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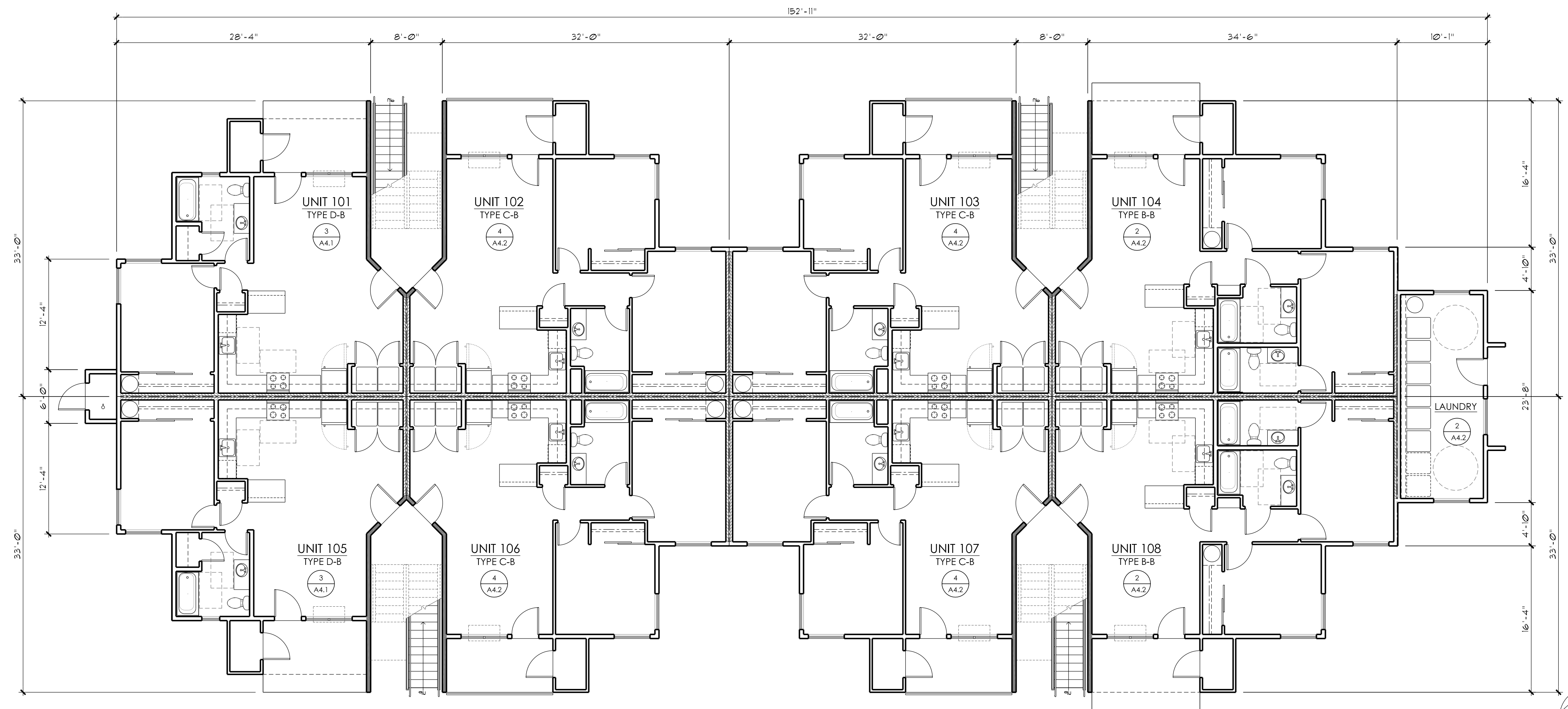
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NORTON LANDING APARTMENTS  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
KWDS, LLC  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

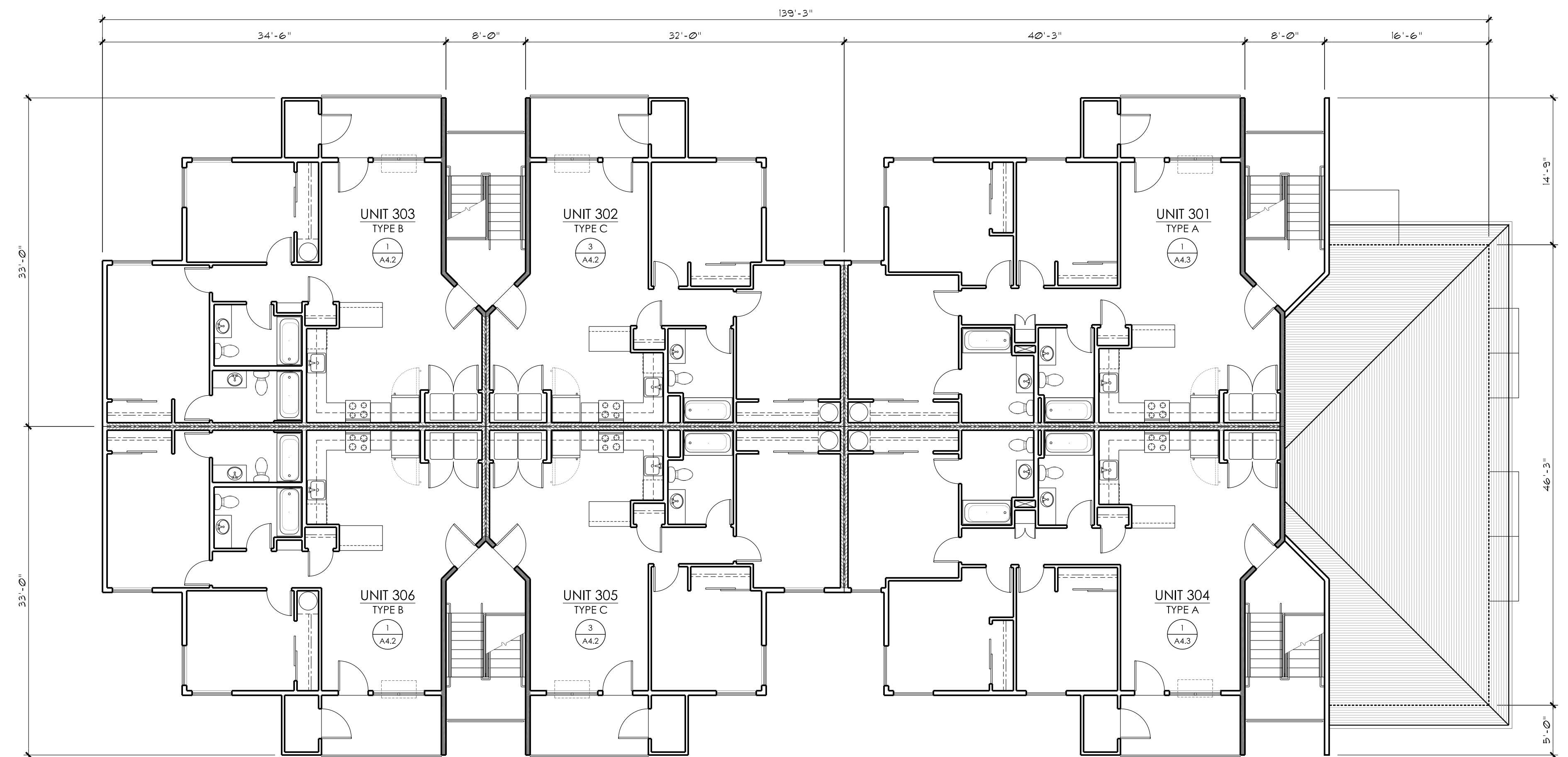
RDA  
REITER DESIGN ARCHITECT  
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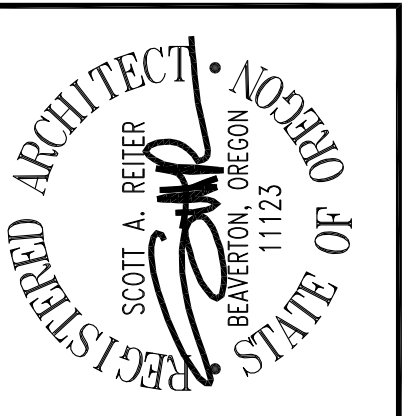




BUILDING 'C' FIRST FLOOR PLAN 2  
Scale: 1/8" = 1'-0" A2.3



BUILDING 'B' THIRD FLOOR PLAN 1  
Scale: 1/8" = 1'-0" A2.3



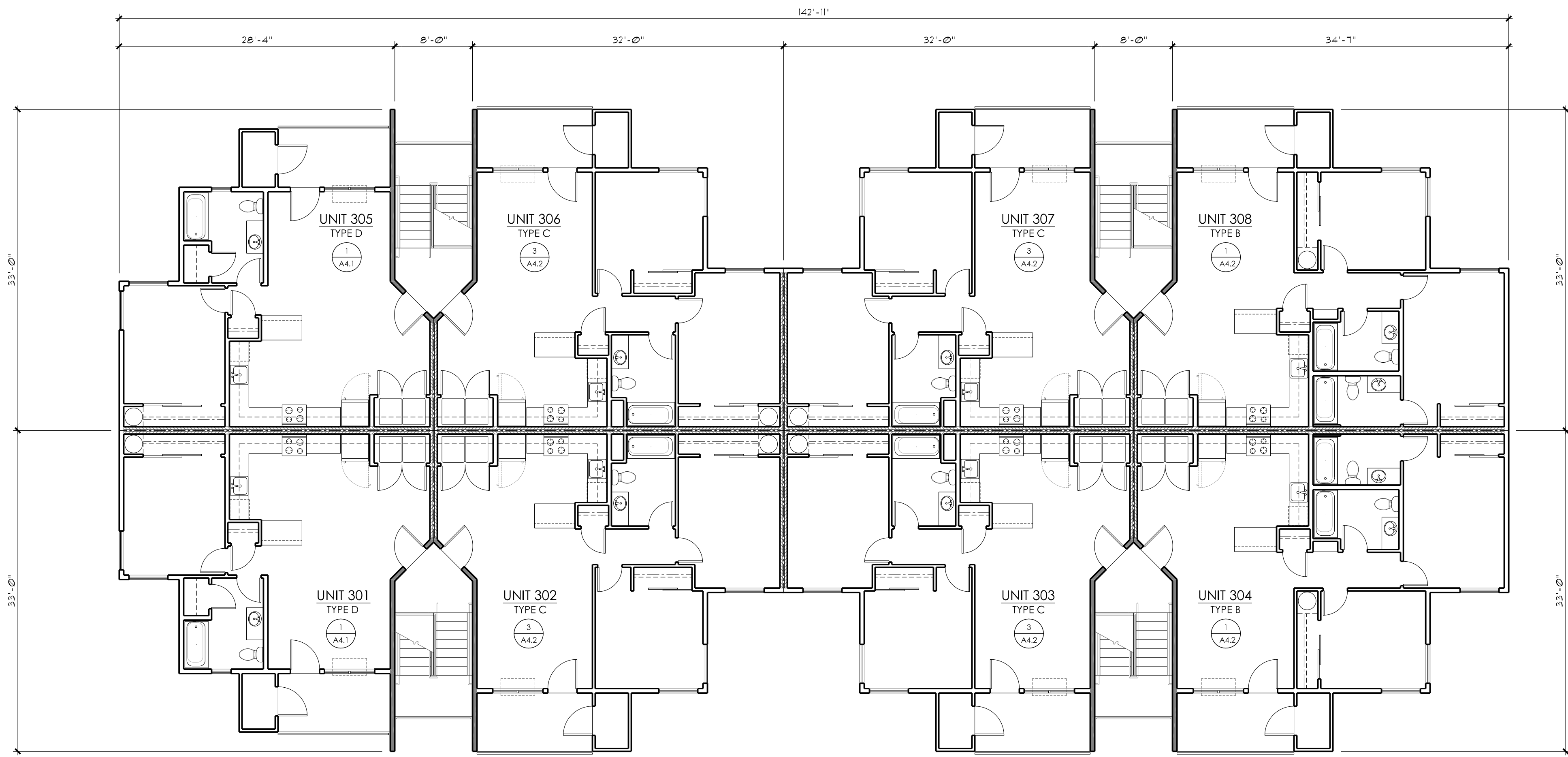
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**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

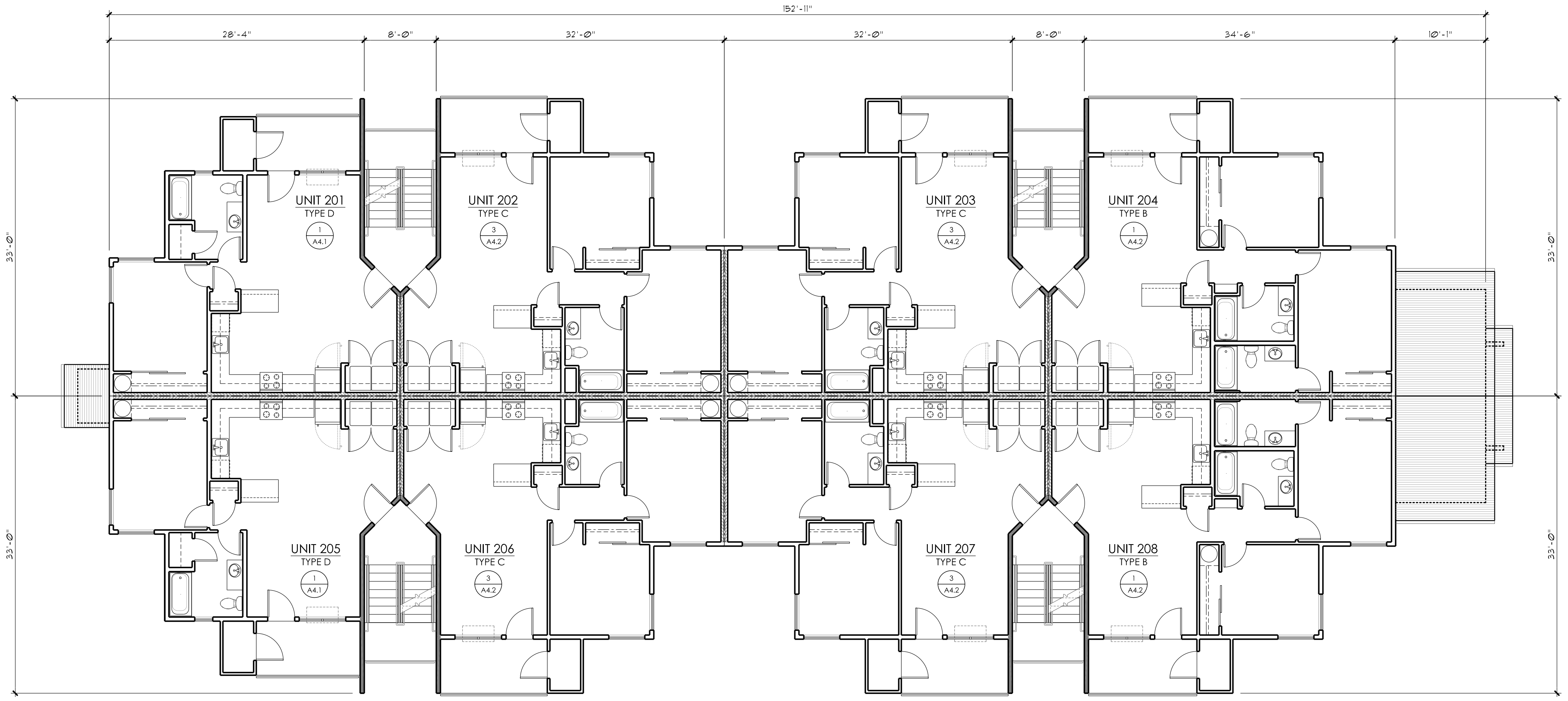
LAND-USE REVIEW

| BUILDING PLANS<br>BUILDING 'B', BUILDING 'C' |               | revisions: |
|--|---------------|------------|
| date:  | JULY 19, 2023 |            |
| scale:                                       | AS NOTED      |            |
| drawn:                                       |               |            |
| job no.:                                     | 2301          |            |

**A**  
**2.3**



BUILDING 'C' THIRD FLOOR PLAN 2  
 Scale: 1/8" = 1'-0" A2.4



BUILDING 'C' SECOND FLOOR PLAN 1  
 Scale: 1/8" = 1'-0" A2.4

LAND-USE REVIEW

BUILDING PLANS  
 BUILDING 'C'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
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A  
 2.4

NORTON LANDING APARTMENTS  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

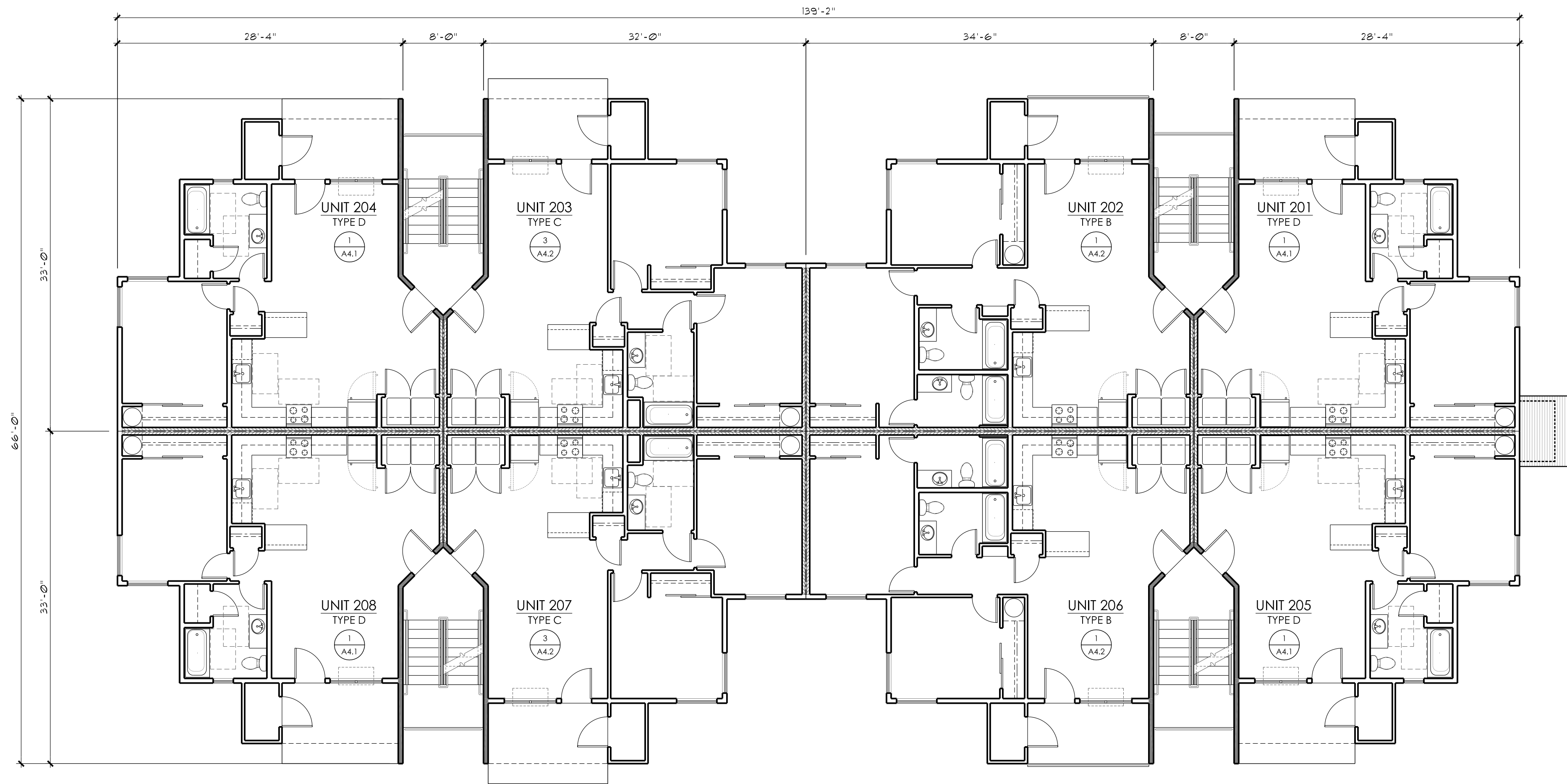
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 KWDS, LLC  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED

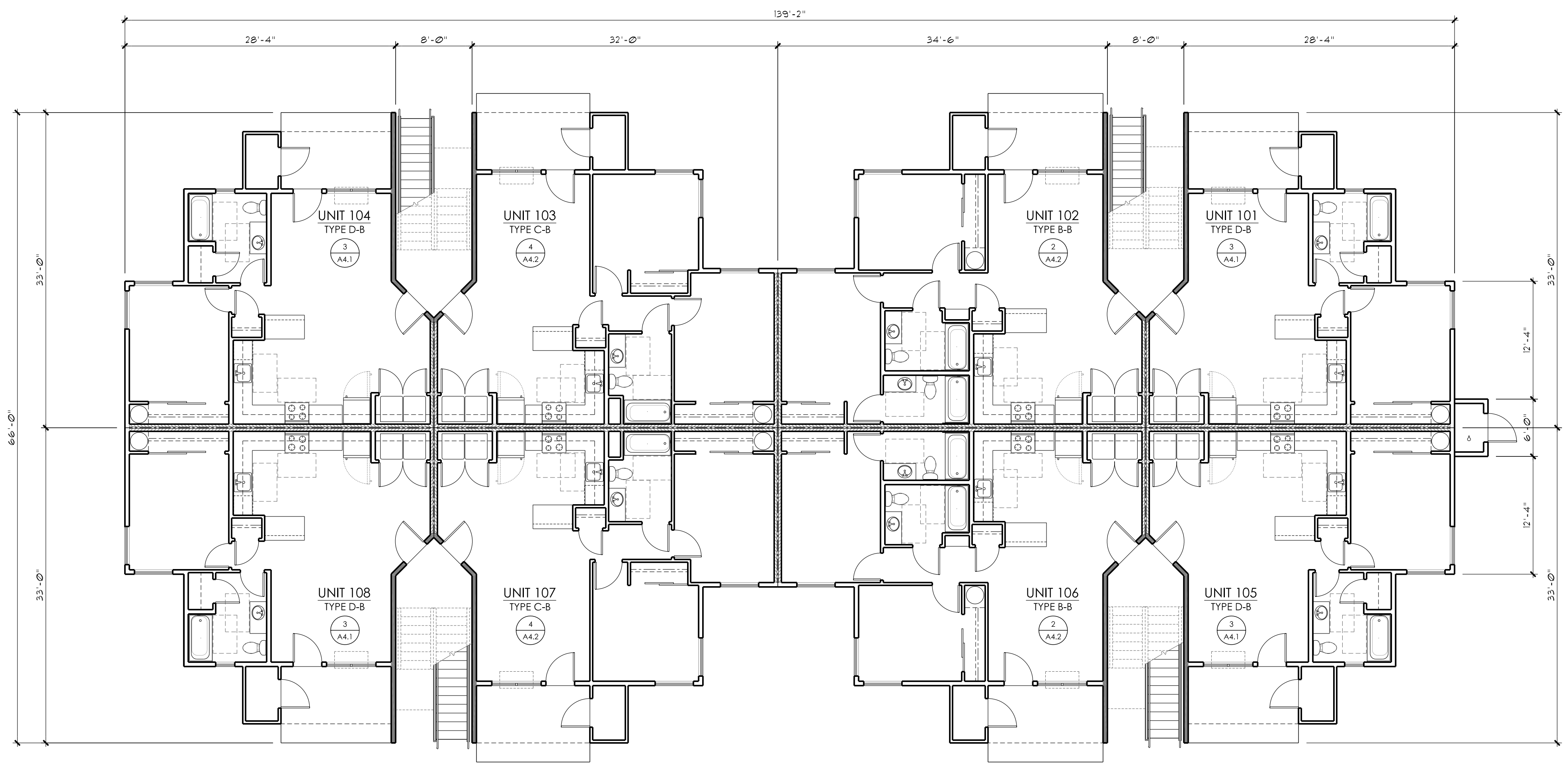
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 BEAVERTON, OREGON 97008 (503) 574-3056



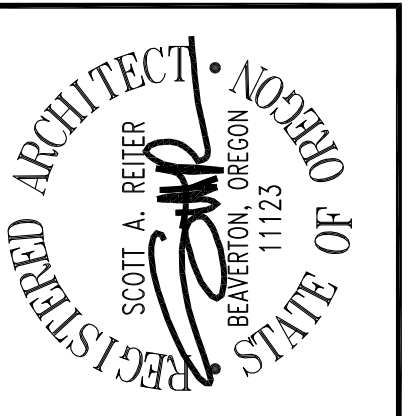




BUILDING 'D' SECOND/THIRD FLOOR PLAN 2  
 Scale: 1/8" = 1'-0" A2.5



BUILDING 'D' FIRST FLOOR PLAN 1  
 Scale: 1/8" = 1'-0" A2.5



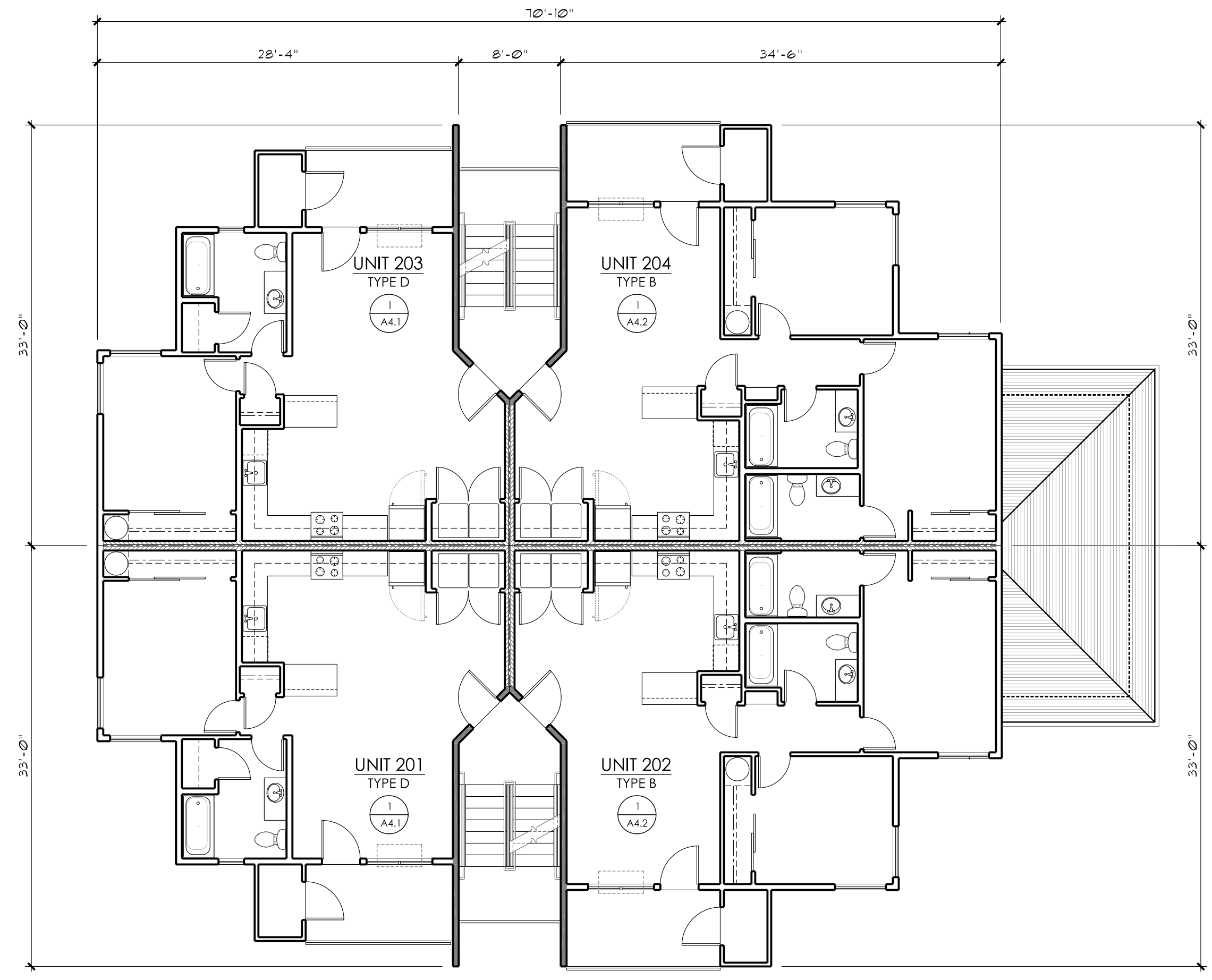
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 INCORPORATED  
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 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

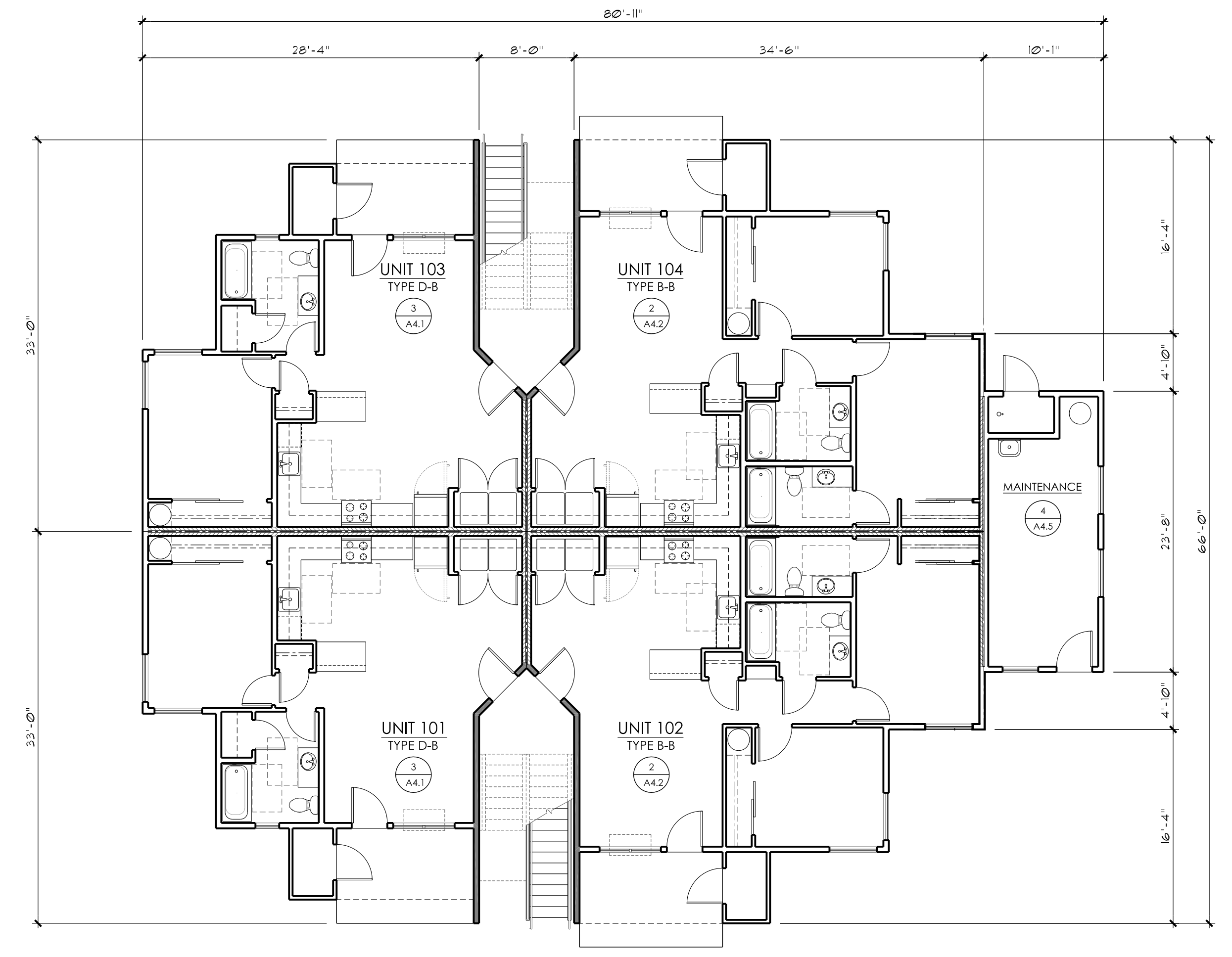
LAND-USE REVIEW

| DATE          | REVISIONS |
|---------------|-----------|
| JULY 19, 2023 | AS NOTED  |
| SCALE:        |           |
| DRAWN:        |           |
| JOB NO.:      | 2301      |

**A**  
**2.5**



BUILDING 'E' - SECOND/THIRD FLOOR PLAN 2  
 Scale: 1/8" = 1'-0" A2.6



BUILDING 'E' - FIRST FLOOR PLAN 1  
 Scale: 1/8" = 1'-0" A2.6

LAND-USE REVIEW

BUILDING PLANS  
 BUILDING 'E'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   |               |            |  |
| job no.: | 2301          |            |  |

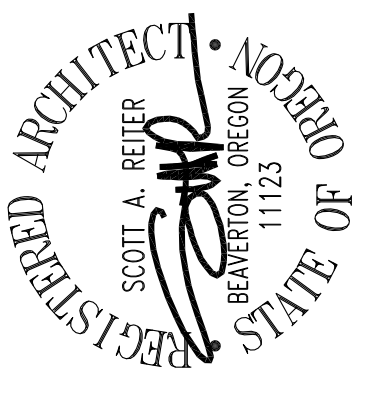
A  
 2.6

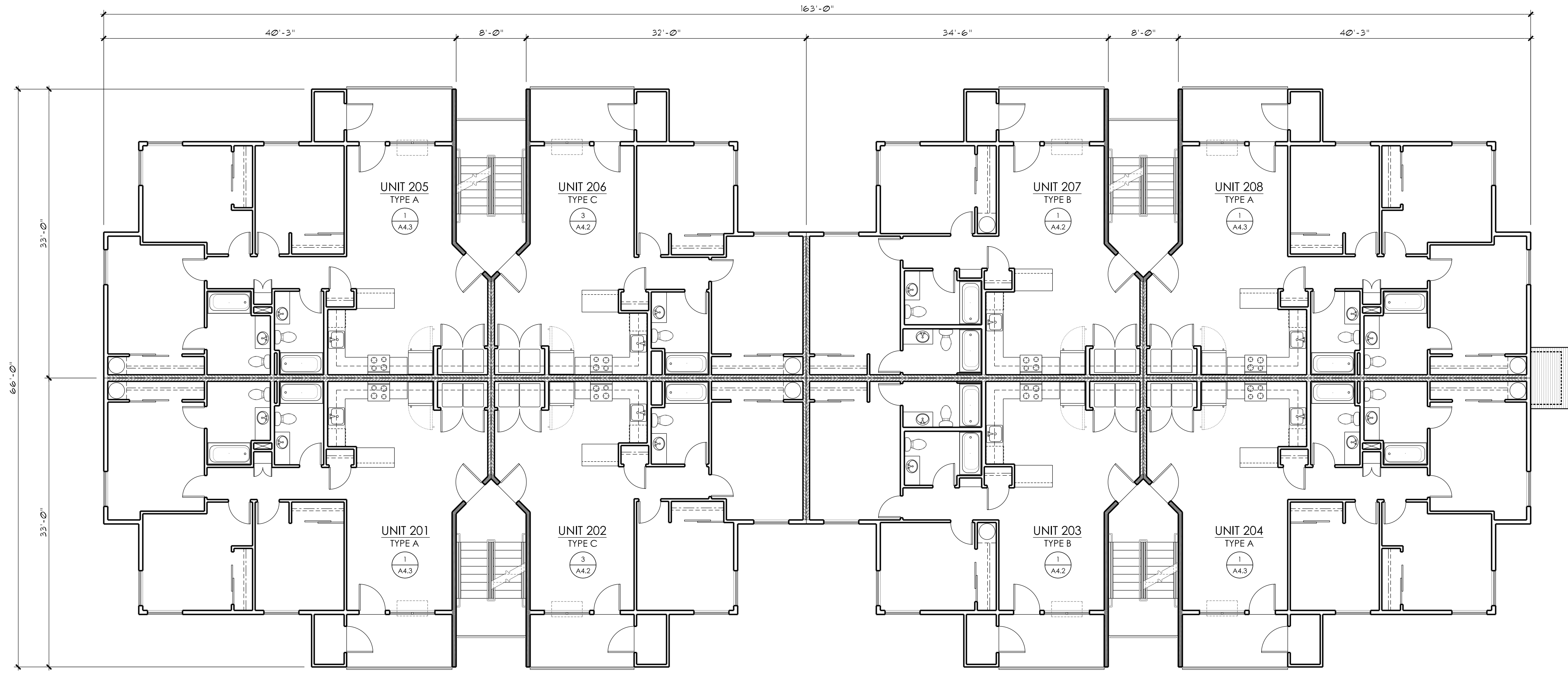
NORTON LANDING APARTMENTS  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
 KWDS, LLC  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED

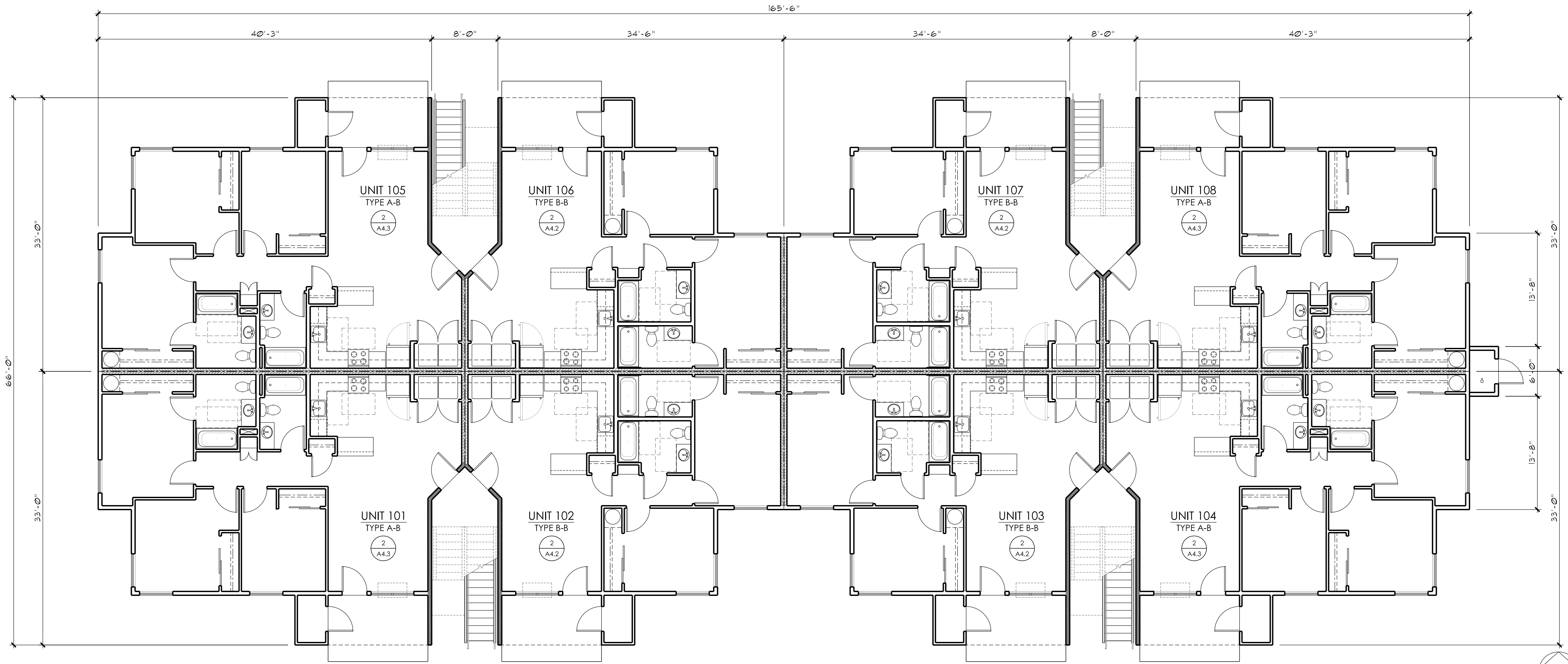
6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056





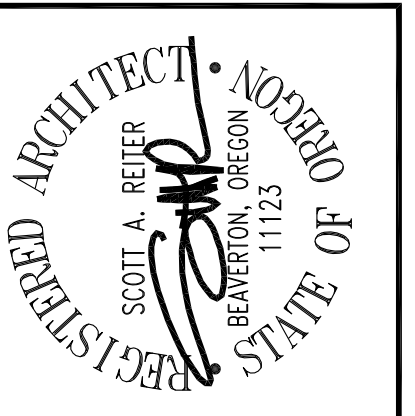
BUILDING 'F' - SECOND/THIRD FLOOR PLAN 2

Scale: 1/8" = 1'-0" A2.7



BUILDING 'F' - FIRST FLOOR PLAN 1

Scale: 1/8" = 1'-0" A2.7



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**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

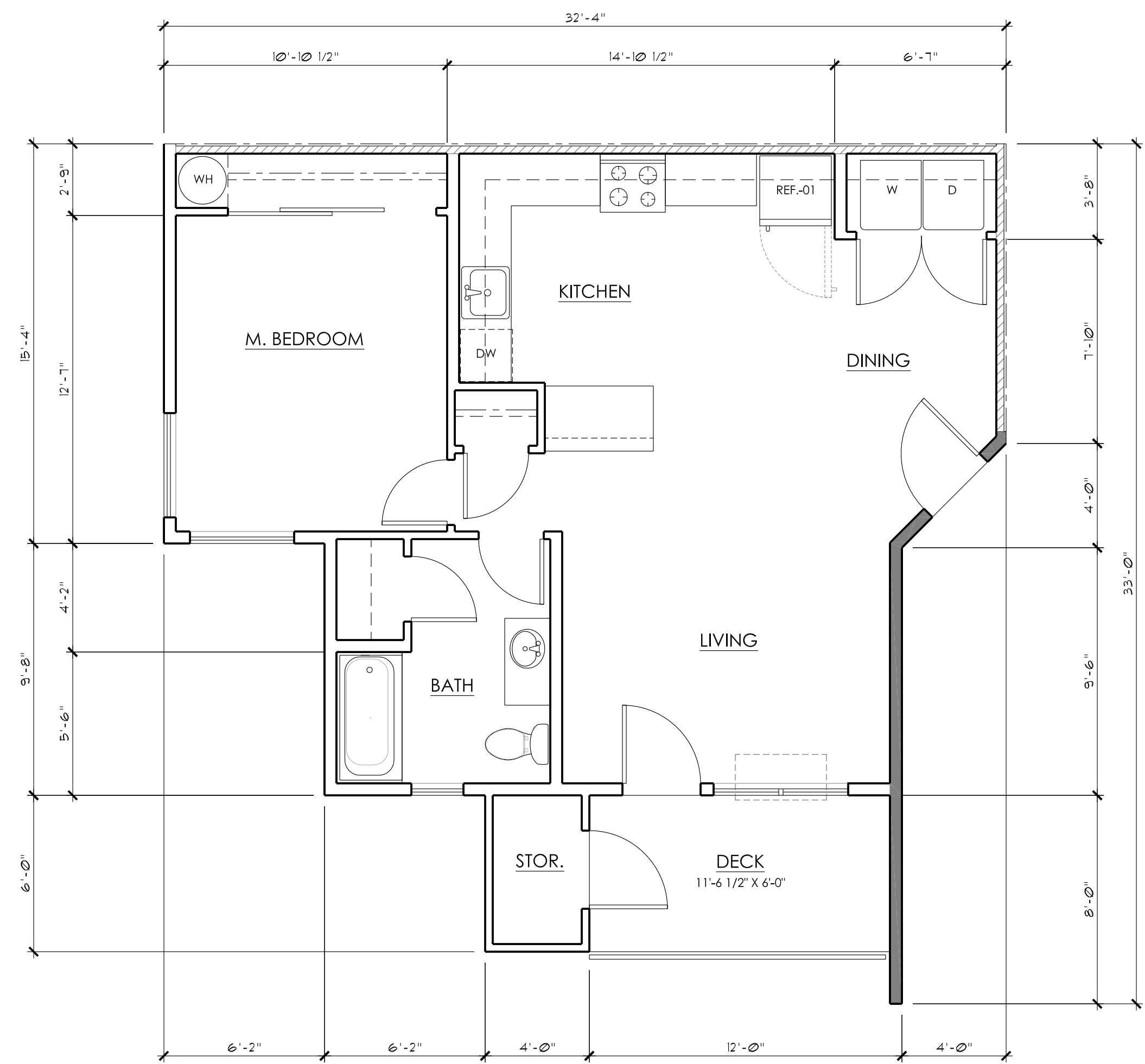
LAND-USE REVIEW

BUILDING PLANS  
BUILDING 'F'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   |               |            |  |
| job no.: | 2301          |            |  |

**A**  
**2.7**





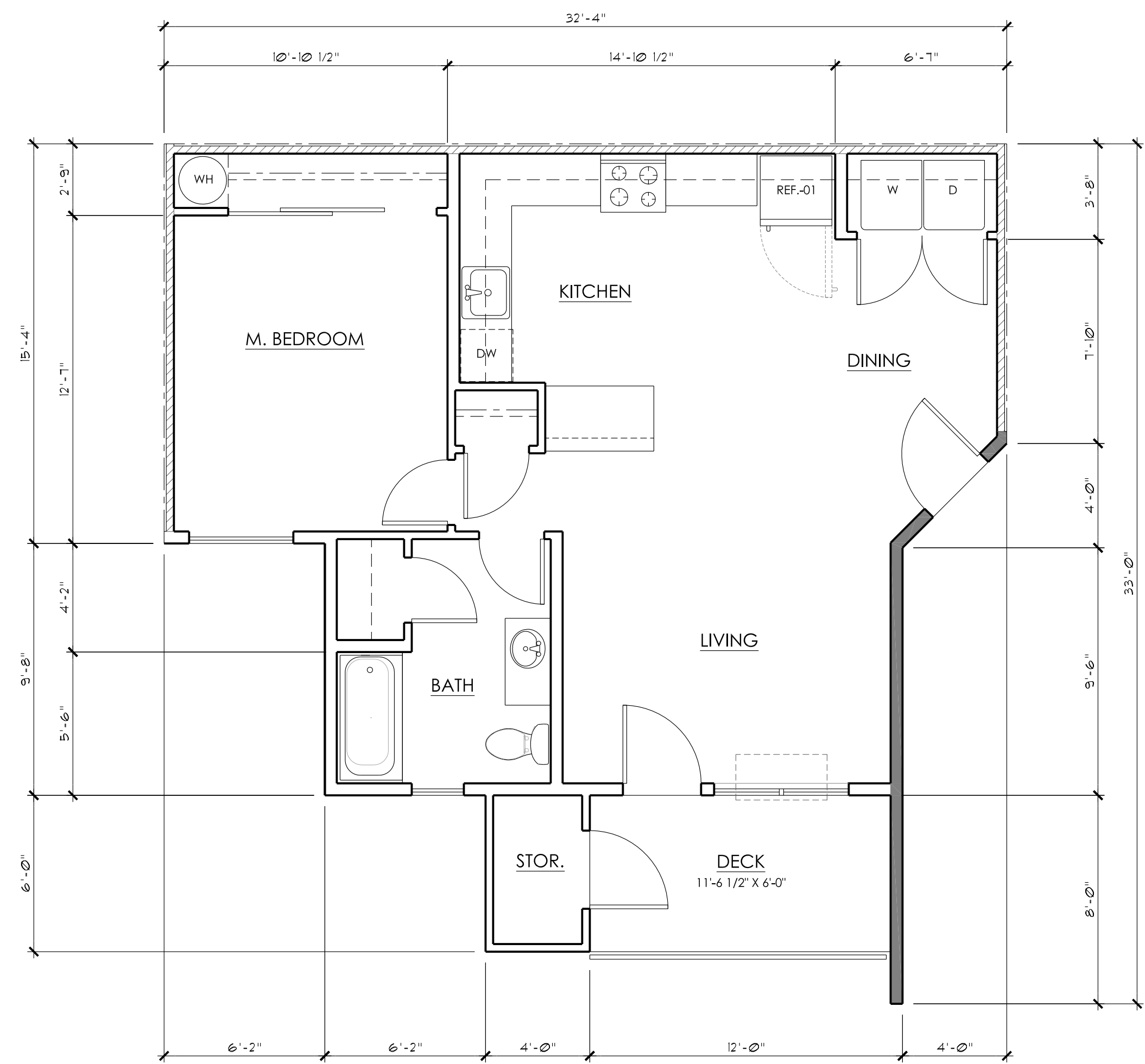
703 SF

UNIT TYPE D - 1BR, 1BATH - UPPER END UNIT

1

Scale: 1/4" = 1'-0"

A4.1



703 SF

UNIT TYPE D - 1BR, 1 BATH UPPER MID-UNIT

2

Scale: 1/4" = 1'-0"

A4.1



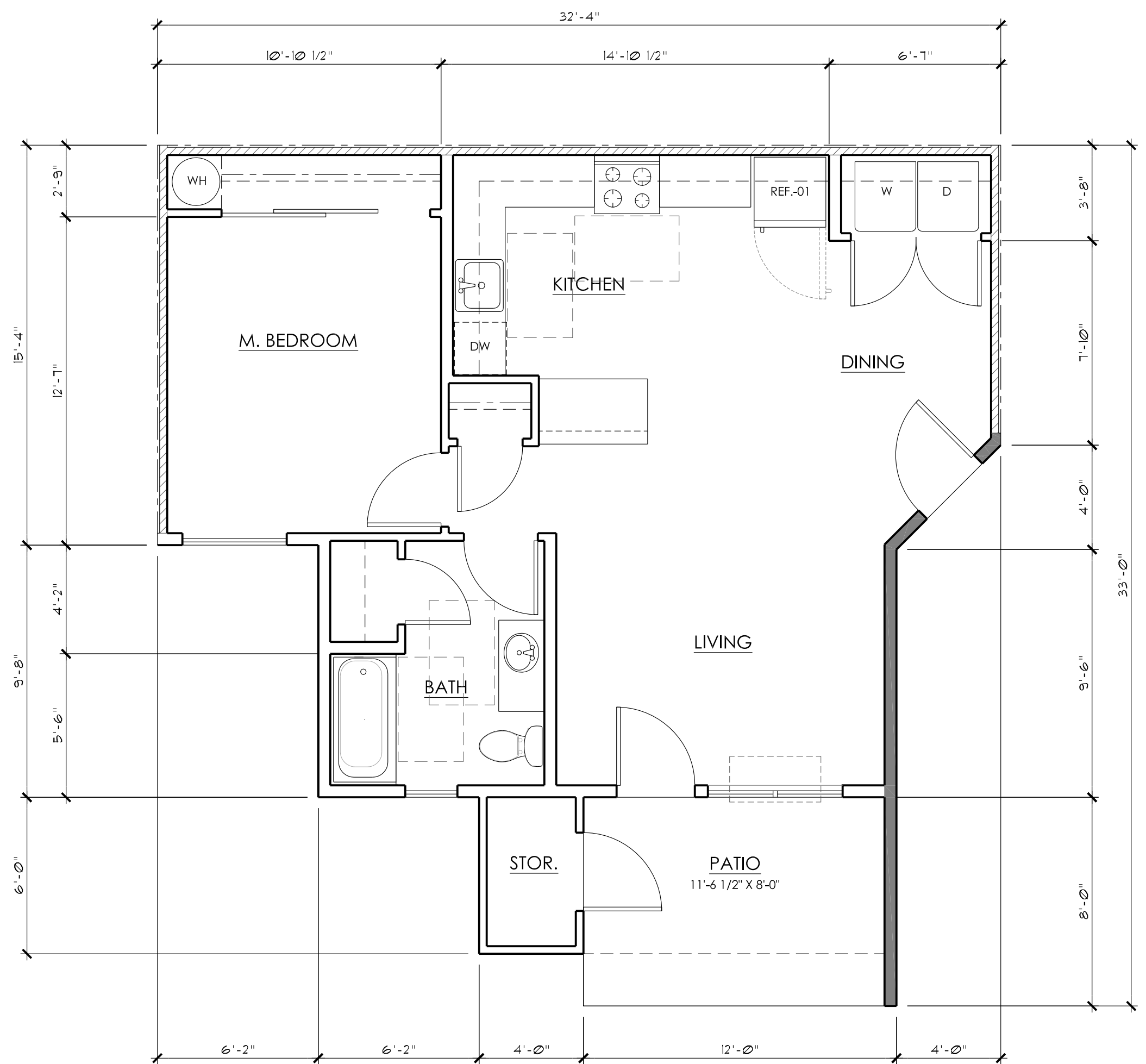
703 SF

UNIT TYPE D-B, 1BR, 1 BATH, ADA TYPE 'B' END UNIT

3

Scale: 1/4" = 1'-0"

A4.1



703 SF

UNIT TYPE D-B, 1BR-1BATH, ADA TYPE 'B' MID-UNIT

4

Scale: 1/4" = 1'-0"

A4.1

LAND-USE REVIEW

CONCEPTUAL UNIT PLANS  
UNIT TYPES D, D-B

|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
| drawn:   | SAR           |
| job no.: | 2301          |

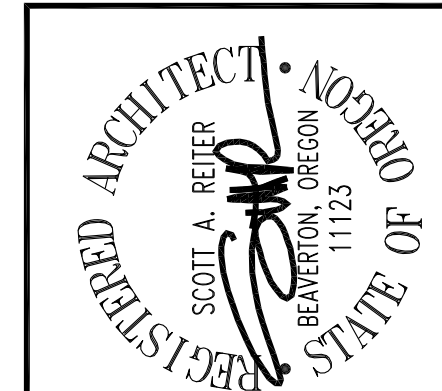
A  
4.1

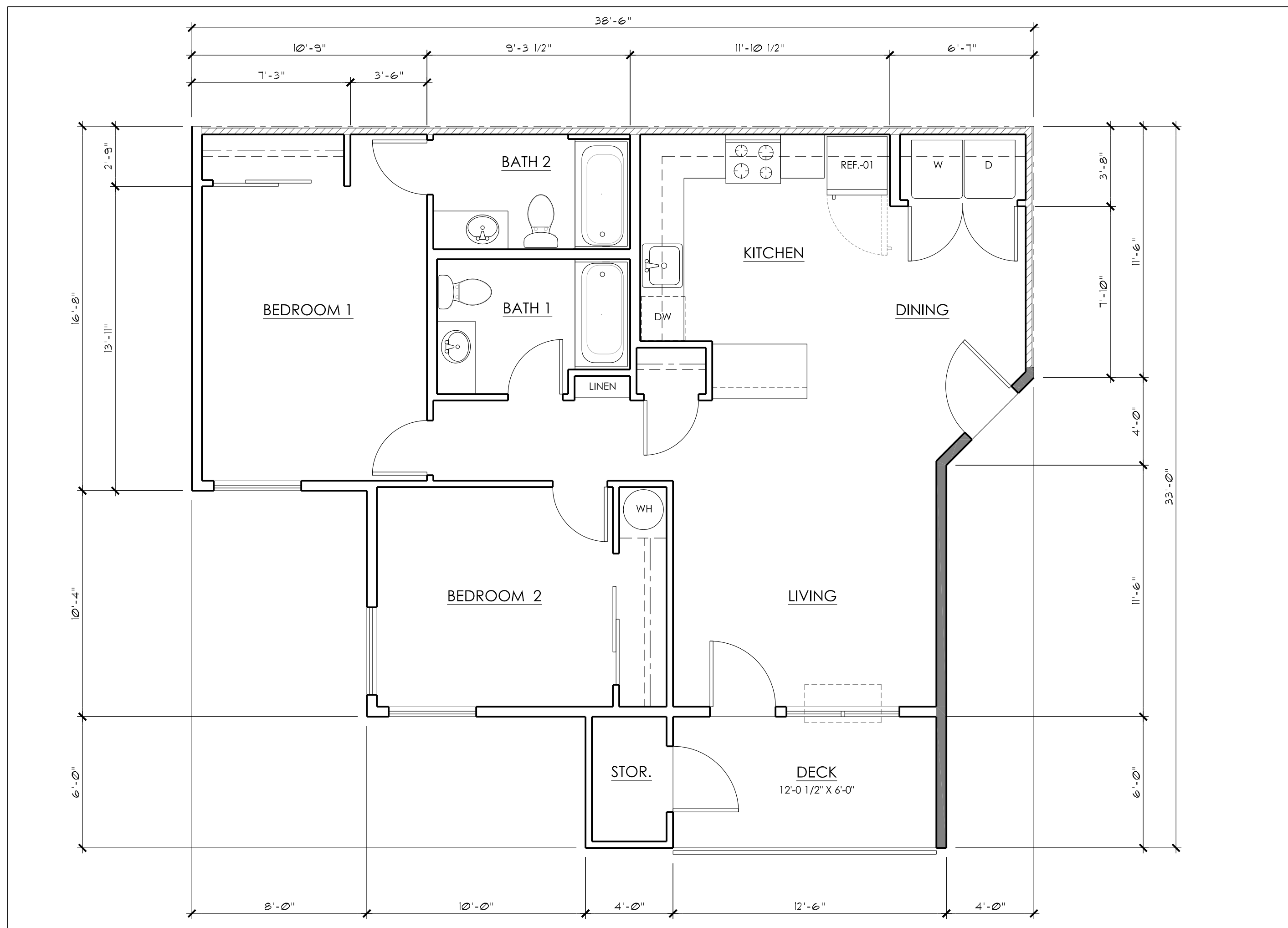
**NORTON LANDING APARTMENTS**  
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**KWDS, LLC**  
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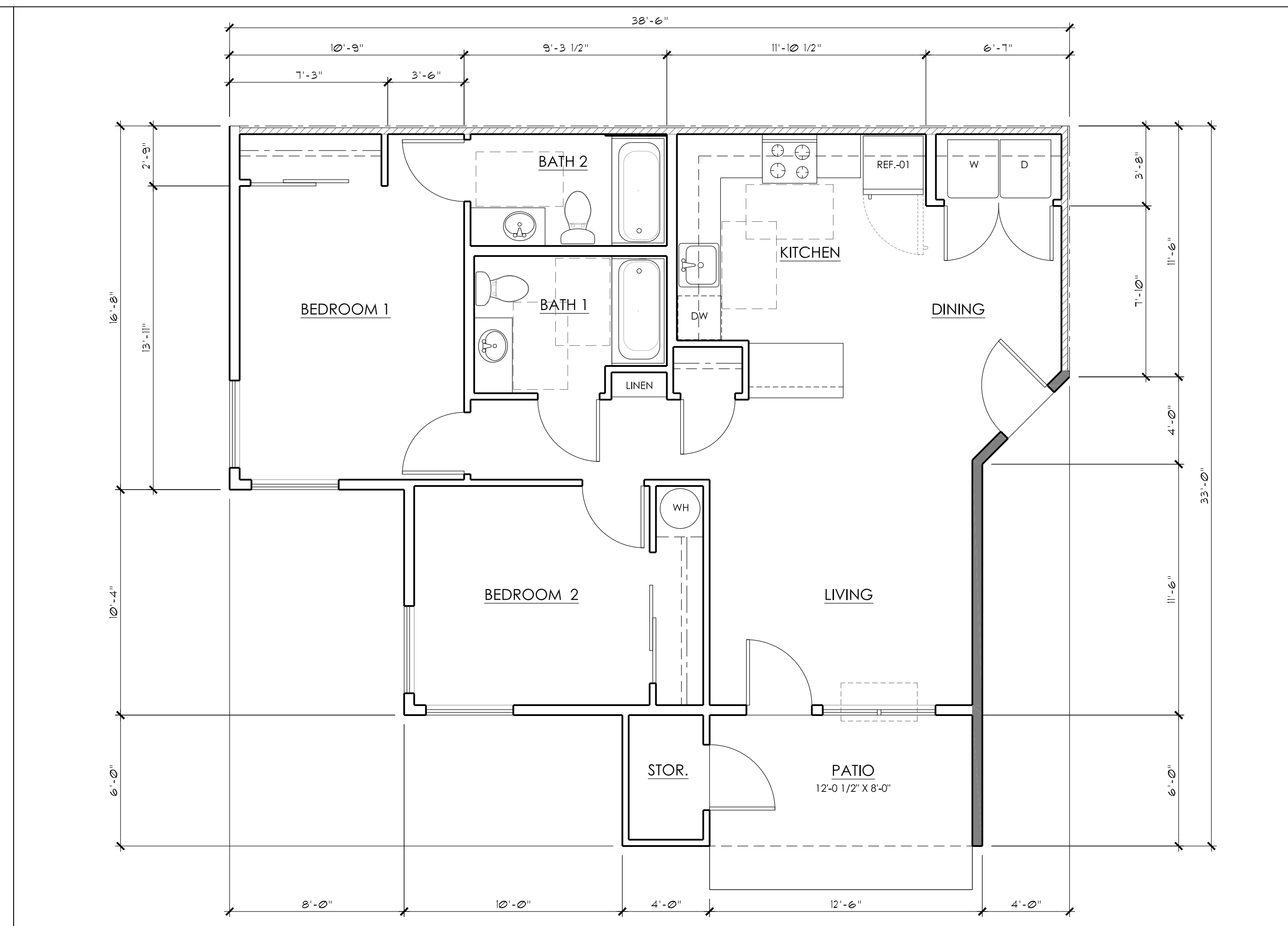
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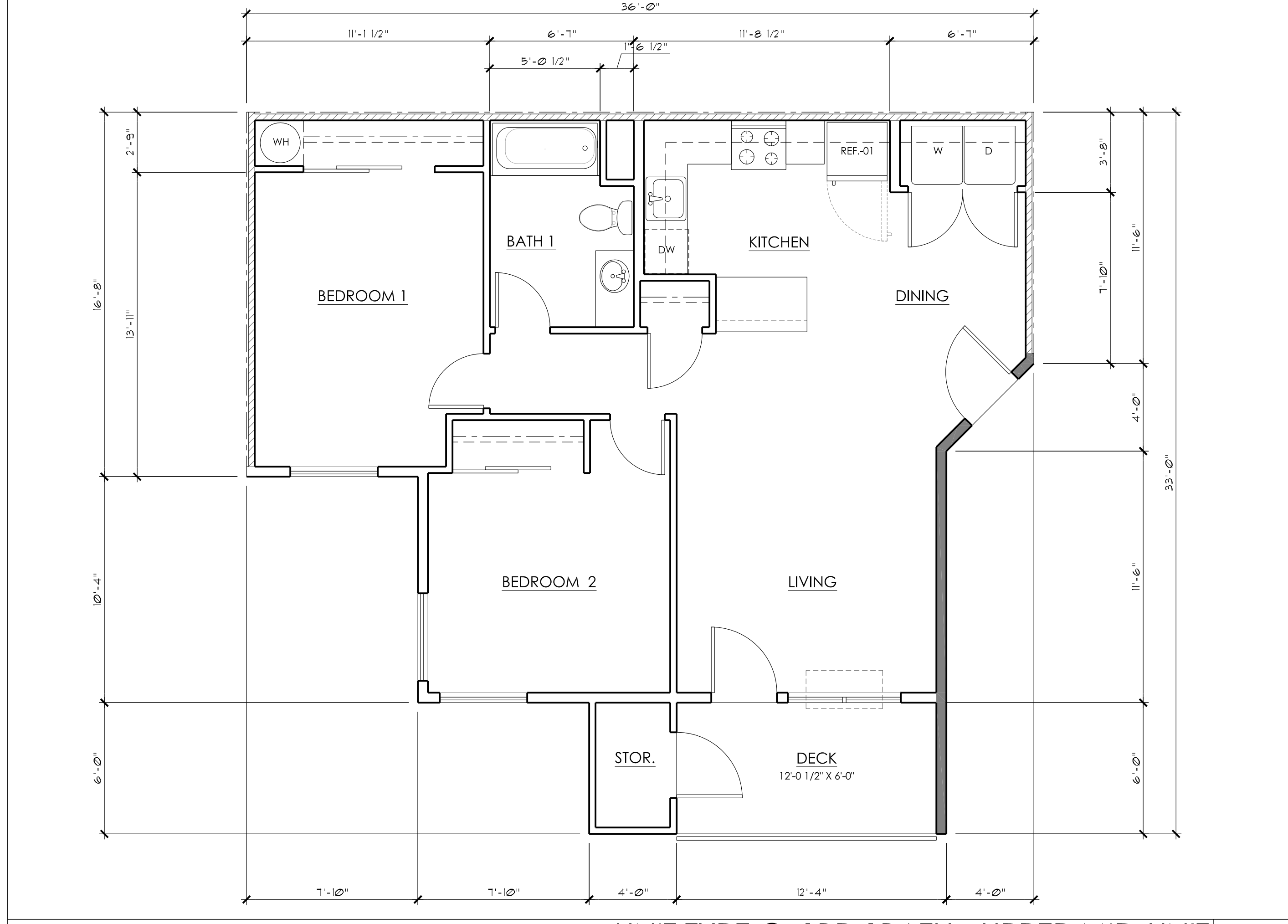




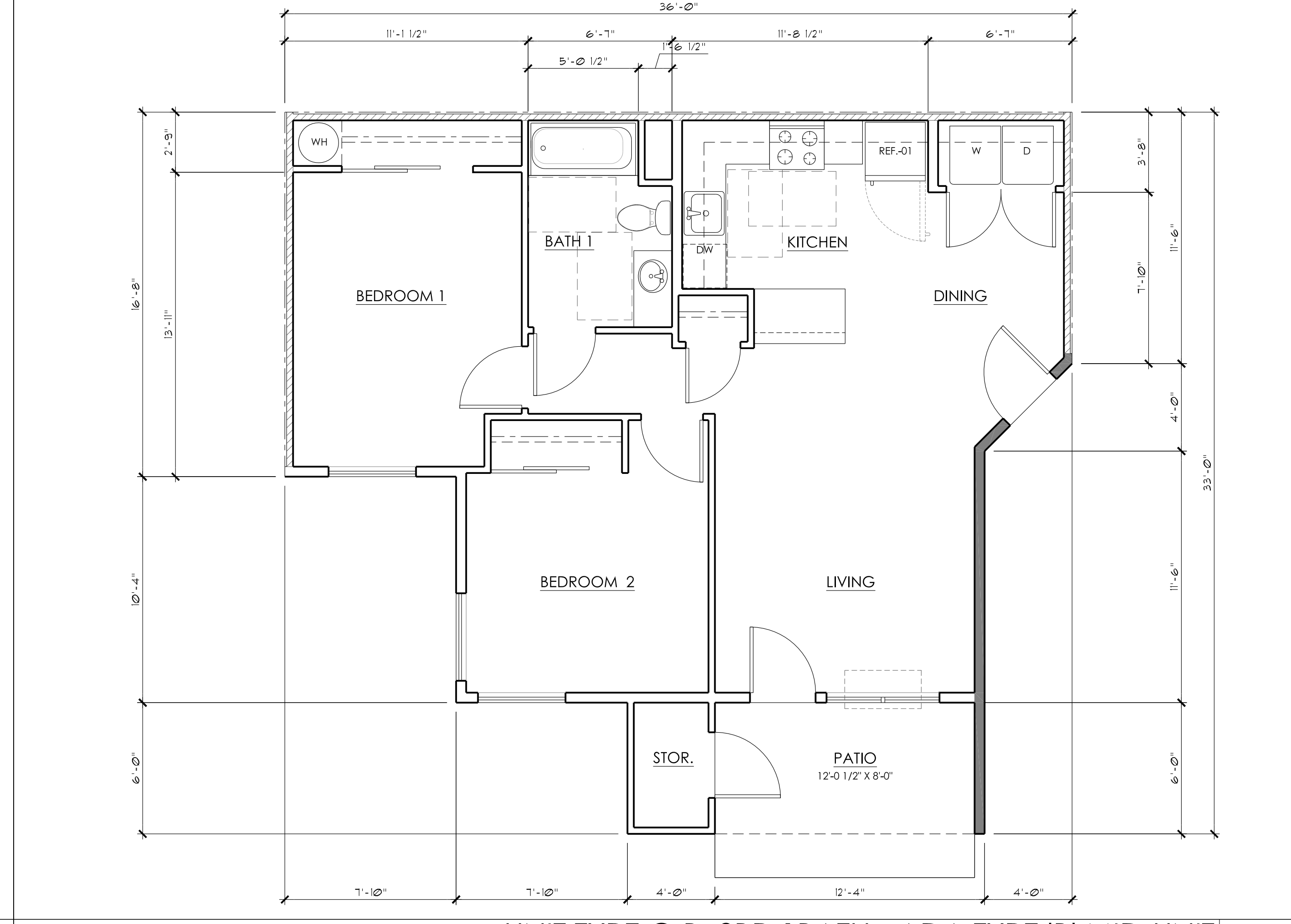
903 SF UNIT TYPE B, 2BR, 2BATH - UPPER END UNIT 1  
Scale: 1/4" = 1'-0" A4.2



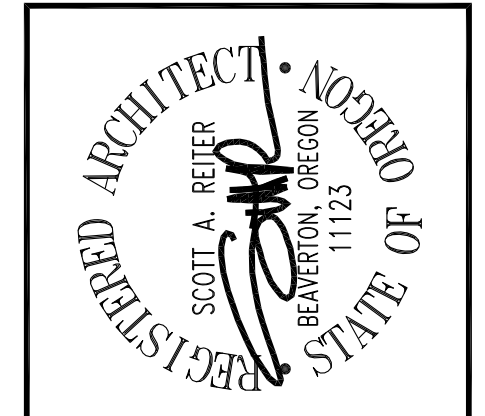
903 SF UNIT TYPE B-B, 2BR, 2 BATH - ADA TYPE 'B' END-UNIT 2  
Scale: 1/4" = 1'-0" A4.2



837 SF UNIT TYPE C, 1BR-1BATH - UPPER MID-UNIT 3  
Scale: 1/4" = 1'-0" A4.2



837 SF UNIT TYPE C-B, 2BR-1BATH - ADA TYPE 'B' MID-UNIT 4  
Scale: 1/4" = 1'-0" A4.2



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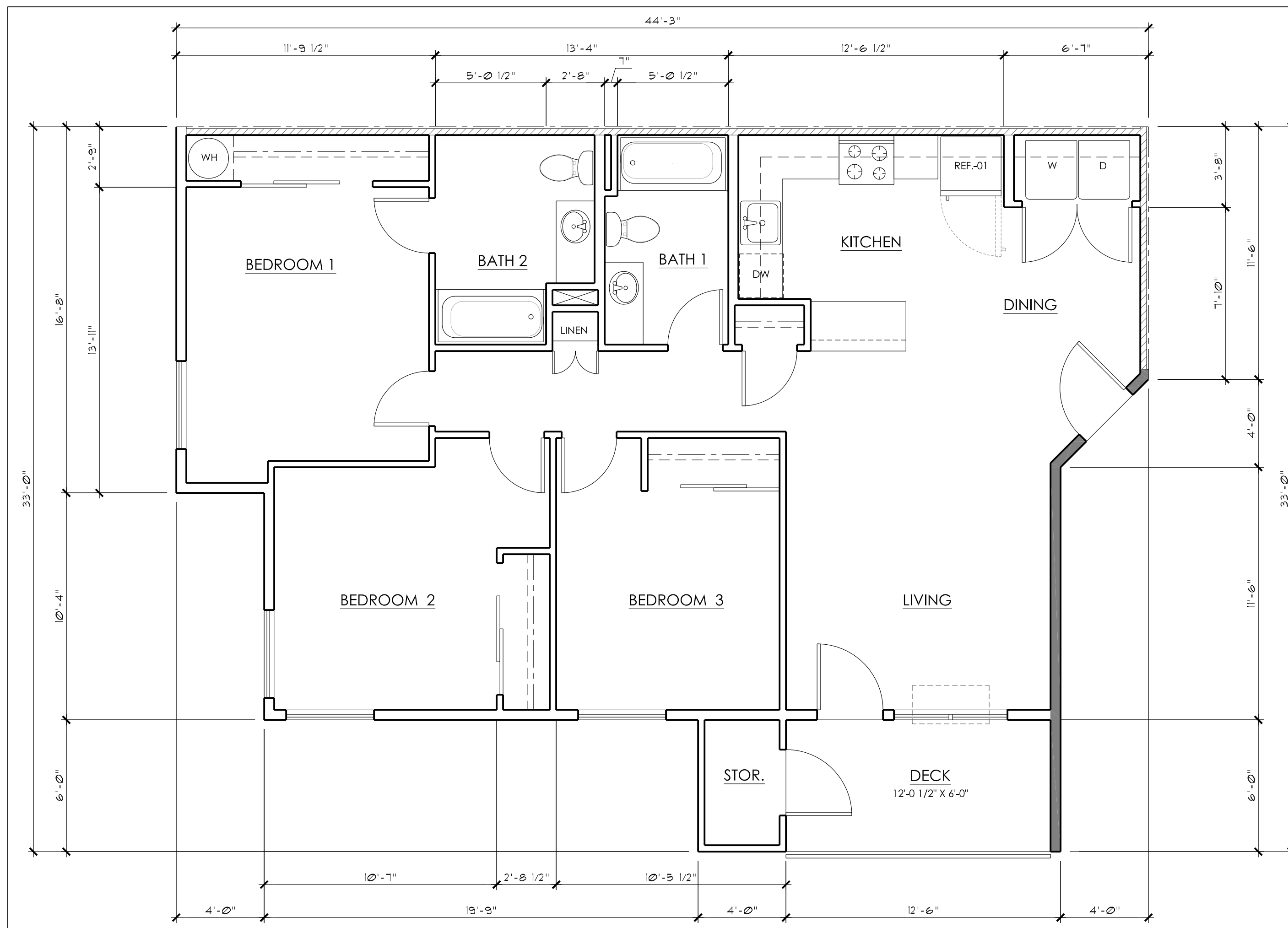
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UNIT PLANS  
UNIT TYPES B, B-B, C, C-B

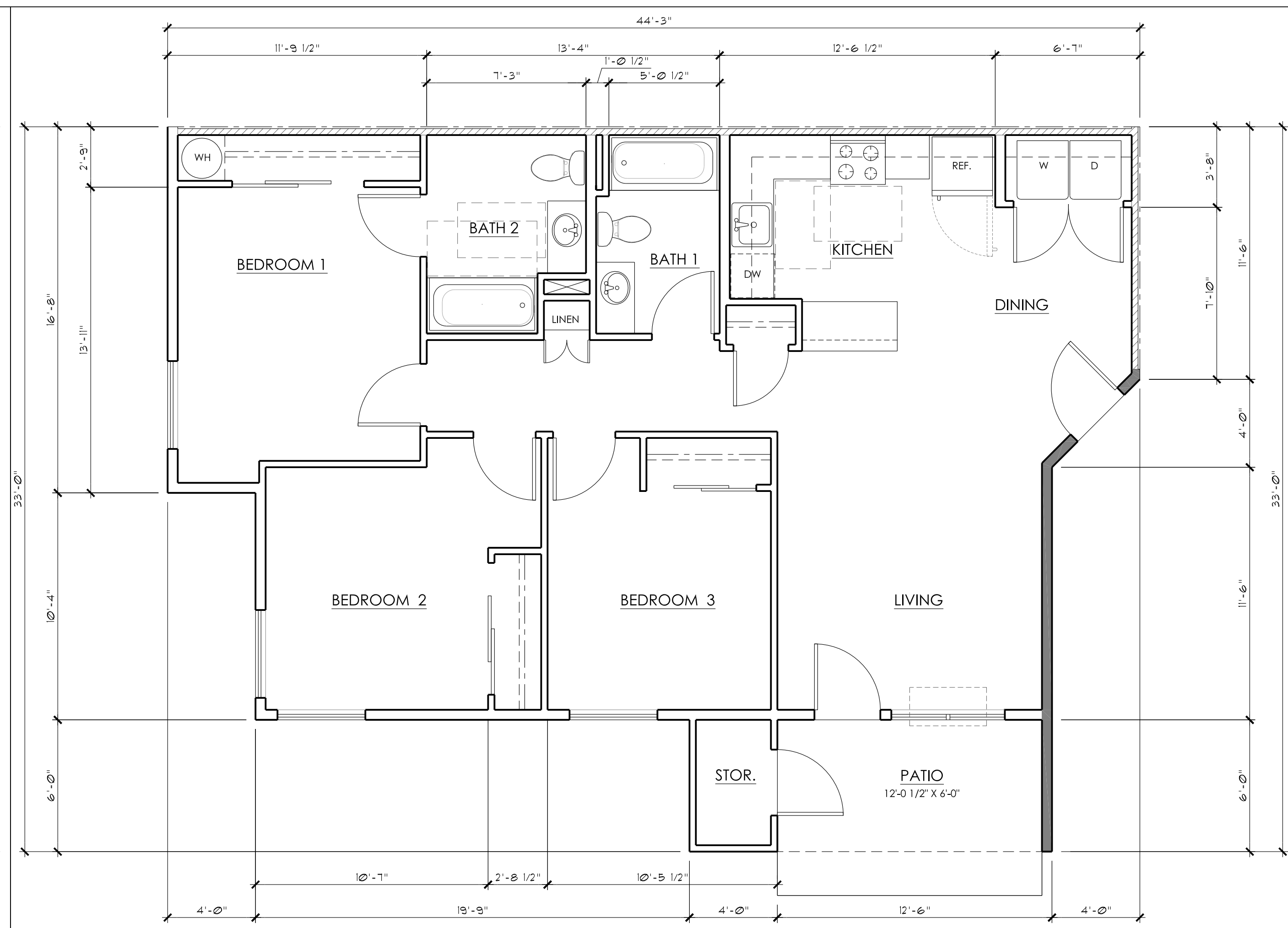
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|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

LAND-USE REVIEW

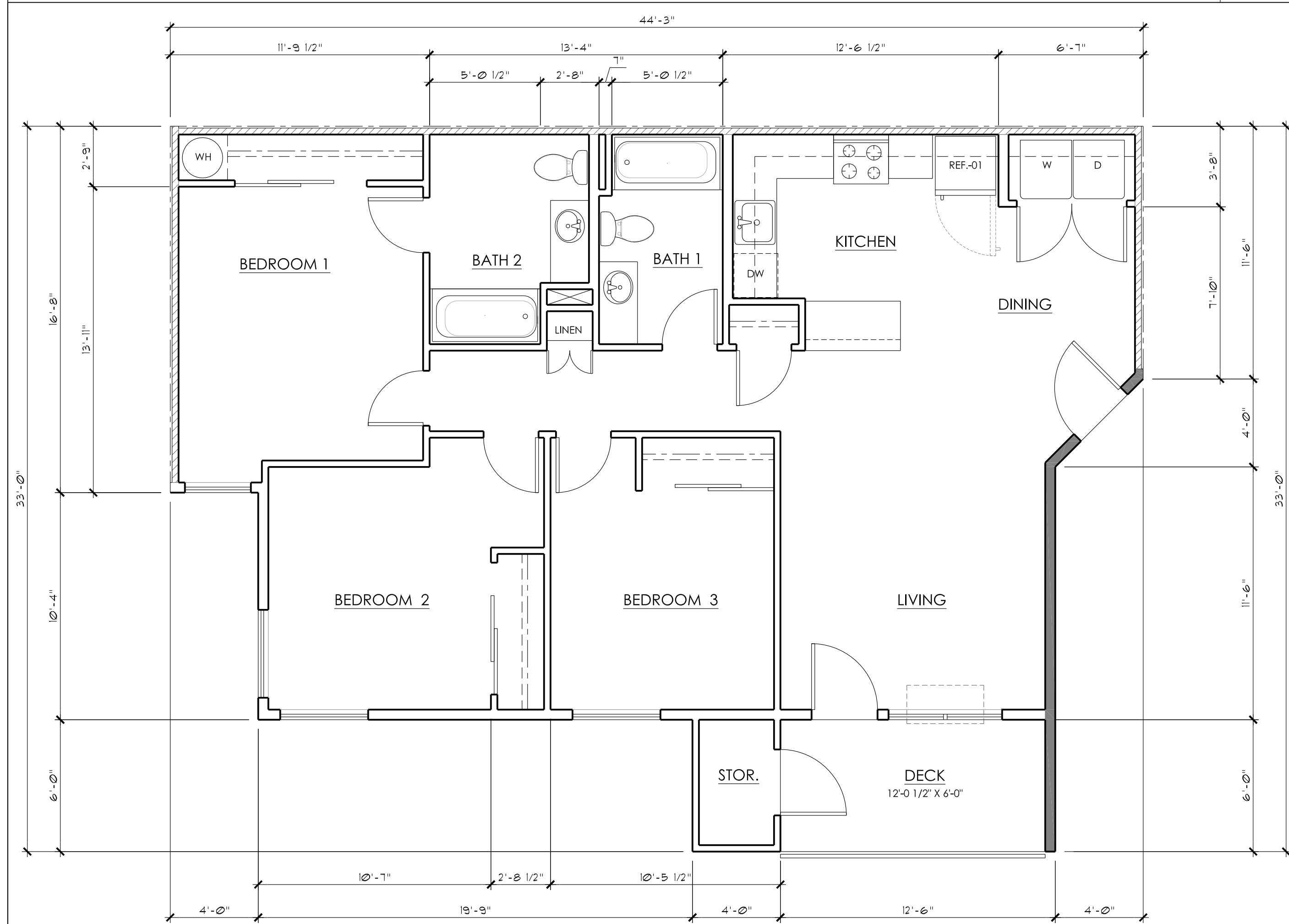
**A**  
**4.2**



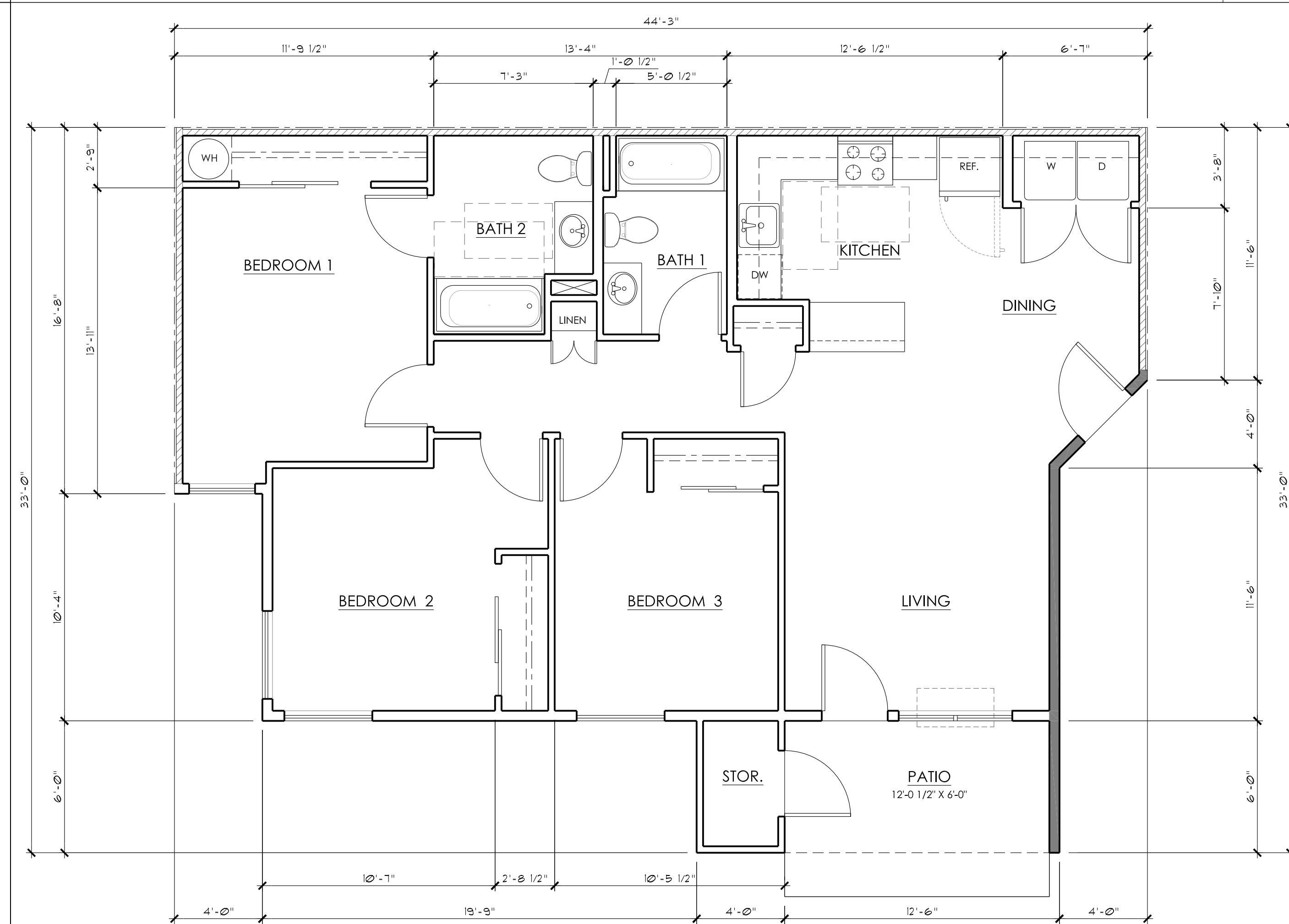
1,100 SF UNIT TYPE A, 3BR, 2BATH - UPPER END UNIT 1  
Scale: 1/4" = 1'-0" A4.3



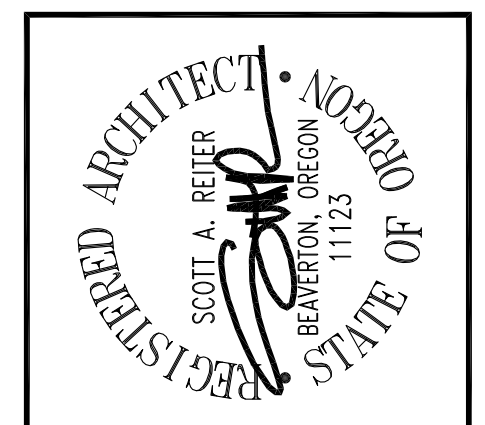
1,100 SF UNIT TYPE A-B, 3BR, 2 BATH - ADA TYPE 'B' END-UNIT 2  
Scale: 1/4" = 1'-0" A4.3



1,100 SF UNIT TYPE A - 3BR, 2 BATH, UPPER MID UNIT 3  
Scale: 1/4" = 1'-0" A4.3



1,100 SF UNIT TYPE A-B, 3BR, 2 BATH, ADA TYPE 'B' MID-UNIT 4  
Scale: 1/4" = 1'-0" A4.3



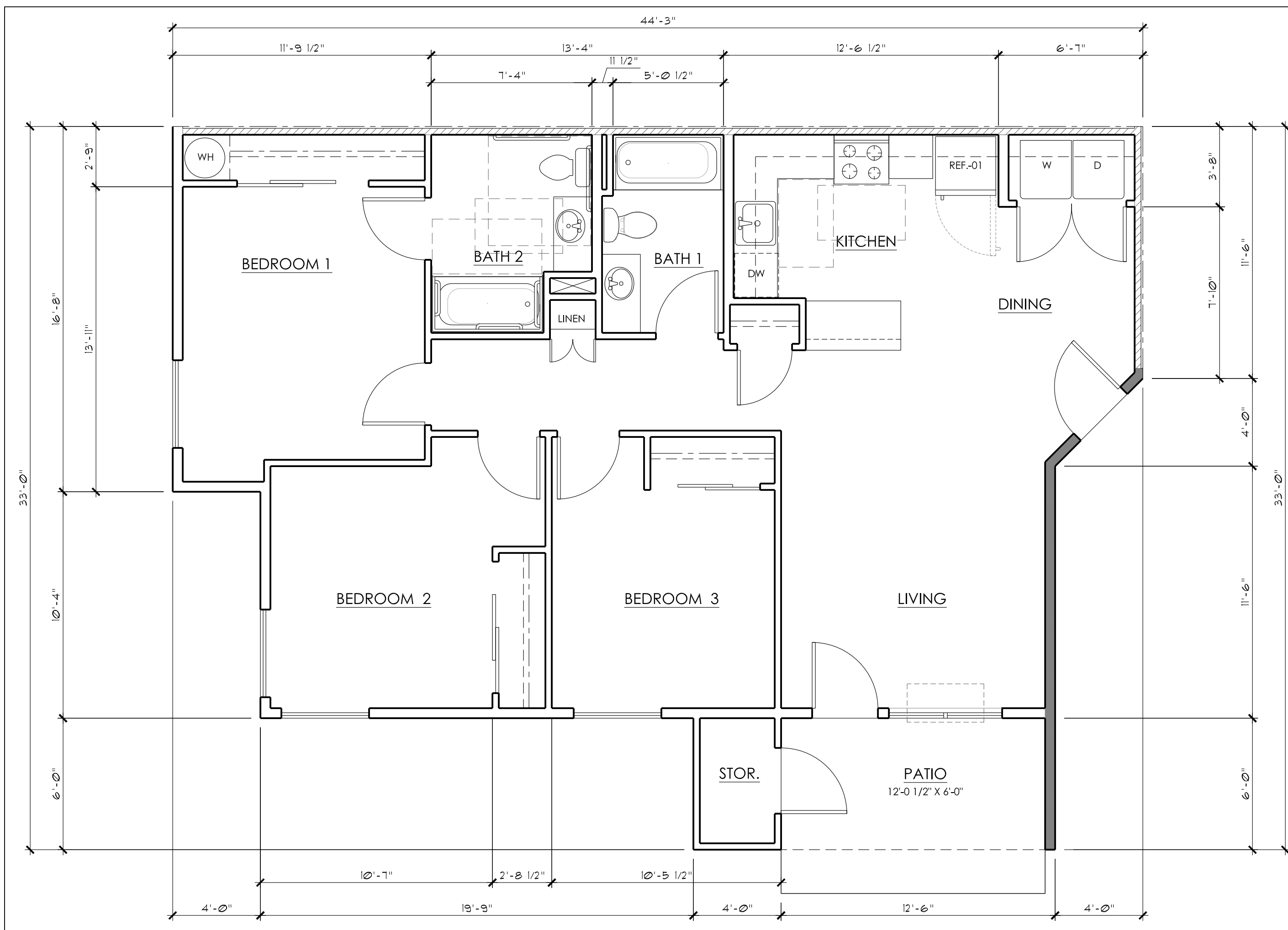
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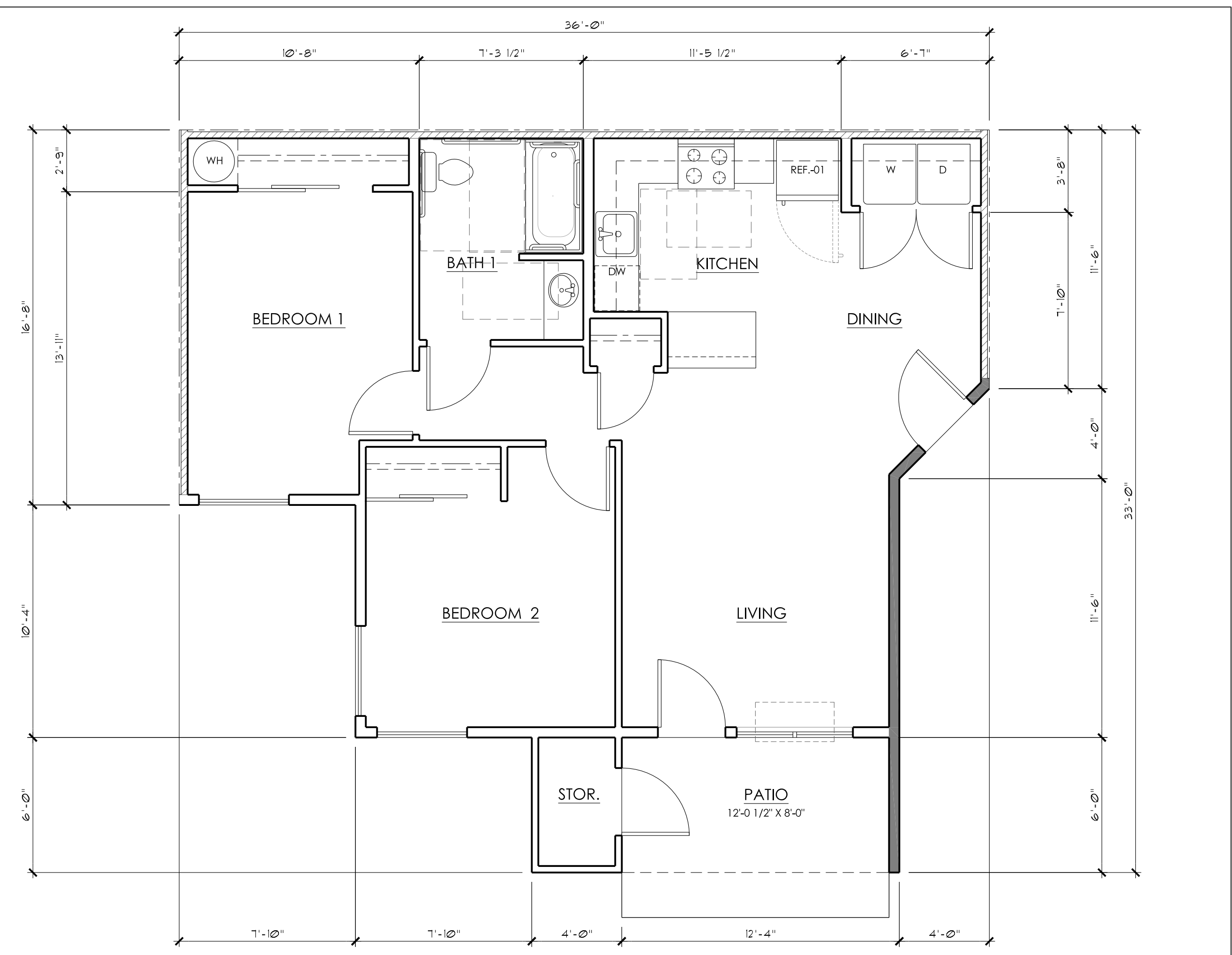
LAND-USE REVIEW

|                                 |                     |            |
|---------------------------------|---------------------|------------|
| UNIT PLANS<br>UNIT TYPES A, A-B | date: JULY 19, 2023 | revisions: |
|                                 | scale: AS NOTED     |            |
|                                 | drawn: SAR          |            |
|                                 | job no: 2301        |            |

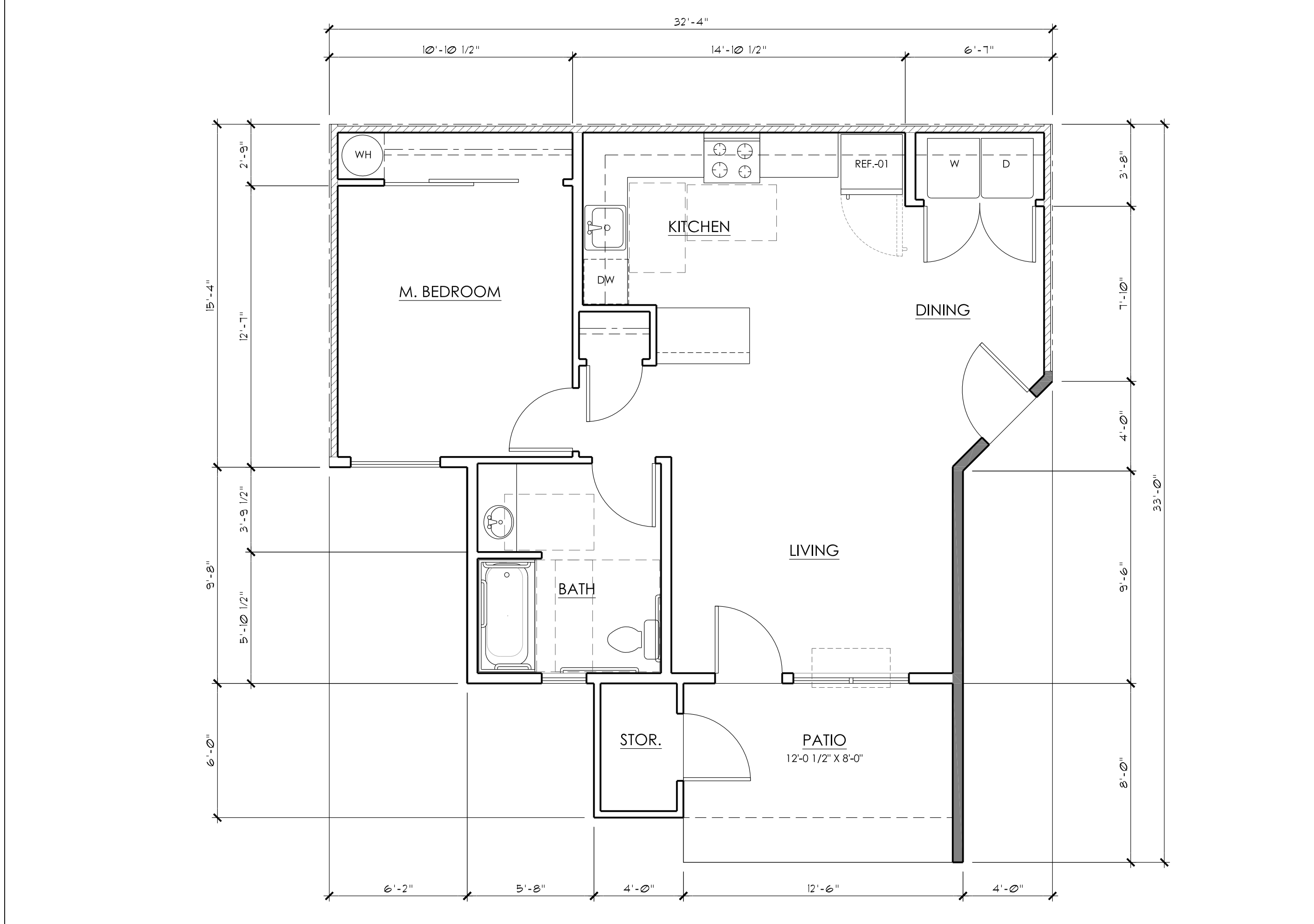
A  
4.3



1,100 SF UNIT TYPE A-A, 3BR, 2BATH - ADA TYPE 'A' END UNIT 1  
Scale: 1/4" = 1'-0" A4.4



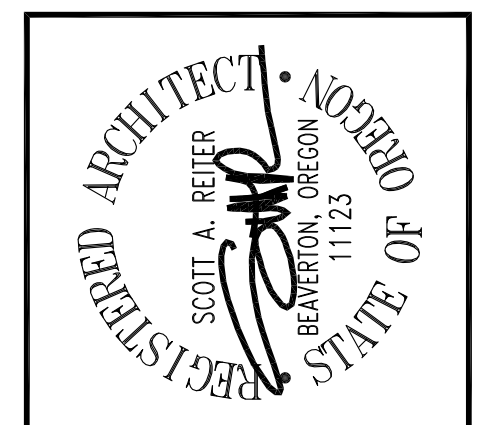
837 SF UNIT TYPE C-A, 2BR, 1 BATH - ADA TYPE 'A' MID-UNIT 2  
Scale: 1/4" = 1'-0" A4.4



703 SF UNIT TYPE D-A, 1BR-1BATH - ADA TYPE 'A' MID-UNIT 3  
Scale: 1/4" = 1'-0" A4.4



837 SF UNIT TYPE A-A, 3BR, 2BATH - ADA TYPE 'A' END UNIT 4  
Scale: 1/4" = 1'-0" A4.4



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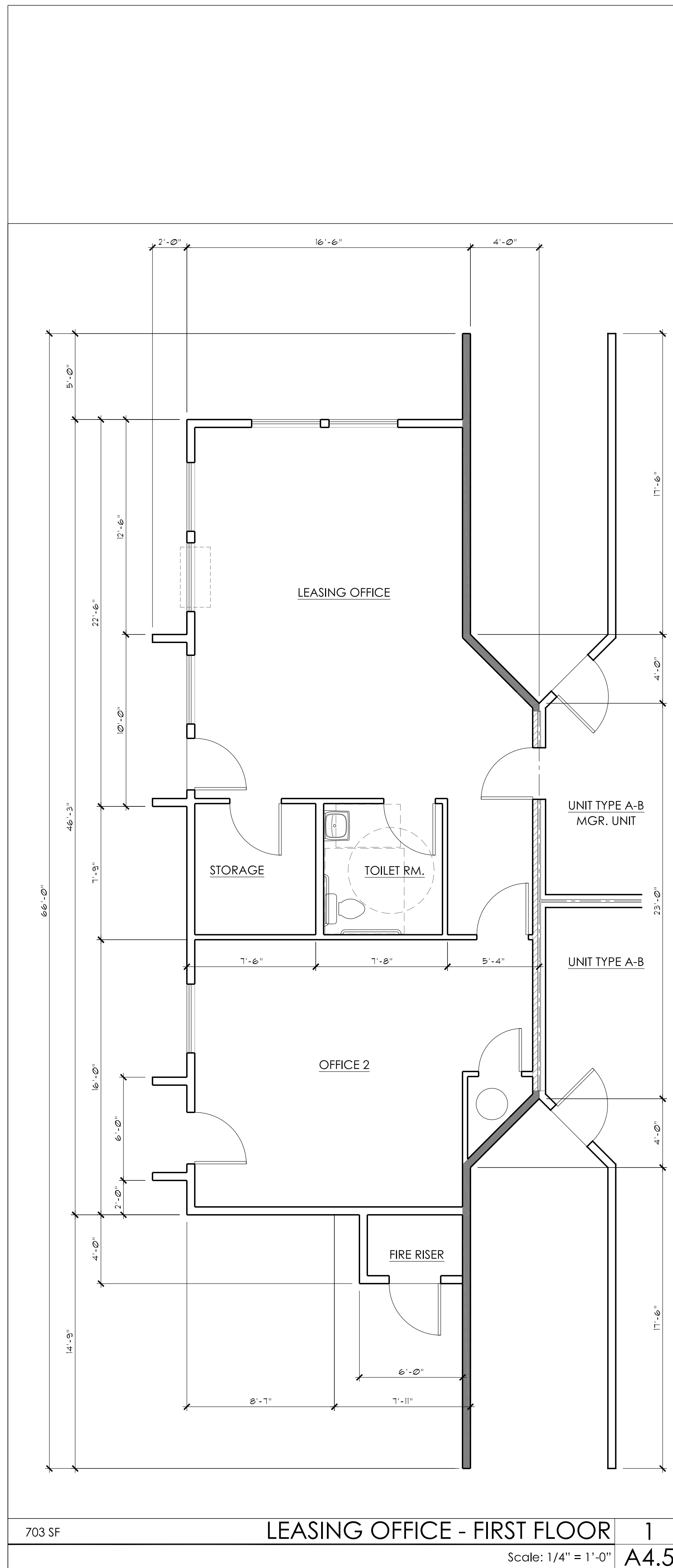
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OWNER:  
**KWDS, LLC**  
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UNIT PLANS, ADA TYPE 'A' UNITS  
UNIT TYPES A-A, C-A, D-A

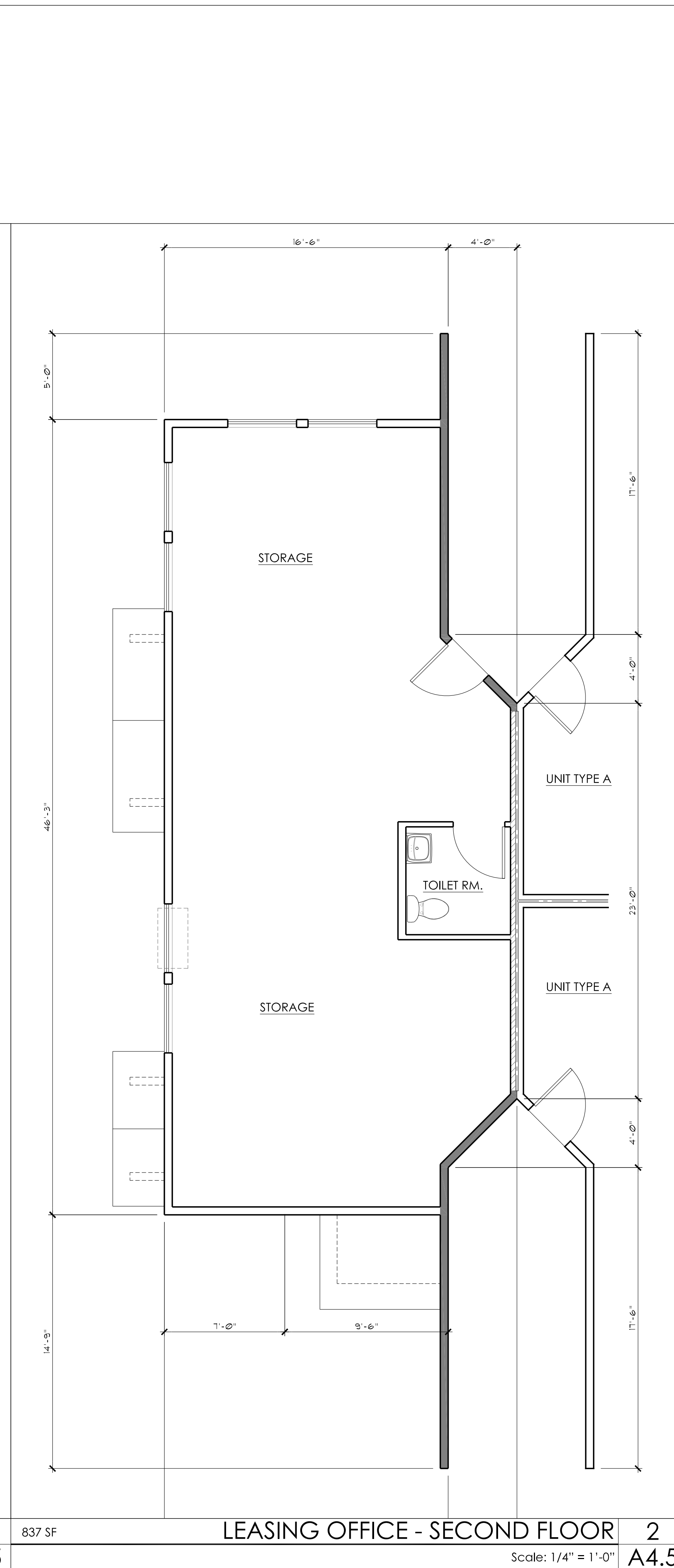
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|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

LAND-USE REVIEW

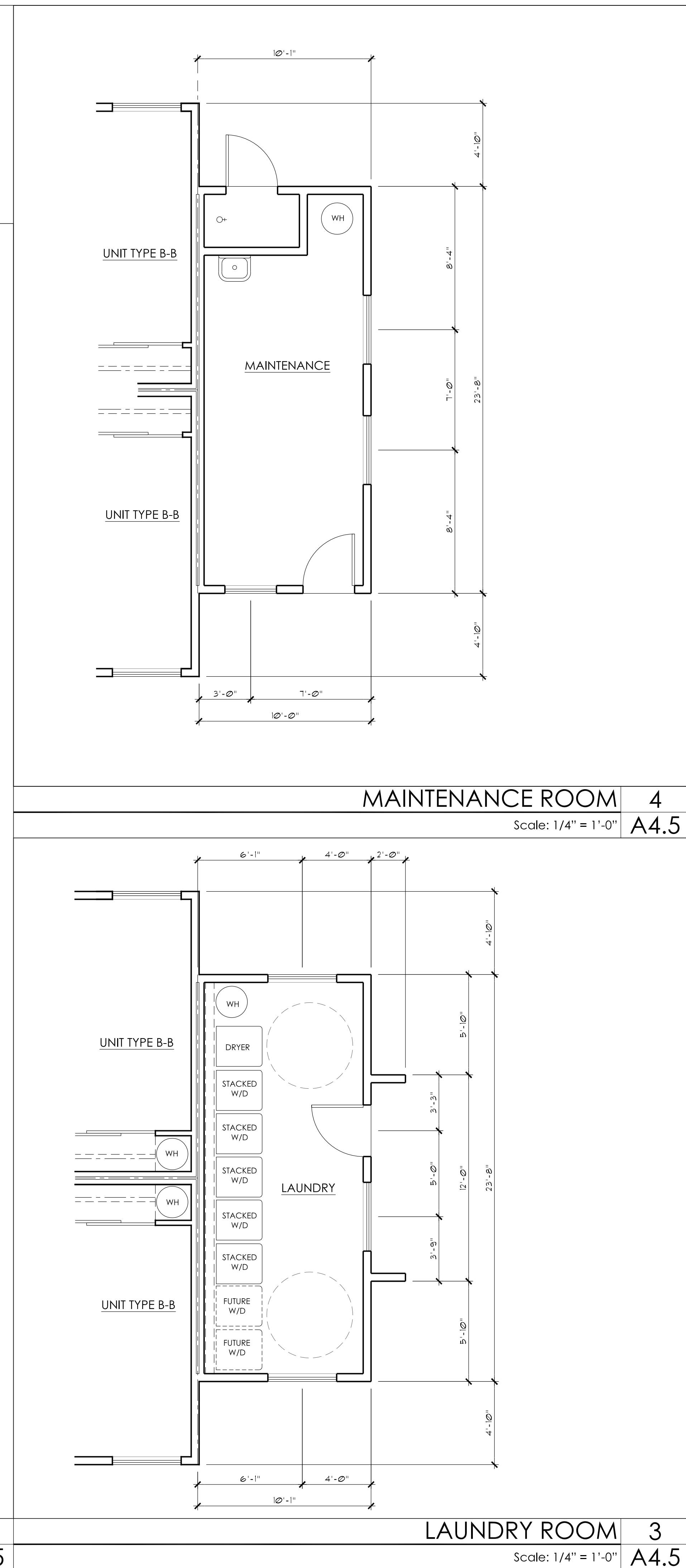
**A**  
**4.4**



703 SF LEASING OFFICE - FIRST FLOOR 1  
Scale: 1/4" = 1'-0" A4.5



837 SF LEASING OFFICE - SECOND FLOOR 2  
Scale: 1/4" = 1'-0" A4.5



LAUNDRY ROOM 3  
Scale: 1/4" = 1'-0" A4.5

LAND-USE REVIEW

LEASING OFFICE PLANS  
LAUNDRY ROOM PLAN  
MAINTENANCE PLAN

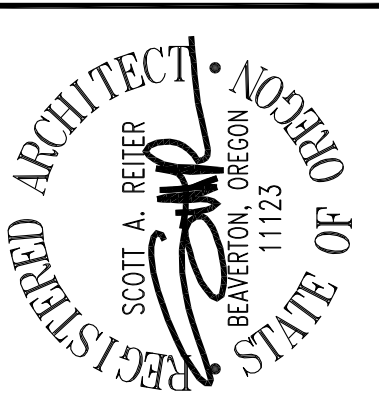
date: JULY 19, 2023  
scale: AS NOTED  
drawn: SAR  
job no: 2301

**A**  
**4.5**

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OWNER:  
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REGISTERED ARCHITECT • NO. 8810  
SCOTT A. REITER  
BEAVERTON, OREGON  
11123  
STATE OF OREGON



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**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

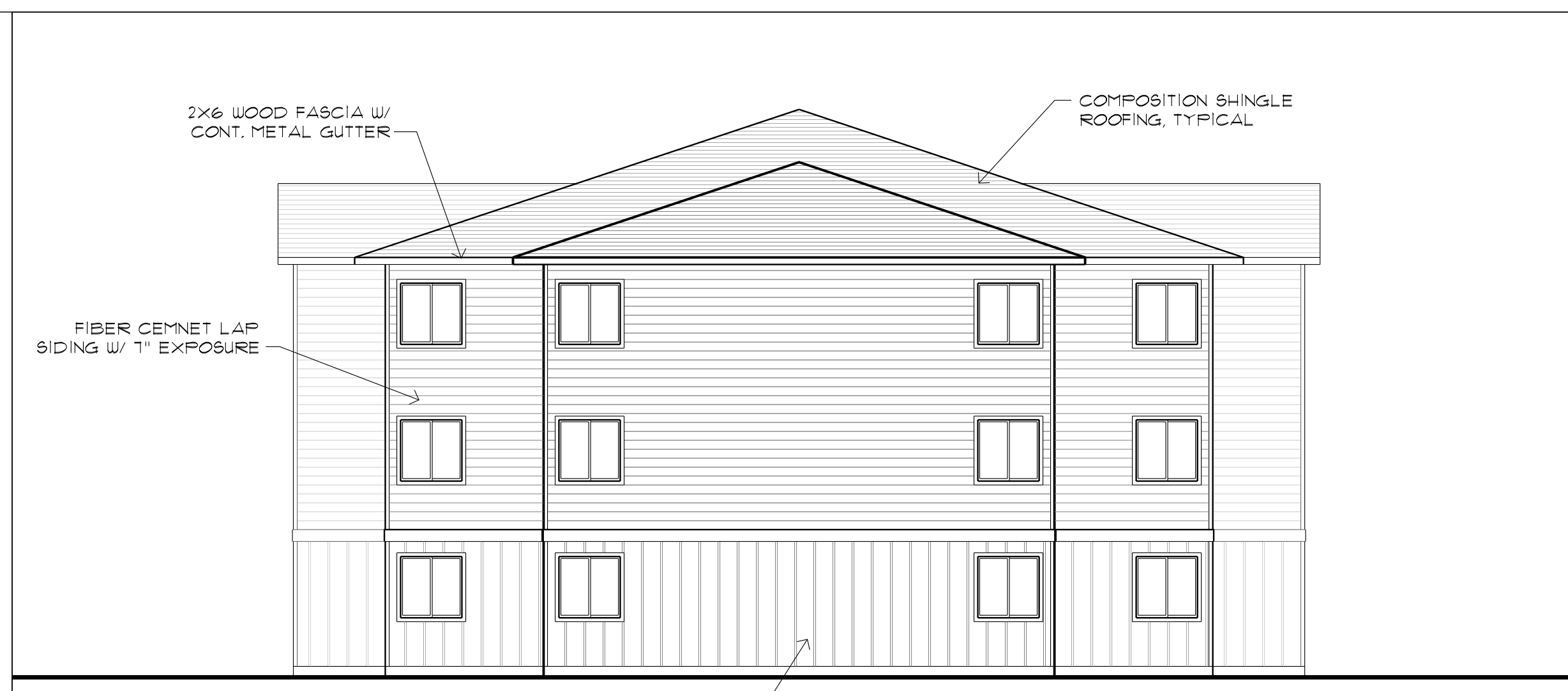
OWNER:  
**KWDS, LLC**  
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EXTERIOR ELEVATIONS  
BUILDING A

|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
| drawn:   | SAR           |
| job no.: | 2301          |

**A**  
**7.1**

LAND-USE REVIEW



**BUILDING 'A' - NORTH ELEVATION** 3  
Scale: 1/8" = 1'-0" **A7.1**



**BUILDING 'A' - WEST ELEVATION** 4  
Scale: 1/8" = 1'-0" **A7.1**



**BUILDING 'A' - SOUTH ELEVATION** 2  
Scale: 1/8" = 1'-0" **A7.1**

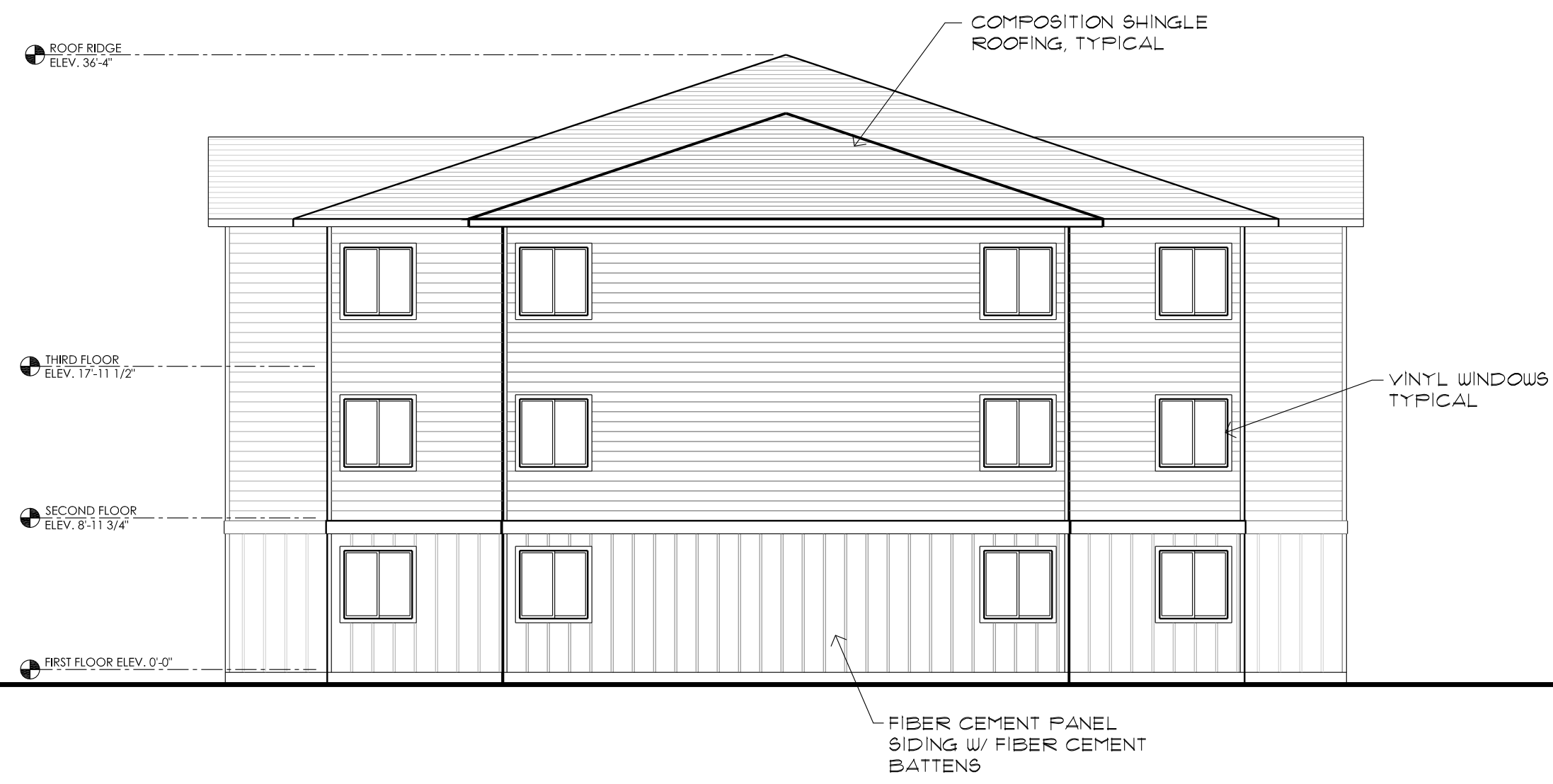


**BUILDING 'A' - EAST ELEVATION** 1  
Scale: 1/8" = 1'-0" **A7.1**





**BUILDING 'B' - WEST ELEVATION** 4  
 Scale: 1/8" = 1'-0" **A7.2**



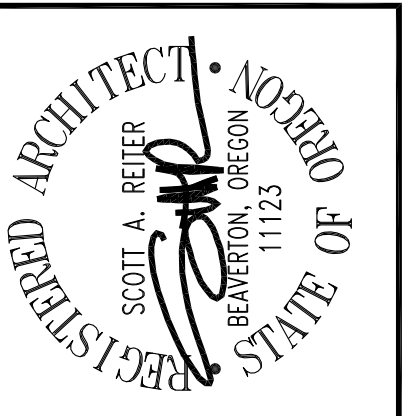
**BUILDING 'B' - SOUTH ELEVATION** 2  
 Scale: 1/8" = 1'-0" **A7.2**



**BUILDING 'B' - NORTH ELEVATION** 3  
 Scale: 1/8" = 1'-0" **A7.2**



**BUILDING 'B' - EAST ELEVATION** 1  
 Scale: 1/8" = 1'-0" **A7.2**



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 OWNER:  
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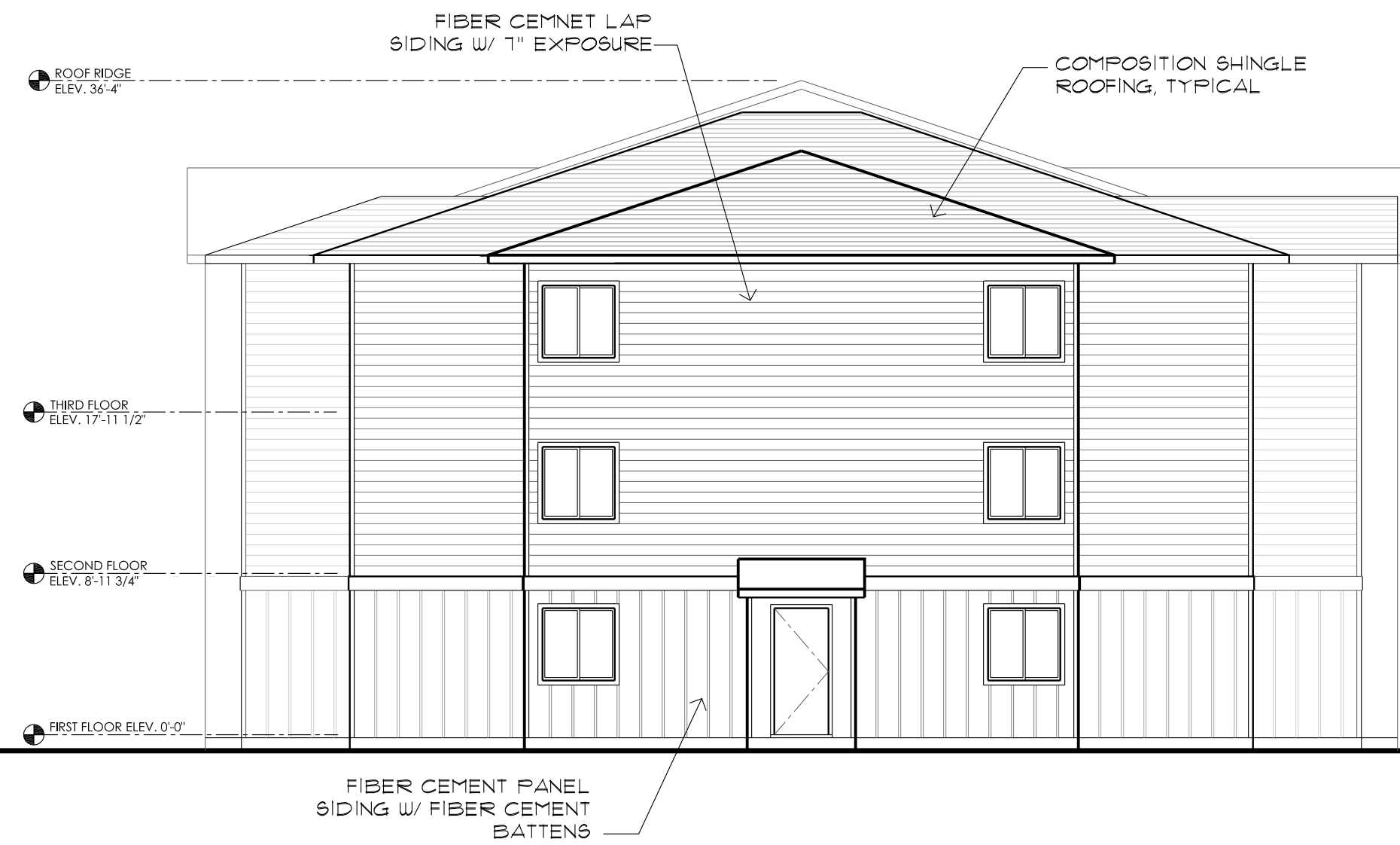
LAND-USE REVIEW

|                                   |  |                     |            |
|-----------------------------------|--|---------------------|------------|
| EXTERIOR ELEVATIONS<br>BUILDING B |  | date: JULY 19, 2023 | revisions: |
|                                   |  | scale: AS NOTED     |            |
|                                   |  | drawn: SAR          |            |
|                                   |  | job no: 2301        |            |

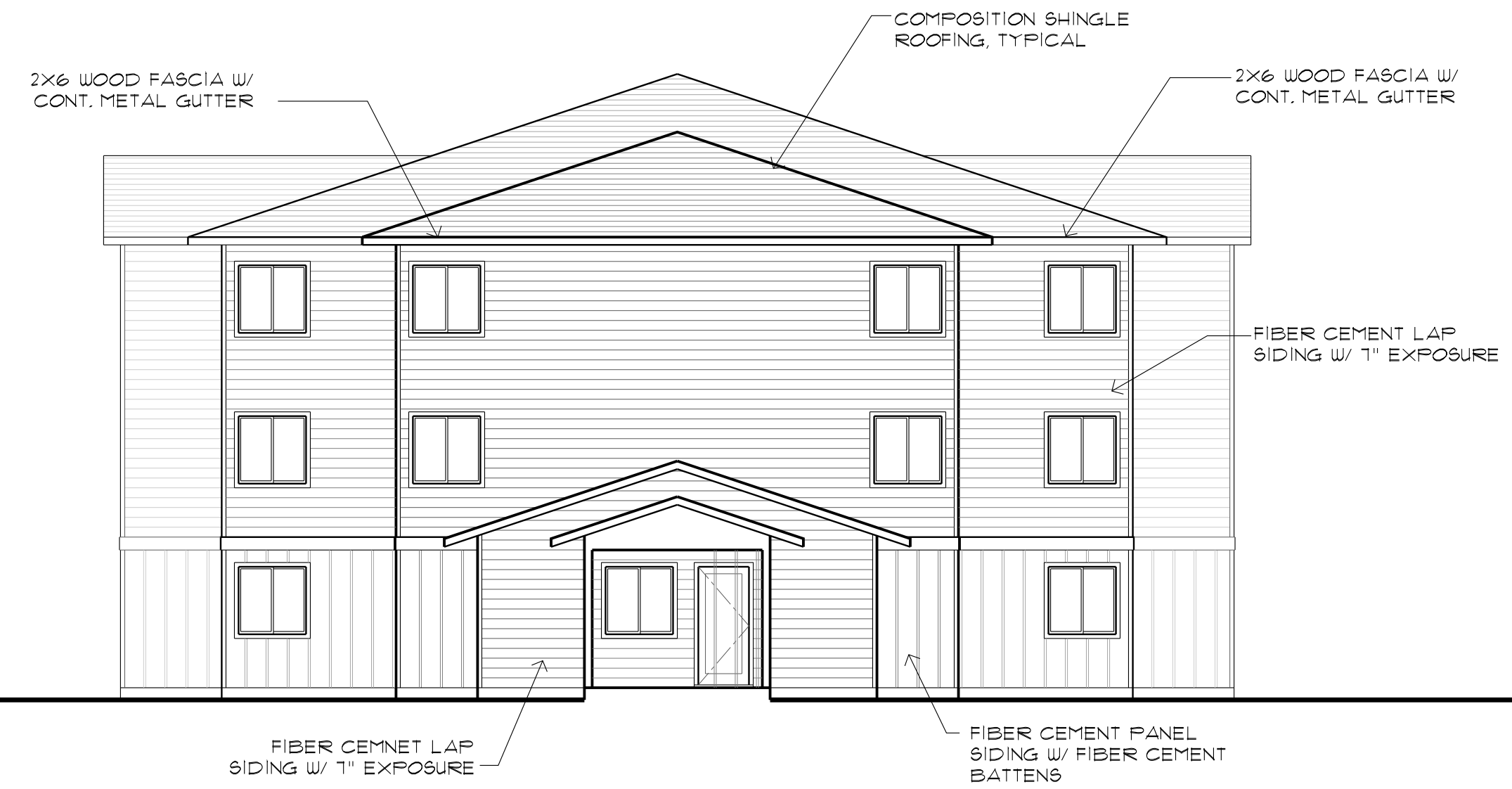
**A**  
**7.2**



**BUILDING 'C' - NORTH ELEVATION** 4  
 Scale: 1/8" = 1'-0" **A7.3**



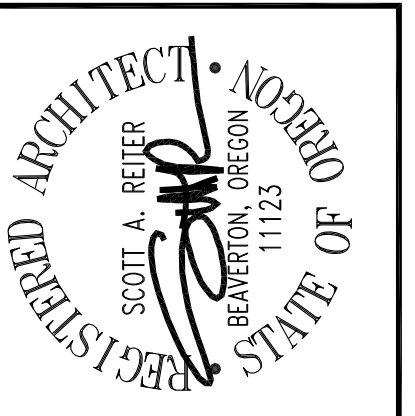
**BUILDING 'C' - WEST ELEVATION** 2  
 Scale: 1/8" = 1'-0" **A7.3**



**BUILDING 'C' - EAST ELEVATION** 3  
 Scale: 1/8" = 1'-0" **A7.3**



**BUILDING 'C' - SOUTH ELEVATION** 1  
 Scale: 1/8" = 1'-0" **A7.3**



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**NORTON LANDING APARTMENTS**  
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LAND-USE REVIEW

|                                   |                     |            |
|-----------------------------------|---------------------|------------|
| EXTERIOR ELEVATIONS<br>BUILDING C | date: JULY 19, 2023 | revisions: |
|                                   | scale: AS NOTED     |            |
|                                   | drawing: SAR        |            |
|                                   | job no: 2301        |            |

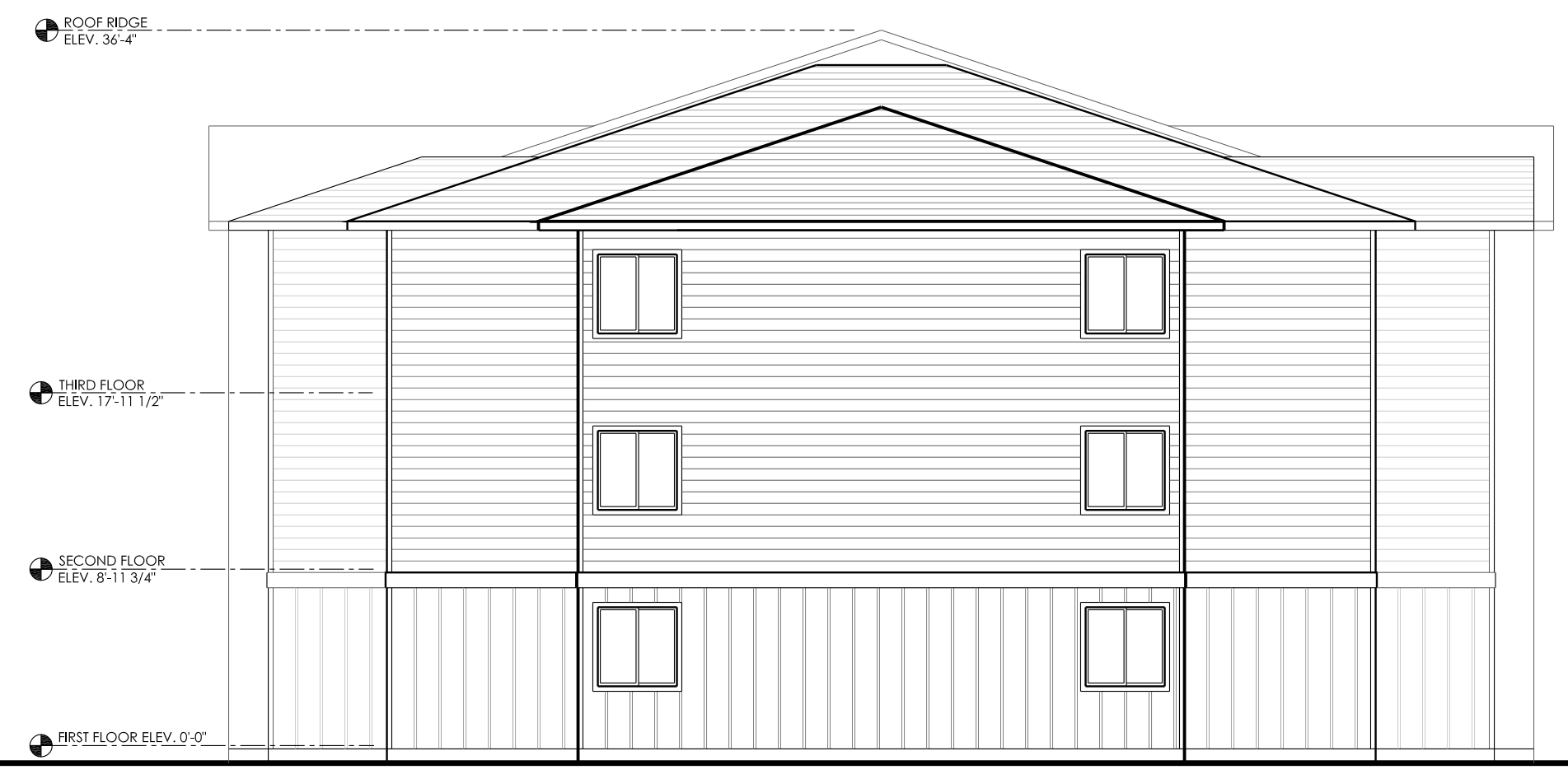
**A**  
**7.3**

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'D' - WEST ELEVATION 4  
Scale: 1/8" = 1'-0" A7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



ROOF RIDGE  
ELEV. 36'-4"  
THIRD FLOOR  
ELEV. 17'-11 1/2"  
SECOND FLOOR  
ELEV. 8'-11 3/4"  
FIRST FLOOR ELEV. 0'-0"

BUILDING 'D' - SOUTH ELEVATION 2  
Scale: 1/8" = 1'-0" A7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



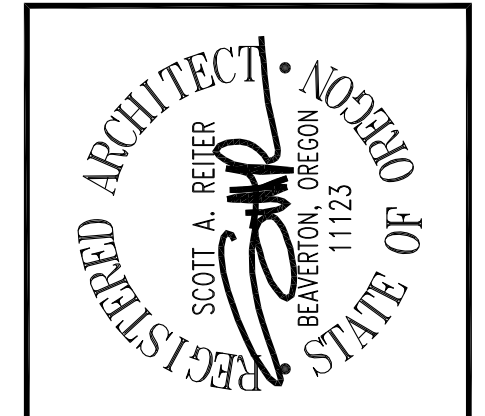
BUILDING 'D' - NORTH ELEVATION 3  
Scale: 1/8" = 1'-0" A7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



ROOF RIDGE  
ELEV. 36'-4"  
THIRD FLOOR  
ELEV. 17'-11 1/2"  
SECOND FLOOR  
ELEV. 8'-11 3/4"  
FIRST FLOOR ELEV. 0'-0"

BUILDING 'D' - EAST ELEVATION 1  
Scale: 1/8" = 1'-0" A7.4



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|                                   |          |               |
|-----------------------------------|----------|---------------|
| EXTERIOR ELEVATIONS<br>BUILDING D | date:    | JULY 19, 2023 |
|                                   | scale:   | AS NOTED      |
|                                   | drawn:   | SAR           |
|                                   | job no.: | 2301          |

LAND-USE REVIEW  
**A**  
**7.4**

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'E' - NORTH ELEVATION 4  
Scale: 1/8" = 1'-0" A7.5

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'E' - WEST ELEVATION 2  
Scale: 1/8" = 1'-0" A7.5

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'E' - EAST ELEVATION 3  
Scale: 1/8" = 1'-0" A7.5

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'E' - SOUTH ELEVATION 1  
Scale: 1/8" = 1'-0" A7.5



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LAND-USE REVIEW

EXTERIOR ELEVATIONS  
BUILDING E

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

**A**  
**7.5**

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - NORTH ELEVATION 4  
Scale: 1/8" = 1'-0" A7.6

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - WEST ELEVATION 2  
Scale: 1/8" = 1'-0" A7.6

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - EAST ELEVATION 3  
Scale: 1/8" = 1'-0" A7.6

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - SOUTH ELEVATION 1  
Scale: 1/8" = 1'-0" A7.6



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED

6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

EXTERIOR ELEVATIONS  
BUILDING F

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

**A**  
**7.6**

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'G' - WEST ELEVATION 4  
Scale: 1/8" = 1'-0" A7.7

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'G' - SOUTH ELEVATION 2  
Scale: 1/8" = 1'-0" A7.7

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS

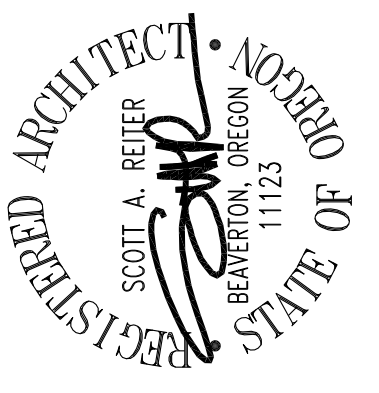


BUILDING 'G' - NORTH ELEVATION 3  
Scale: 1/8" = 1'-0" A7.7

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'G' - EAST ELEVATION 1  
Scale: 1/8" = 1'-0" A7.7



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

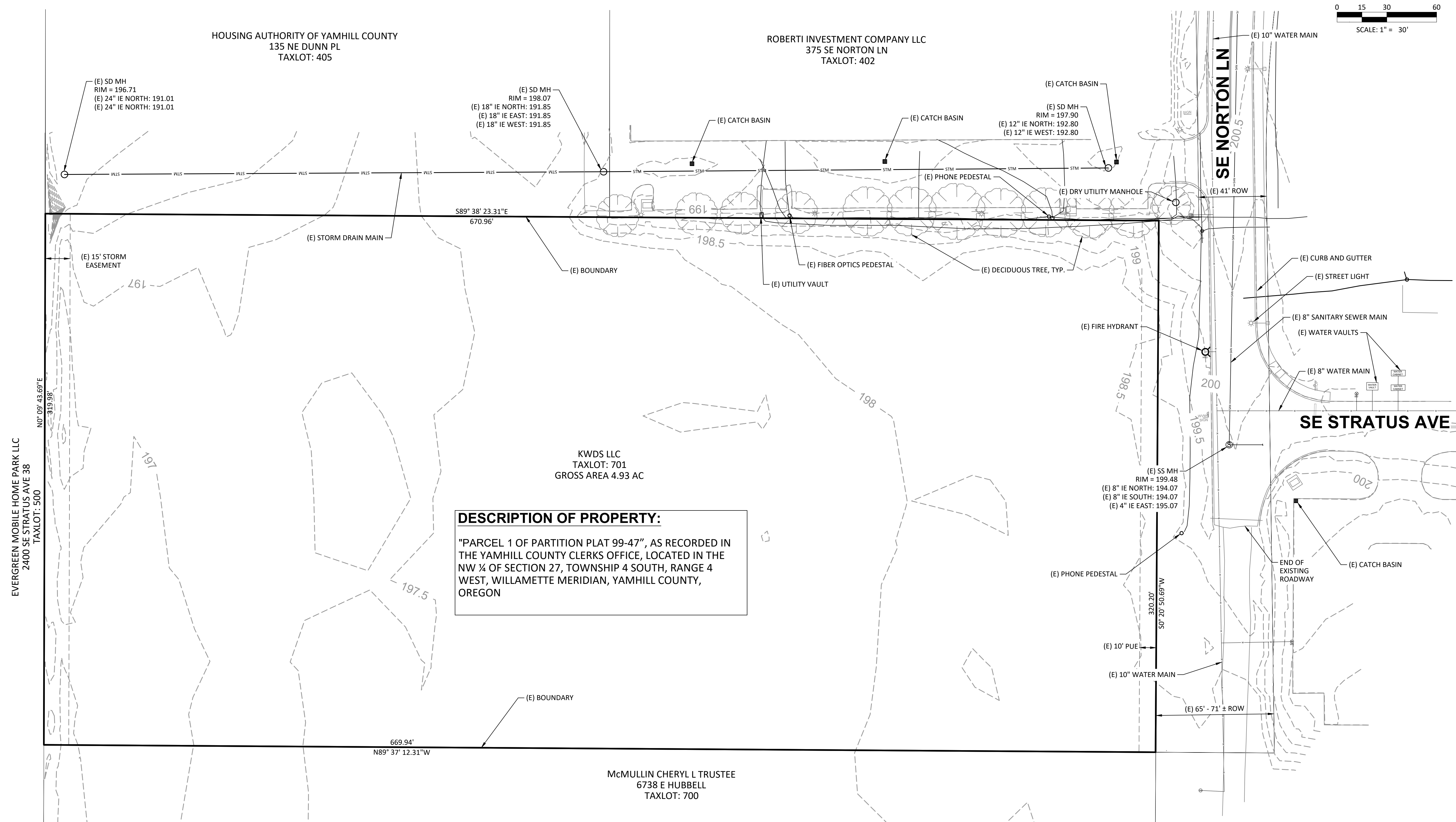
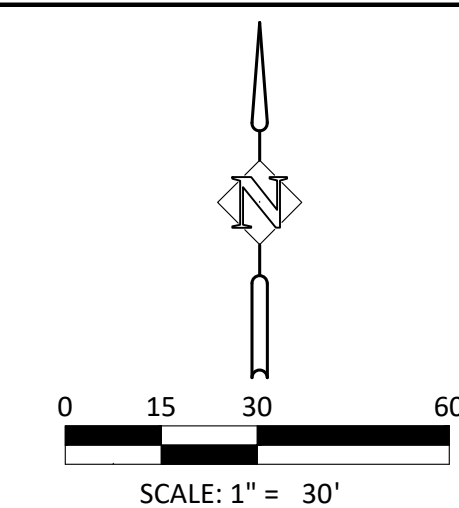
LAND-USE REVIEW

EXTERIOR ELEVATIONS  
BUILDING G

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

A  
7.7





**DESCRIPTION OF PROPERTY:**

"PARCEL 1 OF PARTITION PLAT 99-47", AS RECORDED IN THE YAMHILL COUNTY CLERKS OFFICE, LOCATED IN THE NW ¼ OF SECTION 27, TOWNSHIP 4 SOUTH, RANGE 4 WEST, WILLAMETTE MERIDIAN, YAMHILL COUNTY, OREGON

**EXISTING CONDITIONS**  
**NORTON LANDING**  
 McMINNVILLE, OREGON

**Harper Houf Peterson Righellis Inc.**  
 ENGINEERS + PLANNERS  
 LANDSCAPE ARCHITECTS + SURVEYORS  
 250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
 phone: 541.318.1161 www.hhpr.com fax: 541.318.1141

REGISTERED PROFESSIONAL ENGINEER  
 76,554  
**PRELIMINARY**  
 OREGON  
 DEC. 29, 2023  
 JENNIFER L. YANC AMP  
 EXPIRES: 6/30/2024

|           |            |
|-----------|------------|
| DESIGNED: | MD/JLV     |
| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 08.11.2023 |

| DATE | NO. | DESCRIPTION |
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SHEET NO.  
**C0.1**  
 JOB NO. KWD-11

**EXISTING CONDITIONS**  
 1" = 30"

**LAND USE**

**CONSTRUCTION NOTES:**

- ① PROPOSED 6" CONCRETE CURB.
- ② PROPOSED CONCRETE SIDEWALK WIDTH NOTED ON PLAN.
- ③ PROPOSED 26- FT WIDE CONCRETE DRIVEWAY APRON.
- ④ PROPOSED SAWCUT LINE
- ⑤ PROPOSED AC PAVING SECTION - SEE TYPICAL SECTION
- ⑥ PROPOSED CURB AND GUTTER.
- ⑦ PROPOSED SWALE. SEE CROSS SECTION.

**LEGEND**

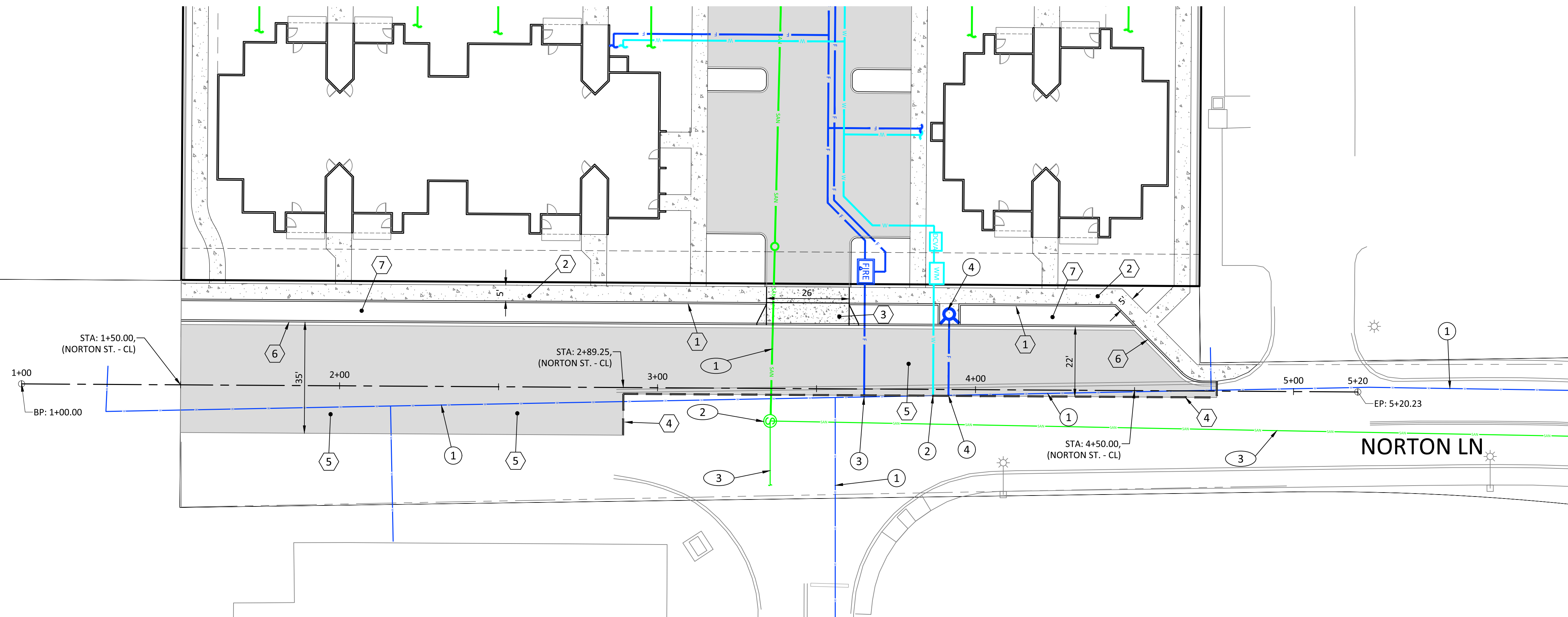
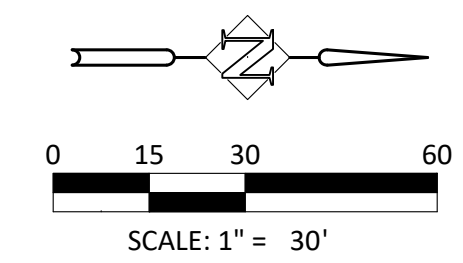
- ①
- ②
- ③
- ④
- ⑤
- ⑥
- ⑦

**WATER MAIN NOTES**

- ① PROTECT EXISTING 10" WATER MAIN.
- ② PROPOSED 4" DOMESTIC WATER SERVICE.
- ③ PROPOSED 6" FIRE SUPPRESSION SERVICE.
- ④ PROPOSED FIRE HYDRANT ASSEMBLY.

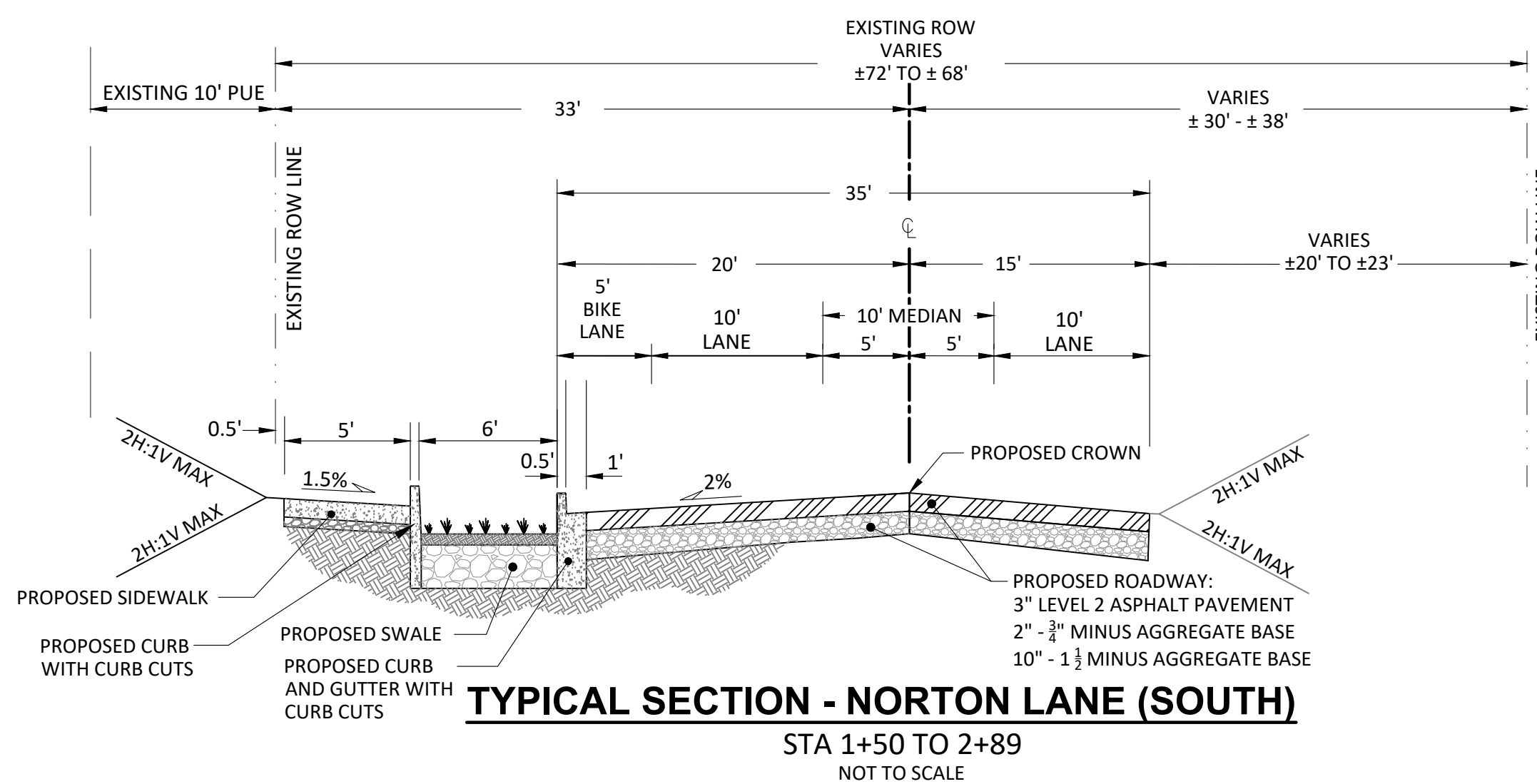
**SANITARY SEWER NOTES**

- ① PROPOSED SANITARY SEWER SERVICE LINE
- ② PROTECT EXISTING PUBLIC SEWER MANHOLE.
- ③ PROTECT EXISTING PUBLIC SEWER MAIN.



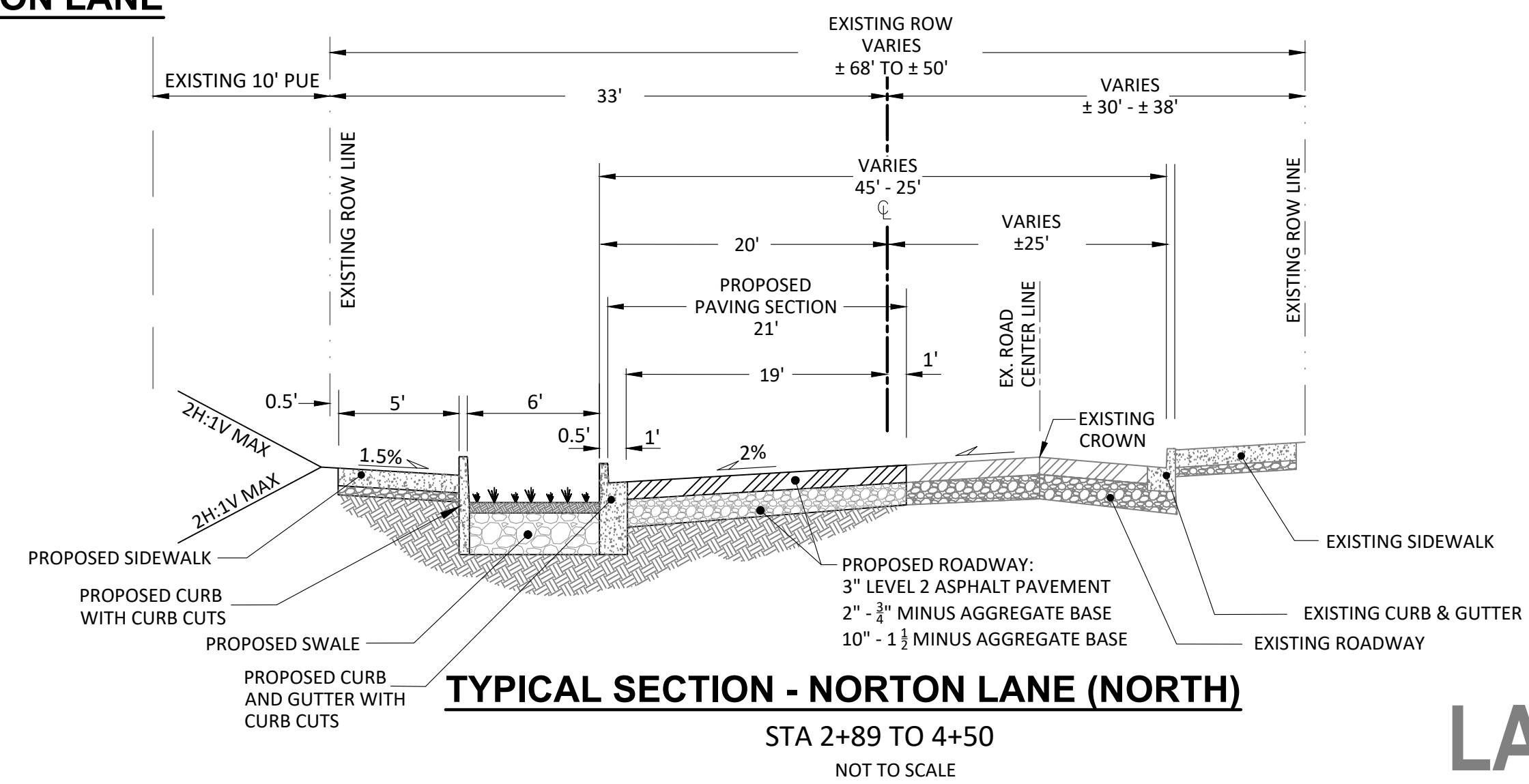
**PRELIMINARY PLAN - NORTON LANE**

1" = 20"



**TYPICAL SECTION - NORTON LANE (SOUTH)**

STA 1+50 TO 2+89  
NOT TO SCALE



**TYPICAL SECTION - NORTON LANE (NORTH)**

STA 2+89 TO 4+50  
NOT TO SCALE

**LAND USE**

**SE NORTON LN - PRELIMINARY PLAN**  
**NORTON LANDING**  
 McMinnville, Oregon

**Harper Houf Peterson Righellis Inc.**  
 ENGINEERS + PLANNERS  
 LANDSCAPE ARCHITECTS + SURVEYORS  
 250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
 phone: 541.318.1161 www.hhpr.com

**REGISTERED PROFESSIONAL ENGINEER**  
 PRELIMINARY  
 JENNIFER L. YANCY  
 DEC. 29, 2009  
 EXPIRES: 6/30/2024

|           |        |    |     |          |            |
|-----------|--------|----|-----|----------|------------|
| DESIGNED: | MD/JLV | MD | JLV | DATE:    | 08.11.2023 |
| DRAWN:    |        |    |     | CHECKED: |            |

| NO. | DATE | DESCRIPTION |
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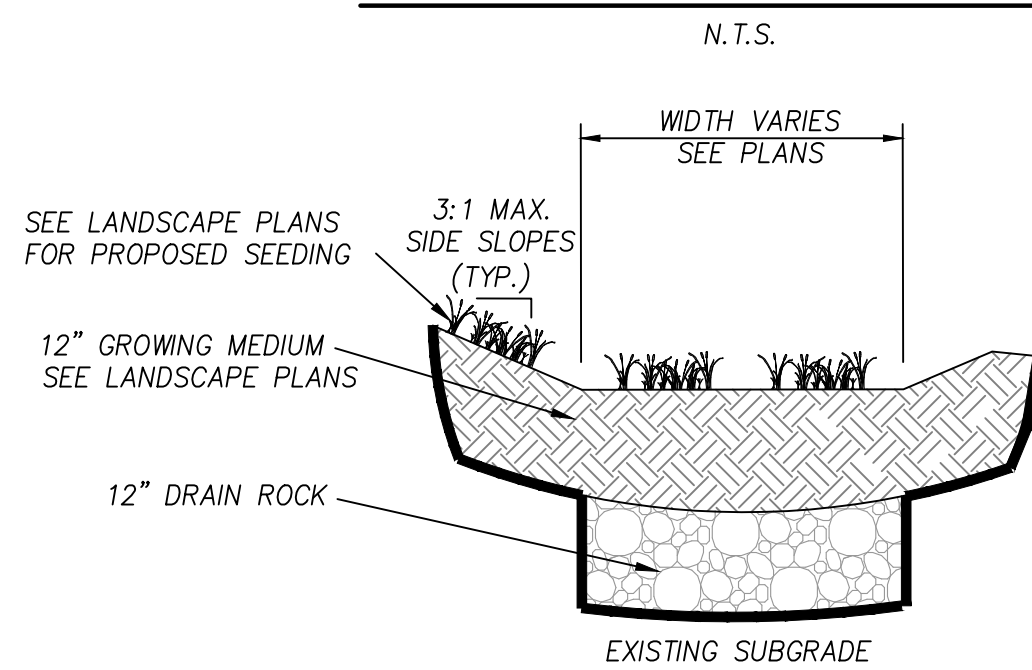
SHEET NO. **C1.0**  
 JOB NO. KWD-11



**PRELIMINARY GRADING & DRAINAGE NOTES:**

1. ALL PROPOSED ELEVATIONS/GRADES SHOWN ON THIS PLAN ARE PRELIMINARY AND SUBJECT TO CHANGE DURING FINAL DESIGN.
2. STORMWATER FACILITY LOCATIONS ARE PRELIMINARY AND SUBJECT TO CHANGE DURING FINAL DESIGN.
3. FINAL DRAINAGE FACILITIES DESIGN WILL BE IN ACCORDANCE WITH CITY OF McMINNVILLE STANDARDS AND OREGON LAW REQUIREMENTS.

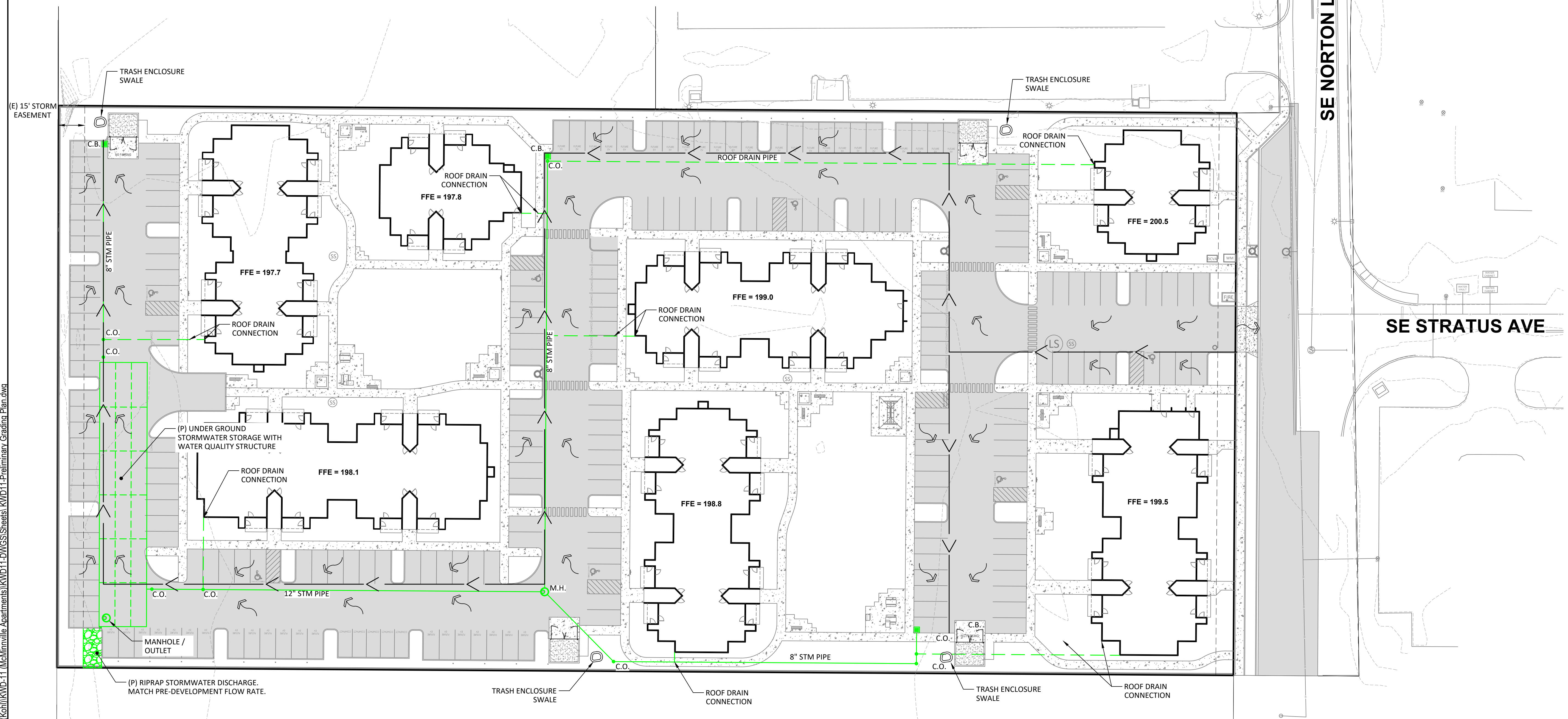
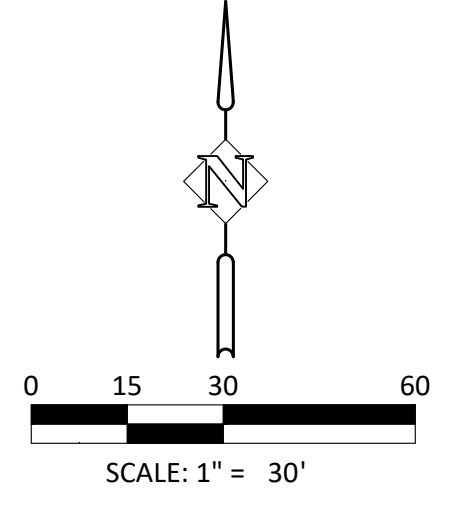
**TRASH ENCLOSURE SWALE**



1. PROVIDE PROTECTION FROM ALL VEHICLE TRAFFIC, EQUIPMENT STAGING, AND FOOT TRAFFIC IN PROPOSED INFILTRATION AREAS PRIOR TO, DURING, AND AFTER CONSTRUCTION.
2. DIMENSIONS:  
A. WIDTH OF BASIN VARIES, SEE PLANS.  
B. DEPTH OF BASIN VARIES, SEE TABLE THIS SHEET.
3. INSTALL 10' X 10' AREA OF WASHED PEA GRAVEL OR RIVER ROCK TO TRANSITION FROM PIPE OUTFALLS TO GROWING MEDIUM.

**LEGEND**

- 200 — (P) MAJOR CONTOUR
- 198 — (P) MINOR CONTOUR
- - - 200 - - - (E) MAJOR CONTOUR
- - - 198 - - - (E) MINOR CONTOUR
- (P) SLOPE ARROW
- V — (P) VALLEY GUTTER DRAINAGE
- - - (P) BUILDING / ROOF DRAIN CONNECTION



P:\03-Bend\KVD - (Koh)\KVD-11 (McMinnville Apartments)\KVD11-DWG\Sheets\KVD11-Preliminary Grading Plan.dwg

**PRELIMINARY GRADING & DRAINAGE PLAN**  
**NORTON LANDING**  
McMINNVILLE, OREGON

**Harper Houf Peterson Righelis Inc.**  
ENGINEERS & PLANNERS  
LANDSCAPE ARCHITECTS & SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhp.com

**REGISTERED PROFESSIONAL ENGINEER**  
76,554  
**PRELIMINARY**  
OREGON  
JANUARY L. YANCAMP  
DEC. 29, 2023  
EXPIRES: 6/30/2024

|           |            |
|-----------|------------|
| DESIGNED: | MD/JLV     |
| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 08.11.2023 |

SHEET NO. **C2.0**  
JOB NO. KWD-11

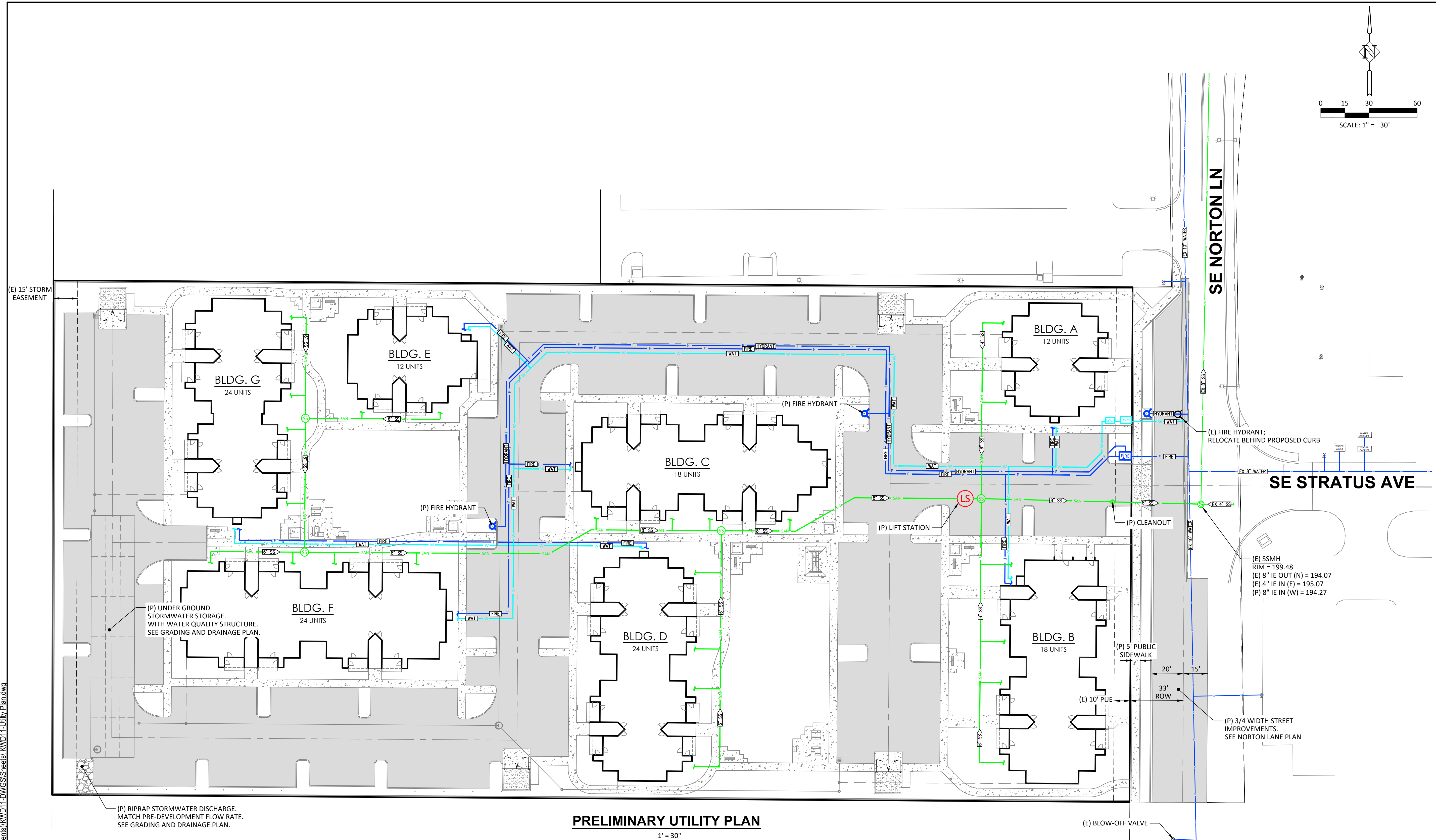
**PRELIMINARY GRADING & DRAINAGE**

1" = 30"

**LAND USE**



P:\03-Bend\KVD (Koh)\KVD-11 (McMinnville Apartments)\KVD11-DWG\Sheets\KVD11-Utility Plan.dwg



**PRELIMINARY UTILITY PLAN**  
1" = 30"

**PRELIMINARY UTILITY PLAN**  
**NORTON LANDING**  
McMINNVILLE, OREGON

**Harper Houf Peterson Righellis Inc.**  
ENGINEERS • PLANNERS  
LANDSCAPE ARCHITECTS • SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhp.com

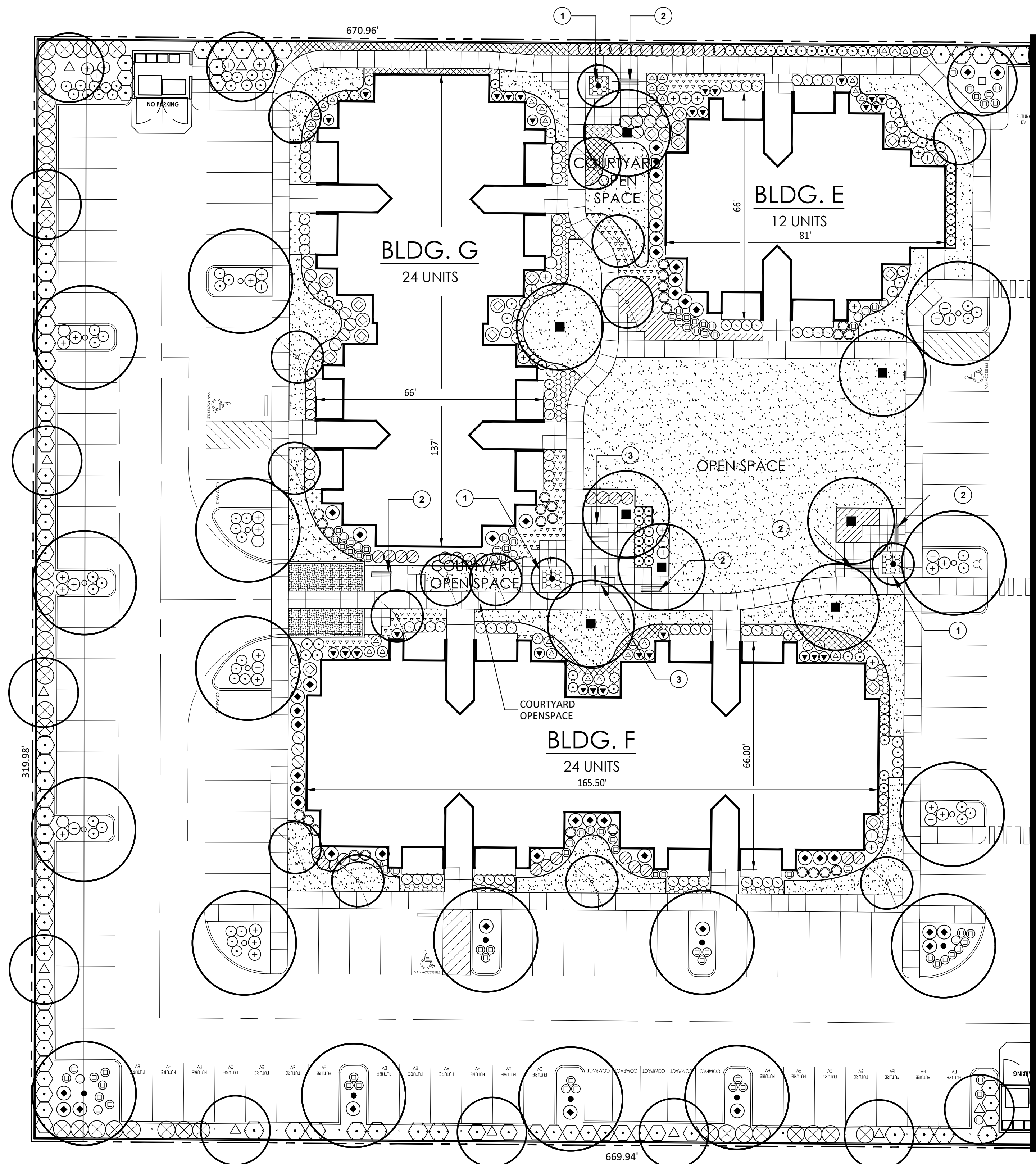
**REGISTERED PROFESSIONAL ENGINEER**  
PRELIMINARY  
JENNIFER L. VANCE  
EXPIRES: 6/30/2024

| DESIGNED: | MD/JLV     |             |
|-----------|------------|-------------|
| DRAWN:    | MD         |             |
| CHECKED:  | JLV        |             |
| DATE:     | 08.11.2023 |             |
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SHEET NO. **C3.0**  
JOB NO. KWD-11

**LAND USE**





LANDSCAPE PLAN  
1" = 20"

PLANT SCHEDULE

| TREES  |   |
|--------|---|
|        | 14 Zelkova serrata 'Green Vase' - Green Vase Zelkova<br>2" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 40' W                   |
|        | 16 Pyrus calleryana 'Aristocrat' - Callery Pear<br>2" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 35' H X 25' W                        |
|        | 4 Acer griseum - Paperbark Maple<br>2" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 25' H X 20' W                                       |
|        | 16 Acer rubrum 'Bowhall' - Bowhall Maple<br>2" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 25' H X 15' W                               |
|        | 7 Lagerstroemia Plum Magic - Plum Magic Crape Myrtle<br>12" HGT B&B, WELL BRANCHED, MULTI-TRUNKED<br>MATURE SIZE: 15' H X 15' W                 |
|        | 19 Carpinus betulus - European Hornbeam<br>X" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 35' W                                |
|        | 13 Ginkgo biloba 'Autumn Gold' - Autumn Gold Ginkgo<br>X" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 35' W                    |
|        | 26 Cornus 'Eddie's White Wonder' - Eddie's White Dogwood<br>X" CAL B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 35' H X 20' W               |
| SHRUBS |   |
|        | 222 Pennisetum orientale - Fountaingrass<br>X GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W                              |
|        | 102 Abelia x grandiflora 'Kaleidoscope' - Kaleidoscope Abelia<br>X GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 3.5' W     |
|        | 220 Helictotrichon sempervirens - Blue Oatgrass<br>X GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 2.5' W                   |
|        | 67 Gardenia jasminoides 'Frostproof' - Frostproof Gardenia<br>X GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 4' H X 3' W            |
|        | 56 Hydrangea macrophylla 'Mini Penny' - Mini Penny Hydrangea<br>X GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 3.5' W      |
|        | 66 Deutzia gracilis 'Nikko' - Dwarf Nikko Deutzia<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2' H X 3' W                     |
|        | 74 Berberis thunbergii 'Monomb' - Cherry Bomb Japanese Barberry<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 4' W       |
|        | 123 Escallonia hybrid 'Compakta' - Compact Escallonia<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 4' W               |
|        | 104 Spiraea x bumalda 'Goldflame' - Goldflame Spirea<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3.5' H X 3.5' W              |
|        | 85 Thuja occidentalis 'Golden Globe' - Golden Globe Arborvitae<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 4' H X 4' W        |
|        | 68 Choisya ternata 'Sundance' - Sundance Orange Blossom<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W               |
|        | 61 Abelia gradiflora - Glossy Abelia<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W                                  |
|        | 61 Viburnum tinus 'Spring Bouquet' - Laurustinus<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W (PRUNNED)            |
|        | 118 Nandina domestica 'Gulf Stream' - Gulf Stream Nandina<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W             |
|        | 185 Prunus laurocerasus 'Otto Luyken' - Dwarf Cherry Laurel<br>5 GAL CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W (PRUNNED) |

GROUND COVER

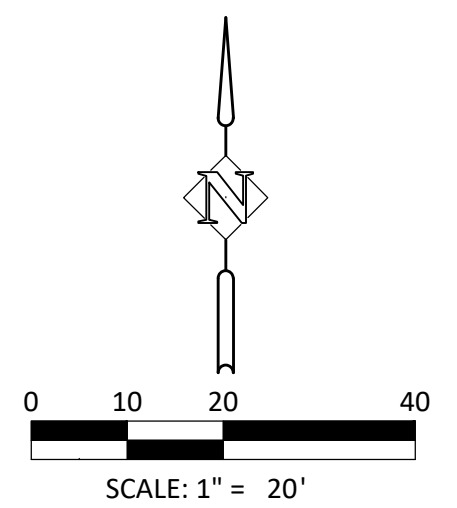
|  |   |
|--|---|
|  | 1 GAL Festuca glauca 'Elijah Blue' - Blue Fescue<br>CONT., FULL PLANTS, 12" O.C.                            |
|  | 1 GAL Liriope muscari 'Big Blue' - Big Blue Lillyturf<br>CONT., FULL PLANTS, 18" O.C.                       |
|  | 1 GAL Pennisetum alopecuroides 'Little Bunny' - Little Bunny Fountain Grass<br>CONT., FULL PLANTS, 12" O.C. |
|  | 1 GAL Carex elata 'Aurea' - Bowles Golden Sedge<br>CONT., FULL PLANTS, 30" O.C.                             |
|  | 1 GAL Carex oshimensis 'Carfit01' - Everest Variegated Sedge<br>CONT., FULL PLANTS, 24" O.C.                |

SEED MIXES

| SEED MIX 1 (LAWN AREAS)         | % PLS | LBS OF PLA/ 1000 SF |
|---------------------------------|-------|---------------------|
| Festuca rubra 'Gibraltar'       | 10    | 0.364               |
| Festuca rubra 'Silhouette'      | 10    | 0.364               |
| Lolium perenne 'Delaware Dwarf' | 40    | 5.563               |
| Lolium perenne 'Amazing'        | 40    | 2.696               |
| <b>TOTAL</b>                    |       | <b>8.987</b>        |

GENERAL PLANTING NOTES

- ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT CITY OF McMinnville STANDARDS AND OREGON BUILDING AND SPECIALITY CODES.
- INSTALL EROSION CONTROL SYSTEMS IN ACCORDANCE WITH CITY OF McMinnville STANDARDS PRIOR TO SITE WORK AND LANDSCAPE INSTALLATION.
- CONTRACTOR SHALL MARK AND PROTECT ALL UTILITIES, SITE FEATURES, AND VEGETATION TO REMAIN IN PLACE.
- CONTRACTOR SHALL REMOVE ALL WEEDS AND INVASIVE SPECIES PRIOR TO PLANTING OR SEEDING.
- ALL DISTURBED AREAS SHALL BE SEEDDED.
- PRIOR TO PLANTING, CONTRACTOR SHALL TEST ON-SITE SOILS FOR SOIL FERTILITY BY CERTIFIED TESTING LAB. IF NECESSARY, BACKFILL SOILS FOR TREE PITS, SHRUB AND GROUND COVER AREAS SHALL BE AMENDED AS RECOMMENDED BY SOIL ANALYSIS REPORT.
- ALL SEEDED AREAS SHALL BE STRIPPED OF VEGETATION, SCARIFIED AND RECEIVE 6" OF TOPSOIL PRIOR TO APPLICATION OF SEED.
- ALL PLANTER BEDS SHALL BE SCARIFIED 12" BELOW FINISHED GRADE AND HAVE 12" OF TOPSOIL ADDED TO BRING BACK TO FINISHED GRADE PRIOR TO PLANTING.
- CONTRACTOR TO INSTALL 3" LAYER OF BARK MULCH AT ALL TREE, SHRUB AND GROUND COVER AREAS.
- LANDSCAPE INSTALLATION SHALL INCLUDE PROVISION OF AN AUTOMATIC IRRIGATION SYSTEM TO SUSTAIN LANDSCAPE PLANTINGS, MEETING LOCAL AND STATE BUILDING CODES.
- PLANT MATERIAL INSTALLED SHALL CONFORM IN SIZE AND GRADE TO THE "AMERICAN STANDARD FOR NURSERY STOCK" CURRENT EDITION.
- QUANTITIES OF PLANT MATERIALS SHALL BE AS DETERMINED BY CONTRACTOR IN ACCORDANCE WITH SPECIFIED SPACING OR LOCATION ON PLAN. MATERIAL QUANTITIES SHOWN ON PLAN ARE FOR CONTRACTOR CONVENIENCE ONLY AND SHALL BE VERIFIED BY CONTRACTOR PRIOR TO INSTALLATION. SURPLUS OR SHORTAGES OF PLANT QUANTITIES SHALL BE RESPONSIBILITY OF CONTRACTOR.
- LANDSCAPE CONTRACTOR SHALL WATER PLANTINGS FOR DURATION OF 1-YEAR WARRANTY PERIOD AFTER INSTALLATION AND GUARANTEE ALL PLANTINGS TO BE IN SATISFACTORY HEALTH. LANDSCAPE CONTRACTOR SHALL REPLACE ALL DAMAGED, DEAD, OR DYING PLANTS COVERED BY WARRANTY WITHIN 30 DAYS OF INITIAL IDENTIFICATION OF CONDITION.



KEY NOTES

- RAISED SEAT WALL PLANTER
- BENCH
- PICNIC TABLE

LANDSCAPE PLAN  
NORTON LANDING  
McMINNVILLE, OREGON

**Harper Houf Peterson Righeliss Inc.**  
 ENGINEERS + PLANNERS  
 LANDSCAPE ARCHITECTS + SURVEYORS  
 250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
 phone: 541.318.1161 www.hhp.com

REGISTERED 747  
**PRELIMINARY**  
 JEFFERY F. CREEL  
 OREGON  
 05/13/11  
 LANDSCAPE ARCHITECT

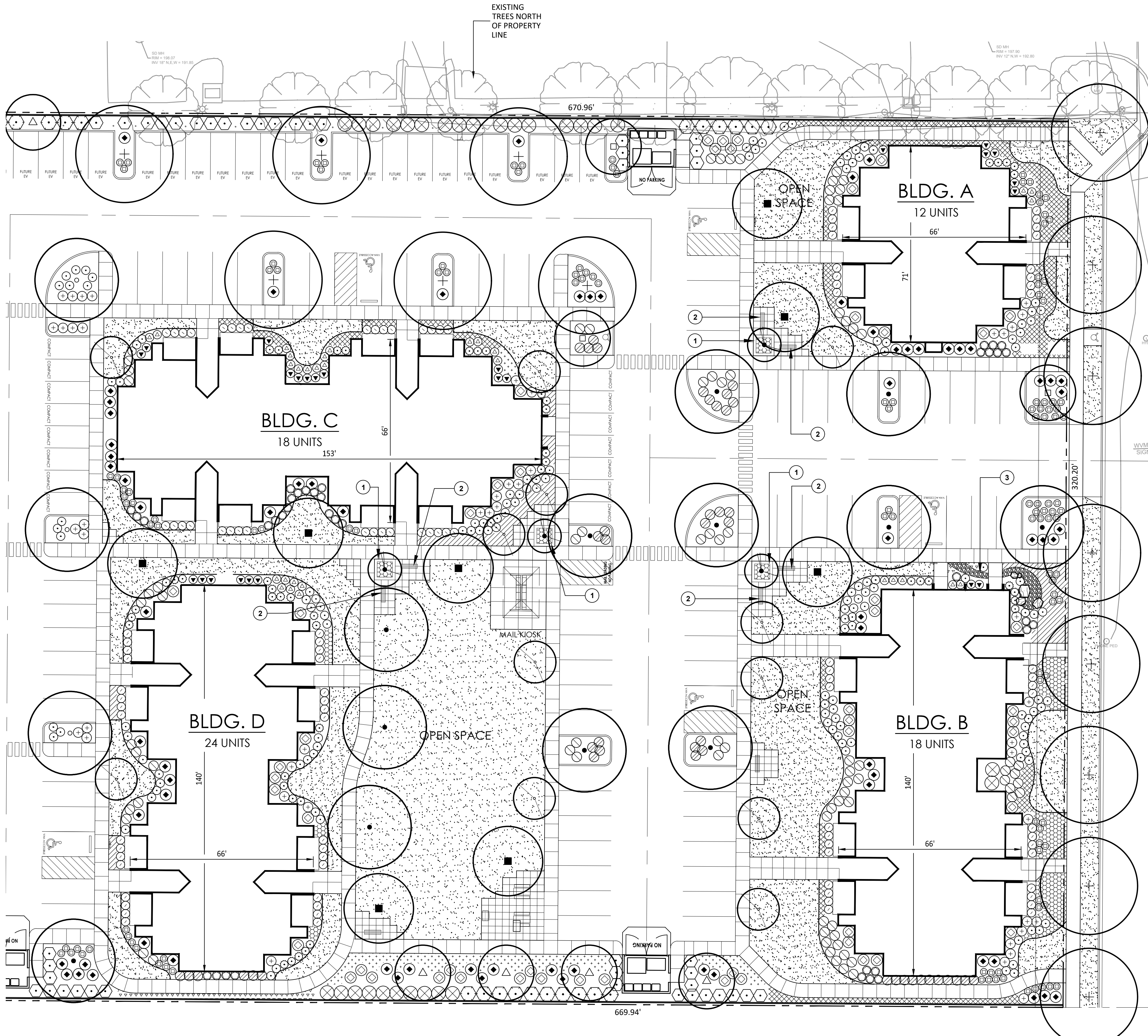
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| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 07.21.2023 |

| DATE | NO. | DESCRIPTION | R | E | V | I | S | I | O | N | S |
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SHEET NO. **L1.1**  
JOB NO. KWD-11



N:\KWD\KWD-11 (McMinnville Apartments)\KWD-11-DWGS\Sheets\L1.1 - Landscape Plans.dwg

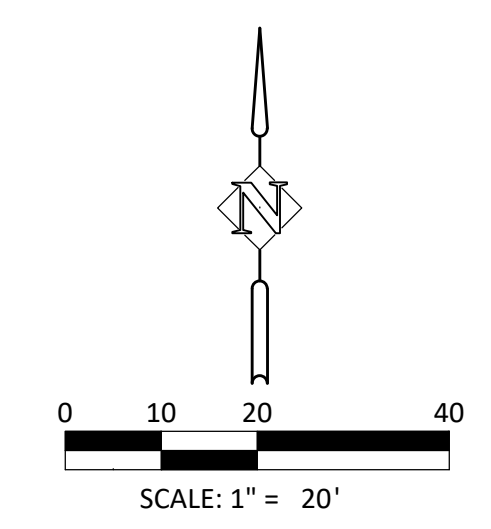


SE NORTON LANE

| CITY OF MCMINNVILLE CODE COMPLIANCE |                              |   |   |   |
|-------------------------------------|------------------------------|---|---|---|
| SECTION #                           | SECTION                      | DESCRIPTION   | REQUIRED  | PROVIDED  |
| 17.11.090(D)                        | LARGE SITE DESIGN STANDARDS  | BUFFER PARKING AREAS WITH LANDSCAPING.  |   | LANDSCAPING AROUND PARKING LOT                    |
|                                     | LARGE SITE DESIGN STANDARDS  | PROVIDE DIFFERENT TYPE OF OPEN SPACE THROUGHOUT THE SITE  |   | DIFFERENT TYPES OF OPEN SPACE PROVIDED            |
|                                     | PARKING LOT DESIGN STANDARDS | >50,000 SF PARKING LOTS = 8% INTERIOR LANDSCAPING REQUIRED.   | PARKING LOT AREA = 75,075 SF X 8% = 6,006 SF        | 6,681 SF INT. PL LANDSCAPING                      |
|                                     | PARKING LOT DESIGN STANDARDS | 1 (NON-COLUMNAR) TREE PER ISLAND  | 37 PARKING LOT ISLANDS                              | 37 TREES  |
|                                     | PARKING LOT DESIGN STANDARDS | FENCE REQUIRED NEXT TO RESIDENTIAL ZONING   | FENCE ON WEST PL                                    | FENCE PROVIDED. SEE ARCHITECTURAL SITE PLANS      |
|                                     | PARKING LOT DESIGN STANDARDS | BUFFER BETWEEN PL AND BUILDINGS   | SW OR LANDSCAPE STRIP                               | 5' SW & LANDSCAPE STRIPS PROVIDED                 |
|                                     | PARKING LOT DESIGN STANDARDS | PLANTINGS ADJACENT TO SW (BETWEEN PL AND BLDGS)   | 6' LANDSCAPE STRIP NEXT TO SW                       | 6' PLANTING STRIP                                 |
|                                     | PARKING LOT DESIGN STANDARDS | TREES ALONG THE THROUGH SIDEWALK-WALKWAY CONNECTIONS  |   | TREES PROVIDED ALONG SIDEWALK CONNECTIONS         |
|                                     | OPEN SPACE                   | PROVIDE DECIDUOUS TREE FOR SHADE  | DEC. TREES IN OPEN SPACES                           | STEWARTIA PROVIDED                                |
|                                     | LANDSCAPING                  | A MIN. OF 20% OF THE SITE SHALL BE LANDSCAPED   | TOTAL SITE 214,606 SF X 20% = 42,921 SF             | 64,157 SF PROVIDED (30%)                          |
| 17.11.090(D). 10                    | FRONT YARD SETBACK           | FRONT YARD SETBACK BETWEEN ENTRANCE AND STREET GATEWAY ZONE   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
| 17.11.090(D). 6.B(3)                | PRIVATE OPEN SPACE           | PRIVATE OUTDOOR SPACE AT GROUND LEVEL REQUIRES FRONT YARD   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
|                                     | OPEN SPACE                   | COMMON OPEN SPACE 15% MIN.  | TOTAL SITE 214,606 SF X 15% = 32,191 SF             | 32,715 SF PROVIDED                                |
|                                     | OPEN SPACE                   | PASSIVE OPEN SPACE NO MORE THAN 5% OF SITE  | TOTAL SITE 214,606 SF X 5% = 10,730 SF MAX. PASSIVE | 21,985 SF ACTIVE OPEN SPACE                       |
| 17.57.070                           | EXISTING TREES               | EXISTING LOCATIONS OF TREES OVER 6" DBH   |   | NO EXISTING TREES ON SITE                         |
| 17.57.070                           | LANDSCAPE SCREENING          | SCREENING TO PROPOSED USE BY SIGHT OBSCURING, EVERGREEN PLANTINGS, SHADE TREES, FENCES OR COMBINATION |   | SIGHT OBSCURING FENCE & SCREENING SHRUBS PROVIDED |
| 17.57.090                           | STREET TREES                 | STREET TREES OVER 40' TALL AND WIDE   | 40' OC  | LARGE STREET TREES PROVIDED. SEE PLANT SCHEDULE   |

**KEY NOTES**

- ① RAISED SEAT WALL PLANTER
- ② BENCH



**LANDSCAPE PLAN  
NORTON LANDING  
MCMINNVILLE, OREGON**

**Harper Houf Peterson  
Righellis Inc.**  
ENGINEERS & PLANNERS  
 LANDSCAPE ARCHITECTS & SURVEYORS  
 250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
 phone: 541.318.1161 www.hhp.com

REGISTERED 747  
**PRELIMINARY**  
 JEFFERY P. CREEL  
 OREGON  
 06/13/11  
 LANDSCAPE ARCHITECT

DESIGNED: MD/JLV  
 DRAWN: MD  
 CHECKED: JLV  
 DATE: 07.21.2023

| DATE | NO. | DESCRIPTION |
|------|-----|-------------|
|      |     |             |
|      |     |             |
|      |     |             |

SHEET NO. **L1.2**  
 JOB NO. KWD-11

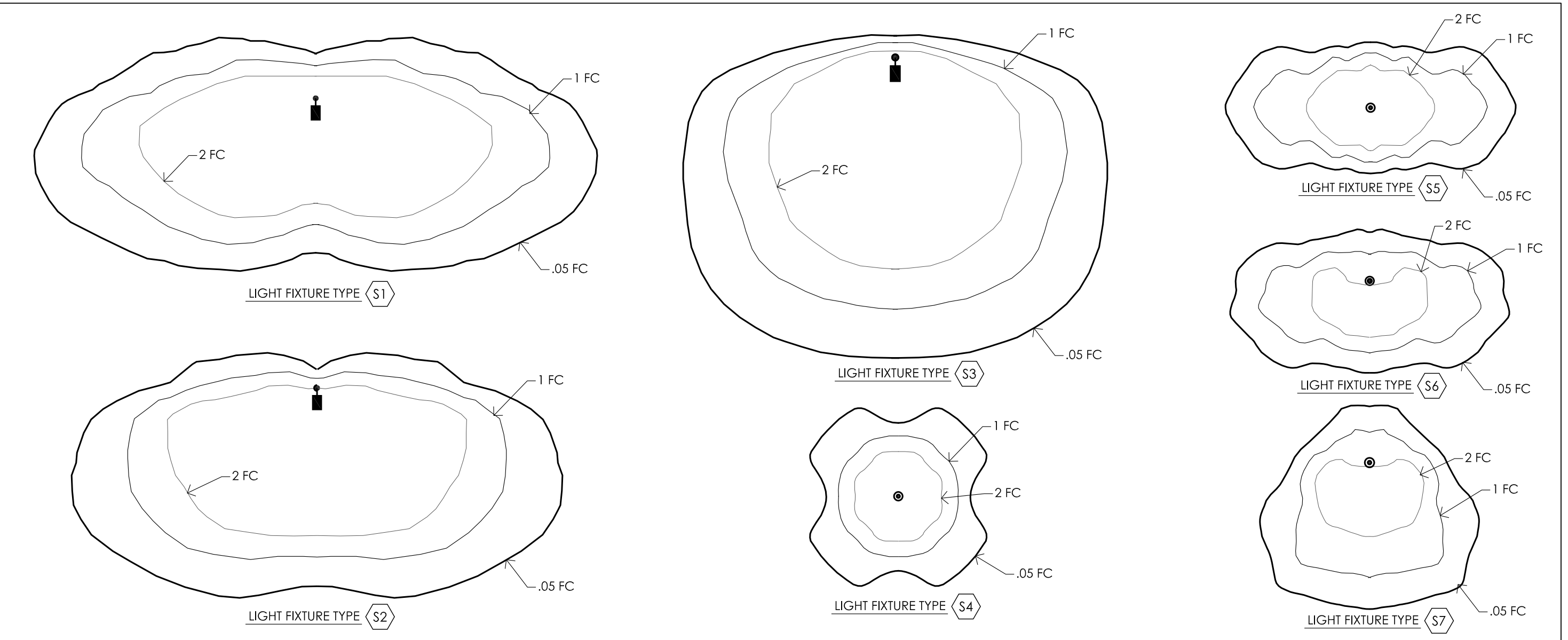
SEE L1.1 FOR PLANTING SCHEDULE

**LAND USE**

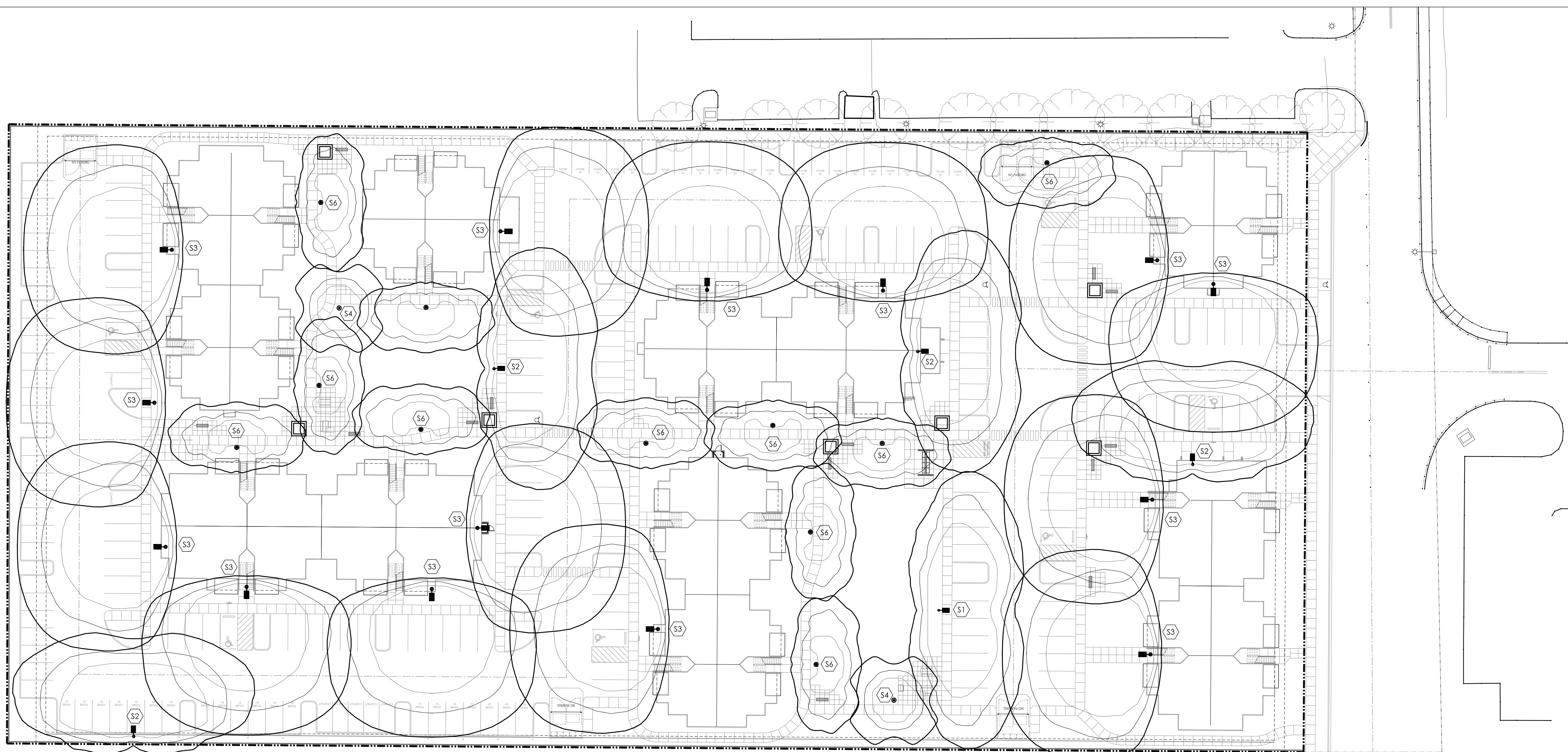


**SITE LIGHT FIXTURE SCHEDULE**

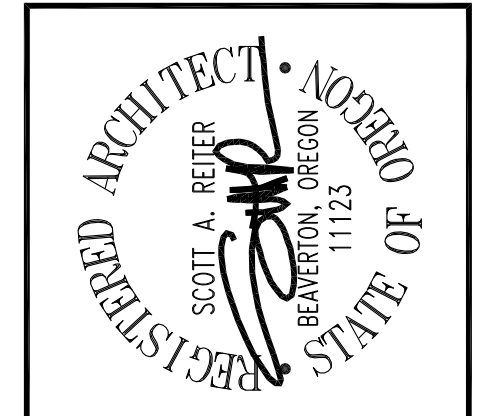
| ITEM | DESCRIPTION                   | MFR. & NUMBER                                 | LAMP | REMARKS  |
|------|-------------------------------|---|------|--|
| S1   | PARKING AREA LIGHT FIXTURE    | RAB ALED-105 W TYPE II LIGHT PATTERN          | LED  | POLE MOUNTED AT 20'-0"                             |
| S2   | PARKING AREA LIGHT FIXTURE    | RAB ALED-105 W TYPE III LIGHT PATTERN         | LED  | POLE MOUNTED AT 20'-0", OR BUILDING MTD. AT 20'-0" |
| S3   | PARKING AREA LIGHT FIXTURE    | RAB ALED-105 W TYPE IV LIGHT PATTERN          | LED  | POLE MOUNTED AT 20'-0", OR BUILDING MTD. AT 20'-0" |
| S4   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE I LIGHT PATTERN   | LED  | POLE MOUNTED AT 12'-0"                             |
| S5   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE II LIGHT PATTERN  | LED  | POLE MOUNTED AT 12'-0"                             |
| S6   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE III LIGHT PATTERN | LED  | POLE MOUNTED AT 12'-0"                             |
| S7   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE IV LIGHT PATTERN  | LED  | POLE MOUNTED AT 12'-0"                             |



**SITE LIGHT FIXTURE LEGEND 2**  
Scale: NOT TO SCALE ME1.0



**SITE LIGHTING PLAN 1**  
Scale: 1" = 30'-0" ME1.0



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

OVERALL SITE LIGHTING PLAN

|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | 1"=30'        |
| drawn:   | SAR           |
| job no.: | 2301          |

**ME 1.0**



Planning Department
231 NE Fifth Street o McMinnville, OR 97128
(503) 434-7311 Office o (503) 474-4955 Fax
www.mcminnvilleoregon.gov

Office Use Only:
File No. TML 4-23
Date Received 8/14/23
Fee \$2,087.00
Receipt No. 208597
Received by AW

569-23-000458-PLNG

Three Mile Lane Development Review

Applicant Information

Applicant is: [ ] Property Owner [ ] Contract Buyer [ ] Option Holder [ ] Agent [x] Other Architect/Planner

Applicant Name Reiter Design Architect Incorporated Phone (503) 574-3036
Contact Name Scott Reiter Phone (503) 789-6461
Address 6107 SW Murray Blvd., #480
City, State, Zip Beaverton, OR, 97008
Contact Email ScottRDG@aol.com

Property Owner Information

Property Owner Name KWDS, LLC Phone (503) 781-5685
Contact Name Chad Juranek Phone
Address PO Box 145
City, State, Zip Wilsonville, OR, 97070
Contact Email Cjuranek@jkmanage.com

Site Location and Description

(If metes and bounds description, indicate on separate sheet)

Property Address SE Norton Lane at SE Stratus Ave.
Assessor Map No. R4 4 - 4 - 27 Total Site Area 214,759 sf 4.93 acres
Subdivision Block Lot 701
Comprehensive Plan Designation Commercial Zoning Designation C-3 General Commercial



Describe the project in detail and how it will be consistent with the applicable requirements of the Three Mile Lane Planned Development Overlay ordinances (Nos. 4131 and 4572 and associated design standards). Please note the architectural features and materials to be used. (*Attach additional pages if necessary*).

See Attached Narrative

In addition to this completed application, the applicant must provide two (2) copies of the following:

- For new construction or for structural modifications, a site plan (drawn to scale, with a north arrow, legible, and of a reproducible size), indicating the existing site conditions including topography, structures, utilities, vegetation, and access.
- Relevant building and construction drawings, including building elevations of all sides visible from a public street, and proposed signage and landscaping.
- A narrative describing the architectural features that will be used in the building's design, including materials and colors.
- Other information deemed necessary to show consistency with the requirements of the Three Mile Lane planned development ordinances and/or required by the Planning Director.

**I certify that statements contained herein, along with the evidence submitted, are in all respects true and are correct to the best of my knowledge and belief.**

  
Applicant's Signature

8/10/23  
Date

  
Property Owner's Signature

Manager KWDS, LLC  
8-11-23  
Date

NORTON LANDING  
APARTMENTS  
McMinnville, Oregon

**LAND-USE APPLICATION**

**THREE MILE LANE DESIGN REVIEW**

AUGUST 14, 2023

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070

**RDA**  

---

REITER DESIGN ARCHITECT  
INCORPORATED

# NORTON LANDING APARTMENTS

## THREE MILE LANE DESIGN REVIEW

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**LAND-USE APPLICATION**  
Three Mile Lane Design Review

**1**



# Three Mile Lane Development Review Information & Submittal Requirements



Planning Department  
231 NE Fifth Street • McMinnville, OR 97128  
(503) 434-7311 Office • (503) 474-4955 Fax  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

## Overview

The area known as Three Mile Lane includes lands to the north and south of Oregon Highway 18, and from the eastern City limits west to the vicinity of the South Yamhill River bridge and Three Mile Lane Spur intersection with Highway 18. The Three Mile Lane Design Review Committee was formed to provide for the protection and enhancement of one of McMinnville's gateways through a review of any proposed development within the area. The review process by the Three Mile Lane Design Review Committee ensures that development, landscaping, and signage along Three Mile Lane is aesthetically pleasing and representative of the City of McMinnville as a whole.

For more information regarding development within the Three Mile Lane area, please refer to [Planned Development Ordinance Nos. 4131](#) and [4572](#).

## Submittal Requirement

The following materials must be provided at the time of submittal, or the application will not be accepted for processing.

- A completed Three Mile Lane Development Review application form.
- For new construction or structural modifications, two copies of the following:
  - A site plan (drawn to scale, with a north arrow, legible, and of a reproducible size), indicating the existing site conditions including topography, structures, utilities, vegetation, and access.
  - Relevant building and construction drawings.
  - Building elevations of all sides visible from a public street.
  - Proposed signage and landscaping.
- A narrative describing the architectural features that will be used in the building's design, including materials and colors.
- Other information deemed necessary to show consistency with the requirements of the Three Mile Lane planned development ordinances and/or as required by the Planning Director.

## Review Process

An application for development within the Three Mile Lane area shall be reviewed by the Three Mile Lane Design Review Committee as stated in Section 17.72.110 (Director's Review with Notification) of the Zoning Ordinance after notification of the application has been provided to property owners within 100 feet of the subject site.

The decision of the Three Mile Lane Design Review Committee may be appealed to the Planning Commission as outlined in Section 17.72.170 (Appeal from Ruling of Planning Director) of the Zoning Ordinance.

# **PROJECT DESIGN TEAM**

# **2**

# NORTON LANDING APARTMENTS

## PROJECT DESIGN TEAM

### **OWNER**

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070  
(503) 781-5685  
Owner's Representative: Chad Juranek

### **ARCHITECT / PLANNER**

REITER DESIGN ARCHITECT, INCORPORATED  
6107 SW Murray Blvd., #480  
Beaverton, Oregon 97008  
(503) 574-3036  
Project Architect, Project Planner: Scott A. Reiter

### **CIVIL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
250 NW Franklin Avenue, Suite 404  
Bend, Oregon 97703  
(541) 318-1161  
Project Civil Engineer: Jennifer VanCamp

### **LANDSCAPE ARCHITECT**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Landscape Architect: Jeffery Creel

### **STRUCTURAL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Engineer: Jeff Schwindt

### **TRANSPORATION ENGINEER**

LANCASTER-MOBLEY  
321 SW 4<sup>th</sup> Ave., Suite 400  
Portland, Oregon 97204  
(503) 248-0313  
Project Engineer: Daniel Stumpf

**TAX MAP  
LEGAL DESCRIPTION  
VICINITY MAP**

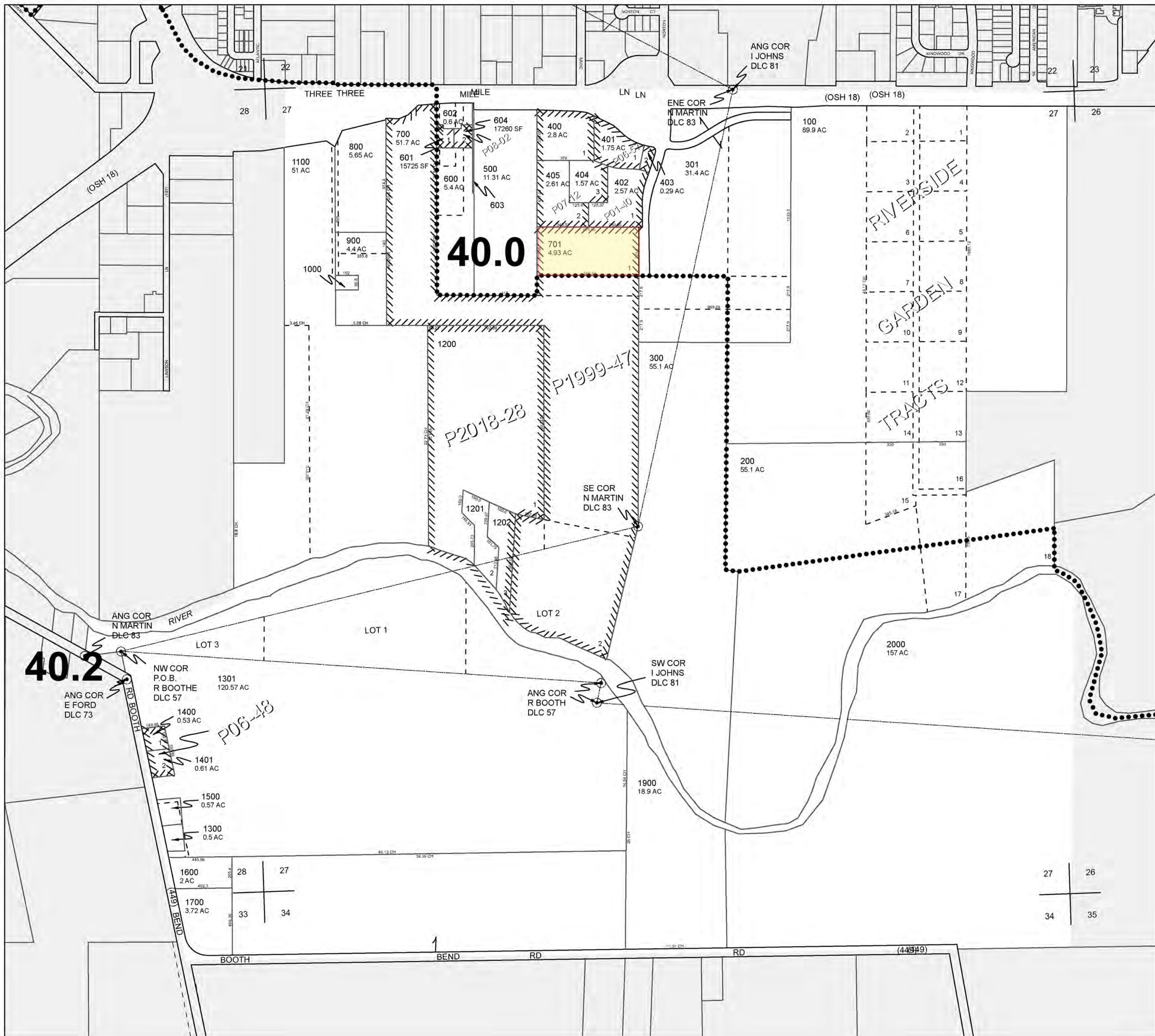
**3**





ASSESSMENT & TAX  
CARTOGRAPHY

SECTION 27 T.4S. R.4W. W.M.  
YAMHILL COUNTY OREGON  
1" = 400'



CANCELLED TAXLOTS:  
1800  
801  
302  
201

DATE PRINTED: 4/12/2019

This product is for Assessment and Taxation (A&T) purposes only and has not been prepared or is suitable for legal, engineering, surveying or any purposes other than assessment and taxation.



**EXHIBIT "A"**  
**LEGAL DESCRIPTION**

Parcel 1 of Partition Plat 99-47, recorded November 02, 1999 as Instrument No. 199921786, Deed and Mortgage Records, Yamhill County, Oregon.



Evergreen  
Mobile Home  
Park

Comfort Inn

Altimus Plaza

Willamette Valley  
Medical Center

Subject Site

THREE MILE LANE (HIGHWAY 18)

18



**SEE TITLE REPORT ATTACHED TO PDA 5-23**

**PRELIMINARY TITLE REPORT**

**4**

**SEE REVISED COPY**

**APPLICATION NARRATIVE**  
Three Mile Lane Design Review

**5**

## **PRIOR LAND-USE DECISIONS**

ORDINANCE 4131

ORDINANCE 4709

ORDINANCE 5072

**6**



## ORDINANCE NO. 4131

### THREE MILE LANE

An Ordinance enacting conditions for development in an area hereinafter described, and commonly known as Three Mile Lane; now, therefore,

#### THE CITY OF McMinnville ORDAINS AS FOLLOWS:

Section 1. Statement of Purpose. The City of McMinnville finds that the development of lands on the north and south sides of Three Mile Lane (Oregon Highway 18) is acceptable if provisions are made to integrate the dual functions of the highway as a bypass around the City and a land service arterial for adjacent properties. The City also finds that development along Three Mile Lane is desirable if it is of good quality and design, if the living and working environments developed are compatible with each other, and if the living environment includes open spaces, parks, and features buffering of residential uses from the highway. The City recognized, during the comprehensive planning process, that to insure that these concerns are met, proper designations on the McMinnville Comprehensive Plan and Zoning Maps must be made, and, further, that a planned development overlay must be placed over the area establishing specific conditions for development.

Section 2. Planned Development Overlay. The planned development overlay which is created and implemented by this ordinance shall be placed over areas to the north and south of Oregon Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18. The affected area is further described by map in Exhibit "A". Areas within and without the city limits shall carry this designation. The policies and procedures set by this ordinance shall be applied to all land use decisions under the jurisdiction of the City of McMinnville. The overlay shall not impinge the legal jurisdiction of Yamhill County for areas outside the city limits.

Section 3. Notation on Zoning and Comprehensive Plan Maps. The perimeters of the areas affected by this ordinance shall be noted on the official McMinnville Comprehensive Plan Map of 1980 and the official McMinnville Zoning Map of 1980.

Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit "A":

- (a) The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- (b) A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- (c) Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:

1. The minimization of entrances onto Three Mile Lane;
  2. The development of on-site circulation systems, connecting to adjoining properties;
  3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
  4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.
- (d) Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- (e) Residentially designated areas covered by this ordinance shall be developed at R-1 zoning classification densities. Mixed housing-type residential developments shall be allowed and encouraged.
- (f) Residential developments shall provide parkland under the provisions of Section 35 of Ordinance No. 3702, unless safe and convenient pedestrian access is provided to an existing park. If no land is required, money in lieu of land shall be required.

The following policies shall apply to specified properties north and south of Three Mile Lane, described on the map in Exhibit "B". The written descriptions, uses, and written policies included below are to apply to the interpretation of the map and be of at least equal weight and importance.

#### Municipal Airport (Area 1)

The airport shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980 to accommodate aviation related facilities in keeping with the Airport Master Plan, as amended. Facilities identified in the plan for upgrading and expanding the aviation aspects of the airport shall be exempted from review by the provisions of the planned development overlay. Any private or public aviation related industrial development that may in the future be contemplated shall be subject to the provisions of this ordinance. The adoption of an airport zoning ordinance in the future may be appropriate, and additional requirements of such a zone shall be applied in conjunction with the requirements of this ordinance.

### West of Municipal Airport (Area 2)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The City shall encourage the location of aviation related industries or industries requiring extensive use of airport services, in this area. Zone changes to M-L (Limited Light Industrial), to M-1 (Light Industrial) and AH (Agricultural Holding) shall be allowed. Zone changes to M-2 (General Industrial) may be allowed upon findings that the intended use of the property shall be an aviation related industry, or an industry which requires extensive use of the airport services for its operation.

### West of Municipal Airport to Mobile Home Park (Area 3)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The City shall allow zone changes to M-L (Limited Light Industrial) and AH (Agricultural Holding) designations.

### East of Pacific Avenue - Industrial Area (Area 4)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980 to accommodate the existing industrial uses. Expansion of those uses may take place if compatibility with surrounding properties is insured.

### Airport Rendezvous (Area 5)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The areas designated AH (Agricultural Holding) on the map shall remain so designated until completion of the update of the Airport Master Plan. The adoption of an airport zoning ordinance, recommended land uses, and development controls for identified noise contours may be appropriate upon completion of the Airport Master Plan; any additional requirements enacted by the City shall be applied in conjunction with the requirements of this ordinance. Redesignation of the properties may be allowed after completion of the Airport Master Plan update and adoption of appropriate Master Plan recommendations, provided that the requirements of this ordinance are met, and upon determination that the proposed use is compatible with surrounding properties.

### Remainder of Planned Development Area

The remainder of the properties within the area covered by this ordinance shall be developed according to the designations on the McMinnville Comprehensive Plan Map. Site plan review as required in Chapter 17.51 of Ordinance No. 3380 shall include conditions to insure compatibility of allowed developments with proposed uses in surrounding areas.

Section 5. Procedures for Review.

- (a) Annexation to the City of lands within the area covered by this ordinance shall meet the requirements of the annexation ordinance.
- (b) Comprehensive Plan Map amendments shall be processed under procedures set out in Ordinance No. 4127.
- (c) Zone changes and land divisions submitted for approval shall be processed under the requirements and standards of Chapter 17.51 of Ordinance No. 3380 (Zoning Ordinance). If standards and requirements of Chapter 17.51 differ with those established elsewhere by the City, the more restrictive standards and requirements shall be adhered to.
- (d) Land division requests shall also be processed under the requirements of Ordinance No. 3702 (Land Division Ordinance).
- (e) Amendments to this ordinance shall be processed under procedures set out in Chapter 17.51 of Ordinance No. 3380 (Zoning Ordinance).

Passed by the City Council on April 7, 1981.



2000 0 2000 Feet

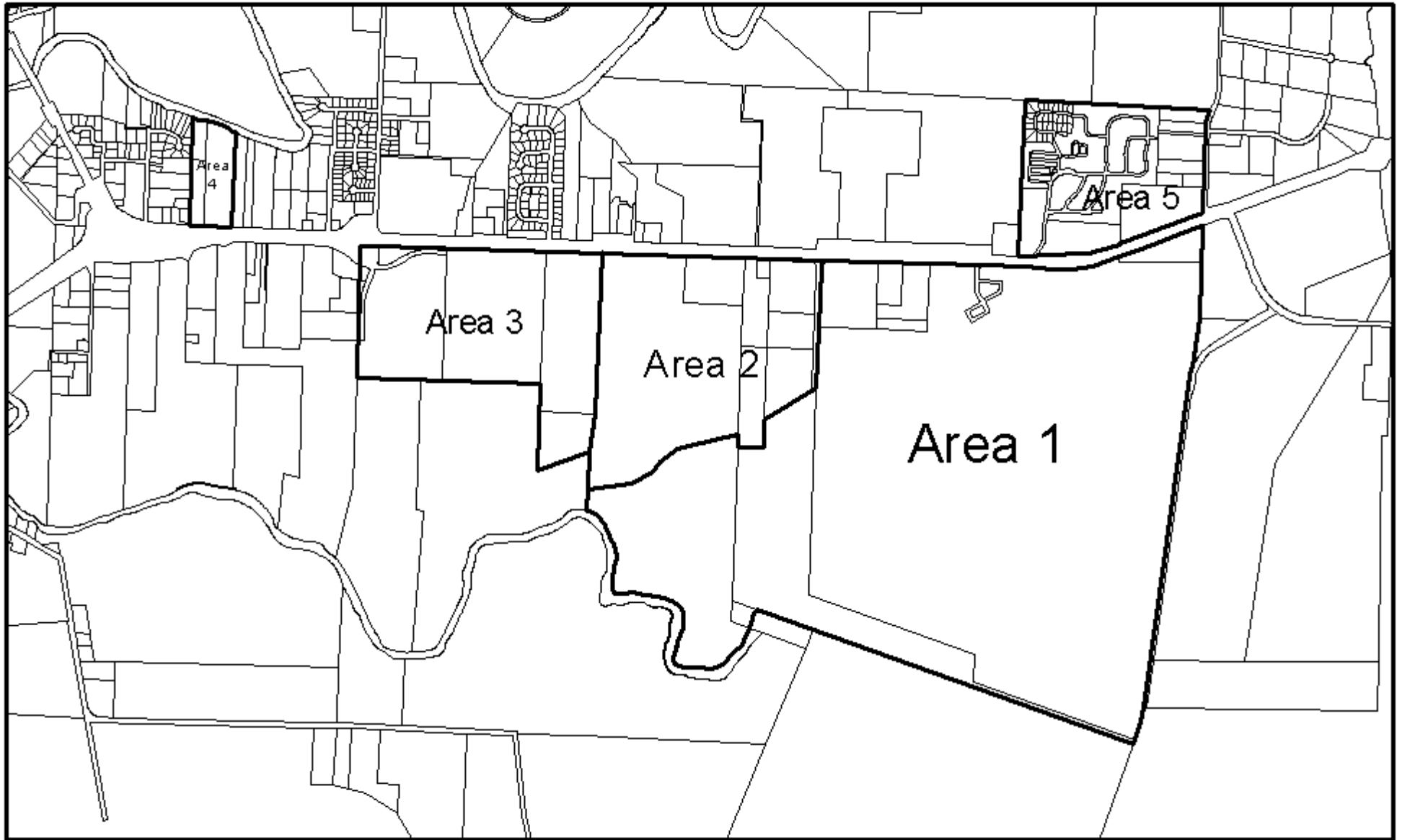


**EXHIBIT A**  
**Three Mile Lane**  
**Planned Development Overlay**

Ordinance No. 4131

**CITY OF McMINNVILLE**





**EXHIBIT B**  
**Three Mile Lane**  
**Planned Development Overlay**

Ordinance No. 4131

**CITY OF McMINNVILLE**

ORDINANCE NO. 4709

An Ordinance amending the City of McMinnville Comprehensive Plan Map from an existing Industrial designation to a Commercial designation and a zone change from a County EF-40 (Exclusive Farm Use - 40-acre minimum) zone to a City C-3 PD (General Commercial Planned Development) zone on a parcel of land approximately five acres in size located south of Three Mile Lane, and immediately west of Norton Lane and the Willamette Valley Medical Center.

RECITALS

The Planning Commission received an application (CPA 6-99 / ZC 11-99) from Gene and Dorothy McMullin and Phyllis Moyer dated July 2, 1999, requesting a comprehensive plan map amendment from an existing Industrial designation to a Commercial designation and a zone change from a County EF-40 (Exclusive Farm Use - 40-acre minimum) zone to a City C-3 PD (General Commercial Planned Development) zone on approximately five acres of land. The subject site is located south of Three Mile Lane, and immediately west of Norton Lane and the Willamette Valley Medical Center and is further described as a portion of Tax Lot 700, Section 27, T.4 S., R. 4 W., W.M.

A public hearing was held August 19, 1999, at 7:30 p.m. before the McMinnville Planning Commission after due notice had been given in the local newspaper on August 14, 1999, and written notice had been mailed to property owners within 300 feet of the affected property; and

At said public hearing, testimony was received, the application materials and a staff report were presented; and

The Planning Commission, being fully informed about said request, found that said changes conformed to the zone change review criteria listed in Chapter 17.72.035 of Ordinance No. 3380 based on the material submitted by the applicant and the findings of fact and the conclusionary findings for approval contained in the staff report, all of which are on file in the Planning Department, and that the plan amendment and zone change are consistent with the Comprehensive Plan; and

The Planning Commission approved said comprehensive plan map amendment and zone change and has recommended said changes to the Council, now, therefore,

THE CITY OF McMINNVILLE ORDAINS AS FOLLOWS:

Section 1. That the Council adopts the findings and conclusions of the Planning Commission, staff report on file in the Planning Department, and the application filed by Gene and Dorothy McMullin and Phyllis Moyer.

Section 2. That the Comprehensive Plan Map shall be amended from an Industrial designation to a Commercial designation for the property described in Exhibit "A" which is attached hereto and incorporated herein by this reference.

Section 3. That the property described in Exhibit "A" is hereby rezoned from a County EF-40 (Exclusive Farm use - 40-acre minimum) zone to a City C-3 PD (General Commercial Planned Development) zone subject to the following conditions:

1. That the zone change request shall not take effect until and unless CPA 6-99 is approved by the City Council.
2. That development and use of the site is subject to the provisions of the Three Mile Lane Planned Development Ordinance, as amended. Further, that a detailed Master Plan for the subject site shall be submitted to the McMinnville Planning Commission for review and approval, pursuant to the procedures of McMinnville Zoning Ordinance 17.51, Planned Development Overlay, prior to any development occurring on the site. The plan shall include, at a minimum, proposed land uses and their location(s), building locations, proposed circulation patterns, proposed open spaces, grading and drainage information, location and size of public utilities and services, off-street parking areas, direct pedestrian access, and other information deemed necessary to convey the details of the proposed development plans to the Planning Commission.
3. That detailed plans for the proposed commercial development showing site layout, proposed circulation pattern, signage, building elevations, landscaping, parking, and lighting must be submitted to and approved by the Three Mile Lane Design Review Committee prior to the issuance of any building permits for said development. Approval or denial of such plans shall be based on findings that, to the extent possible, the building and site design employs principles that will ensure compatibility with adjacent development, and provide an architectural style appropriate to a "gateway" or entrance to the City of McMinnville. In addition, approval or denial of the exterior building design shall be based on a finding that, to the extent possible, the building design incorporates design and architectural features that would serve to break up the building's horizontal plane and provide visual interest. This may include, but is not limited to, the use of vertical columns, gables, variety of compatible and complimentary building materials, providing openings in the building facade, and landscaping at the building perimeter. Submitted plans should include detail as regard building colors and materials (provide texture and visual relief), building height, planting design, window treatment, vertical and horizontal articulation, massing, voids to solids relationships, and other elements appropriate to ensure that the building and site design complies with the objectives and requirements of this planned development approval.

The provisions of Chapter 17.51 of the McMinnville Zoning Ordinance may be used to place conditions on any development and to determine whether or not specific uses are permissible. The applicant may appeal the decision of the Three Mile Lane Design Review Committee to the Planning Commission if

notice of intent to appeal is filed in the Planning Department office within fifteen (15) days of the Committee's decision.

4. That final development plans include landscape plans to be submitted to and approved by the McMinnville Landscape Review Committee and Three Mile Lane Design Review Committee. A minimum of 14 percent of the site must be landscaped. An arborvitae hedge or some similar type of planted visual screen shall be required along the property lines adjacent to the residentially zoned land to the west. Landscaping emphasis shall exist along the site's eastern edge, adjacent to Norton Lane, including required street trees, with particular emphasis at the site's eastern driveway intersection(s) at Norton Lane. In addition, landscape islands are required to be located throughout proposed off-street parking areas.

Street trees within a curbside planting strip along the Norton Lane frontage are required to have a two-inch minimum caliper, exhibit size and growing characteristics appropriate for the particular planting strip, and be spaced as appropriate for the selected species and as may be required for the location of above ground utility vaults, transformers, light poles, and hydrants. All street trees shall be of good quality and shall conform to American Standard for Nursery Stock (ANSI Z60.1). The Planning Director reserves the right to reject any plant material which does not meet this standard.

5. That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of the City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to the issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private rather than public, and private maintenance agreements must be approved by the City for them.
6. That final development plans for the subject site include a detailed sanitary sewage collection plan which incorporates the requirements of the City's Collection System Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to the issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. In addition, existing structures located within the subject site are required to connect to the sanitary sewer system as soon as service is available.
7. That the developer secure from the Department of Environmental Quality (DEQ) applicable storm runoff and site development permits prior to construction of the required site improvements. Evidence of such permit shall be submitted to the City Engineer.
8. That the developer enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and

grading permit for lot fill and grading from the City Building Division. All fill placed in the areas where construction is expected shall be engineered and shall meet with the approval of the City Building Division and the City Engineering Department.

9. That the developer extend water and power services to the subject site in accordance with McMinnville Water and Light requirements, including any necessary contracts and/or easements. Said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.
10. That utilities shall be extended to the property boundaries by the applicant, as may be required by the City Engineer or McMinnville Water and Light.
11. That no building shall exceed the height of 35 feet.
12. That if outside lighting is to be provided, it must be directed down and away from residential areas and public streets.
13. That signs located on the site shall be subject to the requirements of McMinnville Ordinance No. 4572 (B).
14. That all business, storage, or displays shall be conducted wholly within an enclosed building; except for off-street parking and loading.
15. That the subject site is limited to professional office use or medical office use, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility. Uses other than professional office use or medical office use may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the subject site.
16. That if restrictive covenants are proposed for the development they must meet with the approval of the Planning Director.
17. That the approved Master Plan as approved by the Planning Commission shall be placed on file with the Planning Department and become a part of the zone and binding on the owner and developer. The developer will be responsible for requesting permission of the Planning Commission for any major change of the details of the final development plans. Minor changes to the details of the adopted plan may be approved by the City Planning Director. It shall be the Planning Director's decision as to what constitutes a major or minor change. An appeal from a ruling by him may be made only to the Commission. Review of the Planning Director's decision by the Planning Commission may be initiated at the request of any one of the commissioners.



18. That improvements to the Norton Lane frontage, to the southernmost entry drive, or as may otherwise be required by the City of McMinnville, shall be done at the developer's expense and be finalized prior to release of any occupancy permits. Plans for the improvement of Norton Lane shall be submitted to the City Engineer for review and approval prior to its construction.
19. That the developer/owner of the subject site must grant to the Willamette Valley Medical Center and the City of McMinnville the right to cause all airspace above the surface of the subject site such as noise, vibrations, fumes, dust, fuel particles, and all other effects as may be caused by the operation of aircraft landing at or taking off from or operating at or on the Medical Center property and the McMinnville Municipal Airport. The owner must fully waive any right or cause of action which he may now or in the future raise against the Willamette Valley Medical Center and the City of McMinnville due to such circumstances noted above.
20. That prior to the development of the site, the applicant shall sign a waiver of remonstrance against the future improvement of Norton Lane. The waiver shall be prepared by the City.

Passed by the Council this 28<sup>th</sup> day of September 1999, by the following votes:

Ayes: Aleman, Kirchner, Pavne, Rabe, Windle

Nays: \_\_\_\_\_

Approved this 28<sup>th</sup> day of September 1999.

Richard D Windle  
COUNCIL PRESIDENT

Attest:

Kenneth Taylor  
RECORDER PRO TEM

ORDINANCE NO. 5072

**AN ORDINANCE AMENDING AN EXISTING PLANNED DEVELOPMENT OVERLAY DISTRICT TO ADD MULTIPLE FAMILY RESIDENTIAL AS AN ALLOWABLE USE IN THE PLANNED DEVELOPMENT OVERLAY DISTRICT**

RECITALS:

The Planning Department received an application (PDA 6-18) from KWDS, LLC, on behalf of property owners Gene A. McMullin (representing McMullin Family Residual Trust) and Phyllis A. Moyer (representing Phyllis Moyer Living Trust), requesting approval of a Planned Development Amendment. The existing Planned Development Overlay District limits uses, and the Planned Development Amendment request was to add multiple family residential as an allowable use on the subject site; and

The subject site is located west of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.; and

A public hearing was held on May 16, 2019 at 6:30 p.m., before the McMinnville Planning Commission after due notice had been provided in the local newspaper on May 7, 2019, and written notice had been mailed to property owners within 300 feet of the affected property; and

At said public hearing, the application materials and a staff report were presented, and applicant and public testimony was received; and

The Planning Commission, being fully informed about said requests, found that the requested amendments conformed to the applicable Comprehensive Plan goals and policies, as well as the planned development amendment review criteria listing in Section 17.74.070 of the McMinnville Municipal Code based on the material submitted by the applicant and the findings of fact and conclusionary findings for approval contained in Exhibit A; and

The Planning Commission recommended approval of said planned development amendment to the Council;

The City Council having received the Planning Commission recommendation and staff report, and having deliberated; and

**NOW, THEREFORE, THE COMMON COUNCIL FOR THE CITY OF MCMINNVILLE ORDAINS AS FOLLOWS:**

1. That the Council adopts the Findings of Fact, Conclusionary Findings, Decision and Conditions of Approval as documented in Exhibit A; and
2. That the requested amendment to the existing Planned Development Overlay District is approved, subject to the following conditions:
  - 1) That Condition 15 of Ordinance 4709, as amended by Ordinance 4863, be amended as follows (text to be removed is shown with ~~strikeout~~, text to be added is **bold and underlined**):

"That the subject site is limited to professional office use or medical office use, or multiple family dwellings, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, multiple family dwellings, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.

If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped."

- 2) All other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect.

Passed by the Council this 25<sup>th</sup> day of June, 2019, by the following votes:

Ayes: Garvin, Geary, Peralta, Stassens

Nays: \_\_\_\_\_

Scott A. Hill  
MAYOR

Attest:

Melissa Biset  
CITY RECORDER

Approved as to form:

[Signature]  
CITY ATTORNEY



CITY OF MCMINNVILLE  
PLANNING DEPARTMENT  
231 NE FIFTH STREET  
MCMINNVILLE, OR 97128

503-434-7311  
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**DECISION, CONDITIONS, FINDINGS OF FACT AND CONCLUSIONARY FINDINGS FOR THE APPROVAL OF A PLANNED DEVELOPMENT AMENDMENT TO ADD MULTIPLE FAMILY RESIDENTIAL AS AN ALLOWABLE USE WITHIN AN EXISTING PLANNED DEVELOPMENT OVERLAY DISTRICT**

- DOCKET:** PDA 6-18 (Planned Development Amendment)
- REQUEST:** Approval to amend Planned Development Overlay Ordinance 4709, as amended by Ordinance 4863, by amending a condition of approval that currently limits use of the site to professional office, medical office, senior condominiums, senior apartments, assisted living facilities, and other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility. The requested amendment is to add multiple-family residential apartments to that list of allowable uses in the existing Planned Development Overlay condition of approval. The base C-3 zone allows multiple-family residential as a permitted use.
- LOCATION:** West of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.
- ZONING:** C-3 PD (General Commercial Planned Development)
- APPLICANT:** KWDS, LLC, on behalf of property owners Gene A. McMullin (representing McMullin Family Residual Trust) and Phyllis A. Moyer (representing Phyllis Moyer Living Trust)
- STAFF:** Chuck Darnell, Senior Planner
- DATE DEEMED COMPLETE:** March 22, 2019
- HEARINGS BODY & ACTION:** The McMinnville Planning Commission makes a recommendation for approval or denial to the City Council.
- HEARING DATE & LOCATION:** May 16, 2019, Civic Hall, 200 NE 2<sup>nd</sup> Street, McMinnville, Oregon.



**PROCEDURE:** An application for a Planned Development Amendment is processed in accordance with the procedures in Section 17.72.120 of the McMinnville Municipal Code. The application is reviewed by the Planning Commission in accordance with the quasi-judicial public hearing procedures specified in Section 17.72.130 of the McMinnville Municipal Code.

**CRITERIA:** The applicable criteria for a Planned Development Amendment are specified in Section 17.74.070 of the McMinnville Municipal Code. In addition, the goals, policies, and proposals in Volume II of the Comprehensive Plan are to be applied to all land use decisions as criteria for approval, denial, or modification of the proposed request. Goals and policies are mandated; all land use decisions must conform to the applicable goals and policies of Volume II. "Proposals" specified in Volume II are not mandated, but are to be undertaken in relation to all applicable land use requests.

**APPEAL:** As specified in Section 17.72.190 of the McMinnville Municipal Code, the City Council's decision may be appealed to the Land Use Board of Appeals (LUBA) within 21 (twenty-one) days of the date written notice of decision is mailed. The City's final decision is subject to a 120 day processing timeline, including resolution of any local appeal.

**COMMENTS:** This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Engineering Department, Building Department, Parks Department, City Manager, and City Attorney; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Public Works; Yamhill County Planning Department; Frontier Communications; Comcast; Northwest Natural Gas; and Oregon Department of Transportation. Their comments are provided in this document.

**RECOMMENDATION**

Based on the findings and conclusionary findings, the City Council finds the applicable criteria are satisfied with conditions and **APPROVES** the Planned Development Amendment (PDA 6-18), **subject to the conditions of approval provided in this document.**

RECOMMENDATION: APPROVAL WITH CONDITIONS

City Council: Scott Hill  
Scott Hill, Mayor of McMinnville

Date: 6/25/2019

Planning Commission: Roger Hall  
Roger Hall, Chair of the McMinnville Planning Commission

Date: 6/27/2019

Planning Department: Heather Richards  
Heather Richards, Planning Director

Date: 6/25/19

## **I. APPLICATION SUMMARY:**

The applicant has provided information in their application narrative and findings (attached as Attachment 1) regarding the history of land use decisions for the subject site(s) and the request(s) under consideration. City has found the information provided to accurately reflect the current Planned Development Amendment request and the relevant background, and excerpted portions are provided below to give context to the request, in addition to staff's comments.

### ***Subject Property & Request***

The application (PDA 6-18) is a request for a Planned Development Amendment to amend a condition of approval from Ordinances 4709 and 4863 that currently limits use of the site to professional office, medical office, senior condominiums, senior apartments, assisted living facilities, and other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility. The requested amendment is to add multiple-family residential apartments to that list of allowable uses in the existing Planned Development Overlay condition of approval. The base C-3 zone allows multiple-family residential as a permitted use.

The subject property is located west of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits and urban growth boundary. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.

Excerpts from Land Use Application Narrative and Findings:

#### **Existing Conditions**

The Subject Site is currently undeveloped, and used for farming. The site is generally flat, with a minor slope to the northwest. There are no significant or distinguishing natural features associated with this property.

Access to the property is from Highway 18 via the intersection with Norton Lane. There is a signalized intersection of Norton Lane (SE & NE) with Highway 18. Immediately south of the highway, there is a three-way stop intersection of SE Norton Lane and SE Stratus Avenue. Stratus Avenue loops around the Medical Center and intersects back with SE Norton Lane across from the subject site.

#### **Access and Utilities**

The property fronts on west side of SE Norton Lane, just south of the Altimus Plaza medical offices. The existing right-of-way for SE Norton Lane is 60 feet down to and through the Stratus Avenue Loop intersection, south of the Medical Center. The existing improvements extend through this intersection. The street is currently improved with curb and gutter and 28 foot paved section.

SE Norton Lane is improved to City standard south to the intersection with the Stratus Avenue loop. The street improvements extend approximately 180 feet along the frontage of the Subject Site [Staff would note that the street along the subject site's frontage do not include sidewalks, but sidewalks constructed to City standards would be required at the time of development of the site – see Conditions 18 and 20 of Ordinance 4709]. There is an additional 280 feet of site frontage that is currently not improved.



There are existing urban services and utilities within SE Norton Lane. These facilities are all available and were adequately sized to serve development of the subject property, consistent with the allowed commercial uses.

#### Surrounding Uses

North – The property immediately north of the subject property is the Altimus Plaza medical offices. Further to the north at the Stratus Avenue intersection are the Comfort Inn & Suites and the Diner restaurant. The land west of the motel is vacant land, designated for professional and medical offices, or other compatible uses.

East – The property immediately east of the subject property is developed with medical offices, specifically Willamette Valley Medical Center, including a Heliport. To the south of the medical center is vacant farm land, outside of the city limits and UGB.

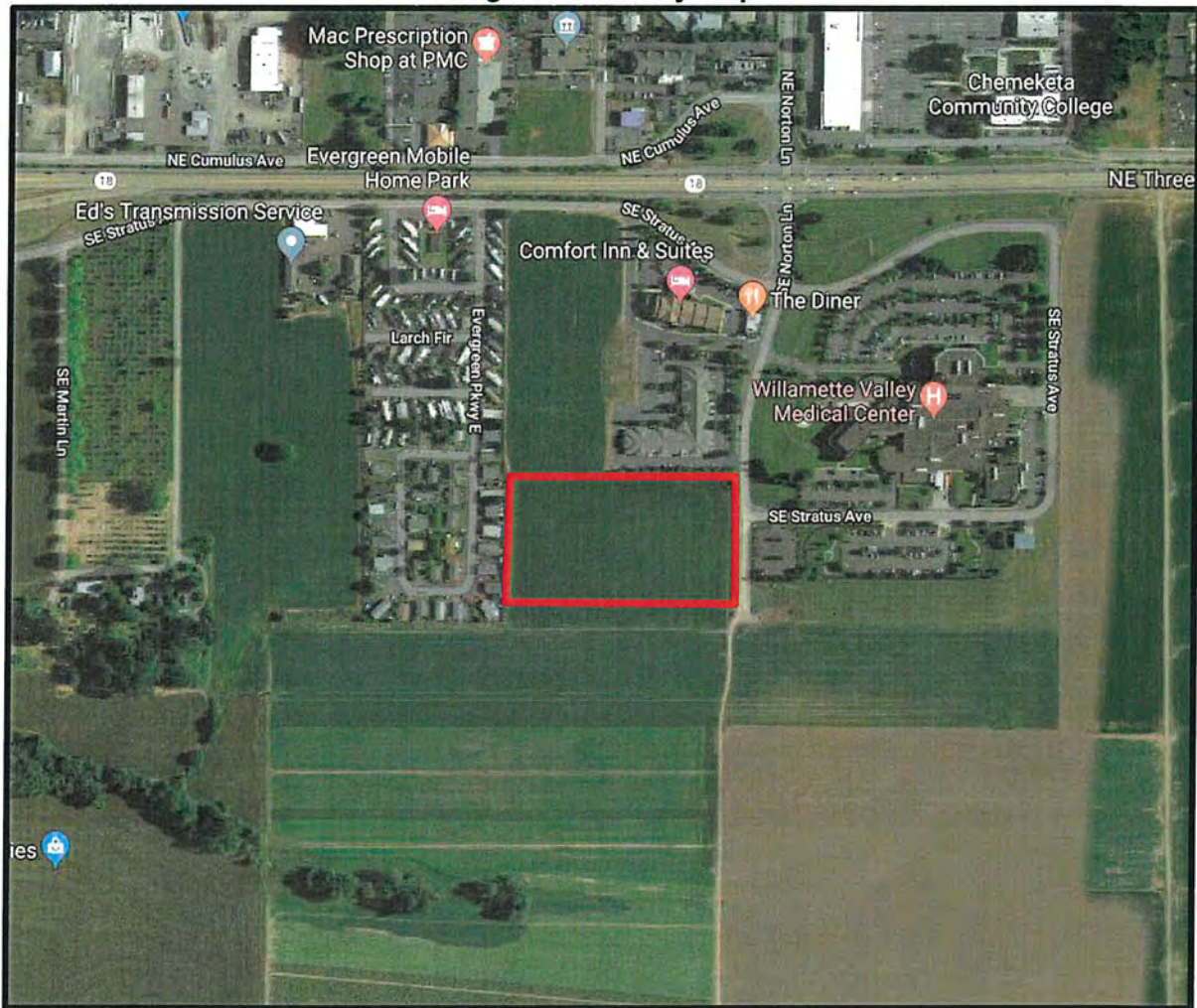
The Evergreen Aviation Museum, north of Hwy 18, and Municipal Airport, south of Hwy 18, are located just over a mile to the east.

South – The property immediately south of the subject property is vacant farm land, outside of the city limits and UGB.

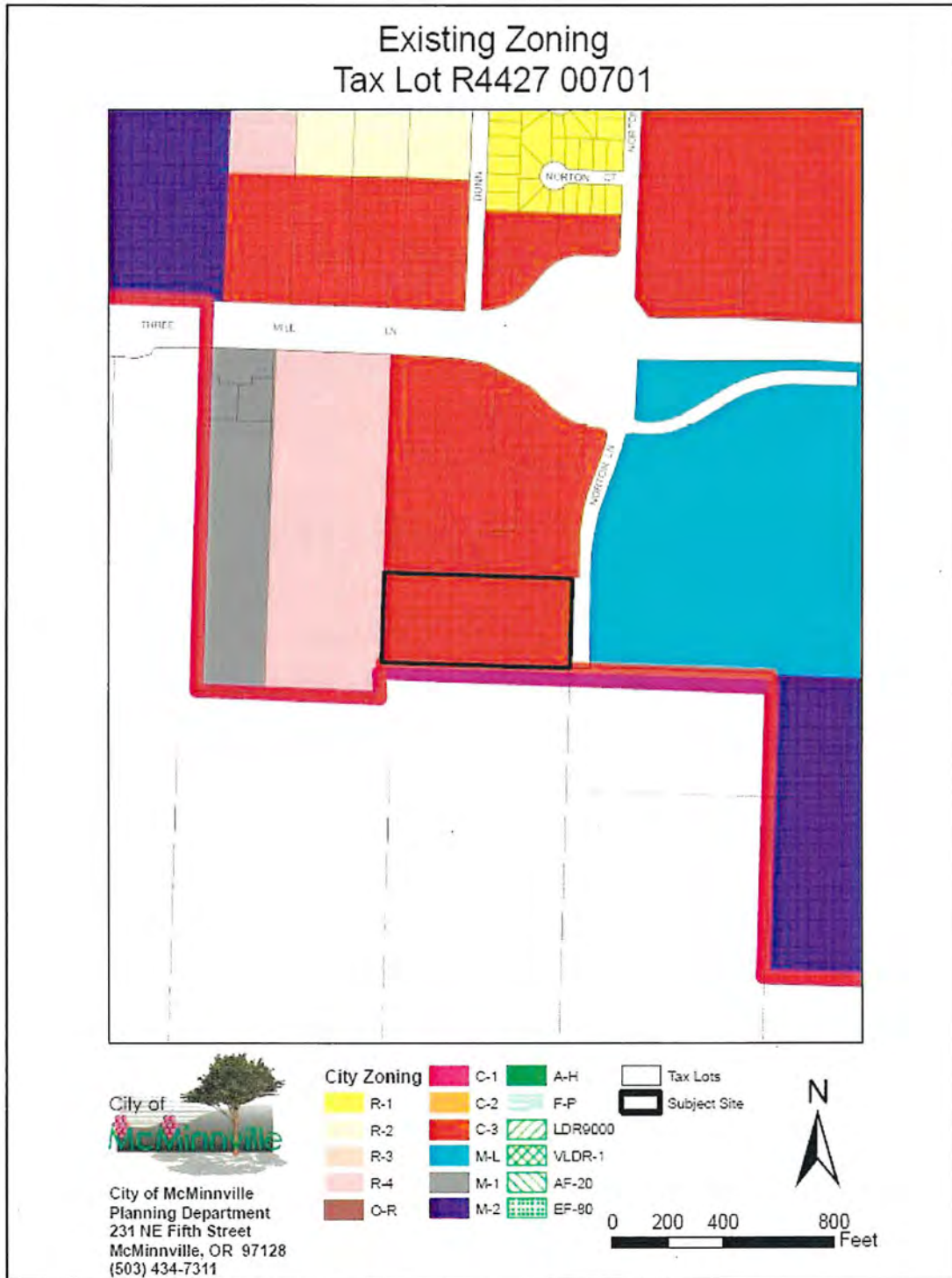
West – The property immediately west of the subject property is the Evergreen Estates Mobile Home Park, which is zoned R-4.

***See Vicinity Map (Figure 1) and Zoning Map (Figure 2) below.***

Figure 1. Vicinity Map



**Figure 2. Zoning Map**



**Background**

Excerpts from Land Use Application Narrative and Findings:

The subject site is within the Three Mile Lane Planned Development Overlay area, originally established by Ordinance 4131 in 1981

By Ordinance 4131, the City of McMinnville established Planned Development requirements for the Three Mile Lane area (Oregon Highway 18 Corridor). The area covered by this requirement extends north and south of Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18.

In 1994, Ordinance 4131 was amended to add specific development policies (Section 4) and signage regulations for Commercial lands (Section 5) by Ordinance 4572.

In 1999, the City amended the Comprehensive Plan re-designating a 5 acre industrial area south of Stratus Avenue for commercial use. With this Plan amendment, the 5 acre area, which is the Subject Site, was zoned C-3 PD, General Commercial Planned Development by Ordinance 4709.

Ordinance 4709 included 20 Conditions applicable to development within the PD boundary. These conditions remain in effect. However Condition 15 specifically limited uses as follows:

*“Condition 15. That the subject site is limited to professional office use or medical office use, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use or medical office use may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.”*

The record indicates the City’s general intent in limiting uses in this area was to avoid creation of strip commercial development, by restricting retail uses.

In 2006, the Three Mile Lane Planned Development (Ord. 4709) was amended, by Ordinance 4863. This amendment added senior condominiums, senior apartments, or assisted living facilities as permitted uses. Under Ordinance 4863 the current allowed uses for the subject property are limited as follows:

*“Condition 15. That the subject site is limited to professional office use or medical office use, **or senior condominiums, senior apartments, or assisted living facilities**, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.”*

The revised Condition 15 applies specifically to Tax Lot 701. In addition to the use restrictions per amended Condition 15, the specific development policies (Section 4) and signage regulations for Commercial lands (Section 5) established in Ordinance 4572 are applicable to the development of the Subject Property, as well as the other 19 Conditions set in Ordinance 4709.

The existing Planned Development originally adopted by Ordinance 4709 in 1999, and later amended by Ordinance 4863 in 2006, contains 19 other conditions of approval. Those conditions of approval are

still applicable to the property and are provided in Ordinance 4709, which is included as Attachment 2 to this Decision Document.

### **Summary of Criteria & Issues**

The application (PDA 6-18) is subject to Planned Development Amendment review criteria in Section 17.74.070 of the Zoning Ordinance. An amendment to an existing planned development may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Major changes to an adopted site plan shall be processed in accordance with Section 17.72.120. The goals and policies in Volume II of the Comprehensive Plan are also independent approval criteria for all land use decisions.

The specific review criteria for Planned Development Amendments in Section 17.74.070 of the McMinnville Zoning Ordinance require the applicant to demonstrate that:

- A. There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;
- B. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;
- C. The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;
- D. The plan can be completed within a reasonable period of time;
- E. The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;
- F. Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;
- G. The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.

The applicant has provided findings to support the request for a Planned Development Amendment. These will be discussed in detail in Section VII (Conclusionary Findings) below.

Generally, the purpose of a planned development is to provide greater flexibility and greater freedom of design in the development of land than may be possible under strict interpretation of the provisions of the zoning ordinance. Further, the purpose of a planned development is to encourage a variety in the development pattern of the community; encourage mixed uses in a planned area; encourage developers to use a creative approach and apply new technology in land development; preserve significant man-made and natural features; facilitate a desirable aesthetic and efficient use of open space; and create public and private common open spaces. A planned development is not intended to be simply a guise to circumvent the intent of the zoning ordinance.

Consideration of a planned development request includes weighing the additional benefits provided to the development and city as a whole through the planned development process that go above and beyond what would be provided through a standard land use application against the applicable zoning requirements. It should be noted in this case that the subject site is already regulated by an existing Planned Development (Ordinance No. 4709), and the request is only to expand the list of uses that are listed as permitted on the subject site. Overall, the proposed planned development amendment would



provide additional benefits to the community and the City as a whole. The proposal would provide an opportunity for increased variety in the development pattern of the community, and would encourage mixed uses in a planned area, specifically allowing for the incorporation of a use that is identified as needed in the community.

## **II. CONDITIONS:**

1. That Condition 15 of Ordinance 4709, as amended by Ordinance 4863, be amended as follows (text to be removed is shown with ~~strikeout~~, text to be added is **bold and underlined**:

"That the subject site is limited to professional office use or medical office use, or **multiple family dwellings**, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, **multiple family dwellings**, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.

**If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.**"

2. All other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect.

## **III. ATTACHMENTS:**

1. PDA 6-18 Application and Attachments (on file with the Planning Department)
2. Ordinance No. 4709 (on file with the Planning Department)
3. Ordinance No. 4863 (on file with the Planning Department)

## **IV. COMMENTS:**

### **Agency Comments**

This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Parks and Recreation Department, Engineering and Building Departments, City Manager, and City Attorney, McMinnville School District No. 40, McMinnville Water and Light, Yamhill County Public Works, Yamhill County Planning Department, Recology Western Oregon, Frontier Communications, Comcast, Northwest Natural Gas, Oregon Department of Transportation. The following comments were received:

- McMinnville Engineering Department

We have completed our review of PDA 6-18. A recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposal.

Thus, we have no comments re: the proposed planned development amendment.

At the time of development, the applicant will need to acquire all necessary permits (including any required by the Oregon Department of Transportation), and will need to construct the appropriate street and utility improvements necessary to support the development.

- McMinnville Fire Department

No comments received

- McMinnville Water and Light

Regarding the above planning development amendment there are no comments from the water or power side on our end.

- Oregon Department of Transportation

Since the site isn't adjacent to state highway, the only thing we would consider are impacts to a state highway intersection. Normally our threshold for a TIA is 50 peak hour trips or 300 ADT at a state highway intersection. This would potentially include the intersections at Stratus/Norton, OR-18/Norton, and maybe farther down OR-18 depending on what the trip distribution looks like.

## **Public Comments**

Notice of this request was mailed to property owners located within 300 feet of the subject site. Notice of the public hearing was also provided in the News Register on Tuesday, May 7, 2019. As of the date of the Planning Commission public hearing on May 16, 2019, no public testimony had been received by the Planning Department.

## **V. FINDINGS OF FACT - PROCEDURAL FINDINGS**

- A. The applicant, KWDS, LLC, held a neighborhood meeting on November 7, 2018.
- B. The applicant submitted the Planned Development Amendment application (PDA 6-18) on December 20, 2018.
- C. During the completeness review, it was determined that the neighborhood meeting had not be held in accordance with Section 17.72.095 of the McMinnville Municipal Code. It was also determined that findings had not been provided for all applicable Comprehensive Plan goals and policies. The application was deemed incomplete on January 18, 2019.
- D. The applicant held a second neighborhood meeting in accordance with Section 17.72.095 of the McMinnville Municipal Code on February 13, 2019.
- E. The applicant submitted a revised application on March 14, 2019 that included evidence of the neighborhood meeting held on February 13, 2019 and the applicable Comprehensive Plan goals and policies identified in the incomplete notification.
- F. The application was deemed complete on March 22, 2019. Based on that date, the 120 day land use decision time limit expires on July 20, 2019.

- G. Notice of the application was referred to the following public agencies for comment in accordance with Section 17.72.120 of the Zoning Ordinance: McMinnville Fire Department, Police Department, Parks and Recreation Department, Engineering and Building Departments, City Manager, and City Attorney, McMinnville School District No. 40, McMinnville Water and Light, Yamhill County Public Works, Yamhill County Planning Department, Recology Western Oregon, Frontier Communications, Comcast, Northwest Natural Gas, Oregon Department of Transportation.  
Comments received from agencies are addressed in the Decision Document.
- H. Notice of the application and the May 16, 2019 Planning Commission public hearing was mailed to property owners within 300 feet of the subject property in accordance with Section 17.72.120 of the Zoning Ordinance on Tuesday, April 23, 2019.
- I. Notice of the application and the May 16, 2019 Planning Commission public hearing was published in the News Register on Tuesday, May 7, 2019, in accordance with Section 17.72.120 of the Zoning Ordinance.
- J. No public testimony was submitted to the Planning Department prior to the Planning Commission public hearing.
- K. On May 16, 2019, the Planning Commission held a duly noticed public hearing to consider the request.

**VI. FINDINGS OF FACT – GENERAL FINDINGS**

- 1. **Location:** West of SE Norton Lane, near the termination of SE Norton Lane at the existing city limits. The property is more specifically described as Parcel 1, Partition Plat 99-47. The property is also identified as Tax Lot 701, Section 27, T. 4 S., R. 4 W., W.M.
- 2. **Size:** Approximately 4.93 acres.
- 3. **Comprehensive Plan Map Designation:** Commercial
- 4. **Zoning:** C-3 PD (General Commercial Planned Development)
- 5. **Overlay Zones/Special Districts:** Three Mile Lane Planned Development Overlay District (Ordinance No. 4131 and Ordinance No. 4572)
- 6. **Current Use:** Undeveloped
- 7. **Inventoried Significant Resources:**
  - a. **Historic Resources:** None
  - b. **Other:** None
- 8. **Other Features:** The site is described by the applicant as being generally flat, with a minor slope to the northwest. There are no significant or distinguishing natural features associated with this property.
- 9. **Utilities:**
  - a. **Water:** Water service is available to the subject site.
  - b. **Electric:** Power service is available to the subject site.

- c. **Sewer:** Sanitary sewer service is available to the subject site.
- d. **Stormwater:** Storm sewer service is available to the subject site.
- e. **Other Services:** Other utility services are available to the subject site. Northwest Natural Gas and Comcast is available to serve the site.

10. **Transportation:** The site is adjacent to SE Norton Lane, which is identified as a minor collector in the McMinnville Transportation System Plan. Section 17.53.101 of the McMinnville Municipal Code identifies the right-of-way width for minor collector streets as 56 feet (with no bike lane) or 66 feet (with bike lane). The existing right-of-way width of SE Norton Lane adjacent to the subject site is 71.47 feet, as shown on Partition Plat 99-47. Therefore, no additional right-of-way dedications are necessary. Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development.

**VII. CONCLUSIONARY FINDINGS:**

The Conclusionary Findings are the findings regarding consistency with the applicable criteria for the application. The applicable criteria for a Planned Development Amendment are specified in Section 17.74.070 of the Zoning Ordinance.

In addition, the goals, policies, and proposals in Volume II of the Comprehensive Plan are to be applied to all land use decisions as criteria for approval, denial, or modification of the proposed request. Goals and policies are mandated; all land use decisions must conform to the applicable goals and policies of Volume II. "Proposals" specified in Volume II are not mandated, but are to be undertaken in relation to all applicable land use requests.

**Comprehensive Plan Volume II:**

The following Goals, Policies, and Proposals from Volume II of the Comprehensive Plan provide criteria applicable to this request:

The implementation of most goals, policies, and proposals as they apply to this application are accomplished through the provisions, procedures, and standards in the city codes and master plans, which are sufficient to adequately address applicable goals, policies, and proposals as they apply to this application.

The following additional findings are made relating to specific Goals and Policies:

GOAL IV 1: TO ENCOURAGE THE CONTINUED GROWTH AND DIVERSIFICATION OF McMINNVILLE'S ECONOMY IN ORDER TO ENHANCE THE GENERAL WELL-BEING OF THE COMMUNITY AND PROVIDE EMPLOYMENT OPPORTUNITIES FOR ITS CITIZENS.

GOAL IV 2: TO ENCOURAGE THE CONTINUED GROWTH OF McMINNVILLE AS THE COMMERCIAL CENTER OF YAMHILL COUNTY IN ORDER TO PROVIDE EMPLOYMENT OPPORTUNITIES, GOODS, AND SERVICES FOR THE CITY AND COUNTY RESIDENTS.

*Policy 21.01 The City shall periodically update its economic opportunities analysis to ensure that it has within its urban growth boundary (UGB) a 20-year supply of lands designated for commercial and industrial uses. The City shall provide an adequate number of suitable, serviceable sites in appropriate locations within its UGB. If it should find that it does not have an adequate supply of lands designated for commercial or industrial use it shall take corrective actions which may include, but are not limited to, redesignation of lands for such*

*purposes, or amending the UGB to include lands appropriate for industrial or commercial use. (Ord.4796, October 14, 2003)*

**APPLICANT'S RESPONSE:** The comprehensive plan map is already consistent with these Goals and this Policy. The subject site and surrounding properties to the north and east are designated commercial and zoned C-3PD.

The proposed amendment does not change this designation, and the property remains zoned C-3PD. Additional vacant commercial land remains north and east of the subject property, to accommodate additional office or senior housing uses. The amendment only adds multi-family apartments to the list of allowed uses, which already includes senior apartments.

**FINDING: SATISFIED.** City concurs with the applicant's findings, and adds that the proposed amendment to Condition 15 would not remove any ability to develop commercial uses on the subject site. Currently, and after the proposed amendment, professional office, medical office, senior condominiums, senior apartments, assisted living facilities, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility, as allowed under the revised condition of approval that was approved by Ordinance No. 4863.

**GOAL IV 3: TO ENSURE COMMERCIAL DEVELOPMENT THAT MAXIMIZES EFFICIENCY OF LAND USE THROUGH UTILIZATION OF EXISTING COMMERCIALY DESIGNATED LANDS, THROUGH APPROPRIATELY LOCATING FUTURE COMMERCIAL LANDS, AND DISCOURAGING STRIP DEVELOPMENT.**

*Policy 24.50 The location, type, and amount of commercial activity within the urban growth boundary shall be based on community needs as identified in the Economic Opportunities Analysis. (Ord.4796, October 14, 2003)*

*Policy 25.00 Commercial uses will be located in areas where conflicts with adjacent land uses can be minimized and where city services commensurate with the scale of development are or can be made available prior to development.*

*Policy 29.00 New direct access to arterials by large-scale commercial developments shall be granted only after consideration is given to the land uses and traffic patterns in the area of development as well as at the specific site. Internal circulation roads, acceleration/deceleration lanes, common access collection points, signalization, and other traffic improvements shall be required wherever necessary, through the use of planned development overlays.*

**APPLICANT'S RESPONSE:** As noted, the Subject Property is designated commercial, consistent with these policies. The designation does not change with this proposed amendment. In approving the prior amendment (Ordinance 4863) the City has already determined that residential use, in the form of senior housing, would be compatible with the underlying C-3 commercial zoning, which allows multi-family housing. The applicant is simply requesting that the residential use not be restricted to senior housing, by also allowing traditional multi-family apartment.

The provisions of Policy 29.00 remain in effect, and have been implemented by the existing transportation improvements in the immediate area. As demonstrated by the traffic impact report attached with this application, the proposed multi-family use will generate less traffic impacts than the base commercial office uses.



**FINDING: SATISFIED.** The existing property is already designated on the Comprehensive Plan as Commercial, and is zoned C-3 PD (General Commercial Planned Development) as determined to meet Goal IV 3 and Policies 24.50 and 25.00 by the previous actions to amend the Comprehensive Plan (CPA 6-99) and adopt the existing Planned Development (ZC 11-99) by Ordinance 4709. The proposed amendment to add multiple family residential as an allowed use would not remove any ability to develop commercial uses on the subject site. Currently, and after the proposed amendment, professional office, medical office, senior condominiums, senior apartments, assisted living facilities, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility, would still be allowed on the subject site under the revised condition of approval that was approved by Ordinance No. 4863.

The existing property has frontage on SE Norton Lane, which is identified as a minor collector in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010. Therefore, the consideration of the impacts of a new direct access on land uses and traffic patterns in the area of the development is not required by Policy 29.00 because the adjacent roadway is not an arterial. However, the applicant provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use.

*Proposal 6.00 A planned development overlay should be placed on the large cluster commercial development areas and the entrances to the City to allow for review of site design, on-site and off-site circulation, parking, and landscaping. The areas to be overlaid by this designation shall be noted on the zoning map and/or comprehensive plan map.*

**APPLICANT'S RESPONSE:** The Three Mile Lane Planned Development is consistent with this proposal. The PD Overlay remains in effect, the amendment simply adds multi-family apartments as an allowed uses, and will maintain consistency with the underlying C-3 zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings, and adds that the existing conditions of approval from Ordinance No. 4709 remain in effect. Conditions 2, 3, and 4 require site plan and landscape plan review and approval by the Planning Commission and Landscape Review Committee, respectively, prior to any development of the subject site. These reviews would ensure review of site design, on-site and off-site circulation, parking, and landscaping.

**GOAL V 1:** TO PROMOTE DEVELOPMENT OF AFFORDABLE, QUALITY HOUSING FOR ALL CITY RESIDENTS.

*Policy 58.00 City land development ordinances shall provide opportunities for development of a variety of housing types and densities.*

*Policy 59.00 Opportunities for multiple-family and mobile home developments shall be provided in McMinnville to encourage lower-cost renter and owner-occupied housing. Such housing shall be located and developed according to the residential policies in this plan and the land development regulations of the City.*

*Policy 61.00 The City of McMinnville shall monitor the conversion of lands to residential use to ensure that adequate opportunities for development of all housing types are assured. Annual reports on the housing development pattern, housing density and mix shall be prepared for city review.*

**APPLICANT'S RESPONSE:** The City has implemented these Policies through adoption of the Zoning Code, which includes residential zones accommodating various densities and housing types. Further, the underlying C-3 zone allows multi-family development, consistent with R-4 standards, per Section 17.33.010.3. The proposed amendment simply adds this allowed use, maintaining consistency with the C-3 code.

As addressed herein, the evidence provided demonstrates that there is no greater need for senior housing over other type of housing. In fact, the City's Code does not even distinguish senior housing as a separate use category. Any senior housing built would be approved consistent with the C-3 and R-4 zoning for multi-family housing.

Further, there is residential R-4 land immediately to the west of the subject site, so there is a basis for concluding that R-4 residential is compatible with the professional and medical office commercial designation for the remainder of the Planned Development Area.

The requested change will not alter the current density allocated for the property. The allowed density will remain consistent with the R-4 standards for multi-family housing.

Therefore the requested amendment maintains consistency with the applicable zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

**GOAL V 2:** TO PROMOTE A RESIDENTIAL DEVELOPMENT PATTERN THAT IS LAND INTENSIVE AND ENERGY-EFFICIENT, THAT PROVIDES FOR AN URBAN LEVEL OF PUBLIC AND PRIVATE SERVICES, AND THAT ALLOWS UNIQUE AND INNOVATIVE DEVELOPMENT TECHNIQUES TO BE EMPLOYED IN RESIDENTIAL DESIGNS.

*Policy 68.00 The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center and to those areas where urban services are already available before committing alternate areas to residential use.*

*Policy 71.00 The City of McMinnville shall designate specific lands inside the urban growth boundary as residential to meet future projected housing needs. Lands so designated may be developed for a variety of housing types. All residential zoning classifications shall be allowed in areas designated as residential on the Comprehensive Plan Map.*

*Policy 71.05 The City of McMinnville shall encourage annexations and rezoning which are consistent with the policies of the Comprehensive Plan so as to achieve a continuous five-year supply of buildable land planned and zoned for all needed housing types. (Ord.4840, January 11, 2006; Ord. 4243, April 5, 1983; Ord. 4218, November 23, 1982)*

**APPLICANT'S RESPONSE:** The area of the proposed amendment is zoned C-3 Commercial and therefore is not committed to low density development. The existing C-3 zoning is consistent with Goal V2 and the Policies 68, 71.05 and 71.09.

**FINDING: SATISFIED.** The proposed Planned Development Amendment would result in the addition of multiple family residential to the list of allowed uses in the existing Planned Development area. The subject property is not designated as residential, but has an underlying zone of C-3 (General Commercial). The underlying C-3 zone allows multiple family residential uses as a permitted use, so the proposed amendment is not inconsistent with the underlying zone and provides an opportunity for the development of residential uses.

The most recently acknowledged Residential Buildable Lands Inventory, which was prepared in 2001, that identified a need for additional land for housing uses. That inventory, which was titled the McMinnville Buildable Land Needs Analysis and Growth Management Plan, identified a deficit of over 1,000 acres of land for housing in Table B-11 of Appendix B. More specifically, the analysis identified a need of 162 acres of R-4 (higher density) zoned land. The proposed amendment to allow multiple family residential on the subject site would not increase the amount of land designated specifically for higher density residential use, but it would increase the potential for the development of higher density residential uses on a property with an existing, underlying zone that already permits multiple family residential use (C-3 General Commercial zone).

*Policy 71.09 Medium and High-Density Residential (R-3 and R-4) – The majority of residential lands in McMinnville are planned to develop at medium density range (4 – 8 units per net acre). Medium density residential development uses include small lot single-family detached uses, single family attached units, duplexes and triplexes, and townhouses. High density residential development (8 – 30 dwelling units per net acre) uses typically include townhouses, condominiums, and apartments. The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center and to those areas where urban services are already available before committing alternate areas to residential use.*

*1. Areas that are not committed to low density development;*

**APPLICANT'S RESPONSE:** The area of the proposed amendment is zoned C-3 Commercial and therefore is not committed to low density development. The existing C-3 zoning is consistent with Goal V2 and the Policies 68, 71.03, and 71.09.

The current Planned Development Overlay is consistent with the criteria for location of medium and high density residential, which is allowed under Policy 71.09 and also specifically allowed in the C-3 zone.

However, Ordinance 4863 imposed a specific limitation restricting residential development to various types of senior housing, per Condition 15. Except for this specific conditioned limitation, the C-3 zone would otherwise allow multi-family development, consistent with R-4 standards, as a permitted use.

The applicant's intent is to develop high density apartments consistent with the R-4 standards, as required by the C-3 zone. So technically, under the C-3 zoning, the density is not being increased, as the R-4 standards will be applied.

**FINDING: SATISFIED.** City concurs with the applicant's findings, specifically that the existing underlying zone of C-3 (General Commercial) is not committed to low density development.

*2. Areas that have direct access from collector or arterial streets;*

**APPLICANT'S RESPONSE:** The subject site has direct access from the Highway (E. Salmon River) via SE Norton Lane. Therefore this criterion is met.

**FINDING: SATISFIED.** The subject site has frontage on SE Norton Lane, which is identified as a minor collector in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010. Therefore, the subject site has direct access from a collector street.

3. *Areas that are not subject to development limitations such as topography, flooding, or poor drainage;*

**APPLICANT'S RESPONSE:** The subject site is an open generally flat field, with no development restrictions. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

4. *Areas where the existing facilities have the capacity for additional development;*

**APPLICANT'S RESPONSE (Provided December 20, 2018):** The commercial area along Norton Lane is served by a full range of urban services including sanitary sewer, water, storm drainage and franchise utilities. All of these services generally have sufficient capacity to serve the proposed development.

However, the City Engineer indicated that the existing sanitary sewer line serving the subject site was designed based on, "*an assumption in the modeling for undeveloped commercial/industrial properties at an equivalent of 12 dwelling units per acre.*"

Consequently, in coordination with the City Engineer, CH2M has conducted an analysis of the sewer system capacity for the immediate area to determine that there is capacity. The system was designed based on medium density residential calculated to serve 59 units (4.93 acres x 12 units per acre).

The applicant has agreed to cover the cost for CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development.

CH2M findings will be submitted separately.

**Revised Response from Applicant (Provided March 14, 2019):** The applicant agreed to cover the cost of CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development

However, Mike Bisset, City Engineer, has informed us that the City has engaged CH2M to conduct a system-wide analysis, rather than just to Norton Lane area.

**FINDING: SATISFIED.** The Engineering Department provided comments (provided on April 5, 2019) on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site. Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction.

Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

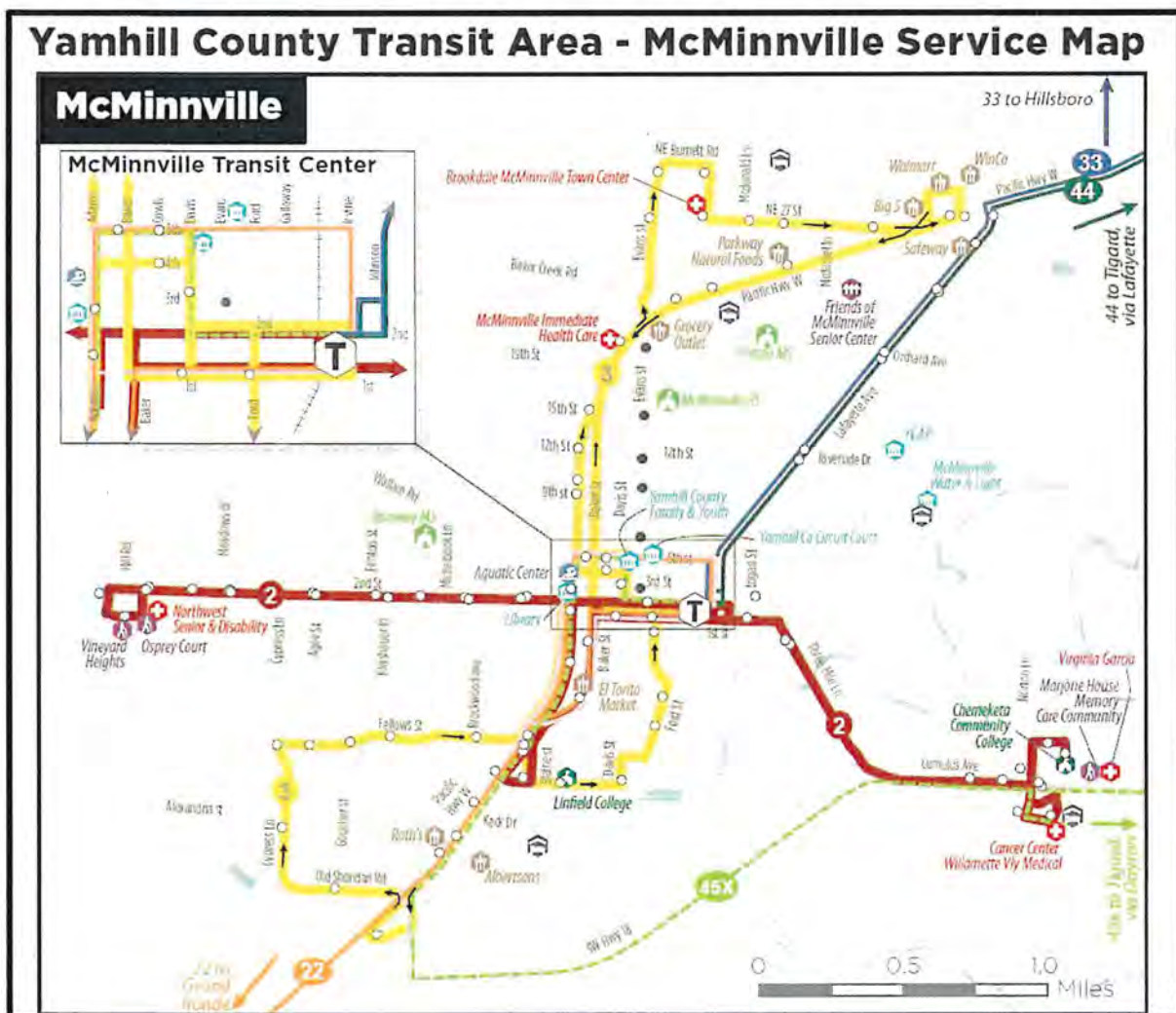
- 5. Areas within one-quarter mile of existing or planned public transportation; and,

**APPLICANT'S RESPONSE:** Local Transit Routes 2 runs on the Highway and on SE Norton Lane to the Willamette Valley Medical Center, which is located across the street from the subject site. See the attached Route Map and Schedule, at the end of this Compliance Narrative.

Route 2 provides regularly scheduled weekday service. This route connects to the Transit Center for connection to the Yamhill County Transit route for access to the other parts of town. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would add that Route 2 provides roughly hourly weekday service, and connects to the McMinnville Transit Center, where riders could transfer to other routes, including commuter routes to surrounding cities.

A view of the McMinnville transit map, which identifies Routes 2 near the Willamette Valley Medical Center, is provided below:



6. *Areas that can be buffered from low density residential areas in order to maximize the privacy of established low density residential areas.*

**APPLICANT'S RESPONSE:** And, there is residential R-4 land immediately to the west of the subject site, which establishes the buffer of lower density residential per criteria 6.

The requested change will not alter the current density allocated to the property. The density will remain as is consistent with the R-4 standards for multi-family housing.

Therefore all of the criteria in Policy 71.09 are met.

**FINDING: SATISFIED.** The subject site is not located adjacent to any existing low density residential areas. Therefore the subject site and area does not require any buffering from low density residential areas.

*Policy 71.13 The following factors should serve as criteria in determining areas appropriate for high-density residential development:*

1. *Areas which are not committed to low or medium density development;*

**APPLICANT'S RESPONSE:** The subject property is zoned C-3 General Commercial with a PD overlay. The C-3 zone allows high density residential use. Therefore it is not committed to low or medium density development.

**FINDING: SATISFIED.** City concurs with the applicant's findings, specifically that the existing underlying zone of C-3 (General Commercial) is not committed to low density development.

2. *Areas which can be buffered by topography, landscaping, collector or arterial streets, or intervening land uses from low density residential areas in order to maximize the privacy of established low density residential areas;*

**APPLICANT'S RESPONSE:** There is no designated low density residential in the surrounding neighborhood. There is R-4 zoning to the west, which provides a buffer between the site and other zoning. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

3. *Areas which have direct access from a major collector or arterial street;*

**APPLICANT'S RESPONSE:** The subject site has direct access from the Highway (E. Salmon River) via SE Norton Lane. Therefore this criterion is met.

**FINDING: SATISFIED.** While Highway 18 (Three Mile Lane) is identified as a major arterial in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010, the subject site does not have direct access from Highway 18. The subject site has frontage and direct access only onto SE Norton Lane, which is identified as a minor collector in the Transportation System Plan, as adopted by Ordinance No. 4922 in 2010. However, the applicant provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development



conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use. Therefore, the use allowed by the proposed amendment would not be more detrimental to the surrounding street network or transportation system than other uses currently allowed on the subject site by Ordinance No. 4709 and Ordinance No. 4863.

4. *Areas which are not subject to development limitations;*

**APPLICANT'S RESPONSE:** The subject site is an open generally flat field, with no development restrictions. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

5. *Areas where the existing facilities have the capacity for additional development;*

**APPLICANT'S RESPONSE (Provided December 20, 2018):** The City Engineer indicates that the existing sanitary sewer line serving the subject site was designed based on, "*an assumption in the modeling for undeveloped commercial/industrial properties at an equivalent of 12 dwelling units per acre.*" Whereas, the applicant is proposing high density multi-family development up to 120 units.

Consequently, in coordination with the City Engineer, CH2M has conducted an analysis of the sewer system capacity for the immediate area to determine that there is capacity. The system was designed based on medium density residential calculated to serve 59 units (4.93 acres x 12 units per acre).

The applicant has agreed to cover the cost for CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development.

CH2M findings will be submitted separately.

**Revised Response from Applicant (Provided March 14, 2019):** The applicant agreed to cover the cost of CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development

However, Mike Bisset, City Engineer, has informed us that the City has engaged CH2M to conduct a system-wide analysis, rather than just to Norton Lane area.

**FINDING: SATISFIED.** The Engineering Department provided comments (provided on April 5, 2019) on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site. Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction.

Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

6. *Areas within a one-half mile wide corridor centered on existing or planned public transit routes;*

**APPLICANT'S RESPONSE:** Local Transit Route 2 runs on the Highway and on SE Norton Lane to the Willamette Valley Medical Center. The Medical Center is located across the street from the subject site. See the attached Route Map and Schedule, at the end of this Compliance Narrative. Route 2 provides regularly scheduled weekday service. Therefore this criterion is met.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would add that Route 2 provides roughly hourly weekday service, and connects to the McMinnville Transit Center, where riders could transfer to other routes, including commuter routes to surrounding cities.

A view of the McMinnville transit map, which identifies Routes 2 near the Willamette Valley Medical Center, is provided in the finding for Policy 71.09 above.

7. *Areas within one-quarter mile from neighborhood and general commercial shopping centers; and*

**APPLICANT'S RESPONSE:** The City's zoning for Neighborhood Commercial is C-1, which General Commercial is C-2.

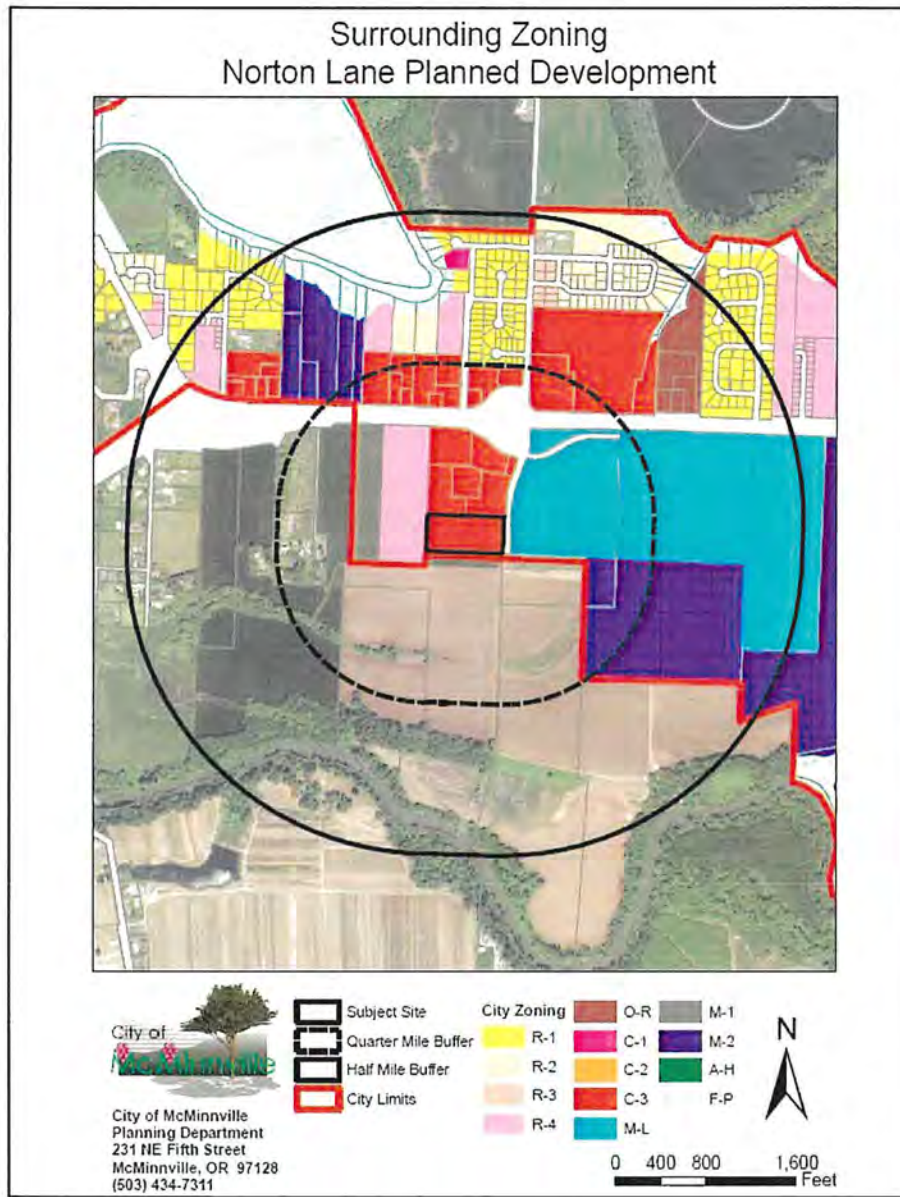
The subject property and adjacent property to the north (770 feet) to Stratus Avenue is zoned C-2, General Commercial, with a PD overlay. There is also C-2 zoning on the north side of the highway on both sides of NE Norton Lane.

The closest C-1 zoning is approximately ¼ mile east of the Norton Lane intersection with the Highway.

Therefore this criterion is met.

**FINDING: SATISFIED.** While the McMinnville Municipal Code (MMC) does have a Neighborhood Business Zone (C-1), that zoning district has been applied very narrowly and is only designated on two parcels in the entire city limits (the two parcels that the applicant notes approximately ¼ mile east of the Norton Lane intersection with Highway 18). Also, the MMC does not have any definition of neighborhood or general commercial shopping centers. Therefore, the commercial designation of property is used in analyzing this Comprehensive Plan policy and high density residential locational factor. The subject site is located within one-quarter mile of commercially zoned property. The commercially zoned property within one-quarter mile of the subject site is zoned C-3 (General Commercial) – which is a correction on the applicant's response where General Commercial is identified as C-2 – to allow general retail, service, and shopping type uses.

A map identifying the subject site and the distances to commercially zoned properties is provided below:



8. *Areas adjacent to either private or public permanent open space.*

**APPLICANT’S RESPONSE:** The closest public open space to the site is Joe Dancer Park, which is located on E. Brooks Street, approximately 1.4 miles to the northwest.

The proposed development will however be designed to provide on-site open space for passive and active recreation for tenants. Therefore this criterion is met.

Therefore all of the criteria in Policy 71.13 are met.

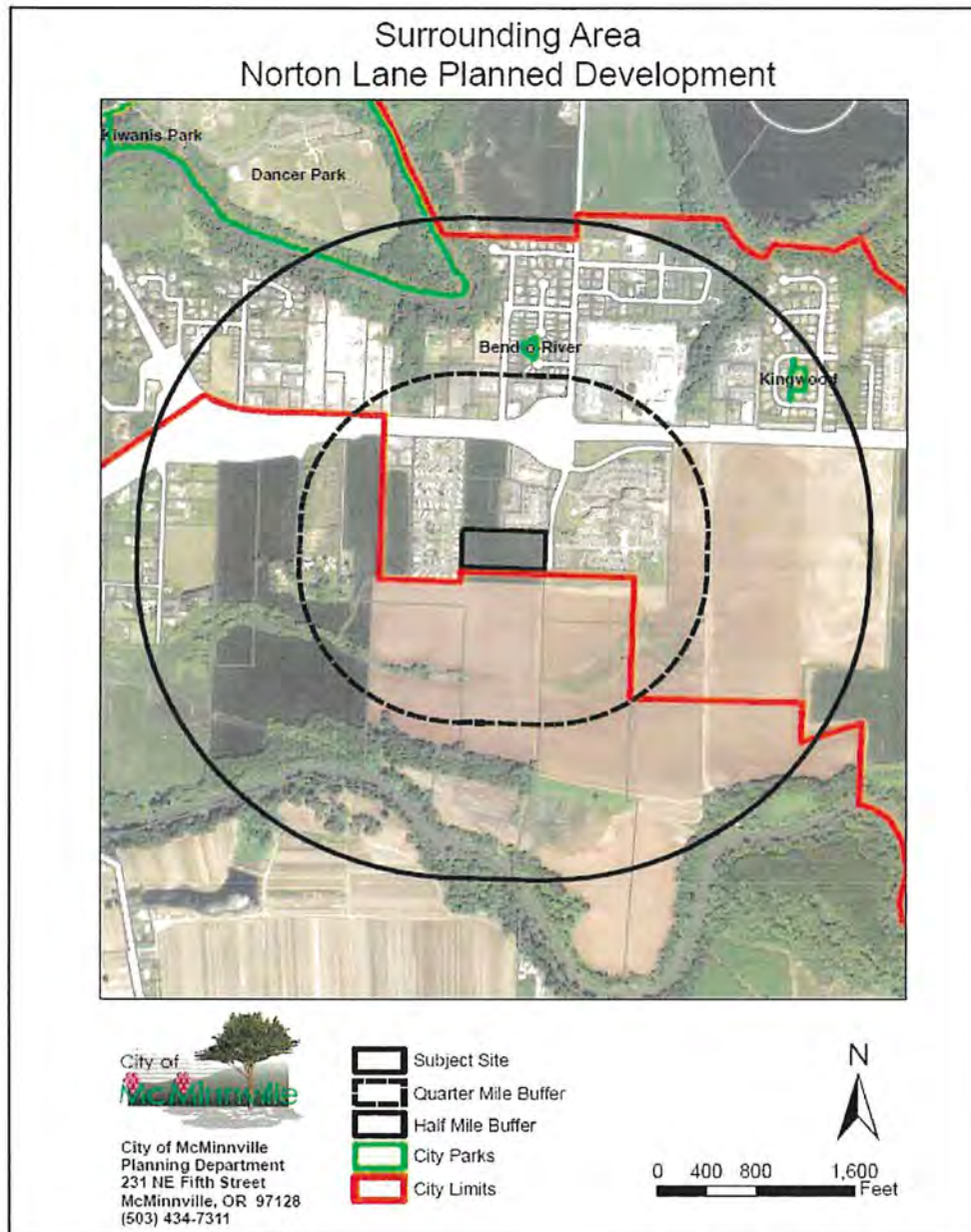
**FINDING: SATISFIED WITH CONDITION #1.** The subject site is not located adjacent to private or public open space. There are only three public parks within ½ mile of the subject site, and two of those parks (Bend-o-River and Kingwood) are what would be classified as Mini-Parks in the McMinnville Parks, Recreation, and Open Space Master Plan. The other park within ½ mile of the subject site is Joe Dancer Park, which is identified as a Community Park in the McMinnville

Parks, Recreation, and Open Space Master Plan. However, Joe Dancer Park is located across the Yamhill River and is not actually accessible within a ½ mile distance of the subject site.

The factor for high density residential development calls for open space to be adjacent to the site, not within a certain distance. While there some opportunities for access to open space within one-half mile of the subject site, there is not any usable open space adjacent to the site. Not only is there no open space adjacent to the site, there is a lack of connectivity to open space in the surrounding area. As a planned development, open space needs to be provided that provides a benefit to the residents of the site. Therefore, language is included in the amended condition of approval to require that, if the site is developed with multiple-family residential uses, an area equivalent to 10 percent of the gross size of the site shall be reserved for usable open space for residents of the multiple-family development site. The condition of approval states that the usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.

A map identifying the subject site and the distances to surrounding parks is provided below:





*Planned Development Policies*

*Policy 72.00 Planned developments shall be encouraged as a favored form of residential development as long as social, economic, and environmental savings will accrue to the residents of the development and the city.*

**APPLICANT’S RESPONSE:** The existing Three Mile Lane Planned Development has been established consistent with these Planned Development Policies, per Ordinance 4709. The 20 conditions included with the ordinance remain in effect, and are applicable to any development of the Subject Site.

**FINDING: SATISFIED.** City concurs with the applicant’s findings. The existing Planned Development overlay, approved by Ordinance No. 4709, is not specifically residential, but is of an underlying commercial zone that allows for some types of residential uses, including the proposed multiple family residential use. The proposed amendment to allow multiple family

residential use, along with the other commercial and senior living residential uses currently allowed by the existing Planned Development condition of approval, which would provide an opportunity for a wider variety of housing types than is currently allowed.

*Policy 73.00 Planned residential developments which offer a variety and mix of housing types and prices shall be encouraged.*

**APPLICANT'S RESPONSE:** The requested amendment will add opportunity for a wider variety of housing types, than is currently allowed. However, this general area is not identified by the City for lower density housing alternatives, just medium and high density residential, consistent with the C-3 zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would add that the existing Planned Development overlay, approved by Ordinance No. 4709, is not specifically residential, but is of an underlying commercial zone that allows for some types of residential uses, including the proposed multiple family residential use. The proposed amendment to allow multiple family residential use, along with the other commercial and senior living residential uses currently allowed by the existing Planned Development condition of approval, which would provide an opportunity for a wider variety of housing types than is currently allowed.

*Policy 74.00 Distinctive natural, topographic, and aesthetic features within planned developments shall be retained in all development designs.*

**APPLICANT'S RESPONSE:** There are no particularly distinctive natural, topographic or aesthetic features associated with the Subject Site. The property is relatively flat, and currently abuts farm land to the south, which is outside of the city limits and UGB.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

*Policy 75.00 Common open space in residential planned developments shall be designed to directly benefit the future residents of the developments. When the open space is not dedicated to or accepted by the City, a mechanism such as a homeowners association, assessment district, or escrow fund will be required to maintain the common area.*

**APPLICANT'S RESPONSE:** No specific development plans are being proposed at this time. However, the applicant is proposing an apartment complex. Their typical development plans provide landscaped open space for passive recreational use, and depending upon the specific target market, other amenities may be provided. Two examples of the applicant's most recent apartment projects have been provided to show how open space and amenities are typically provided.

Specific details regarding site amenities will be provided at the time of development review, assuming this requested PD amendment is approved. Such facilities or amenities are always located for convenient use of the tenants, as reflected in the site plan examples.

The applicant will maintain ownership and management of the facilities, with costs covered by rents. Therefore no HOA, assessment district or escrow funds are necessary to ensure proper maintenance.

**FINDING: SATISFIED WITH CONDITION #1.** City concurs with the applicant's findings. City would add that, while there are some opportunities for access to open space within one-half mile of the subject site, there is not any usable open space adjacent to the site. Not only is there no



open space adjacent to the site, there is a lack of connectivity to open space in the surrounding area. As a planned development, open space needs to be provided that provides a benefit to the residents of the site. Therefore, a condition of approval related to Policy 71.13 will require that, if the site is developed with multiple-family residential uses, an area equivalent to 10 percent of the gross size of the site shall be reserved for usable open space for residents of the multiple-family development site. The condition of approval states that the usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.

*Policy 76.00 Parks, recreation facilities, and community centers within planned developments shall be located in areas readily accessible to all occupants.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED WITH CONDITION #1.** While there some opportunities for access to open space within one-half mile of the subject site, there is not any usable open space adjacent to the site. Not only is there no open space adjacent to the site, there is a lack of connectivity to open space in the surrounding area. As a planned development, open space needs to be provided that provides a benefit to the residents of the site, and in areas readily accessible to all occupants. Therefore, a condition of approval related to Policy 71.13 will require that, if the site is developed with multiple-family residential uses, an area equivalent to 10 percent of the gross size of the site shall be reserved for usable open space for residents of the multiple-family development site. The condition of approval states that the usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped.

Further, the existing conditions of approval from Ordinance No. 4709 remain in effect. Conditions 2, 3, and 4 require site plan and landscape plan review and approval by the Planning Commission and Landscape Review Committee, respectively, prior to any development of the subject site. These reviews would ensure review of site design, on-site and off-site circulation, parking, and landscaping, which will allow an opportunity to review whether the required open space is located in an area readily accessible to all occupants.

*Policy 77.00 The internal traffic system in planned developments shall be designed to promote safe and efficient traffic flow and give full consideration to providing pedestrian and bicycle pathways.*

**APPLICANT'S RESPONSE:** Any development of the subject property will be required to meet these design criteria. So this policy will be equally implemented with or without the proposed amendment. Again, the two site plans provided show how site circulation is typically provided.

**FINDING: SATISFIED.** The existing conditions of approval from Ordinance No. 4709 remain in effect. Conditions 2, 3, and 4 require site plan and landscape plan review and approval by the Planning Commission and Landscape Review Committee, respectively, prior to any development of the subject site. These reviews would ensure review of site design, on-site and off-site circulation, parking, and landscaping, which will allow an opportunity to review whether the internal traffic system promotes safe and efficient traffic flow and gives consideration to providing pedestrian and bicycle pathways.

*Policy 78.00 Traffic systems within planned developments shall be designed to be compatible with the circulation patterns of adjoining properties.*

**APPLICANT'S RESPONSE:** Traffic system improvements have been developed in the immediate neighborhood, including the Highway 18 intersection with Norton Lane, and the SE Norton Lane intersection with SE Stratus Avenue. These improvements provide basic and adequate circulation into, out of, and throughout the immediate neighborhood. These street improvements were made to meet the anticipated development within the Three Mile Lane Planned Development area.

As reflected in Table 1 herein, and the accompanying Traffic Impact Report, the traffic impacts of the proposed multi-family development will be slightly more than those associated with senior housing, but will be considerably lower than traffic generated by allowed professional and medical offices, thereby maintaining consistency with this criterion.

**FINDING: SATISFIED.** The site is adjacent to SE Norton Lane, which is identified as a minor collector in the McMinnville Transportation System Plan. Section 17.53.101 of the McMinnville Municipal Code identifies the right-of-way width for minor collector streets as 56 feet (with no bike lane) or 66 feet (with bike lane). The existing right-of-way width of SE Norton Lane adjacent to the subject site is 71.47 feet, as shown on Partition Plat 99-47. Therefore, no additional right-of-way dedications are necessary. Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure the traffic system within planned developments will be compatible with adjoining properties. In addition, the applicant provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

#### *Residential Design Policies*

*Policy 79.00 The density allowed for residential developments shall be contingent on the zoning classification, the topographical features of the property, and the capacities and availability of public services including but not limited to sewer and water. Where densities are determined to be less than that allowed under the zoning classification, the allowed density shall be set through adopted clear and objective code standards enumerating the reason for the limitations, or shall be applied to the specific area through a planned development overlay. Densities greater than those allowed by the zoning classification may be allowed through the planned development process or where specifically provided in the zoning ordinance or by plan policy. (Ord. 4796, October 14, 2003)*

**APPLICANT'S RESPONSE:** The requested amendment to allow multi-family development, only amends Condition 15 of Ord. 4709, and does not alter the allowed density, which will remain consistent with the R-4 standards and the base C-3 zoning.

**FINDING: SATISFIED.** City concurs with the applicant's findings. The proposed amendment to the existing Planned Development condition would add multiple family residential as an allowed use. Multiple family residential is a permitted use in the underlying C-3 (General Commercial) zone, subject to the standards and provisions of the R-4 (Multiple Family Residential) zone. Therefore, the future development, if it includes multiple family residential dwelling units, will be subject to the density requirements of the R-4 zone.

Further, existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services.

#### *Urban Policies*

*Policy 99.00 An adequate level of urban services shall be provided prior to or concurrent with all proposed residential development, as specified in the acknowledged Public Facilities Plan. Services shall include, but not be limited to:*

- 1. Sanitary sewer collection and disposal lines. Adequate municipal waste treatment plant capacities must be available.*
- 2. Storm sewer and drainage facilities (as required).*
- 3. Streets within the development and providing access to the development, improved to city standards (as required).*
- 4. Municipal water distribution facilities and adequate water supplies (as determined by City Water and Light). (as amended by Ord. 4796, October 14, 2003)*
- 5. Deleted as per Ord. 4796, October 14, 2003.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

Specifically related to sanitary sewer service, the Engineering Department provided comments on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site.

**GOAL VI 1:** TO ENCOURAGE DEVELOPMENT OF A TRANSPORTATION SYSTEM THAT PROVIDES FOR THE COORDINATED MOVEMENT OF PEOPLE AND FREIGHT IN A SAFE AND EFFICIENT MANNER.

## Streets

*Policy 117.00 The City of McMinnville shall endeavor to insure that the roadway network provides safe and easy access to every parcel.*

*Policy 118.00 The City of McMinnville shall encourage development of roads that include the following design factors:*

- 1. Minimal adverse effects on, and advantageous utilization of, natural features of the land.*
- 2. Reduction in the amount of land necessary for streets with continuance of safety, maintenance, and convenience standards.*
- 4. Emphasis placed on existing and future needs of the area to be serviced. The function of the street and expected traffic volumes are important factors.*
- 5. Consideration given to Complete Streets, in consideration of all modes of transportation (public transit, private vehicle, bike, and foot paths). (Ord.4922, February 23, 2010)*

*Policy 119.00 The City of McMinnville shall encourage utilization of existing transportation corridors, wherever possible, before committing new lands.*

*Policy 120.00 The City of McMinnville may require limited and/or shared access points along major and minor arterials, in order to facilitate safe access flows.*

*Policy 121.00 The City of McMinnville shall discourage the direct access of small-scale residential developments onto major or minor arterial streets and major collector streets.*

**APPLICANT'S RESPONSE:** The City's transportation Goal and Policies are further refined and implemented through the Transportation System Plan (TSP). The TSP provides the functional classification of streets, including arterial, collector and local designations.

The TSP identifies corridor improvements for the Highway 18 Corridor:

*ODOT, the City of McMinnville and Yamhill County have mutually approved the Highway 18 Corridor Refinement Plan, which was completed in 1996. The Plan includes a series of traffic control and frontage roads improvements north and south of Highway 18, to include closing of the existing Norton Lane intersection, construction of a new interchange near the Evergreen Air Museum, and redesign of the current East McMinnville (Three Mile Lane) interchange for full, directional access.*

*Some frontage road improvements have been completed since the 1996 Plan was adopted. Exhibit 4-10 illustrates the current state of the Plan. It is important to note, however, that the northernmost collector access road depicted in the Highway 18 Corridor Refinement Plan is not shown in the TSP due to the fact that it is located outside of McMinnville's current urban growth boundary. Under Oregon Administrative Rule (OAR 660-012), transportation facilities outside of urban growth boundaries are not permitted as part of a TSP unless a "reasons" exception to the applicable goal(s) has been approved by the City. In this case, McMinnville finds such action premature due to the lack of certainty as to the street corridor's location and design. An*

*amendment to this plan, and a Goal 2 (Land Use) exception, would be part of any future proposal to add this element to the TSP and permit its construction and use for urban purposes.*

Some of the corridor improvements have already been completed, including the Norton Lane intersection and Stratus Avenue frontage road. The future interchange discussed is further to the east, at Cumulus Avenue. However, no decision regarding an interchange has yet been made.

The proposed amendment does not substantially alter the demand for transportation and public service improvement to any greater degree than the currently allowed uses.

Primary access to the property is from Salmon River Highway 18, designated as an arterial street. SE Norton Lane intersects with Highway 18 north of the subject property. The property fronts on the west side of SE Norton Lane, just south of the intersection with SE Stratus Avenue. Stratus Avenue is classified as a collector.

**FINDING: SATISFIED.** City concurs with the applicant's findings. City would clarify that the site is adjacent to SE Norton Lane, which is identified as a minor collector in the McMinnville Transportation System Plan. Section 17.53.101 of the McMinnville Municipal Code identifies the right-of-way width for minor collector streets as 56 feet (with no bike lane) or 66 feet (with bike lane). The existing right-of-way width of SE Norton Lane adjacent to the subject site is 71.47 feet, as shown on Partition Plat 99-47. Therefore, no additional right-of-way dedications are necessary.

Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

Further, Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure review of site design and on-site and off-site circulation to ensure that the proposed access to serve future development will be compatible with the surrounding transportation system.

*Policy 122.00 The City of McMinnville shall encourage the following provisions for each of the three functional road classifications.*

*2. Major, minor collectors.*

*-Designs should minimize impacts on existing neighborhoods.*

*-Sufficient street rights-of-way should be obtained prior to development of adjacent lands.*

*-On-street parking should be limited wherever necessary.*

*-Landscaping should be required along public rights-of-way. (Ord.4922, February 23, 2010)*

*-As far as is practical, residential collector streets should be no further than 1,800 feet apart in order to facilitate a grid pattern of collector streets in residential areas.*



**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

#### *Parking*

*Policy 126.00 The City of McMinnville shall continue to require adequate off-street parking and loading facilities for future developments and land use changes.*

*Policy 127.00 The City of McMinnville shall encourage the provision of off-street parking where possible, to better utilize existing and future roadways and rights-of-way as transportation routes.*

#### *Bike Paths*

*Policy 130.00 The City of McMinnville shall encourage implementation of the Bicycle System Plan that connects residential areas to activity areas such as the downtown core, areas of work, schools, community facilities, and recreation facilities. (Ord.4922, February 23, 2010)*

*Policy 131.00 The City of McMinnville shall encourage development of bicycle and footpaths in scenic and recreational areas as part of future parks and activities.*

*Policy 132.00 The City of McMinnville shall encourage development of subdivision designs that include bike and foot paths that interconnect neighborhoods and lead to schools, parks, and other activity areas. (Ord. 4922, February 23, 2010; Ord. 4260, August 2, 1983)*

*Policy 132.15 The City of McMinnville shall require that all new residential developments such as subdivisions, planned developments, apartments, and condominium complexes provide pedestrian connections with adjacent neighborhoods.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure review of site design, including off-street parking and bike and pedestrian connections associated with the future development plan.

#### *Supportive of General Land Use Plan Designations and Development Patterns*

*Policy 132.27.00 The provision of transportation facilities and services shall reflect and support the land use designations and development patterns identified in the McMinnville Comprehensive Plan. The design and implementation of transportation facilities and*

services shall be based on serving current and future travel demand—both short-term and long-term planned uses. (Ord. 4922, February 23, 2010)

**APPLICANT'S RESPONSE:** See response to Policies 117.00 – 121.00 above.

**FINDING: SATISFIED.** City concurs with the applicant's findings, along with the additional findings provided for Policies 117.00 – 121.00 above.

#### *Public Safety*

*Policy 132.32.00 The safe, rapid movement of fire, medical, and police vehicles shall be an integral part of the design and operation of the McMinnville transportation system. (Ord. 4922, February 23, 2010)*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will be completed to existing City standards, which are of a design and operation standard that allows for required movements for fire, medical, and police vehicles.

#### *Livability*

*Policy 132.35.00 Transportation facilities in the McMinnville planning area shall be, to the degree possible, designed and constructed to mitigate noise, energy consumption, and neighborhood disruption, and to encourage the use of public transit, bikeways, sidewalks, and walkways. (Ord. 4922, February 23, 2010)*

*Policy 132.41.20 Modal Balance – The improvement of roadway circulation must not impair the safe and efficient movement of pedestrians and bicycle traffic. (Ord. 4922, February 23, 2010)*

*Policy 132.41.25 Consolidate Access – Efforts should be made to consolidate access points to properties along major arterial, minor arterial, and collector roadways. (Ord. 4922, February 23, 2010)*

#### *Pedestrian Programs*

*Policy 132.54.00 Promoting Walking for Health and Community Livability – The City will encourage efforts that inform and promote the health, economic, and environmental benefits of walking for the individual and McMinnville community. Walking for travel and recreation should be encouraged to achieve a more healthful environment that reduces pollution and noise to foster a more livable community. (Ord. 4922, February 23, 2010)*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards.

Further, Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure review of site design and on-site and off-site circulation to ensure that the proposed access to serve future development will be compatible with the surrounding transportation system.

**GOAL VII 1:** TO PROVIDE NECESSARY PUBLIC AND PRIVATE FACILITIES AND UTILITIES AT LEVELS COMMENSURATE WITH URBAN DEVELOPMENT, EXTENDED IN A PHASED MANNER, AND PLANNED AND PROVIDED IN ADVANCE OF OR CONCURRENT WITH DEVELOPMENT, IN ORDER TO PROMOTE THE ORDERLY CONVERSION OF URBANIZABLE AND FUTURE URBANIZABLE LANDS TO URBAN LANDS WITHIN THE McMinnville Urban Growth Boundary.

#### *Sanitary Sewer System*

*Policy 136.00 The City of McMinnville shall insure that urban developments are connected to the municipal sewage system pursuant to applicable city, state, and federal regulations.*

*Policy 139.00 The City of McMinnville shall extend or allow extension of sanitary sewage collection lines within the framework outlined below:*

- 1. Sufficient municipal treatment plant capacities exist to handle maximum flows of effluents.*
- 2. Sufficient trunk and main line capacities remain to serve undeveloped land within the projected service areas of those lines.*
- 3. Public water service is extended or planned for extension to service the area at the proposed development densities by such time that sanitary sewer services are to be utilized.*
- 4. Extensions will implement applicable goals and policies of the comprehensive plan.*

#### *Storm Drainage*

*Policy 142.00 The City of McMinnville shall insure that adequate storm water drainage is provided in urban developments through review and approval of storm drainage systems, and through requirements for connection to the municipal storm drainage system, or to natural drainage ways, where required.*

*Policy 143.00 The City of McMinnville shall encourage the retention of natural drainage ways for storm water drainage.*

#### *Water System*

*Policy 144.00 The City of McMinnville, through McMinnville Water and Light, shall provide water services for development at urban densities within the McMinnville Urban Growth Boundary.*

**APPLICANT'S RESPONSE:** The surrounding development along SE Norton Lane has been constructed consistent with the Public Facilities Goal and these Policies for specific utilities. All required urban services are currently available and adequate to support development of the

property consistent with these policies, assuming that CH2M makes positive findings with their sewer system analysis, being paid for by the applicant.

The proposed amendment will not result in development that will create substantially greater demands on the available services than the types of development that is already allowed.

The City has confirmed that the sanitary sewer line has sufficient capacity to accommodate the proposed increase in density from senior housing to apartments. The net difference is 61 units.

**Revised Response from Applicant:** The applicant agreed to cover the cost of CH2M re-evaluating the available capacity of the sewer system serving the surrounding area, and accounting for the proposed increase in density, using 120 units for the proposed development

However, Mike Bisset, City Engineer, has informed us that the City has engaged CH2M to conduct a system-wide analysis, rather than just to Norton Lane area.

**FINDING: SATISFIED.** Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

Specifically related to sanitary sewer service, the Engineering Department provided comments on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site.

*Policy 145.00 The City of McMinnville, recognizing McMinnville Water and Light as the agency responsible for water system services, shall extend water services within the framework outlined below:*

- 1. Facilities are placed in locations and in such a manner as to insure compatibility with surrounding land uses.*
- 2. Extensions promote the development patterns and phasing envisioned in the McMinnville Comprehensive Plan.*
- 3. For urban level developments within McMinnville, sanitary sewers are extended or planned for extension at the proposed development densities by such time as the water services are to be utilized.*
- 4. Applicable policies for extending water services, as developed by the City Water and Light Commission, are adhered to.*

*Policy 147.00 The City of McMinnville shall continue to support coordination between city departments, other public and private agencies and utilities, and McMinnville Water and Light to insure*

*the coordinated provision of utilities to developing areas. The City shall also continue to coordinate with McMinnville Water and Light in making land use decisions.*

*Water and Sewer – Land Development Criteria*

*Policy 151.00 The City of McMinnville shall evaluate major land use decisions, including but not limited to urban growth boundary, comprehensive plan amendment, zone changes, and subdivisions using the criteria outlined below:*

- 1. Sufficient municipal water system supply, storage and distribution facilities, as determined by McMinnville Water and Light, are available or can be made available, to fulfill peak demands and insure fire flow requirements and to meet emergency situation needs.*
- 2. Sufficient municipal sewage system facilities, as determined by the City Public Works Department, are available, or can be made available, to collect, treat, and dispose of maximum flows of effluents.*
- 3. Sufficient water and sewer system personnel and resources, as determined by McMinnville Water and Light and the City, respectively, are available, or can be made available, for the maintenance and operation of the water and sewer systems.*
- 4. Federal, state, and local water and waste water quality standards can be adhered to.*
- 5. Applicable policies of McMinnville Water and Light and the City relating to water and sewer systems, respectively, are adhered to.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** An existing conditions of approval from Ordinance No. 4709, which remains in effect, will require that final development plans include the provision of water and power services (Condition 9). This will ensure that the density of the future development is shown to be able to be served by McMinnville Water and Light services. McMinnville Water and Light was provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

*Police and Fire Protection*

*Policy 153.00 The City shall continue coordination between the planning and fire departments in evaluating major land use decisions.*

*Policy 155.00 The ability of existing police and fire facilities and services to meet the needs of new service areas and populations shall be a criterion used in evaluating annexations, subdivision proposals, and other major land use decisions.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Emergency services departments were provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

*Parks and Recreation*

**GOAL VII 3:** TO PROVIDE PARKS AND RECREATION FACILITIES, OPEN SPACES, AND SCENIC AREAS FOR THE USE AND ENJOYMENT OF ALL CITIZENS OF THE COMMUNITY.

*Policy 163.00 The City of McMinnville shall continue to require land, or money in lieu of land, from new residential developments for the acquisition and/or development of parklands, natural areas, and open spaces.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Park fees shall be paid for each housing unit at the time of building permit application as required by McMinnville Ordinance 4282, as amended.

*Energy Conservation*

**GOAL VIII 1:** TO PROVIDE ADEQUATE ENERGY SUPPLIES, AND THE SYSTEMS NECESSARY TO DISTRIBUTE THAT ENERGY, TO SERVICE THE COMMUNITY AS IT EXPANDS.

*Energy Supply Distribution*

*Policy 173.00 The City of McMinnville shall coordinate with McMinnville Water and Light and the various private suppliers of energy in this area in making future land use decisions.*

*Policy 177.00 The City of McMinnville shall coordinate with natural gas utilities for the extension of transmission lines and the supplying of this energy resource.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** McMinnville Water and Light and Northwest Natural Gas were provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

**GOAL X 1:** TO PROVIDE OPPORTUNITIES FOR CITIZEN INVOLVEMENT IN THE LAND USE DECISION MAKING PROCESS ESTABLISHED BY THE CITY OF McMINNVILLE.

**GOAL X 2:** TO MAKE EVERY EFFORT TO ENGAGE AND INCLUDE A BROAD CROSS SECTION OF THE COMMUNITY BY MAINTAINING AN ACTIVE AND OPEN CITIZEN INVOLVEMENT PROGRAM THAT IS ACCESSIBLE TO ALL MEMBERS OF THE COMMUNITY AND ENGAGES THE COMMUNITY DURING DEVELOPMENT AND IMPLEMENTATION OF LAND USE POLICIES AND CODES.

*Policy 188.00 The City of McMinnville shall continue to provide opportunities for citizen involvement in all phases of the planning process. The opportunities will allow for review and comment by community residents and will be supplemented by the availability of information on planning requests and the provision of feedback mechanisms to evaluate decisions and keep citizens informed.*

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The process for a planned development amendment provides an opportunity for citizen involvement throughout the process through the neighborhood meeting provisions, the public notice, and the public hearing process. Throughout the process, there are



opportunities for the public to review and obtain copies of the application materials and the completed City report prior to the advertised public hearing(s). All members of the public have access to provide testimony and ask questions during the public review and hearing process.

### **McMinnville Zoning Ordinance**

The following Sections of the McMinnville Zoning Ordinance (Ord. No. 3380) provide criteria applicable to the request:

#### **Chapter 17.03. General Provisions**

**17.03.020 Purpose.** The purpose of this ordinance is to encourage appropriate and orderly physical development in the City through standards designed to protect residential, commercial, industrial, and civic areas from the intrusions of incompatible uses; to provide opportunities for establishments to concentrate for efficient operation in mutually beneficial relationship to each other and to shared services; to provide adequate open space, desired levels of population densities, workable relationships between land uses and the transportation system, and adequate community facilities; to provide assurance of opportunities for effective utilization of the land resource; and to promote in other ways public health, safety, convenience, and general welfare.

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The purpose of the Zoning Ordinance is met by the proposal as described in the Conclusionary Findings contained in this Decision Document.

#### **Chapter 17.06. Definitions**

##### **17.06.015 General Definitions**

**Apartment House** – See "Dwelling, Multifamily."

**Assisted Living** – A living arrangement where the elderly or other persons are provided assistance with daily activities such as dressing, grooming and bathing.

**Condominium** – Ownership of a single unit in a multi-unit structure that includes common areas and facilities; includes residential, commercial, and industrial condominiums and regulated, in part, by Oregon State Law (ORS Chapter 100). (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Dwelling, Multi-Family** – A building containing three or more dwelling units. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Dwelling Unit** – A residence containing one or more rooms designed for occupancy by one family and having not more than one cooking facility. This includes both buildings constructed on-site and off-site, such as manufactured homes. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Family** – For the purpose of this Zoning Ordinance, "family" refers to:  
An individual or two or more persons related by blood, marriage, adoption, or legal guardianship, or other duly-authorized custodial relationship, living together as one housekeeping unit using one kitchen, and providing means of lodging to not more than two additional persons, excluding servants, or a group of not more than five unrelated persons, excluding servants, living together as one housekeeping unit using one kitchen. (Ord. 4988 §1, 2015; Ord. 4479A §1, 1991; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**APPLICANT'S RESPONSE:** A general definition of senior housing is as follows (factfinder2.census.gov):

**Senior housing definition**

*Senior Housing Definition. Senior housing is housing that is suitable for the needs of an aging population. It ranges from independent living to 24-hour care. In senior housing there is an emphasis on safety, accessibility, adaptability, and longevity that many conventional housing options may lack.*

Relative to the proposed amendment, there are a few general factors that affect site development and potential impacts when comparing senior housing to general multi-family housing, as follows:

- Senior housing typically provides smaller 1 and 2 bedroom units, while multi-family facilities may include 3 or more bedroom units in the mix.
- Seniors (retired or assisted living) tend to have fewer cars, relying upon public transit or family to making necessary trips. Those seniors that do still drive tend to make fewer trips than younger tenants, particularly families with children and/or two working adults. Consequently senior housing typically has a lower demand for off-street parking, and tends to generate lower trip volumes, both daily and during peak hours, as reflected in Table 1 above [See Table 1 from Traffic Impact Report].
- The lower parking demand for senior housing, combined with smaller units, tends to yield a slightly higher density than general multi-family apartments.

While multi-family development will result in slightly higher trip generation, than senior housing, it is important to recognize that the office commercial uses allowed under the C-3PD zoning generally have higher trip generation than either form of housing.

**FINDING: SATISFIED.** City concurs with the applicant's clarification that senior condominiums or senior apartments, which are currently listed as allowed uses in Condition 15, are not specifically defined in the Section 17.06.015 of the McMinnville Zoning Ordinance. Section 17.60.060(A)(4) of the McMinnville Zoning Ordinance does regulate off-street parking differently for dwelling units that are "expressly reserved for senior or handicapped persons". However, the same development standards, including setbacks, density, and height, would apply to the development of apartments on the site, whether they were age-restricted or not.

The use that is added to the amended Condition 15 of Ordinance 4709 is "multiple-family dwelling" because that is the specific use that is listed as permitted in the C-3 (General Commercial) zone in Section 17.33.010 of the McMinnville Zoning Ordinance.

## **Chapter 17.21. R-4 Multiple-Family Residential Zone**

**17.21.030 Lot size.** In an R-4 zone, the lot size shall not be less than five thousand square feet, except that the lot area for common wall, single-family lots shall not be less than two thousand five hundred square feet per family. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**17.21.040 Yard requirements.** In an R-4 zone, each lot shall have yards of the following size unless otherwise provided for in Section 17.54.050:

- A. A front yard shall not be less than fifteen feet;
- B. A side yard shall not be less than six feet, except an exterior side yard shall not be less than fifteen feet;

- C. A rear yard shall not be less than twenty feet;
- D. Whether attached to a residence or as a separate building, a covered storage facility for a vehicle on which the main opening is toward a street shall be located not less than twenty feet to the property line bordering the street;
- E. All yards shall be increased, over the requirements of this section, one foot for each two feet of building height over thirty-five feet. (Ord. 4912 §3, 2009; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**17.21.050 Building height.** In an R-4 zone, a building shall not exceed sixty feet in height. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**17.21.060 Density requirements.** In an R-4 zone, the lot area per family shall not be less than fifteen hundred square feet for each unit with two bedrooms or less, and not less than seventeen hundred fifty square feet for each unit with three bedrooms, and an additional five hundred square feet for each additional bedroom in excess of three in any one unit. The above requirements may be waived if the provisions of Section 17.21.020(M) are utilized. (Ord. 4796 §1(b), 2003; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**APPLICANT’S RESPONSE:** The base C-3 zone allows for multi-family apartments as a permitted use, subject to the R-4 zoning standards and density, per Chapter 17.21.

Under Ordinance 4863, senior housing, including apartments and condominiums area an allowed use. However, the City’s code does not specifically define senior housing, differentiated from multi-family apartments, other than assisted living, per Chapter 17.06.015.

No specific site development plan is proposed at this time. Any housing development of the Subject Property will be subject to the R-4 zoning provisions, including density, and will be subject to Site Plan review for compliance with these standards.

**FINDING: SATISFIED.** City concurs with the applicant’s clarification that senior condominiums or senior apartments, which are currently listed as allowed uses in Condition 15, are not specifically defined in the Section 17.06.015 of the McMinnville Zoning Ordinance. Therefore, the same development standards, including setbacks, density, and height, would apply to the development of apartments on the site, whether they were age-restricted or not. Conditions 2 and 3 of Ordinance 4709 require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure that any future development of multiple family residential uses would meet the necessary R-4 development standards.

### **Chapter 17.33. C-3 General Commercial Zone**

**17.33.010 Permitted Uses.** In a C-3 zone, the following uses and their accessory uses are permitted:

- 3. Multiple-family dwelling subject to the provisions of the R-4 zone; [..]

**APPLICANT’S RESPONSE:** The property is zoned C-3 PD. The underlying C-3 zone allows multi-family development, per 17.33.010.3, consistent with the R-4 standards.

There is residential R-4 land immediately to the west of the subject site, so there is a basis for concluding that R-4 residential is compatible with the medical commercial designation for the remainder of the Planned Area.

The requested change will not alter the current density allocated to the property. It will remain as is consistent with the R-4 standards, as set forth below.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

#### **Chapter 17.51. Planned Development Overlay**

**17.51.010 Purpose.** The purpose of a planned development is to provide greater flexibility and greater freedom of design in the development of land than may be possible under strict interpretation of the provisions of the zoning ordinance. Further, the purpose of a planned development is to encourage a variety in the development pattern of the community; encourage mixed uses in a planned area; encourage developers to use a creative approach and apply new technology in land development; preserve significant man-made and natural features; facilitate a desirable aesthetic and efficient use of open space; and create public and private common open spaces. A planned development is not intended to be simply a guise to circumvent the intent of the zoning ordinance.

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The proposed amendment to allow multiple family residential use on the subject site would provide for greater flexibility and greater freedom of design in the development of the subject site than is currently allowed by the existing condition of approval. The proposed amendment to add multiple family residential as an allowed use would not remove any ability to develop other uses on the subject site. Currently, and after the proposed amendment, professional office, medical office, senior condominiums, senior apartments, assisted living facilities, or other compatible, small-scale commercial uses such as a delicatessen, florist, or day care facility, would still be allowed on the subject site under the revised condition of approval that was approved by Ordinance No. 4863, along with multiple family residential. This increases the opportunity for mixed uses in the planned development area. The proposed amendment to allow multiple family residential use on the subject site is also not a guise to circumvent the intent of the zoning ordinance, because multiple family residential is a permitted use in the existing, underlying C-3 (General Commercial) zoning district.

**17.51.020 Standards and requirements.** The following standards and requirements shall govern the application of a planned development in a zone in which it is permitted:

- A. The principal use of land in a planned development shall reflect the type of use indicated on the comprehensive plan or zoning map for the area. Accessory uses within the development may include uses permitted in any zone, except uses permitted only in the M-2 zone are excluded from all other zones. Accessory uses shall not occupy more than twenty-five percent of the lot area of the principal use;
- B. Density for residential planned development shall be determined by the underlying zone designations. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** The proposed amendment to allow multiple family residential use on the subject site will not be inconsistent with the comprehensive plan or zoning designation of the subject site, because multiple family residential is a permitted use in the existing, underlying C-3 (General Commercial) zoning district. The density of any eventual residential development will be determined by the existing regulations in the underlying C-3 zone.

**17.51.030 Procedure.** The following procedures shall be observed when a planned development proposal is submitted for consideration:

- A. The Commission shall consider the preliminary development plan at a meeting at which time the findings of persons reviewing the proposal shall also be considered. In reviewing the plan, the Commission shall need to determine that:
  - a. There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;
  - b. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;
  - c. The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;
  - d. The plan can be completed within a reasonable period of time;
  - e. The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;
  - f. Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;
  - g. The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole;

**APPLICANT'S RESPONSE:** None.

**FINDING: SATISFIED.** Section 17.51.030 is satisfied in that the Commission will have an opportunity to review a future development plan, per Conditions 2 and 3 of Ordinance 4709, which require site plan review and approval by the Planning Commission prior to any development of the subject site. These reviews would ensure that any future development of multiple family residential uses would meet the necessary R-4 development standards. Discussion of the criteria listed in subsection C is provided below, as those criteria are the review criteria for a Planned Development Amendment, as found in 17.74.070 of the Zoning Ordinance.

**17.74.070. Planned Development Amendment – Review Criteria.** An amendment to an existing planned development may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Major changes to an adopted site plan shall be processed in accordance with Section 17.72.120, and include the following:

- An increase in the amount of land within the subject site;
- An increase in density including the number of housing units;
- A reduction in the amount of open space; or
- Changes to the vehicular system which results in a significant change to the location of streets, shared driveways, parking areas and access.

**APPLICANT'S RESPONSE:** While none of the listed changes are specifically applicable, the Planning Director has determined that the proposed PD amendment, which changes allowed uses, is considered to be a Major amendment and is therefore subject to Section 17.72.120, as addressed herein.

**FINDING: SATISFIED.** The applicant has provided a Planned Development Amendment request to be reviewed by the Planning Commission in accordance with Section 17.72.120.

**17.74.070. Planned Development Amendment – Review Criteria.** [...] An amendment to an existing planned development may be authorized, provided that the proposal satisfies all relevant requirements of this ordinance, and also provided that the applicant demonstrates the following:

**17.74.070(A).** *There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;*

**APPLICANT'S RESPONSE:** The applicants desire to develop multi-family apartments. While senior housing remains an appropriate use, the applicant submits that the need for housing alternatives, within the Planned Development, is greater than just the narrow segment currently allowed for senior housing.

While there is a general modest trend of an aging population, the median age for Yamhill County, including McMinnville is 37.5 (2015). This is slightly higher than the state in general. Consequently the applicant submits that there is no greater demand for senior housing over general housing or non-age restricted apartments.

McMinnville's building permit activity over the past couple of years reflects this trend with similar permit activity rates for both single and multi-family residential construction,

#### Historic Perspective

Based on a review of the historic records associated with the Three Mile Lane Planned Development it appears that the City's intent was initially to limit development to professional and medical offices, or other compatible commercial uses. General commercial and retail were not allowed with the intent to avoid creating strip commercial development in this area.

The current development within this Planned Development is predominantly medical facilities and professional offices, as originally targeted. However a motel and restaurant have also been developed, considered as compatible uses.

The 2006 amendment (Ord 4683) allowed for various types of senior housing as permitted uses. This amendment was based on findings that this type of housing was compatible with the developed properties and other allowed uses under Ordinance 4709, in particular the compatibility of supportive native of medical services. The decision was also based on consideration that traffic impacts from senior housing would generally be lower than those created by professional or medical offices.

While the prior amendment allowed for senior housing, the decision does not appear to have been made based on any specific findings of need for this particular housing type relative to types of housing. Rather, the decision was based more on consistency with the underlying C-3 zone, and compatibility with the allowed professional and medical office uses.

However, since the amendment, the anticipated senior housing has not come to fruition, at least on the subject site. Consequently the land has remained vacant. While the subject site was not actually developed for the anticipated senior housing, the need for such housing has been met, in part, by other projects developed in the vicinity northeast of the site. These developments include:

1. Fircrest Community, assisted living and retirements apartments; and
2. Parkland Village, assisted living and retirements apartments; and
3. The Virginia Garcia Memorial Health Center has also been developed in close proximity to these senior housing projects. This facility provides various senior medical services.

#### Continued Need for Housing – General Multi-family



The City's 2001 Housing Needs Analysis projected growth from 2000 to 2020 to increase from 25,153 to 38,720. It is noted that the City has not completed an update of the Needs Analysis, but is working on an update. Consequently the 2001 projections of need are still considered valid.

The 2001 projects identified a need for both single family and multi-family units. City staff confirmed that the projected need remains unfulfilled. For example, staff noted that local business, such as the Medical Center, Chemeketa Community College, and others have identified a need for apartments for transitional housing for new employees and students. Generally people moving to McMinnville continue to create a need for more housing. This growth is consistent with the 2001 projects, just delayed due to the economic recessions over the past 18 years.

Based on data from Portland State University's Population Research Center (PRC) the City's population in 2016 was 33,405, below the 2020 projection. The PRC's most current data shows McMinnville's 2017 population at 34,293, but still below the 2020 projection.

The slower than projected growth is primarily attributed to the economic recessions of in the 1990s and the Great Recession 2008-2012. However, in-migration has also affected the County's and City's population, as noted by PSU research.

Since 2012, the end of the "Great Recession," the economy has shown considerable strength. Given the current continuing strong economy, growth is expected to increase over the rates of the previous 10 years, more on track with the projections prior to the recessions. The Center's updated 2035 projection is for McMinnville to grow to a population of 44,122.

*Portland State University's Population Research Center (PRC)*

*"Yamhill County's total population is forecast to grow by a little more than 70,000 persons from 2017 to 2067, which translates into a total countywide population of 177,170 in 2067 (Figure 15). The population is forecast to grow at the highest rate—just below one and a half percent per year—in the near-term (2017-2025). This anticipated population growth in the near-term is based on three core assumptions: (1) Yamhill County's economy will continue to strengthen in the next 10 years; (2) middle-aged persons will continue migrating into the county—bringing their families or having more children; and (3) empty nesters and retirees will continue migrating into the county, thus increasing deaths. The largest component of growth in this initial period is net in-migration. Over 1,300 more births than deaths are forecast for the 2017 to 2025 period. At the same time roughly 13,000 net in-migrants are also forecast, combining with a diminishing natural increase for continued population growth.*

*Yamhill County's two largest UGBs—McMinnville and Newberg—are forecast to experience a combined population growth of nearly 20,000 from 2017 to 2035 and nearly 37,000 from 2035 to 2067 (Figure 16). McMinnville is expected to increase by 9,829 persons from 2017 to 2035 (1.4% AAGR), growing from a total population of 34,293 in 2017 to 44,122 in 2035. Newberg's population is expected to increase at a slightly faster rate (1.9% AAGR), growing from 24,296 persons in 2017 to 34,021 in 2035. McMinnville and Newberg are forecast to grow more slowly during the second part of the forecast period at 1.1 and 1.3 percent, respectively. We expect both sub-areas to capture increasing shares of the county's total population."*

*Portland State University's Population Research Center (PRC) released the preliminary 2016 population estimates for Oregon and its cities and counties on November 15.*

*According to the preliminary July 1 population estimates, Oregon's population increased from 4,013,845 in 2015 to 4,076,350 in 2016, or by 62,505. This increase represents a 1.6 percent change, slightly higher than in the previous year (1.3 percent). The increase in 2016 is over 11,000 higher than added in 2015, and surpasses the peak pre-recession growth of 58,000 in 2006 and it is closer to growth experienced in the early and mid-1990s.*

*Population growth consists of two factors: natural increase (the number of births minus the number of deaths) and net migration (movers-in minus movers-out). From 2015 to 2016 net migration accounted for roughly 83 percent of Oregon's population growth.*

*Generally, net in-migration has either boosted population growth around the state or has prevented population losses. In counties where a natural decrease is occurring (over a third of the counties experience a natural decrease, meaning there are more deaths than births), net immigration has offset overall population decreases. Net in-migration this past year is estimated to have accelerated in most counties statewide from last year.*

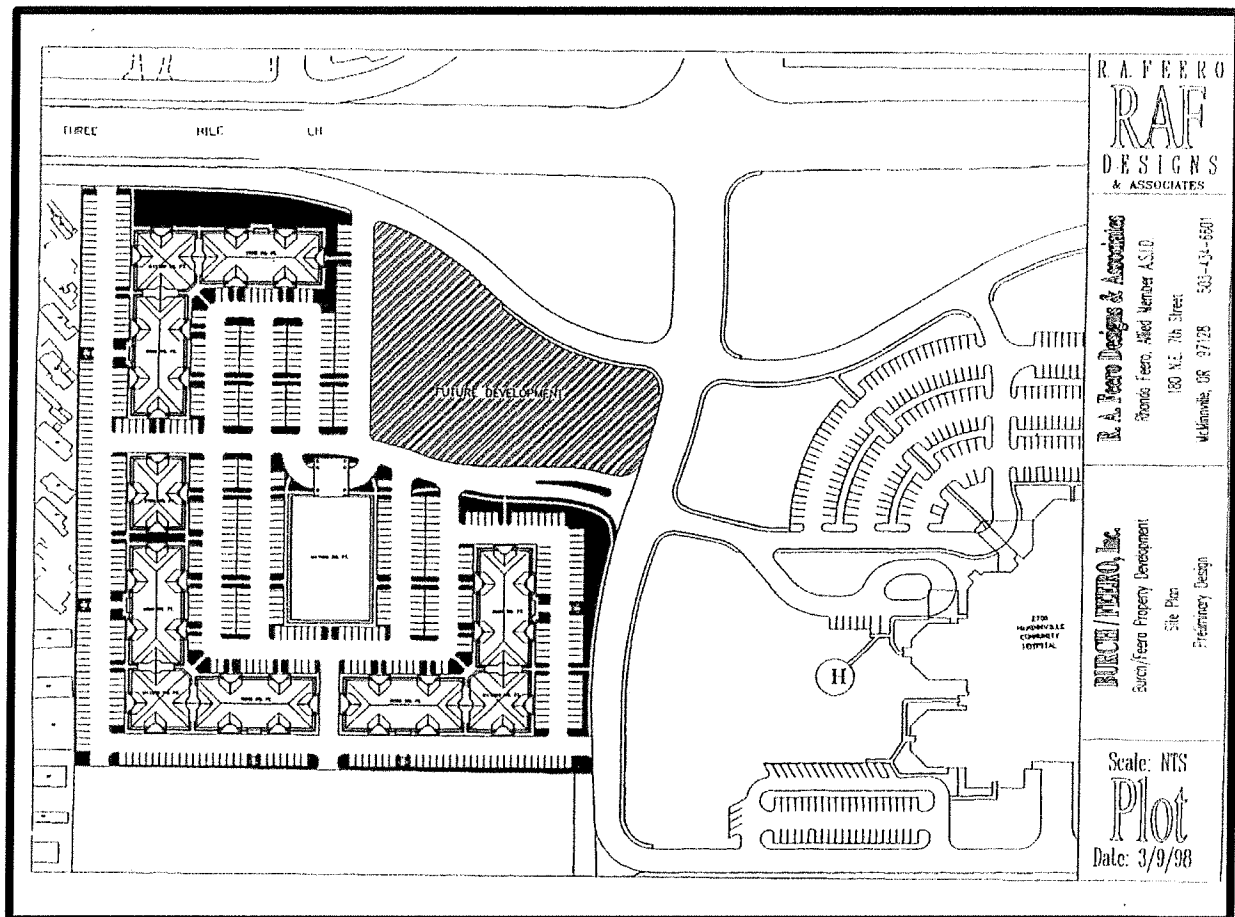
Vacancy rates in 1990 were 2.2% for single family and 3.7% for multi-family. The 2020 projections were based on 2.5% for single family and 5.0% for multi-family. PRC indicates the current vacancy rate is 3.32% for multi-family, which is below the earlier projected rate, thereby indicating a need for more multi-family housing.

For McMinnville currently 40.7% of households are renters. Generally, statewide, there is a trend for renter ship moving towards an even split 50/50 with home ownership. The City's building permit activity for multi-family development has followed a similar pattern as single family over the past couple of years, which is consistent with this trend towards a 50/50 split.

**FINDING: SATISFIED.** City would generally concur with the applicant's findings, but does provide some clarifying points and additional findings below.

The most recently acknowledged Residential Buildable Lands Inventory, which was prepared in 2001, that identified a need for additional land for housing uses. That inventory, which was titled the McMinnville Buildable Land Needs Analysis and Growth Management Plan, identified a deficit of over 1,000 acres of land for housing in Table B-11 of Appendix B. More specifically, the analysis identified a need of 162 acres of R-4 (higher density) zoned land. The City of McMinnville is currently updating its Residential Buildable Lands Inventory and Housing Needs Analysis, and while these reports are not finalized or acknowledged, the analyses are identifying a deficit of land zoned for residential dwelling units. The proposed amendment to allow multiple family residential on the subject site would not increase the amount of land designated specifically for higher density residential use, but it would increase the potential for the development of higher density residential uses on a property with an existing, underlying zone that already permits multiple family residential use (C-3 General Commercial zone).

In terms of compatibility with other uses allowed on the same site and the surrounding area, it is important to note that the subject site is currently regulated by a Planned Development (Ordinance 4709, as amended by Ordinance 4863), but other land immediately adjacent to the subject site is regulated by another Planned Development. The area to the north is subject to a Planned Development (Ordinance No. 4667) that is similar to the subject site, in that there was a limitation of uses on the site to office and medical uses and other compatible uses. The approved development plan for that site is identified below:



Only portions of that approved development plan have been constructed, including the portions of the site closest to SE Norton Lane. The original Planned Development (Ordinance No. 4667) was amended to allow for the development of other compatible uses, including the existing hotel and restaurant located in the area of the originally approved development plan that was identified as "Future Development". The most recent Planned Development amendment (Ordinance No. 4937) actually amended the allowable uses on the site to be any use allowed in the C-3 zone, other than a few storage and auto-oriented uses that were still listed as being prohibited. The commercial Planned Development area to the north, and the buildings that have been constructed, can be seen below:



This commercial development to the north of the subject site will serve as a buffer from Highway 18, which is identified in the McMinnville Transportation System Plan as a major arterial roadway, for any future potential development of a multiple family residential use on the subject site. Also, the proposed amendment to allow multiple family residential use on the subject site will provide an opportunity for more of a variety and mix of uses in the Planned Development areas along SE Norton Lane.

**17.74.070(B).** *Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;*

**APPLICANT'S RESPONSE:** Adding apartments to the list of allowed uses will not be inconsistent with the Comprehensive Plan, including general Goals, Policies and Objectives. This decision will be consistent with the prior amendment, which allowed limited residential uses.

[Note – the applicant also provided findings and responses to Comprehensive Plan goals and policies, which are provided above.]

**FINDING: SATISFIED.** City concurs with the applicant's findings. The proposed Planned Development Amendment would not be inconsistent with the applicable goals and policies of the Comprehensive Plan, as described in more detail above in the specific findings for each Comprehensive Plan goal and policy.

**17.74.070(C).** *The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;*

**APPLICANT'S RESPONSE:** Access to the property is from Salmon River Highway 18 and south on SE Norton Lane. The property fronts on the west side of SE Norton Lane, just south of the intersection with the SE Stratus Avenue Loop. The property abuts the city limits and UGB to the south, but any site development, regardless of use, will extend the street improvements to the city limits, or as determined by the City.

The proposed amendment does not substantially alter the demand for transportation and public service improvement to any greater degree than the currently allowed uses.

The existing right-of-way for SE Norton Lane is 60 feet down to SE Stratus Avenue, which is the extent of current improvements. Site development of the subject site will complete improvements across the site frontage.

The full range of urban services is currently available to the property, thereby ensuring timely and efficient provision of services. Utilities will be extended as required to the adjoining property, which to the south is outside of the city limits.

**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards.

**17.74.070(D).** *The plan can be completed within a reasonable period of time;*

**APPLICANT'S RESPONSE:** The applicant intends to proceed with development review and site development as quickly as possible, assuming this PD amendment is approved. Full site development would be expected to be completed by 2020-2021.

**FINDING: SATISFIED.** City concurs with the applicant's findings.

**17.74.070(E).** *The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;*

**APPLICANT'S RESPONSE:** The existing streets serving the property are designed and constructed to City standards, with adequate capacity, based on the C-3 zoning. The accompanying Traffic Impact Report, prepared by Lancaster Engineering compares the multi-family traffic impacts to those of the allowed commercial offices and senior housing.

[See Table 1 in Traffic Impact Report]

The proposed multi-family residential use will actually generate significantly fewer total trips and peak hour trips than originally anticipated medical office development. The proposed apartment will, however, generate slightly more trips than senior housing, which is currently allowed.

Therefore the requested amendment is not expected to result in any significant increase in new traffic demands, as compared to the current allowed uses.



**FINDING: SATISFIED.** Conditions 18 and 20 of Ordinance 4709 will require right-of-way improvements to City standards on the site's SE Norton Lane frontage at the time of development. These required right-of-way improvements will ensure that adequate access will be provided to the subject site, and the improvements will be completed to existing City standards. At the time of development, all necessary permits will be required to be obtained by Oregon Department of Transportation (ODOT), as Norton Lane is a facility under ODOT jurisdiction. Depending on intensity of development and specific development type, ODOT permitting and review may require additional analysis of the eventual proposed use's impact on the surrounding transportation network.

The applicant has also provided a traffic impact analysis report that analyzed the trip generation of the proposed multiple family residential use against other uses currently allowed under the existing Planned Development conditions of approval. The analysis determined that some of the existing uses, specifically medical office, could generate more daily trips than the proposed multiple family residential use. Therefore, the use allowed by the proposed amendment would not be more detrimental to the surrounding street network or transportation system than other uses currently allowed on the subject site by Ordinance No. 4709 and Ordinance No. 4863.

**17.74.070(F).** *Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;*

**APPLICANT'S RESPONSE:** The full range of urban services is currently available to the property, thereby ensuring timely and efficient provision of services. Utilities will be extended as required to the adjoining properties. However, land to the south is outside of the city limits and UGB.

Utilities requirements for the proposed residential development will be generally consistent with the demands created by the current allowed senior housing. Therefore this amendment will not substantially alter the demand for urban services.

**FINDING: SATISFIED.** Existing conditions of approval from Ordinance No. 4709, which remain in effect, will require that final development plans include a detailed storm drainage plan (Condition 5), a detailed sanitary sewer collection plan (Condition 6), provision of water and power services (Condition 9), and right-of-way improvements (Conditions 4, 18, and 20). This will ensure that the density of the future development is shown to be able to be served by public services.

Specifically related to sanitary sewer service, the Engineering Department provided comments on the proposed Planned Development Amendment, and noted that a recently completed analysis of the sanitary sewer conveyance system indicated that there is adequate system capacity to support the proposed amendment and eventual development of multiple family residential use on the site. In addition, McMinnville Water and Light and other utility providers were provided an opportunity to review the proposal, and offered no comments or objections to the proposed amendment to the allowable uses on the subject site.

**17.74.070(G).** *The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.*

**APPLICANT'S RESPONSE:** The noise, air, and water pollutants caused by the multi-family development allowed by this amendment will not result in any substantial change as compared to the current list of allowed uses, in particular the senior housing, which is substantially similar in potential impacts.



**FINDING: SATISFIED.** City concurs with the applicant's findings.

CD

**SEE TIA ATTACHMENT TO PDA 5-23**

## **TRAFFIC IMPACT ANALYSIS**

**7**

**APPLICATION DRAWINGS**  
ARCHITECTURAL,  
CIVIL AND LANDSCAPE

**8**



**PROJECT DATA**

**SITE INFORMATION:**  
 ZONING: C3 - GENERAL COMMERCIAL, THREE-MILE LANE PLANNED DEVELOPMENT OVERLAY  
 MAP NO: T4SR4W-27, TAX LOT 701  
 SITE AREA: 214,759 SF, 4.93 ACRES

**BUILDING INFORMATION**  
 CONSTRUCTION TYPE: TYPE V-B  
 BUILDING HEIGHT: THREE-STORIES  
 OCCUPANCY GROUPS: R-2 RESIDENTIAL, APARTMENTS

**LIVING UNITS:**  
 ONE-BEDROOM, 1 BATH: 42 UNITS  
 TWO-BEDROOM, 1 BATH: 36 UNITS  
 TWO-BEDROOM, 2 BATH: 42 UNITS  
 THREE-BEDROOM, 2 BATH: 18 UNITS  
 TOTAL UNITS: 138 UNITS

**PARKING:**  
 REQUIRED:  
 1BR AND 2BR UNITS, 1.5 SPACES/UNIT= 180 SPACES  
 3BR UNITS, 2 SPACES/UNIT= 36 SPACES  
 TOTAL REQUIRED: 216 SPACES

**PROVIDED:**  
 STANDARD SPACES: 188 SPACES  
 COMPACT SPACES: 22 SPACES  
 ACCESSIBLE SPACES: 8 SPACES  
 TOTAL PARKING PROVIDED: 218 SPACES

**PROJECT TEAM**

**OWNER/DEVELOPER:**  
 KWDS, LLC  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**GENERAL CONTRACTOR:**  
 KOHL INC.  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**ARCHITECT/PLANNER:**  
 REITER DESIGN ARCHITECT, INC.  
 7965 SW CIRRIUS DRIVE  
 BEAVERTON, OREGON 97008  
 (503) 574-3036  
 PROJECT ARCHITECT: SCOTT REITER

**CIVIL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 250 NW FRANKLIN AVE., SUITE 404  
 BEND, OREGON 97703  
 (541) 318-1161  
 PROJECT ENG.: JENNIFER VAN CAMP, PE

**STRUCTURAL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT ENGINEER: JEFF SCHWINDT, PE

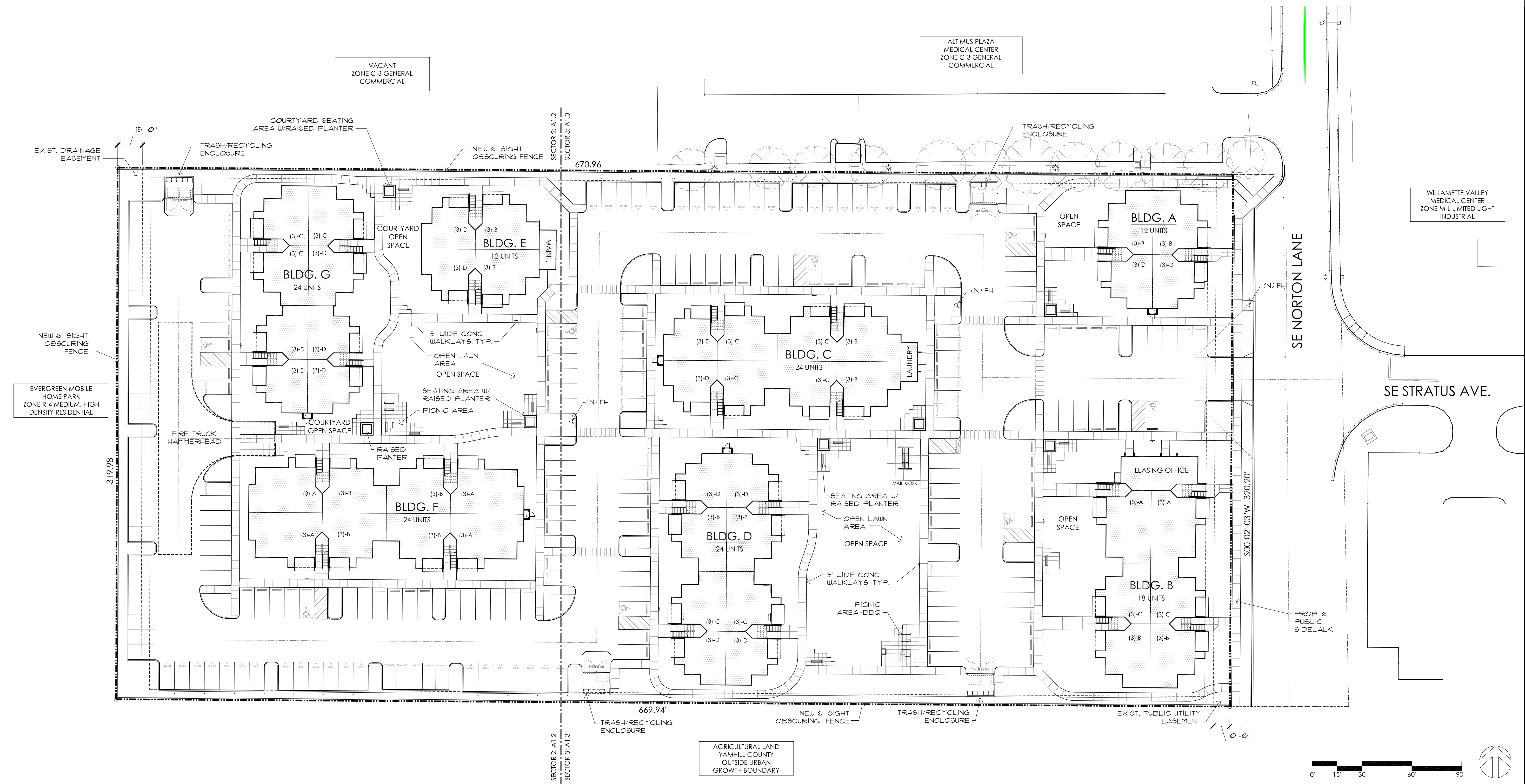
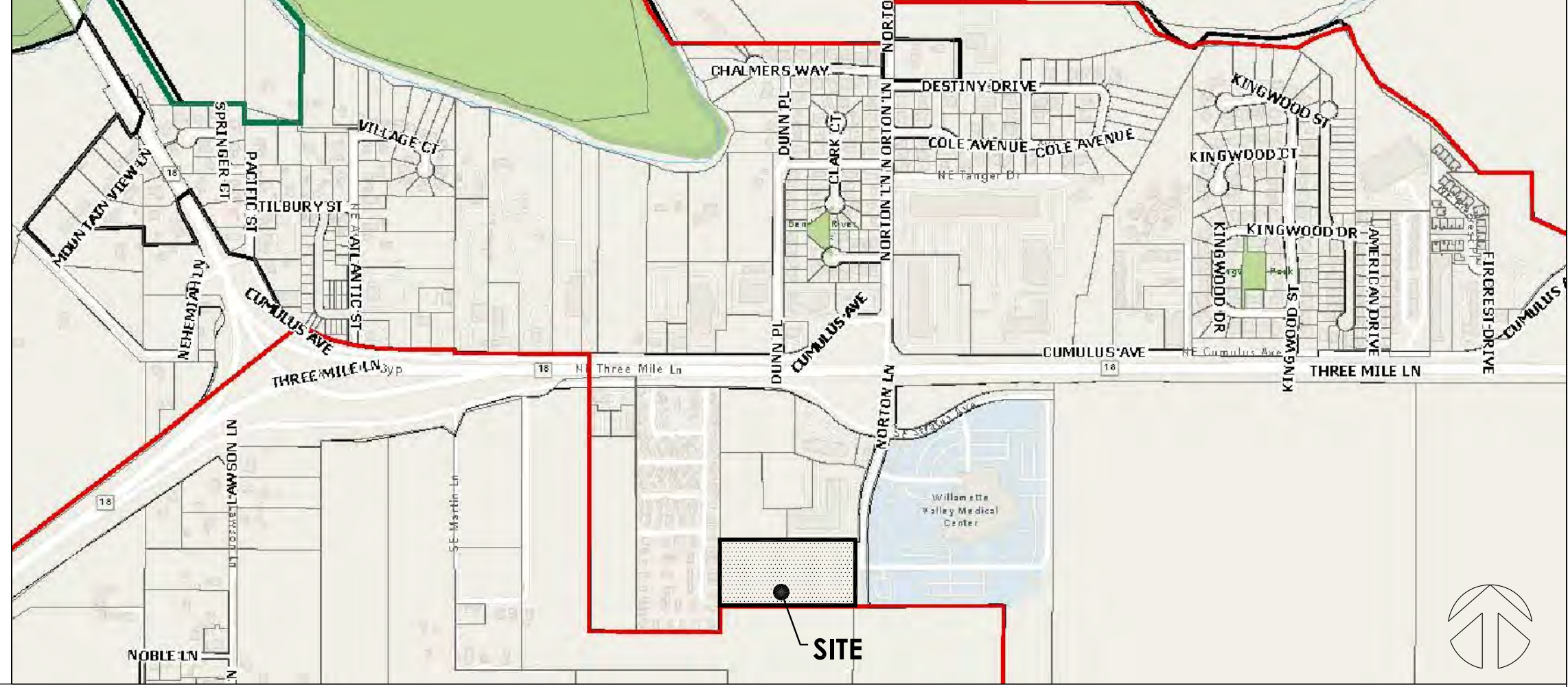
**TRANSPORTATION ENGINEER:**  
 LANCASTER-MOBY  
 321 SW 4TH, AVE, SUITE 400  
 PORTLAND, OREGON 97204  
 (503) 248-0313  
 CONTACT: DANIEL STUMPF

**LANDSCAPE ARCHITECT:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT LANDSCAPE ARCH.: JEFFERY CREEL

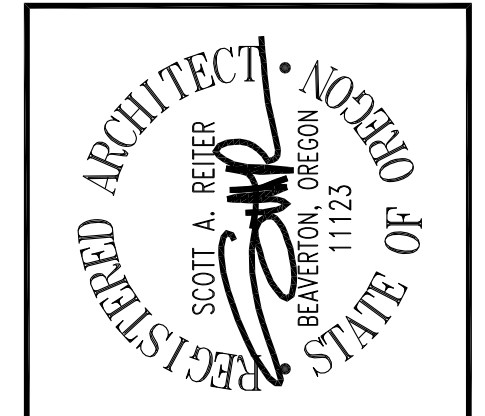
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**VICINITY MAP**



**CONCEPTUAL SITE PLAN** 1  
 Scale: 1" = 30'-0" A1.0



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3036

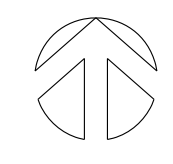
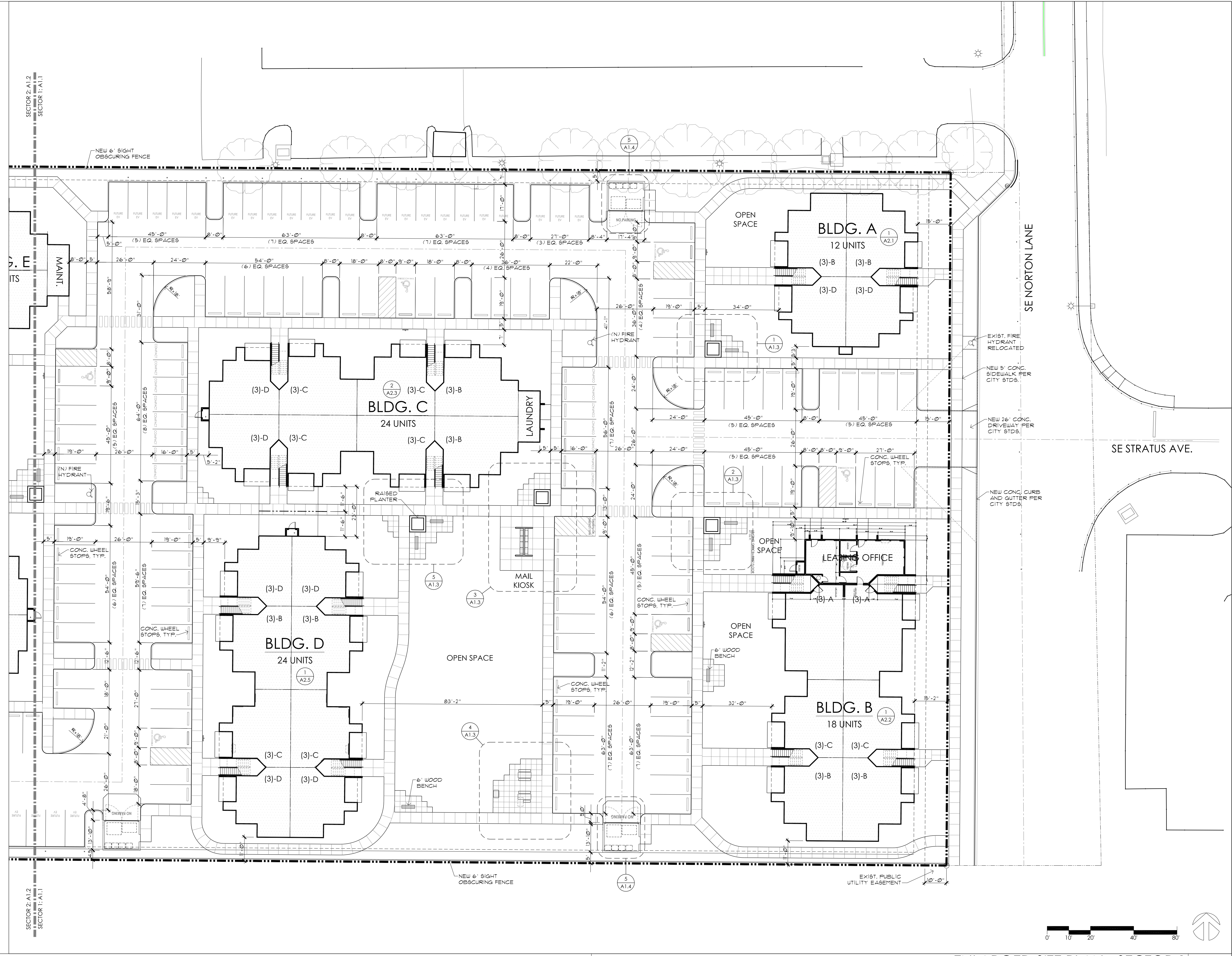
**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER: KWDS, LLC  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

CONCEPTUAL SITE PLAN

|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
| drawn:   |               |
| job no.: | 2301          |

**A**  
**1.0**





ENLARGED SITE PLAN - SECTOR 1 1  
 Scale: 1" = 20'-0" A1.1

LAND-USE REVIEW

|                               |               |
|-------------------------------|---------------|
| ENLARGED SITE PLAN - SECTOR 1 |               |
| date:                         | JULY 19, 2023 |
| scale:                        | AS NOTED      |
| drawn:                        |               |
| job no.:                      | 2301          |

A  
1.1

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

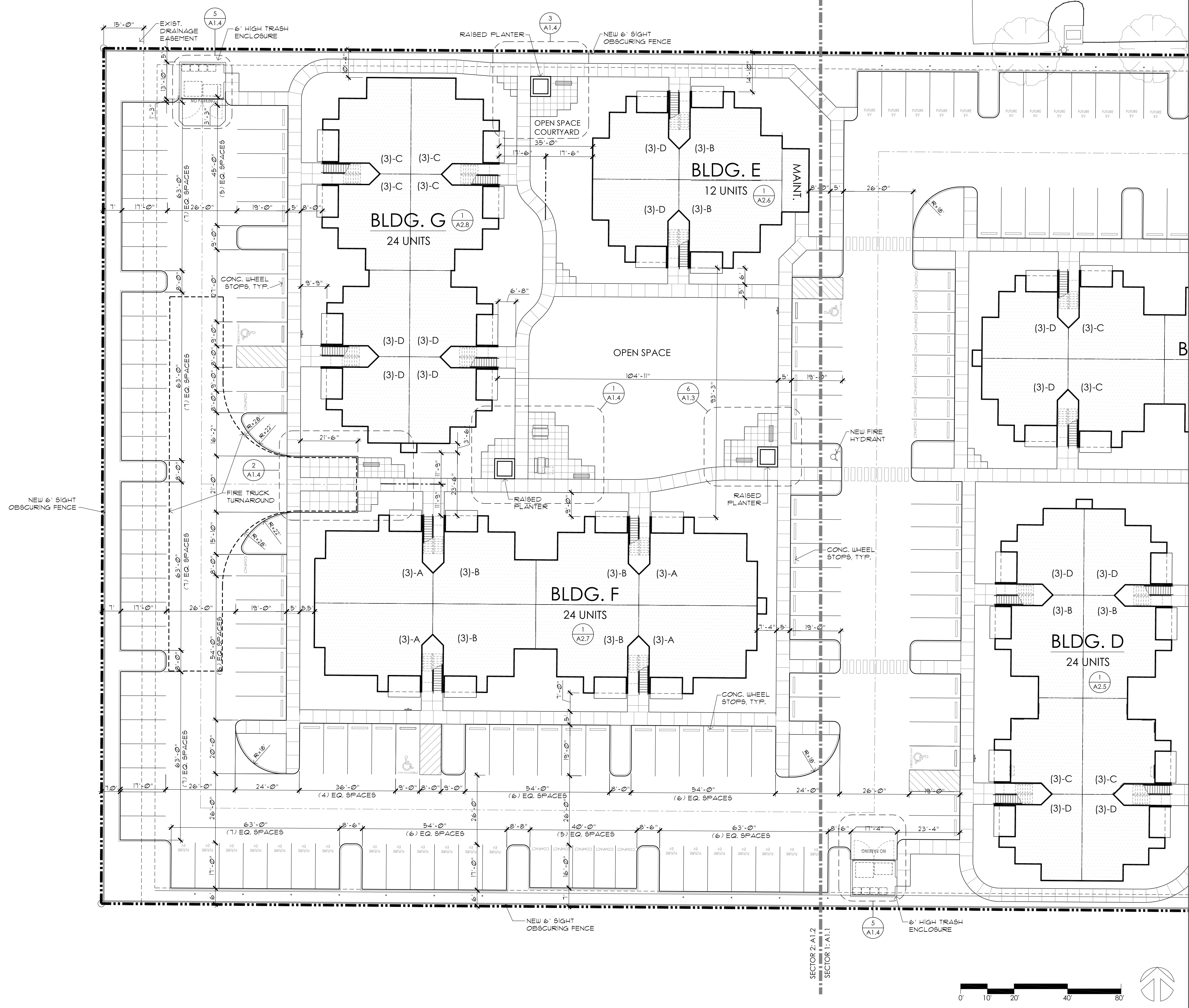
OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

RDA

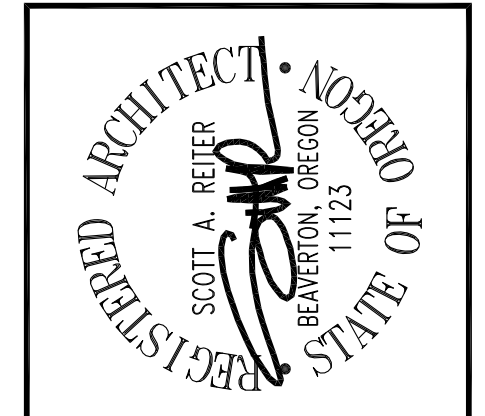
REGISTERED ARCHITECT  
 SCOTT A. REITER  
 BEAVERTON, OREGON 97008  
 11123  
 STATE OF OREGON

6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056





ENLARGED SITE PLAN - SECTOR 2 1  
 Scale: 1" = 20'-0" A1.2



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

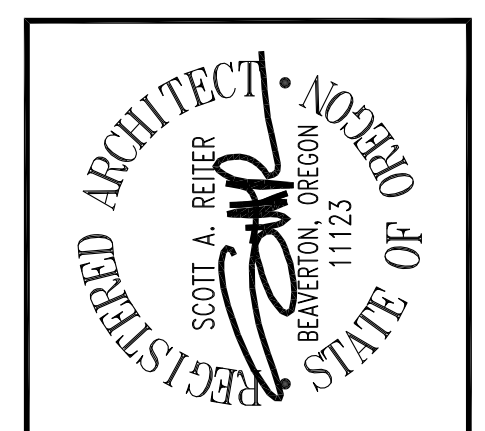
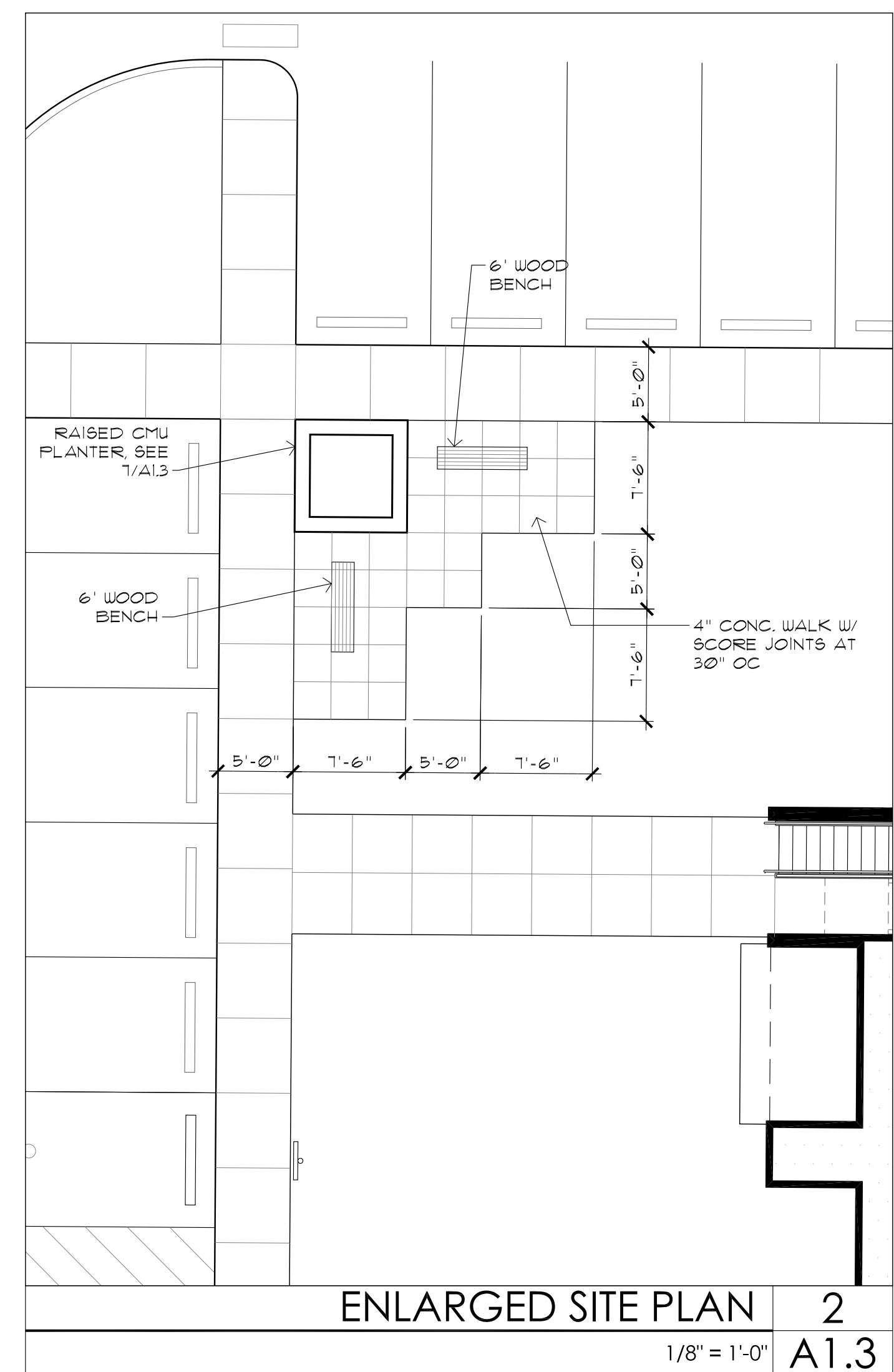
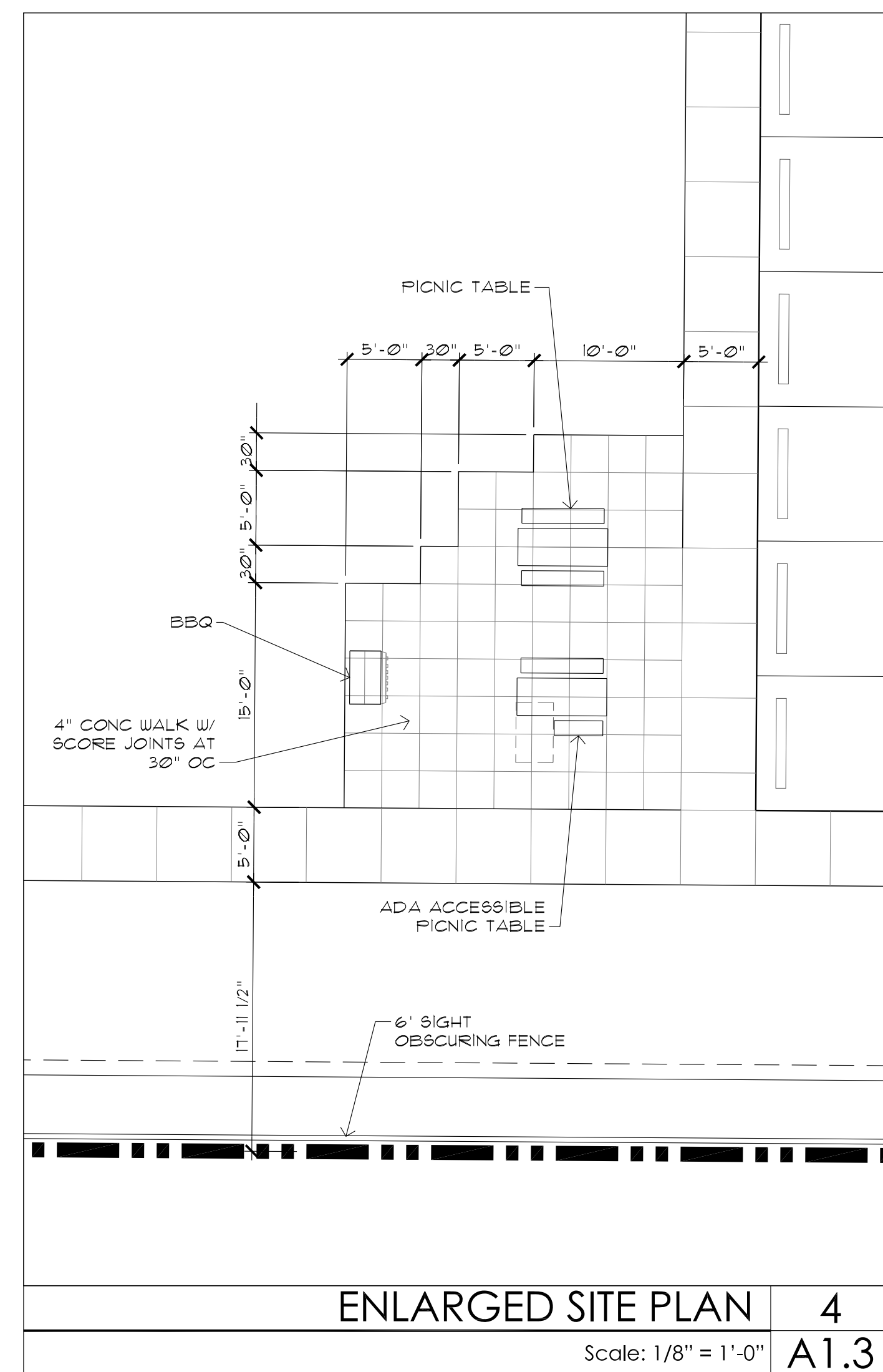
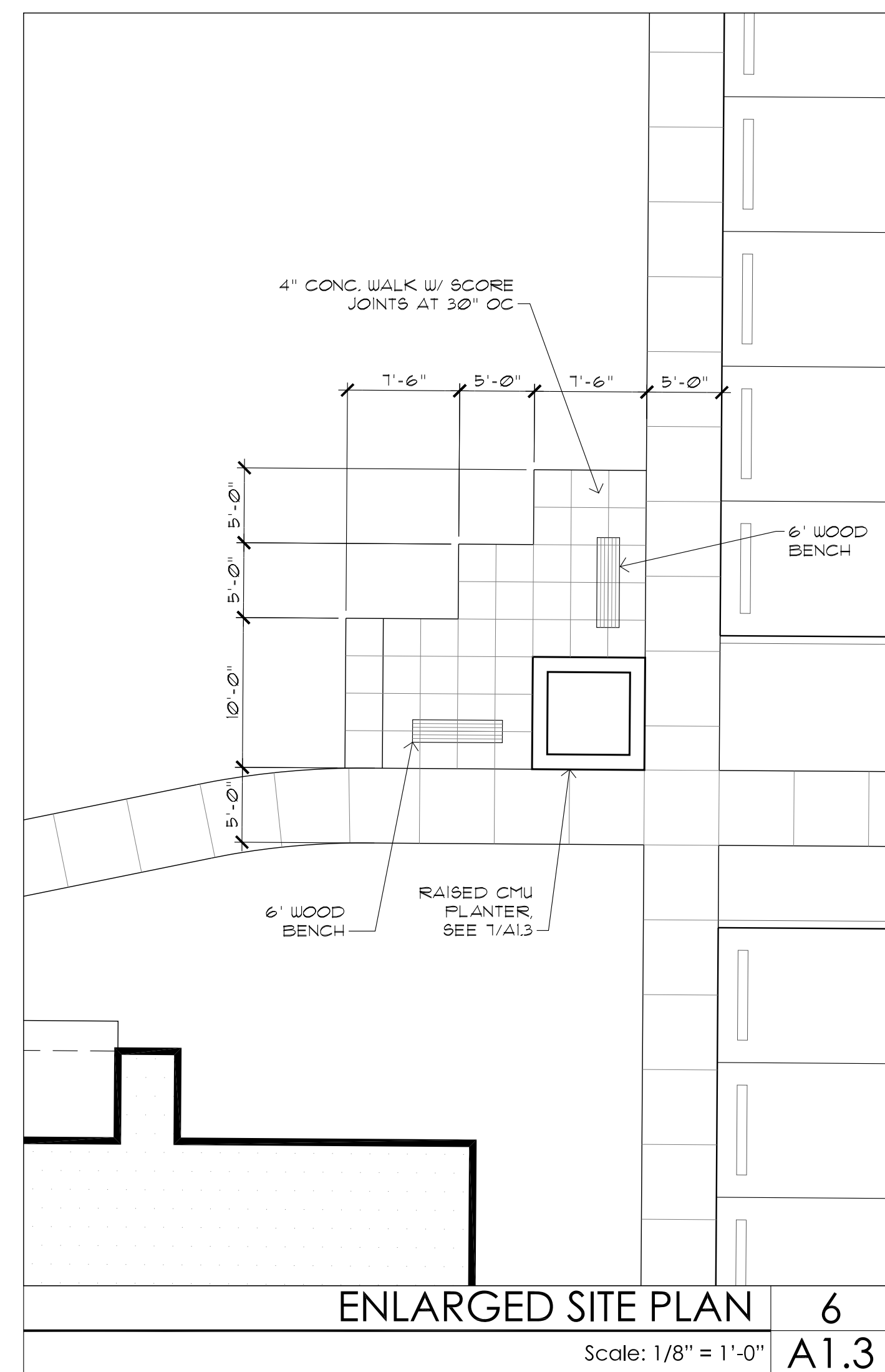
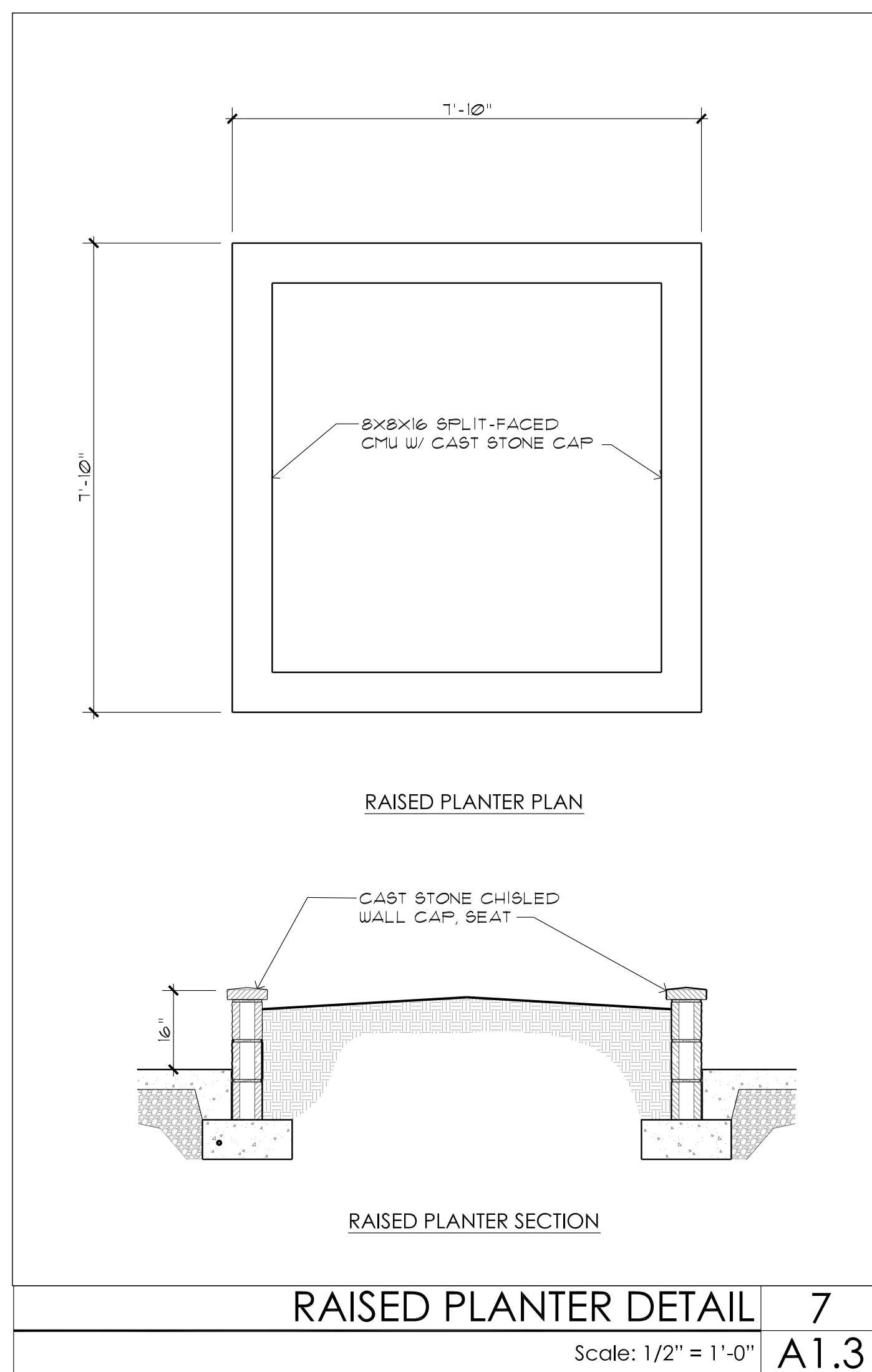
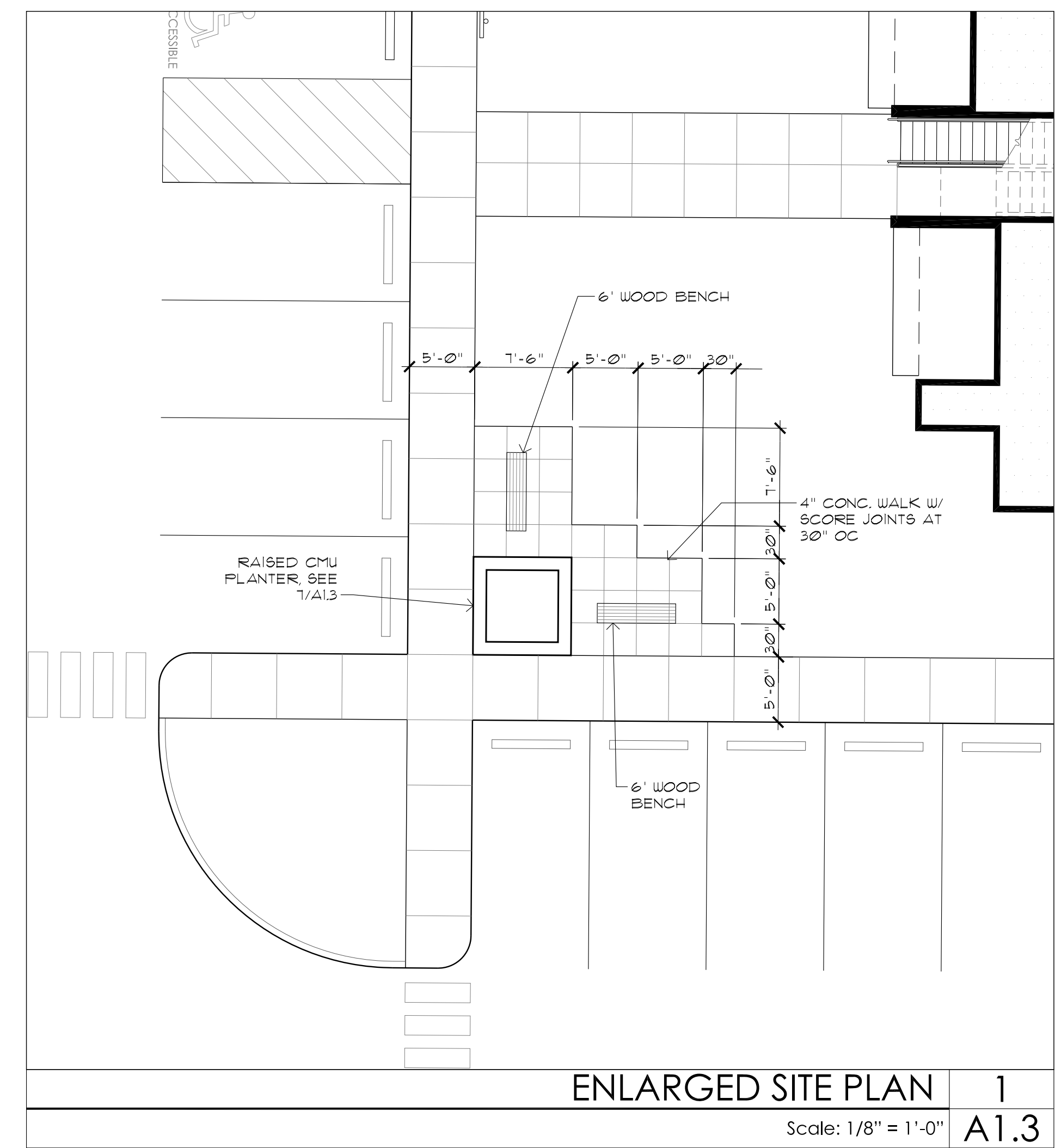
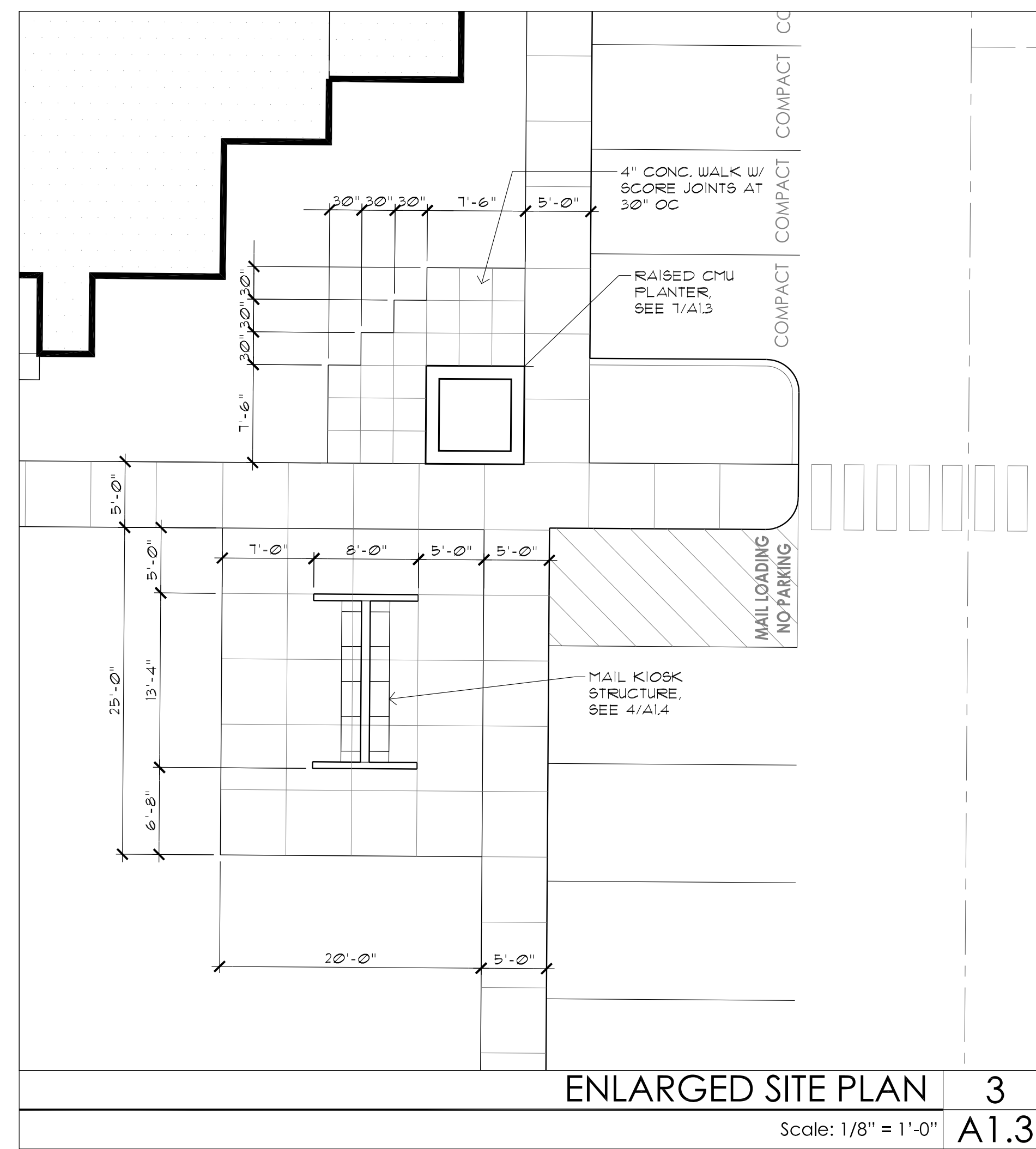
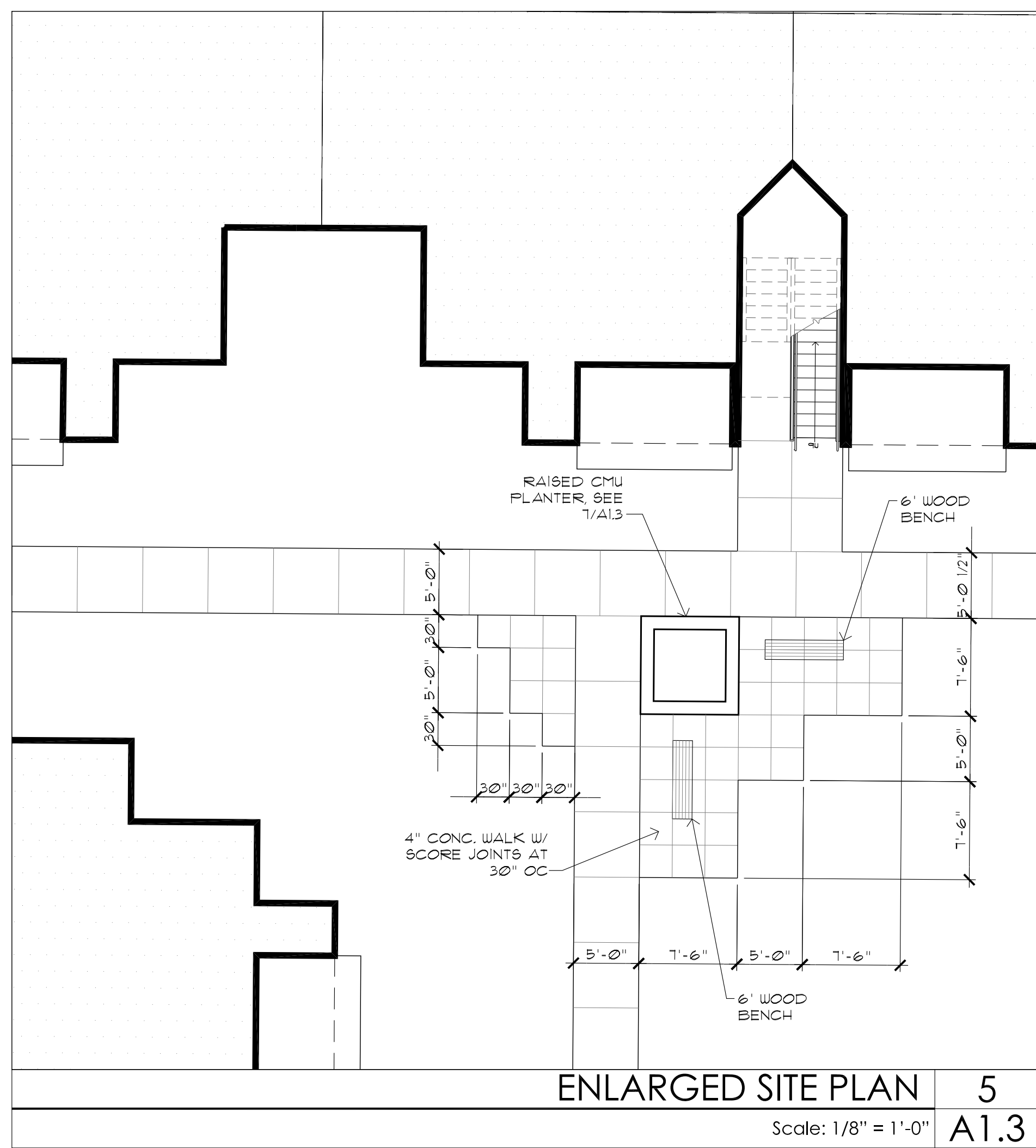
ENLARGED SITE PLAN - SECTOR 2

| DATE          | REVISIONS    |
|---------------|--------------|
| JULY 19, 2023 | AS NOTED     |
|               | DRAWN:       |
|               | JOB NO. 2301 |

**A**  
**1.2**

LAND-USE REVIEW





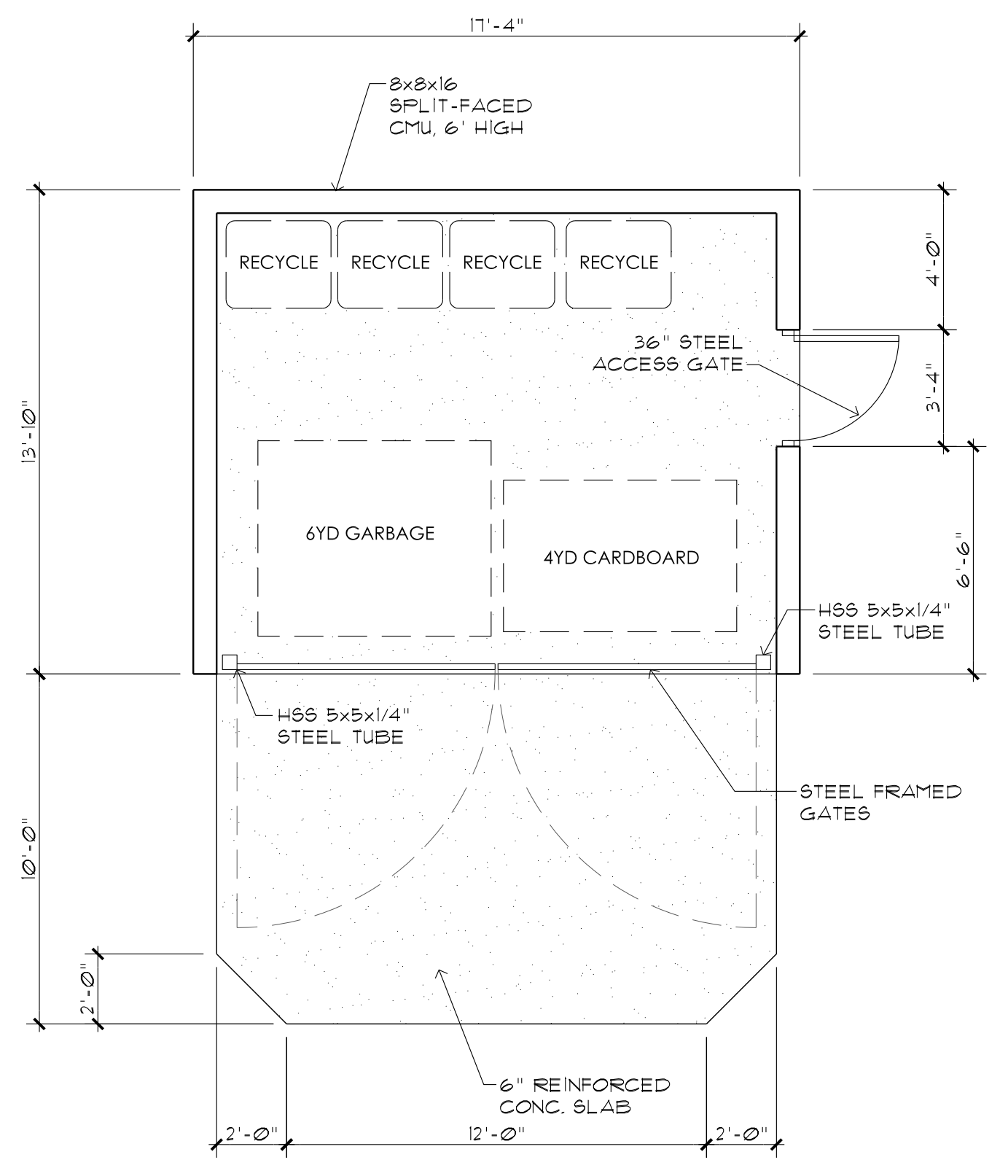
**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

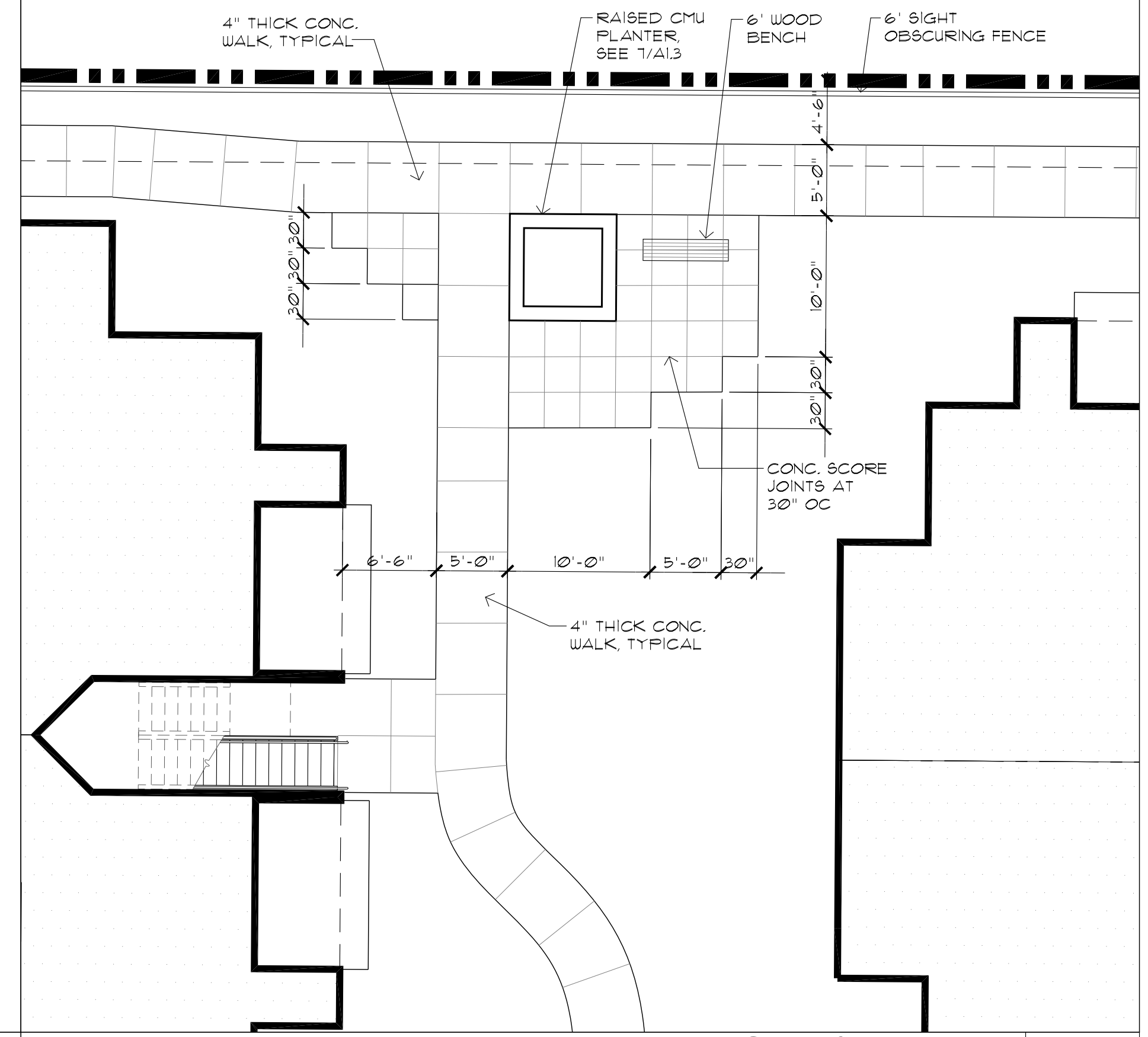
ENLARGED SITE PLANS

|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
| drawn:   |               |
| job no.: | 2301          |

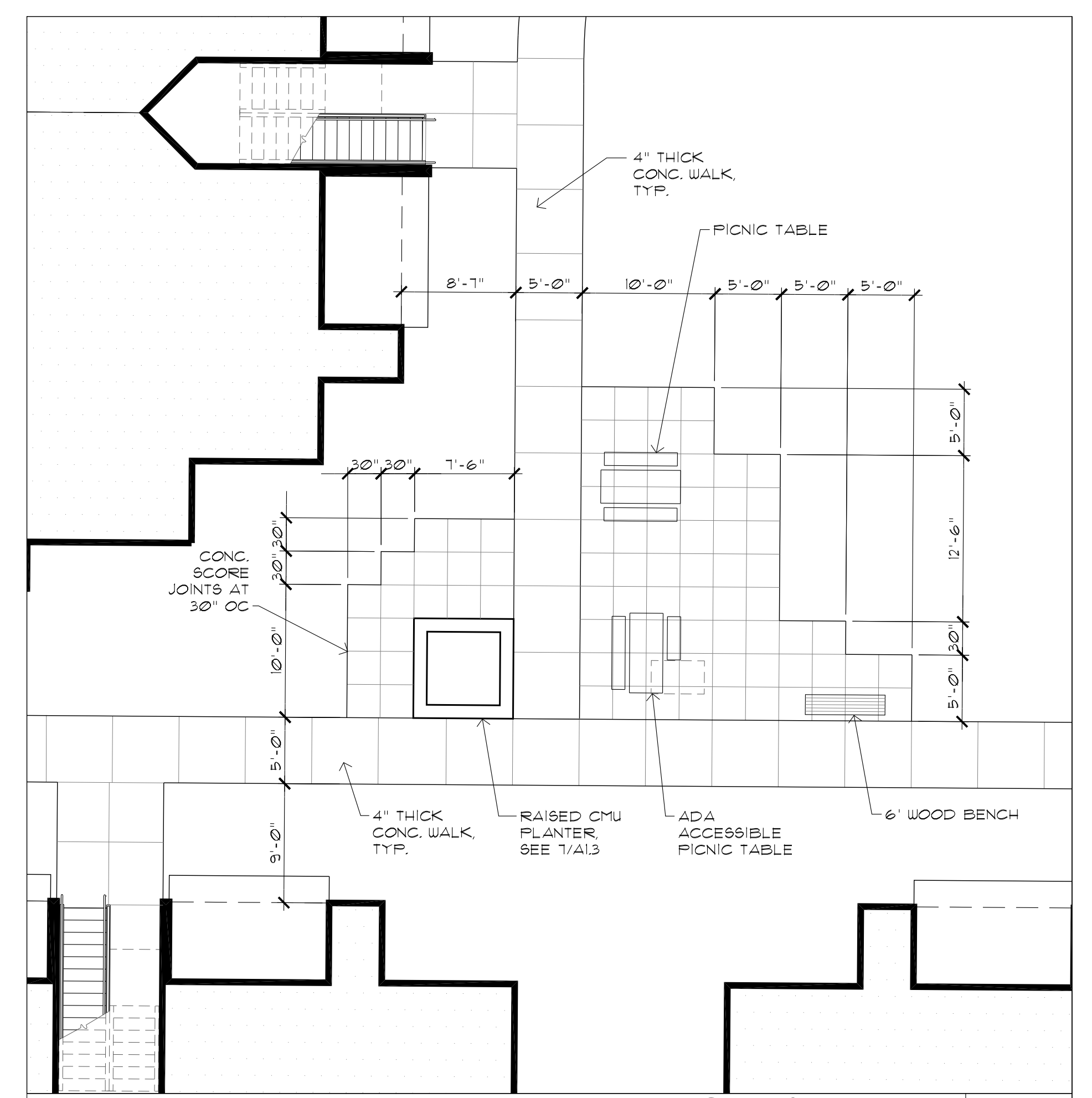
**A**  
**1.3**



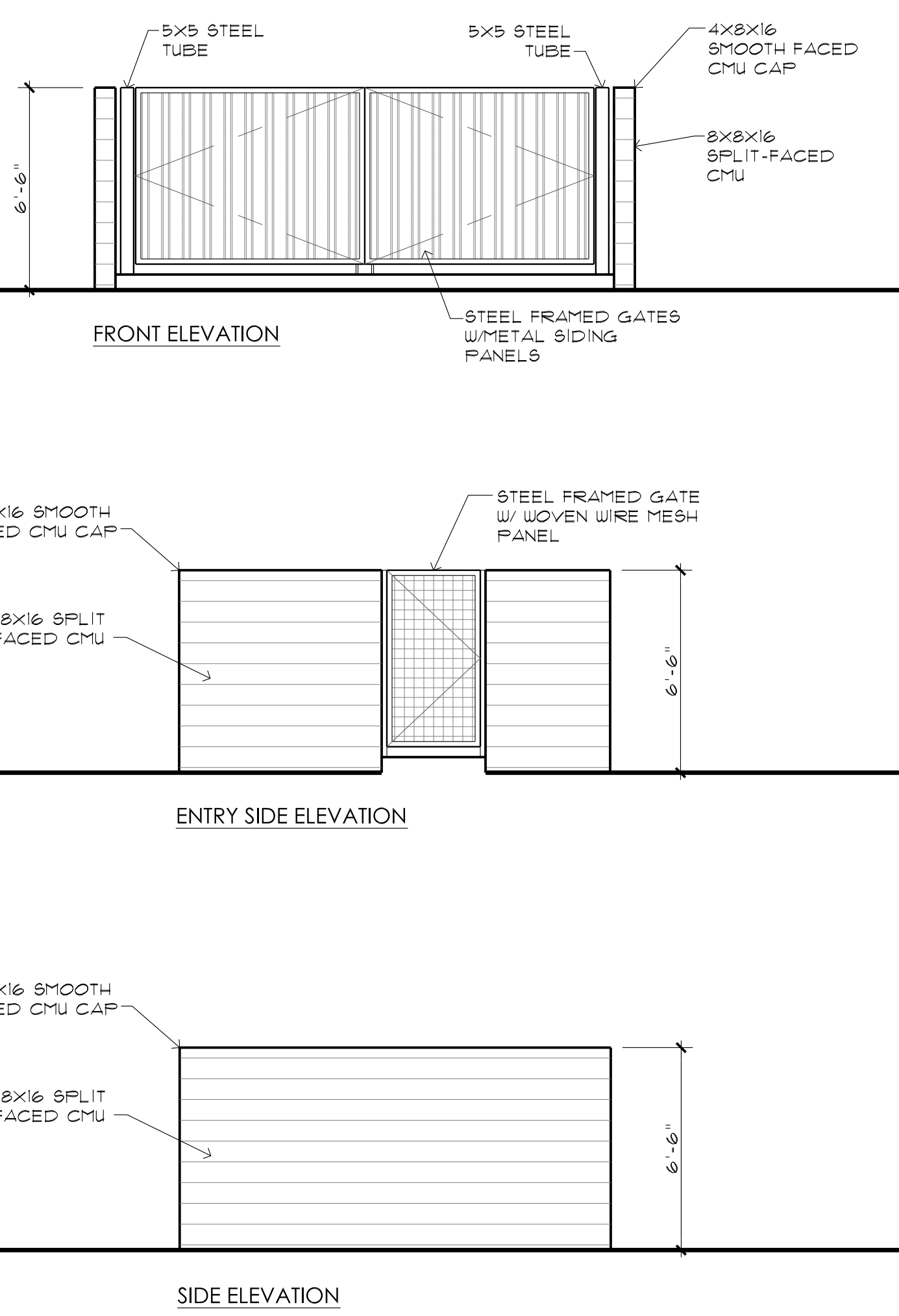
TRASH ENCLOSURE PLAN 5  
Scale: 1/4" = 1'-0" A1.4



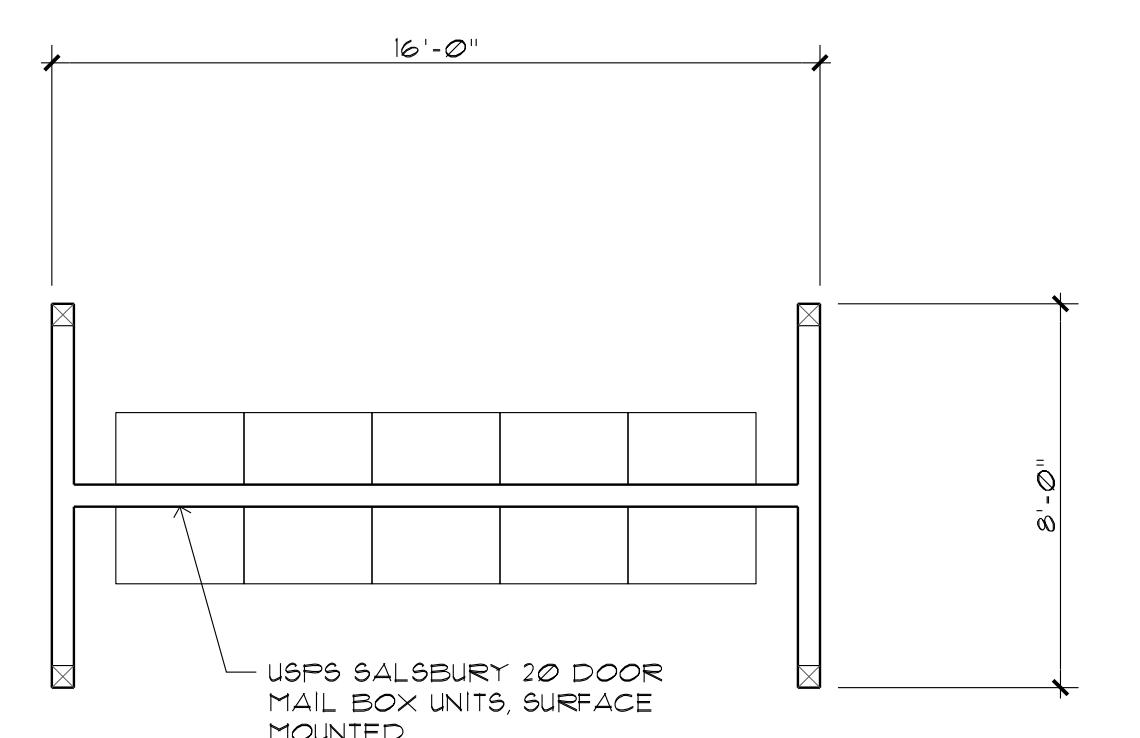
ENLARGED SITE PLAN 3  
Scale: 1/8" = 1'-0" A1.4



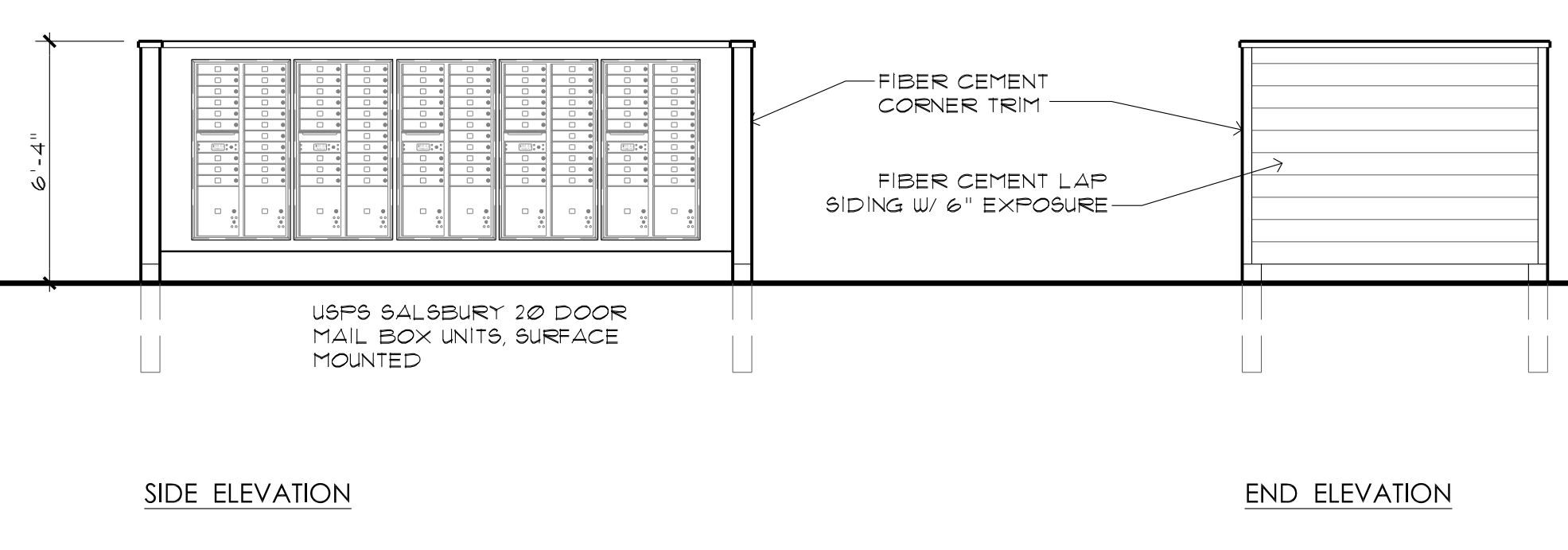
ENLARGED SITE PLAN 1  
Scale: 1/8" = 1'-0" A1.4



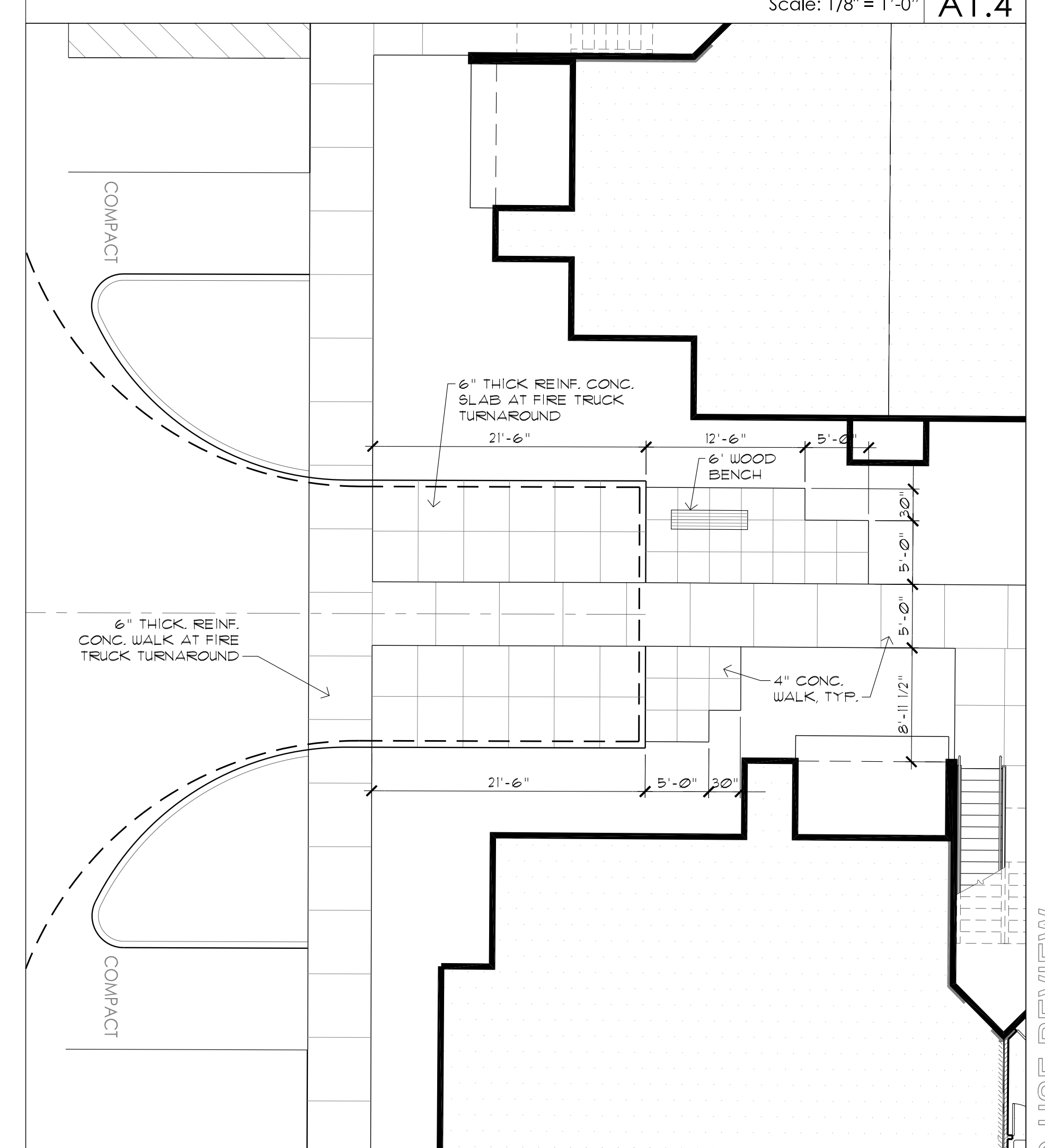
TRASH ENCLOSURE ELEVATIONS 6  
Scale: 1/4" = 1'-0" A1.4



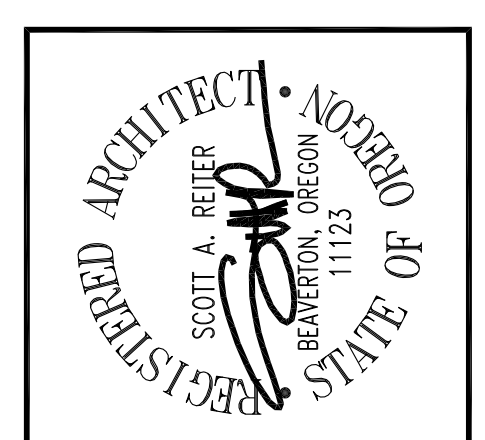
MAIL KIOSK PLAN



MAIL KIOSK PLAN, ELEVATIONS 4  
Scale: 1/4" = 1'-0" A1.4



ENLARGED SITE PLAN 2  
Scale: 1/8" = 1'-0" A1.4



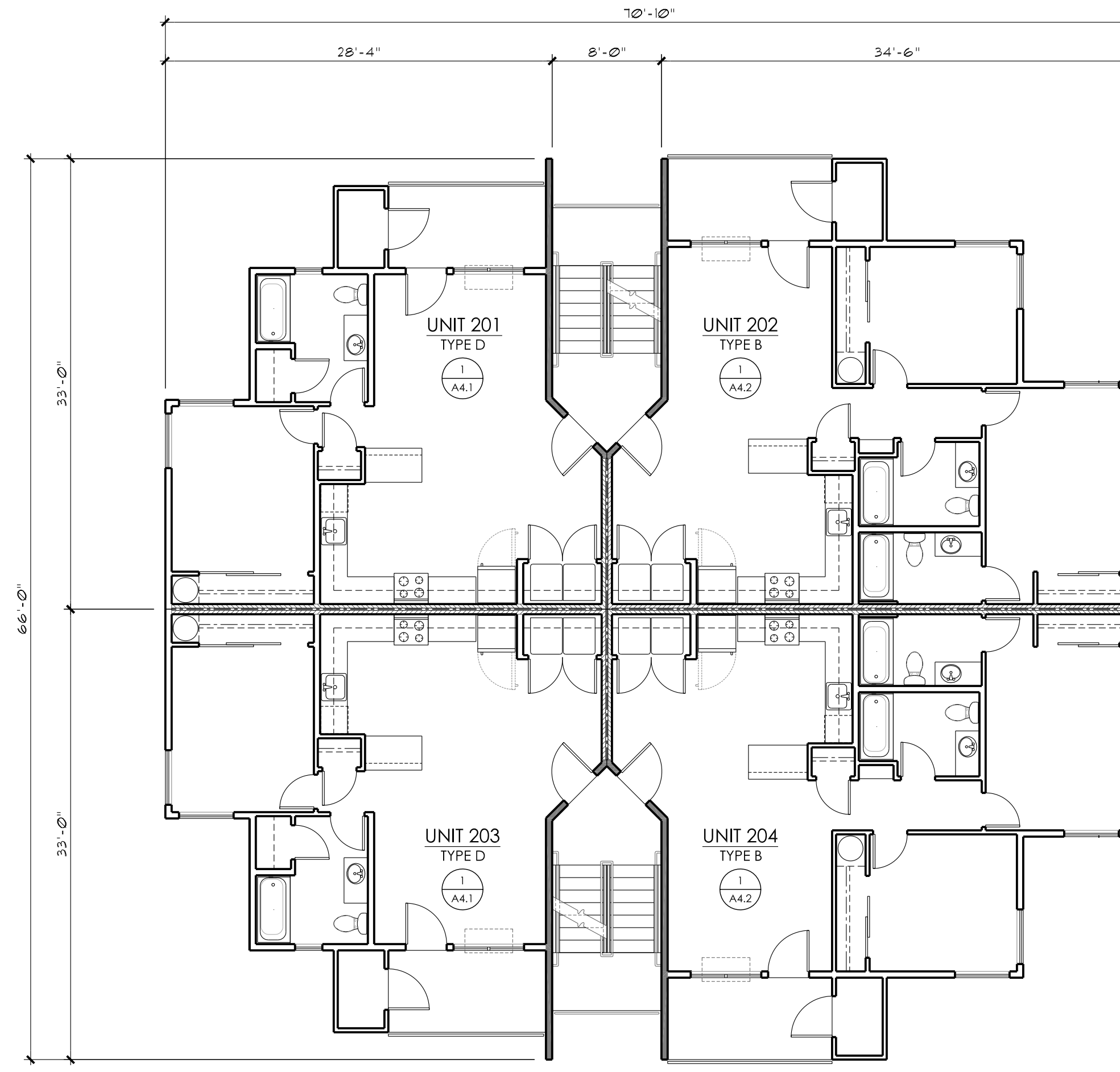
**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLANS  
MAIL KIOSK PLANS, ELEVATIONS  
TRASH ENCLOSURE PLAN, ELEVATIONS

|            |               |
|------------|---------------|
| date:      | JULY 19, 2023 |
| scale:     | AS NOTED      |
| drawn:     |               |
| job no.:   | 2301          |
| revisions: |               |

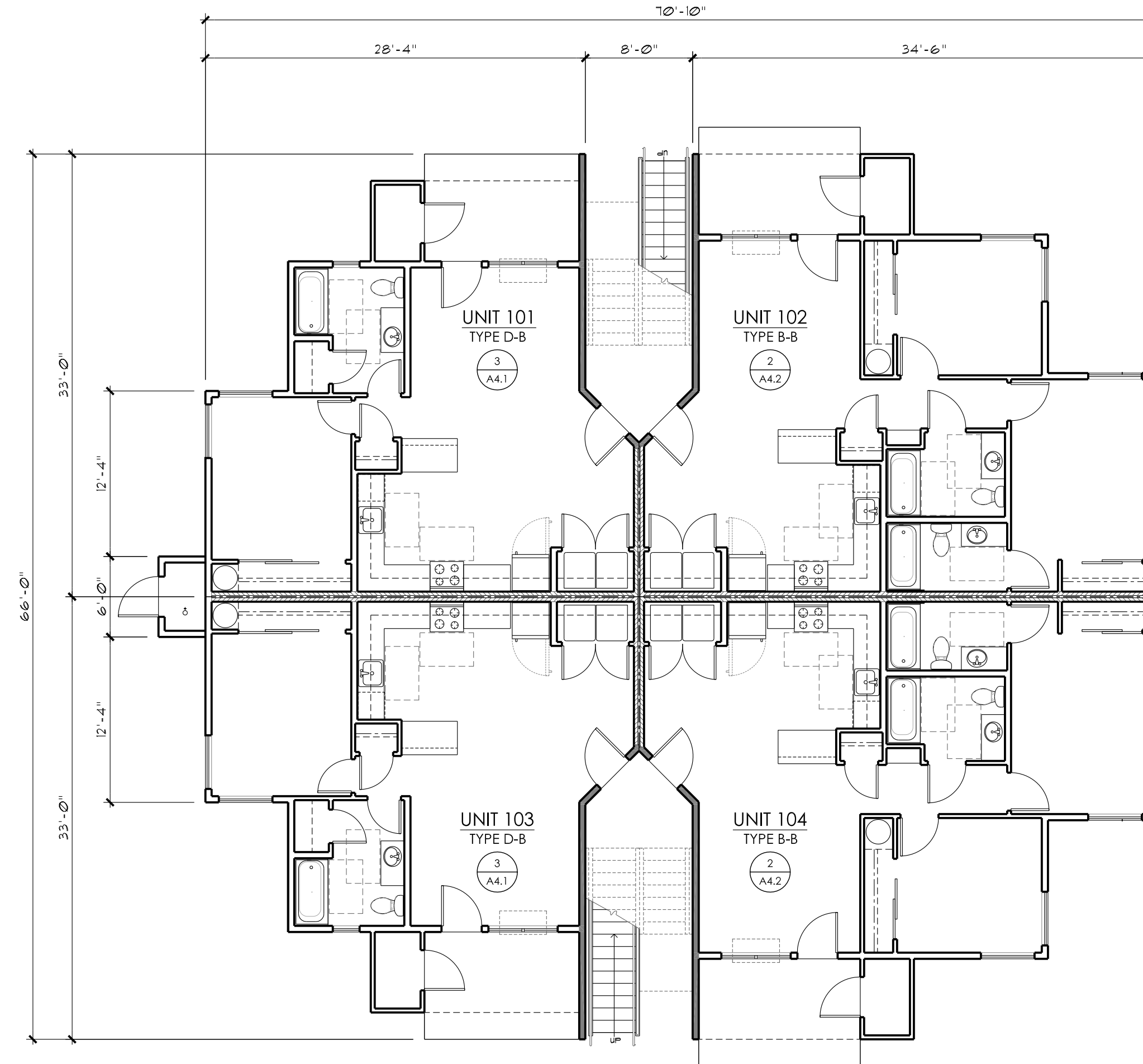
LAND-USE REVIEW  
**A**  
**1.4**



BUILDING 'A' - SECOND/THIRD FLOOR PLAN 2

Scale: 1/8" = 1'-0"

A2.1



BUILDING 'A' - FIRST FLOOR PLAN 1

Scale: 1/8" = 1'-0"

A2.1

LAND-USE REVIEW

BUILDING PLANS  
BUILDING 'A'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   |               |            |  |
| job no.: | 2301          |            |  |

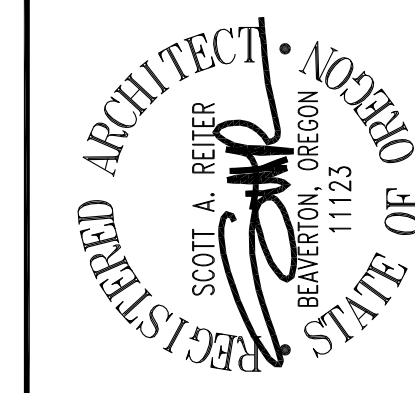
**A**  
**2.1**

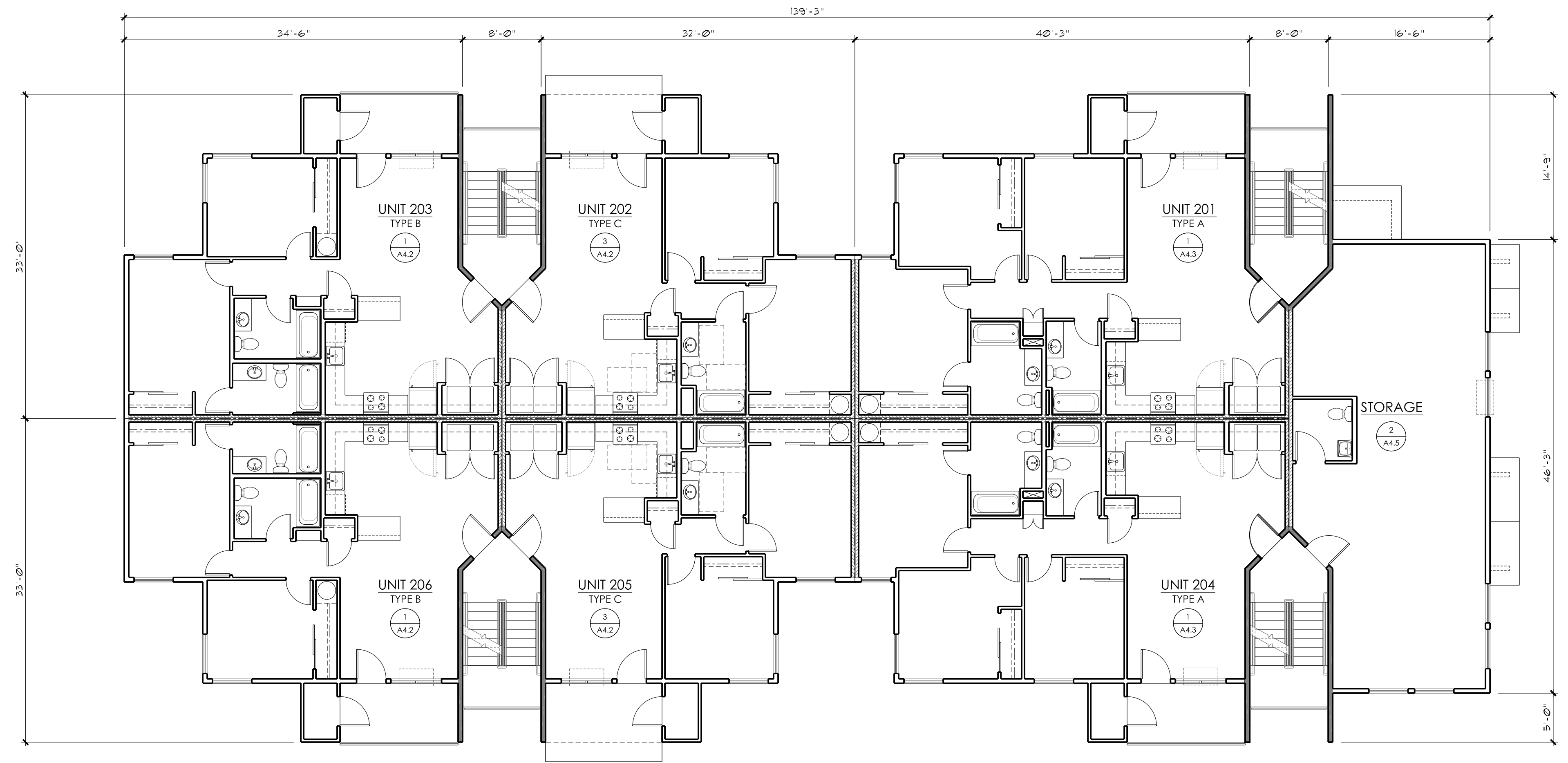
**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED

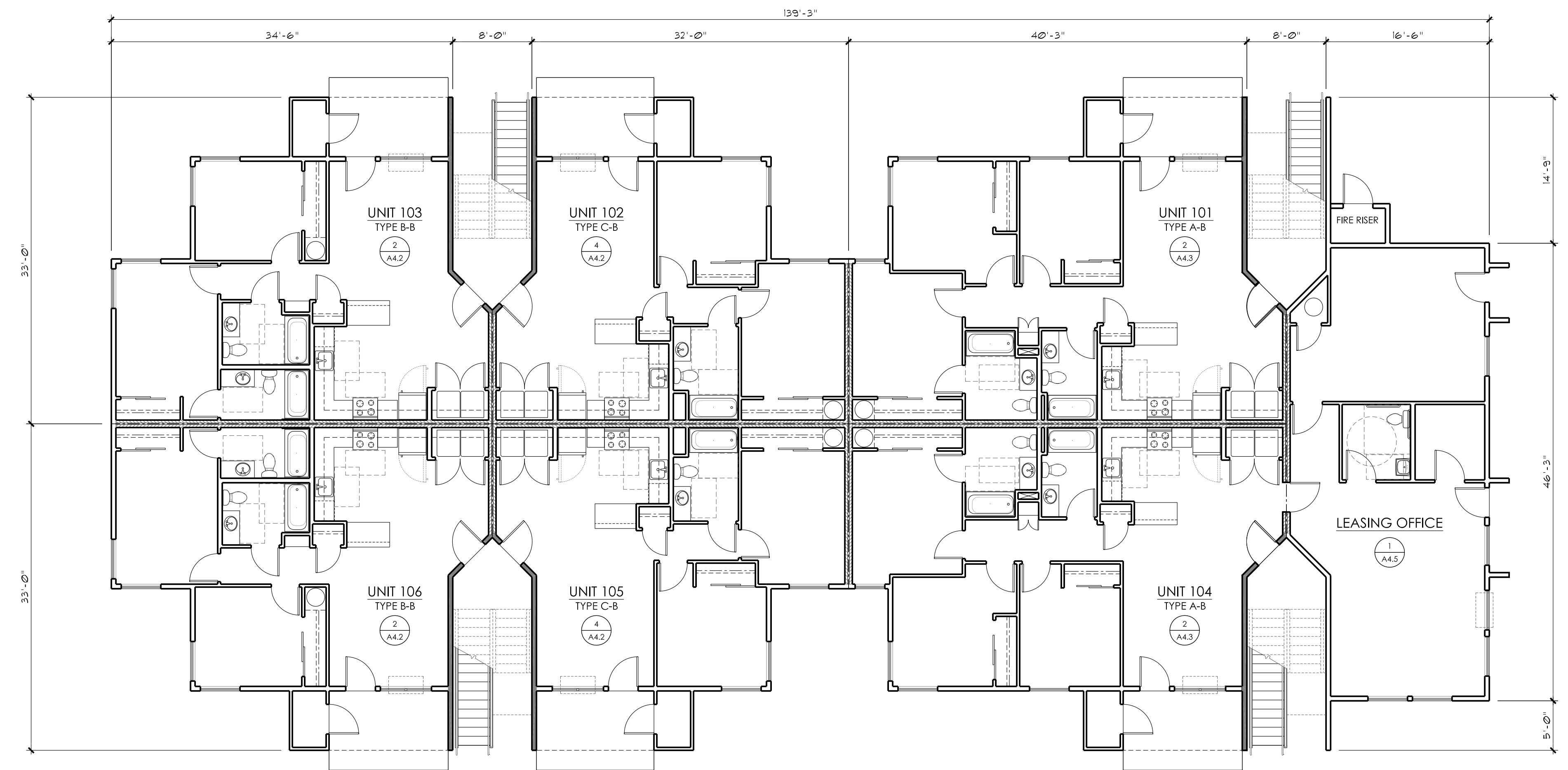
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056





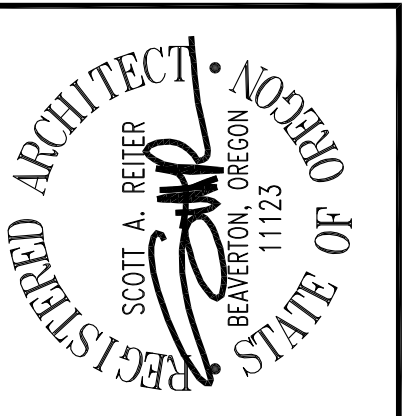
BUILDING 'B' SECOND FLOOR PLAN 2

Scale: 1/8" = 1'-0" A2.2



BUILDING 'B' FIRST FLOOR PLAN 1

Scale: 1/8" = 1'-0" A2.2



**RDA**  
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**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

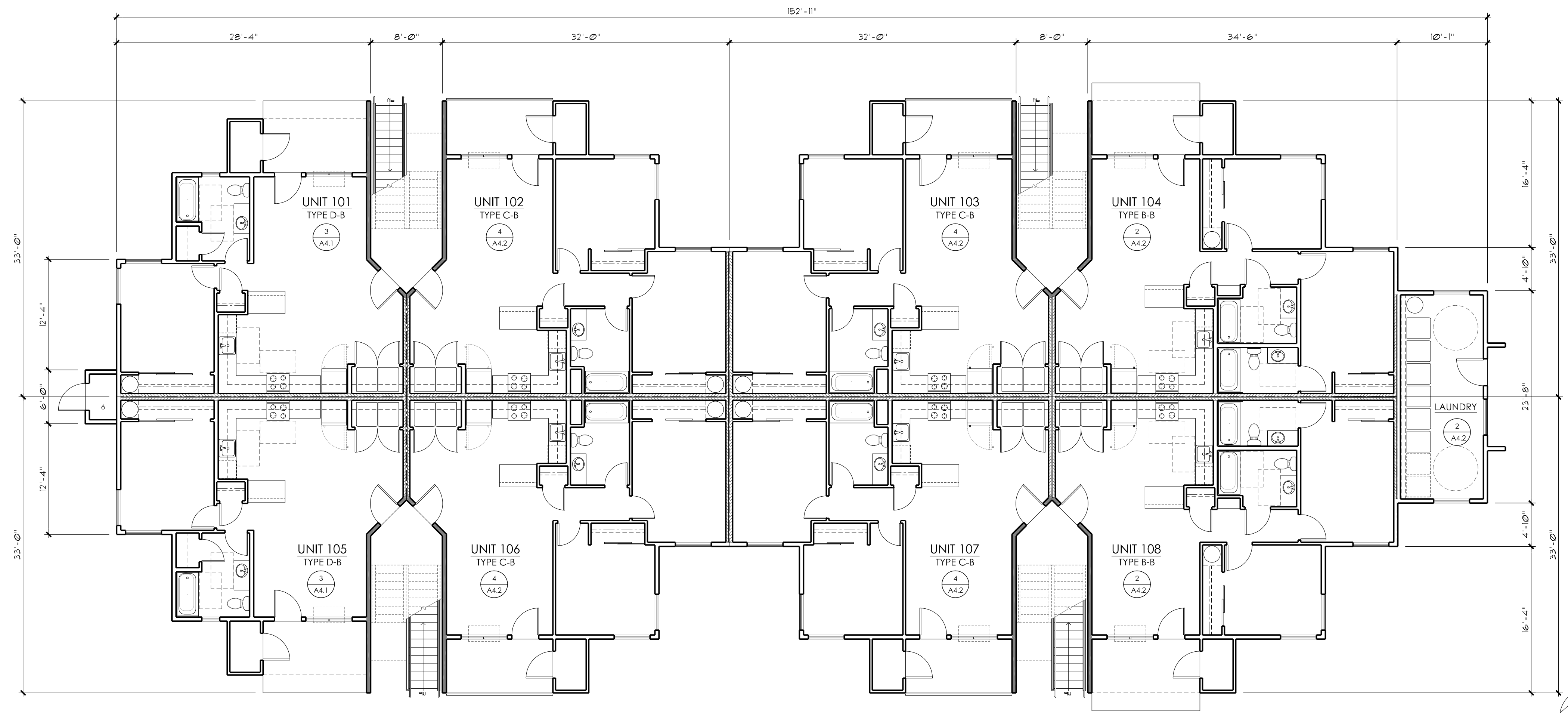
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

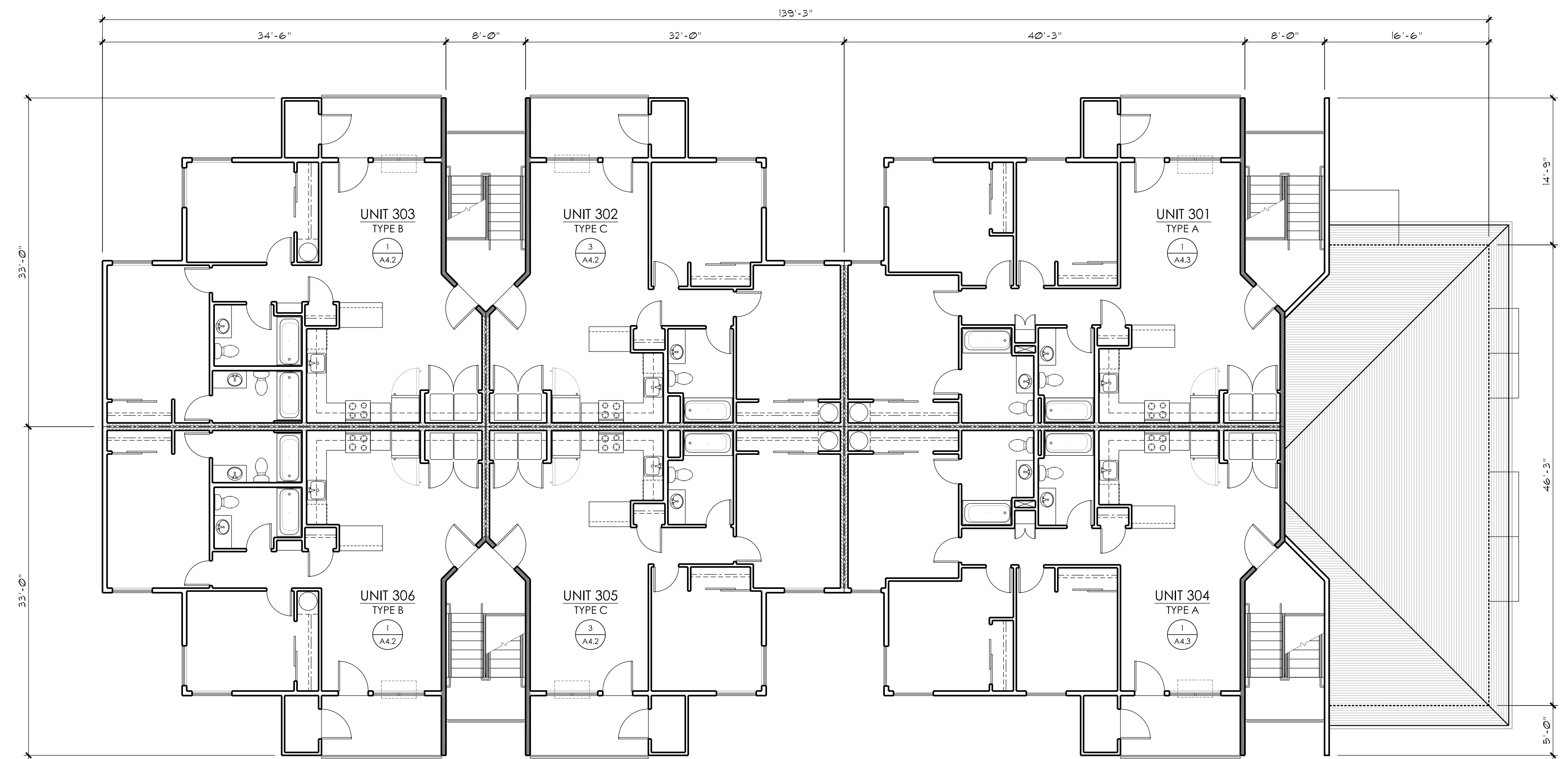
BUILDING PLANS  
BUILDING 'B'

| date:         | scale:   | drawn: | revision: |
|---------------|----------|--------|-----------|
| JULY 19, 2023 | AS NOTED |        |           |
|               |          |        |           |
|               |          |        |           |

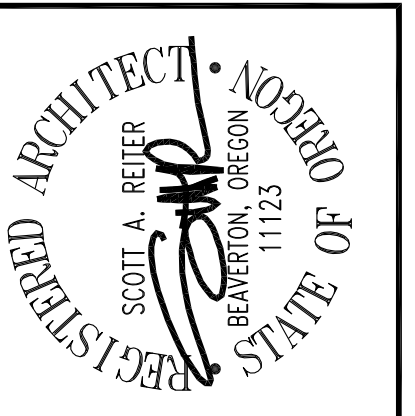
**A**  
**2.2**



BUILDING 'C' FIRST FLOOR PLAN 2  
Scale: 1/8" = 1'-0" A2.3



BUILDING 'B' THIRD FLOOR PLAN 1  
Scale: 1/8" = 1'-0" A2.3



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
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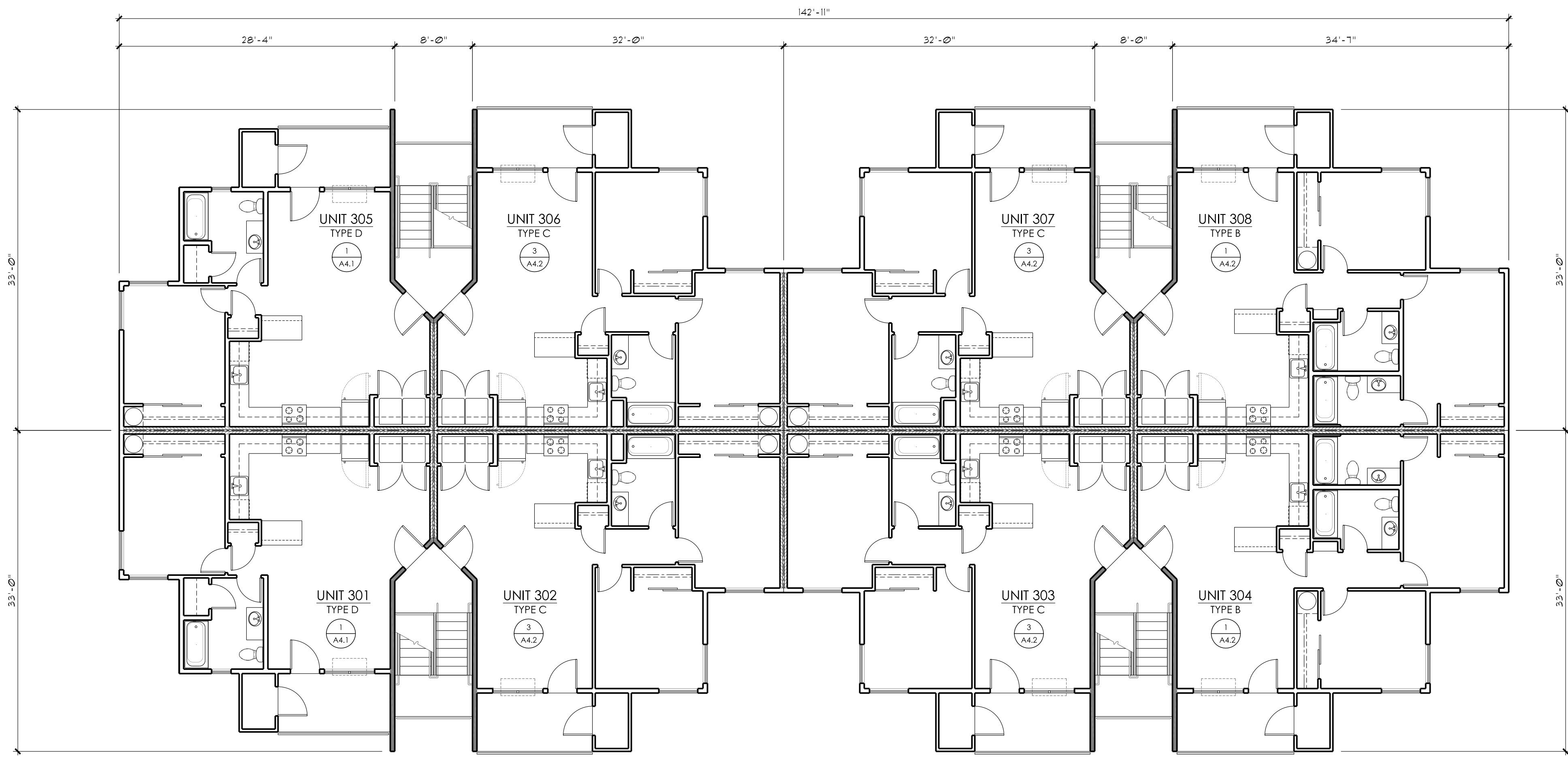
**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

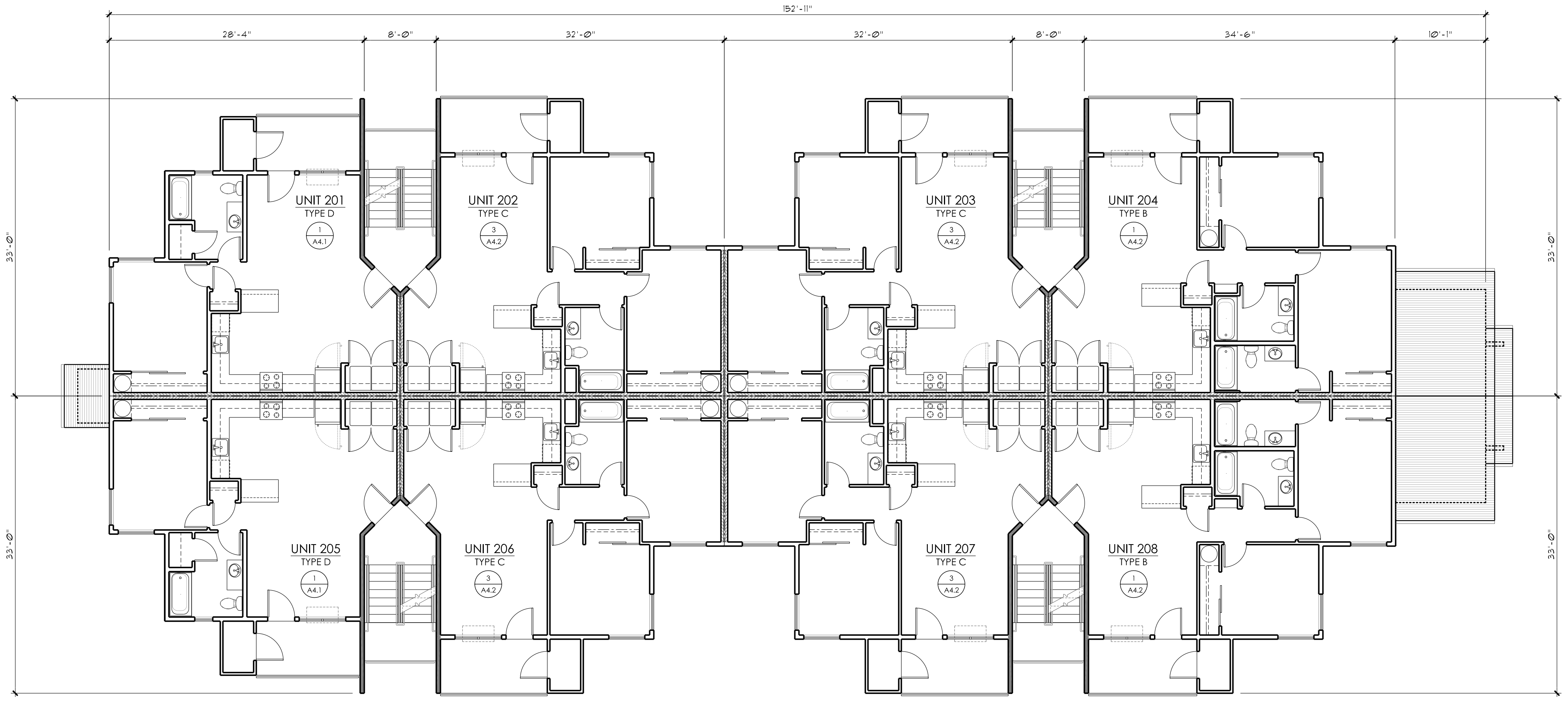
| BUILDING PLANS<br>BUILDING 'B', BUILDING 'C' |               | revisions: |
|--|---------------|------------|
| date:  | JULY 19, 2023 |            |
| scale:                                       | AS NOTED      |            |
| drawn:                                       |               |            |
| job no.:                                     | 2301          |            |

**A**  
**2.3**

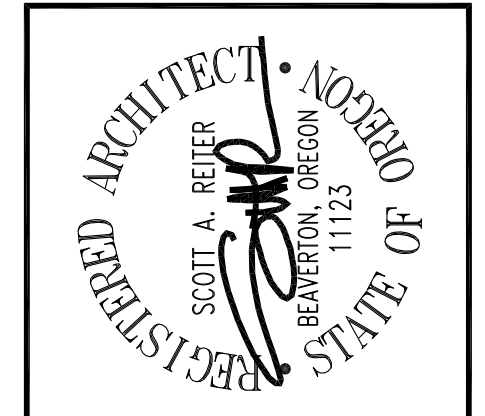




BUILDING 'C' THIRD FLOOR PLAN 2  
 Scale: 1/8" = 1'-0" A2.4



BUILDING 'C' SECOND FLOOR PLAN 1  
 Scale: 1/8" = 1'-0" A2.4



**RDA**  
 REITER DESIGN ARCHITECT  
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 6107 SW MURRAY BLVD. #480  
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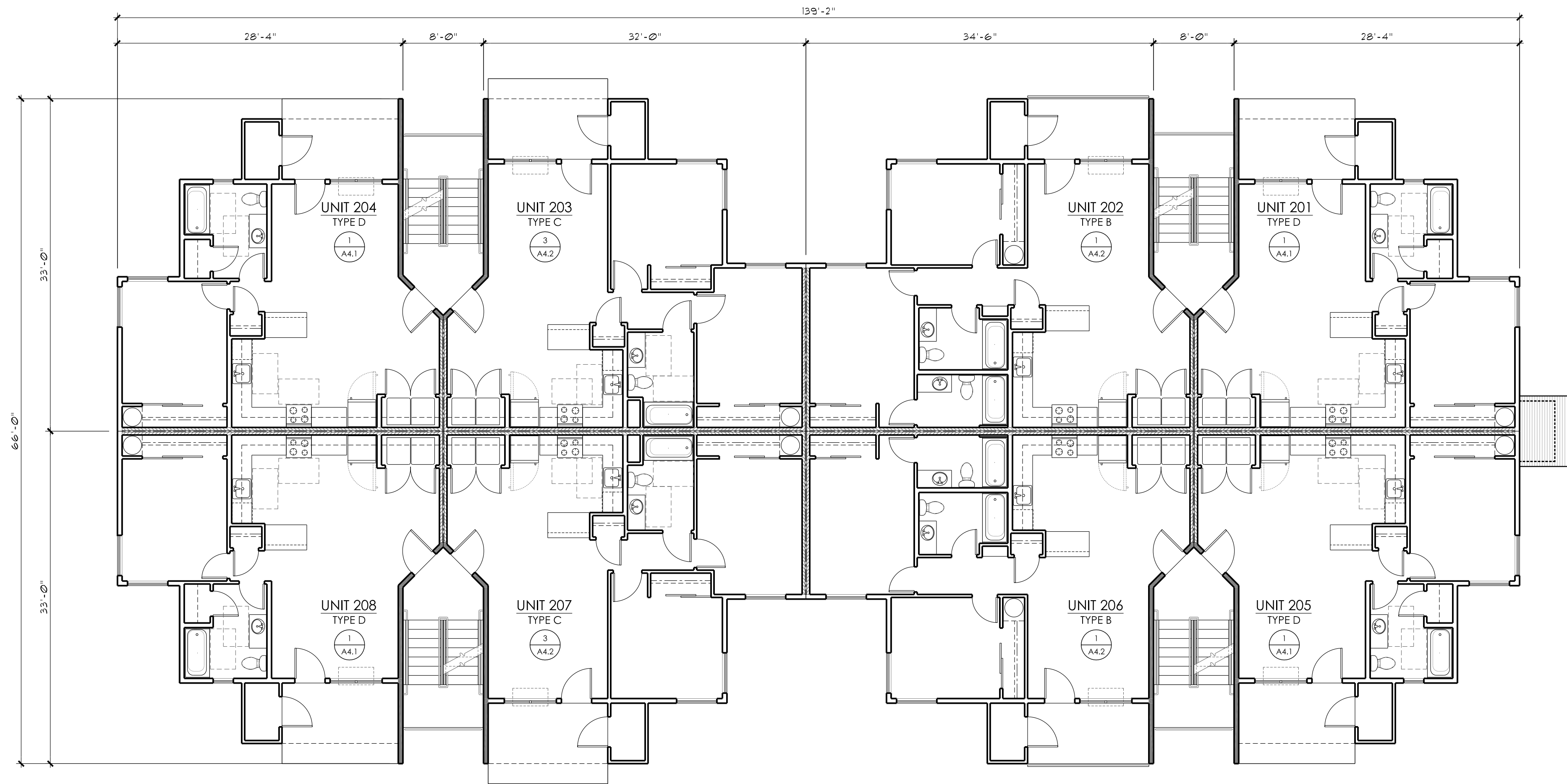
**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
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LAND-USE REVIEW

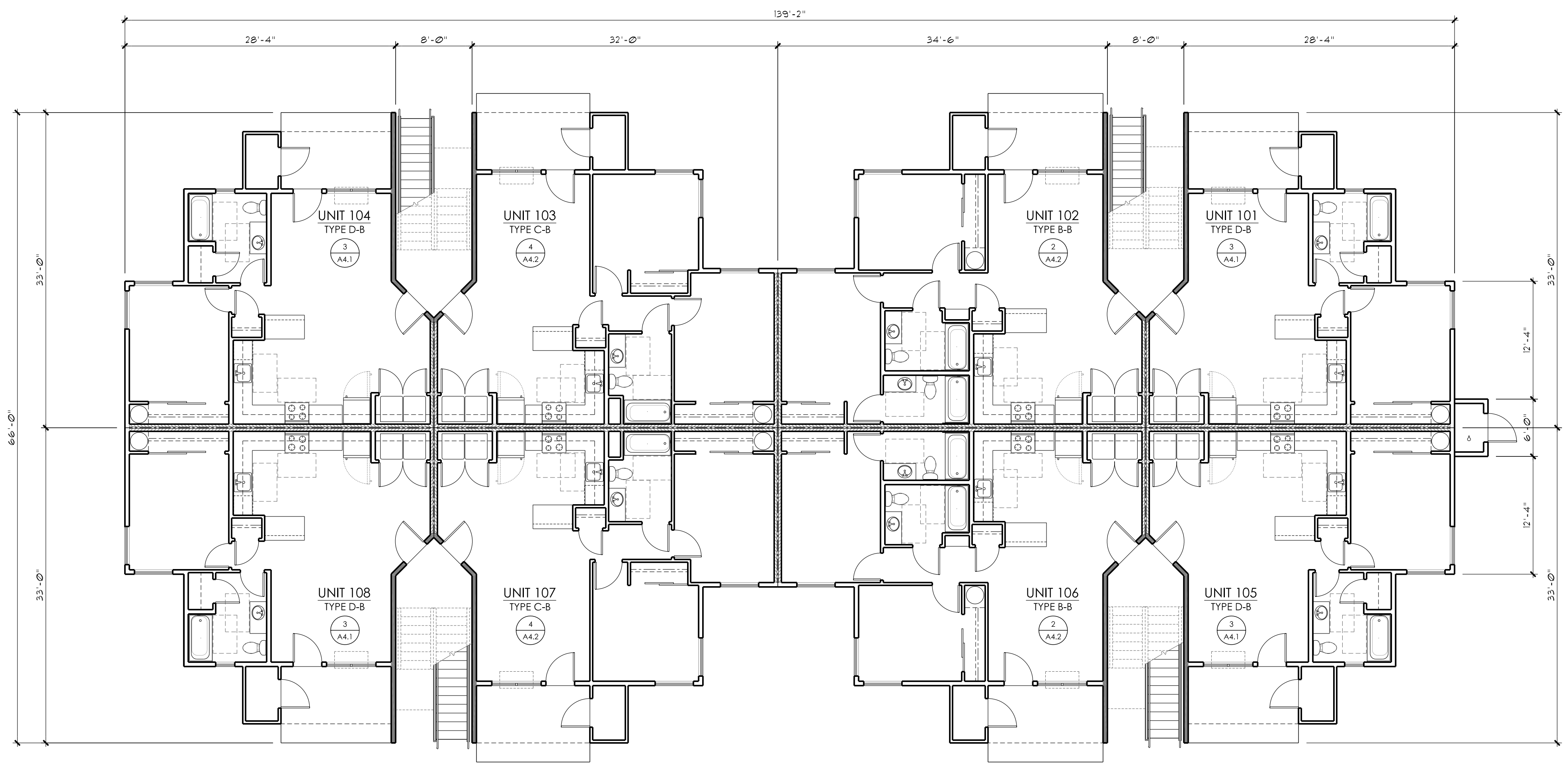
| DATE          | REVISIONS |
|---------------|-----------|
| JULY 19, 2023 | AS NOTED  |
| SCALE:        |           |
| DRAWN:        |           |
| JOB NO.:      | 2301      |

**A**  
**2.4**

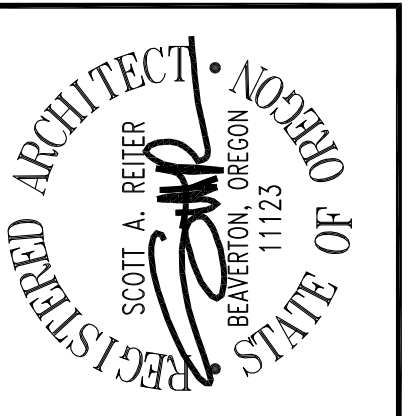




BUILDING 'D' SECOND/THIRD FLOOR PLAN 2  
 Scale: 1/8" = 1'-0" A2.5



BUILDING 'D' FIRST FLOOR PLAN 1  
 Scale: 1/8" = 1'-0" A2.5



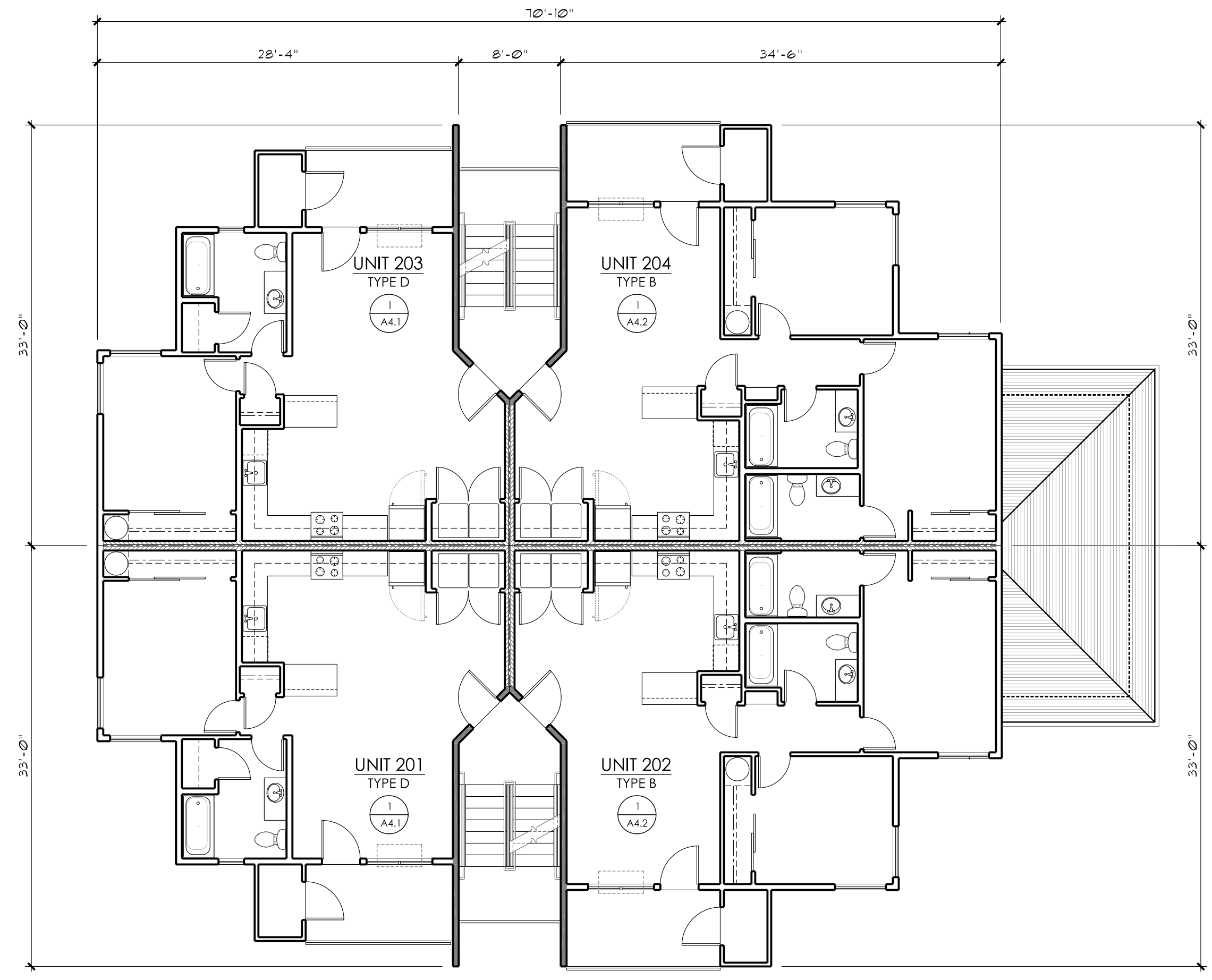
**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

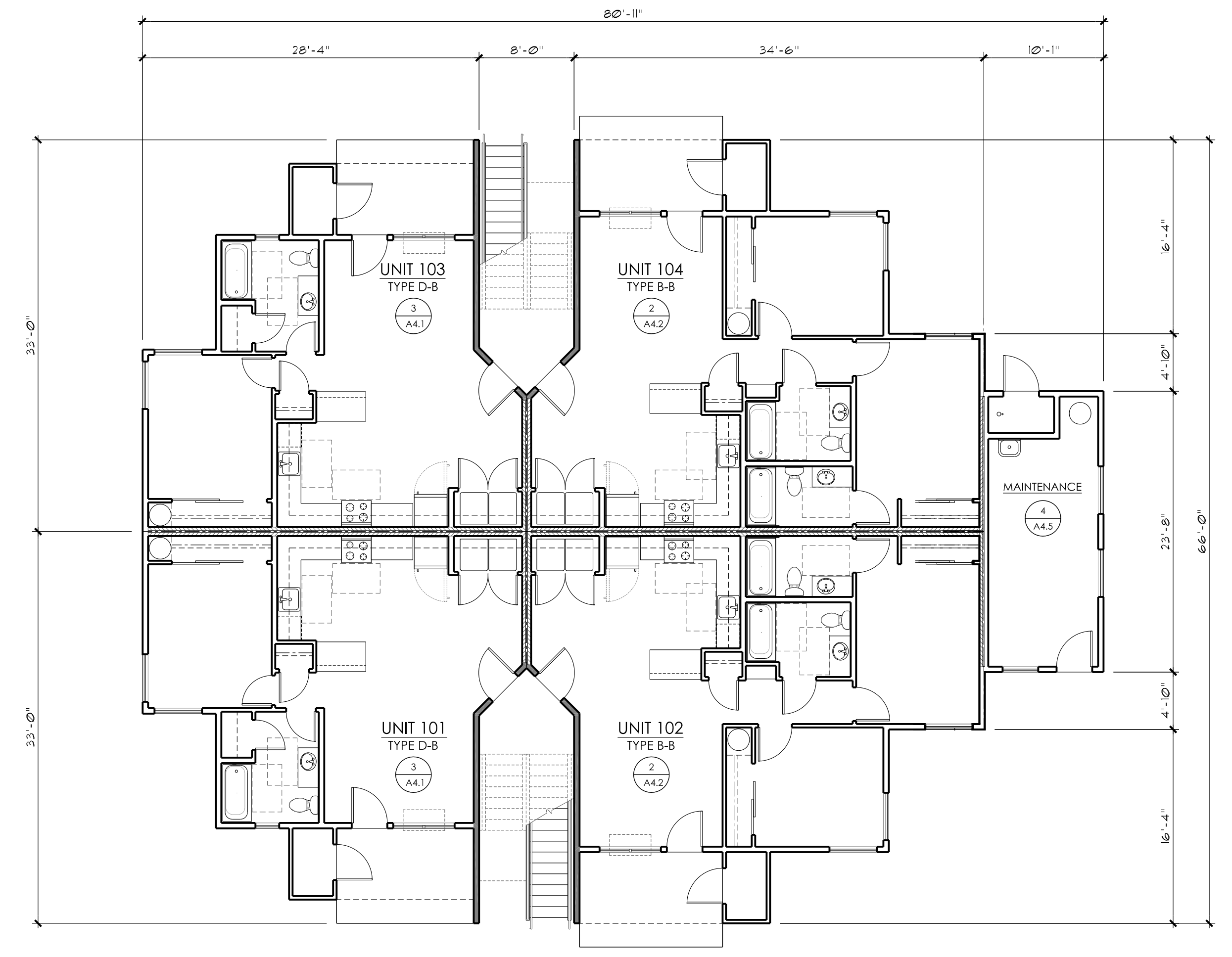
LAND-USE REVIEW

| BUILDING PLANS<br>BUILDING 'D' |               | revisions: |
|--------------------------------|---------------|------------|
| date:                          | JULY 19, 2023 |            |
| scale:                         | AS NOTED      |            |
| drawn:                         |               |            |
| job no.:                       | 2301          |            |

**A**  
**2.5**



BUILDING 'E' - SECOND/THIRD FLOOR PLAN 2  
 Scale: 1/8" = 1'-0" A2.6



BUILDING 'E' - FIRST FLOOR PLAN 1  
 Scale: 1/8" = 1'-0" A2.6

LAND-USE REVIEW

BUILDING PLANS  
 BUILDING 'E'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   |               |            |  |
| job no.: | 2301          |            |  |

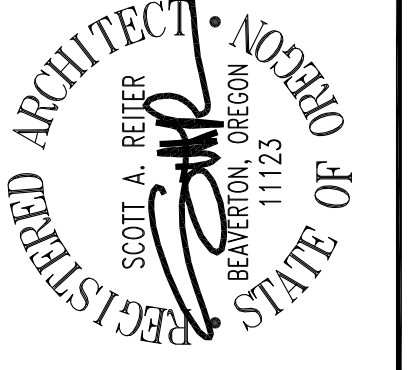
A  
 2.6

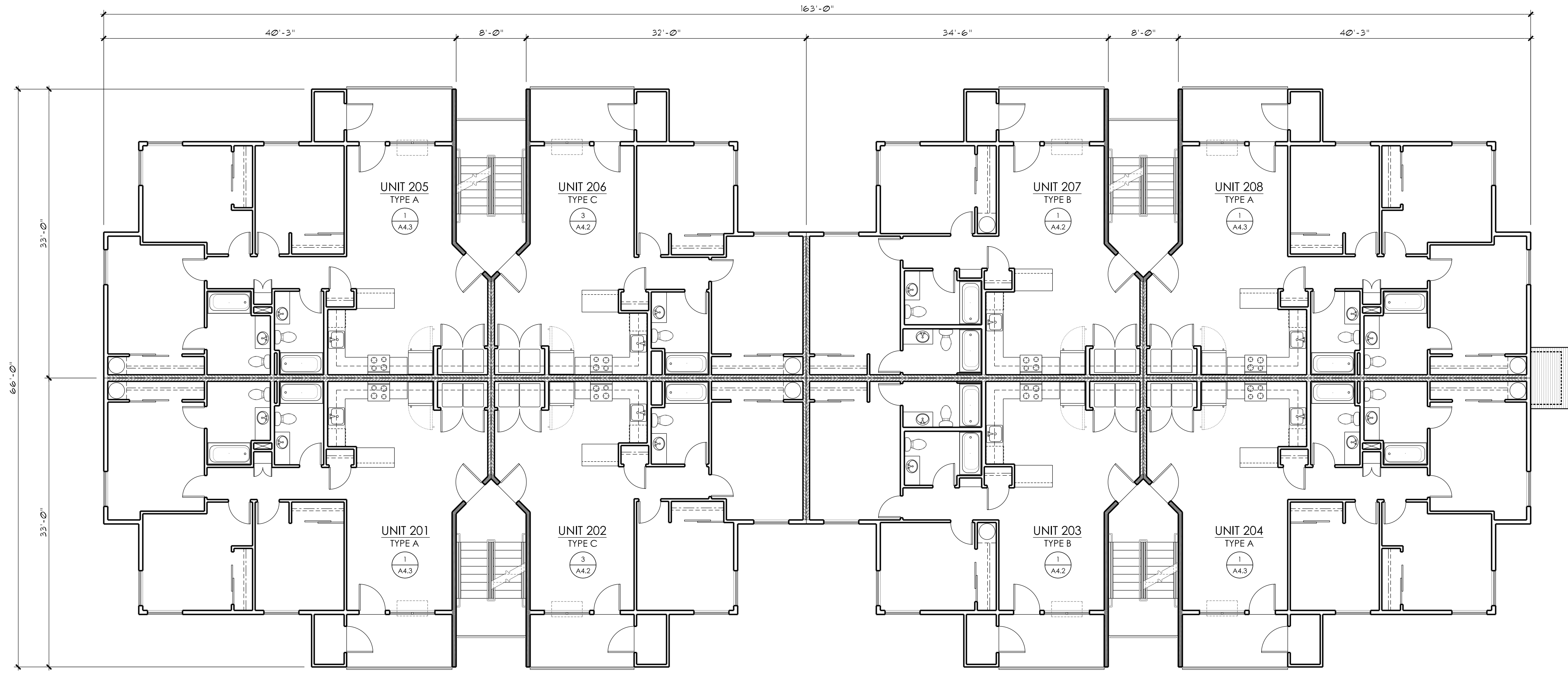
NORTON LANDING APARTMENTS  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED

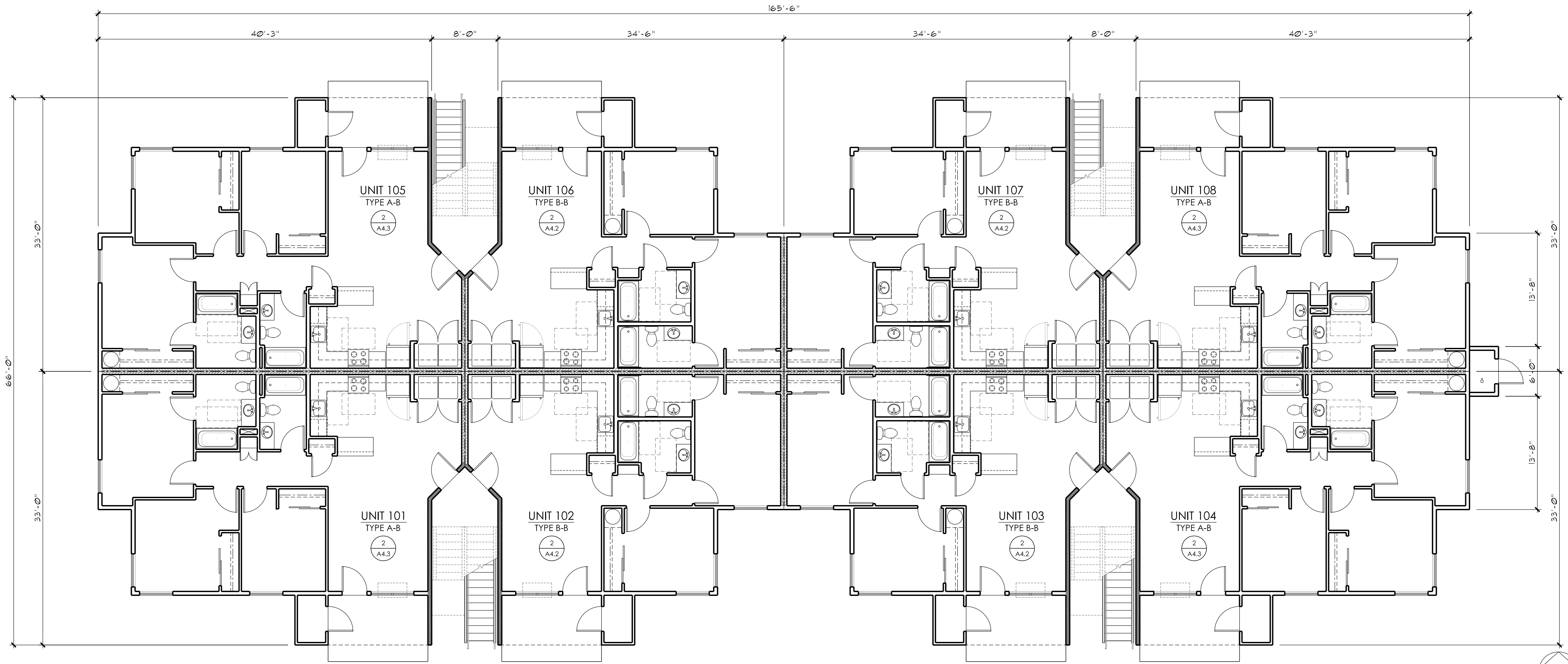
6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056





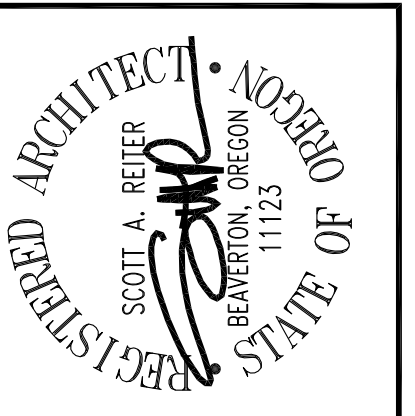
BUILDING 'F' - SECOND/THIRD FLOOR PLAN 2

Scale: 1/8" = 1'-0" A2.7



BUILDING 'F' - FIRST FLOOR PLAN 1

Scale: 1/8" = 1'-0" A2.7



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**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

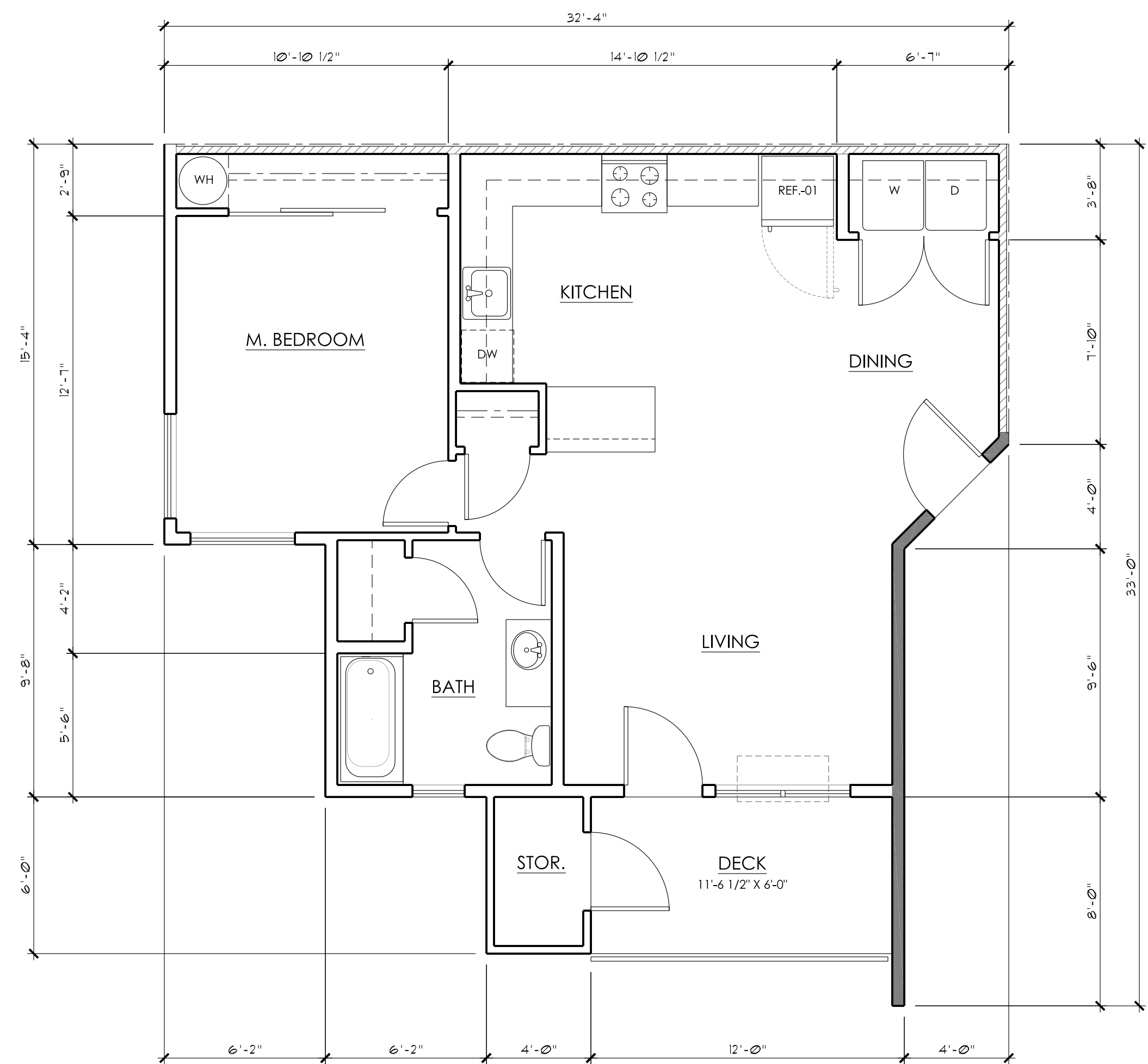
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

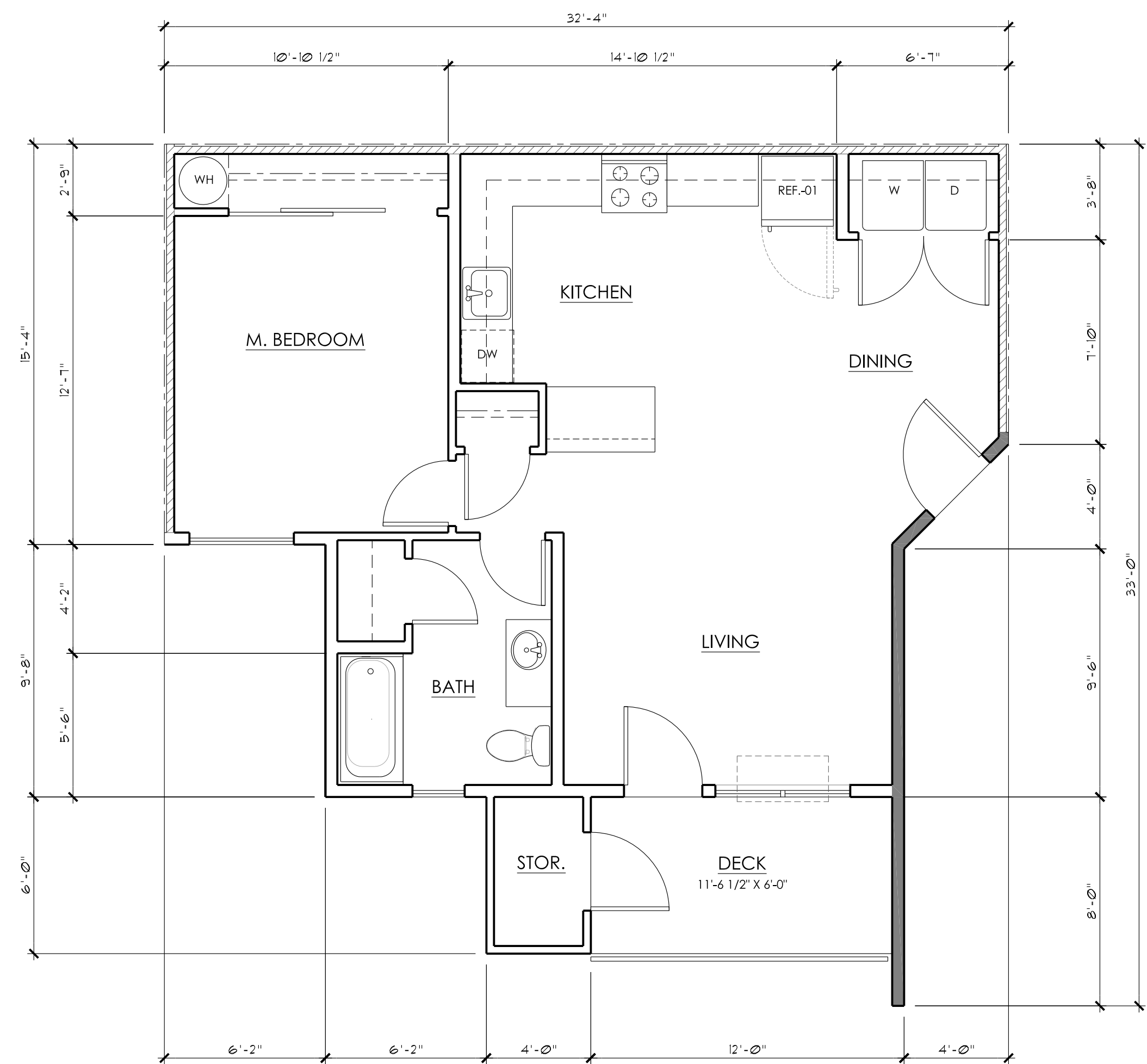
BUILDING PLANS  
BUILDING 'F'

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   |               |            |  |
| job no.: | 2301          |            |  |

**A**  
**2.7**



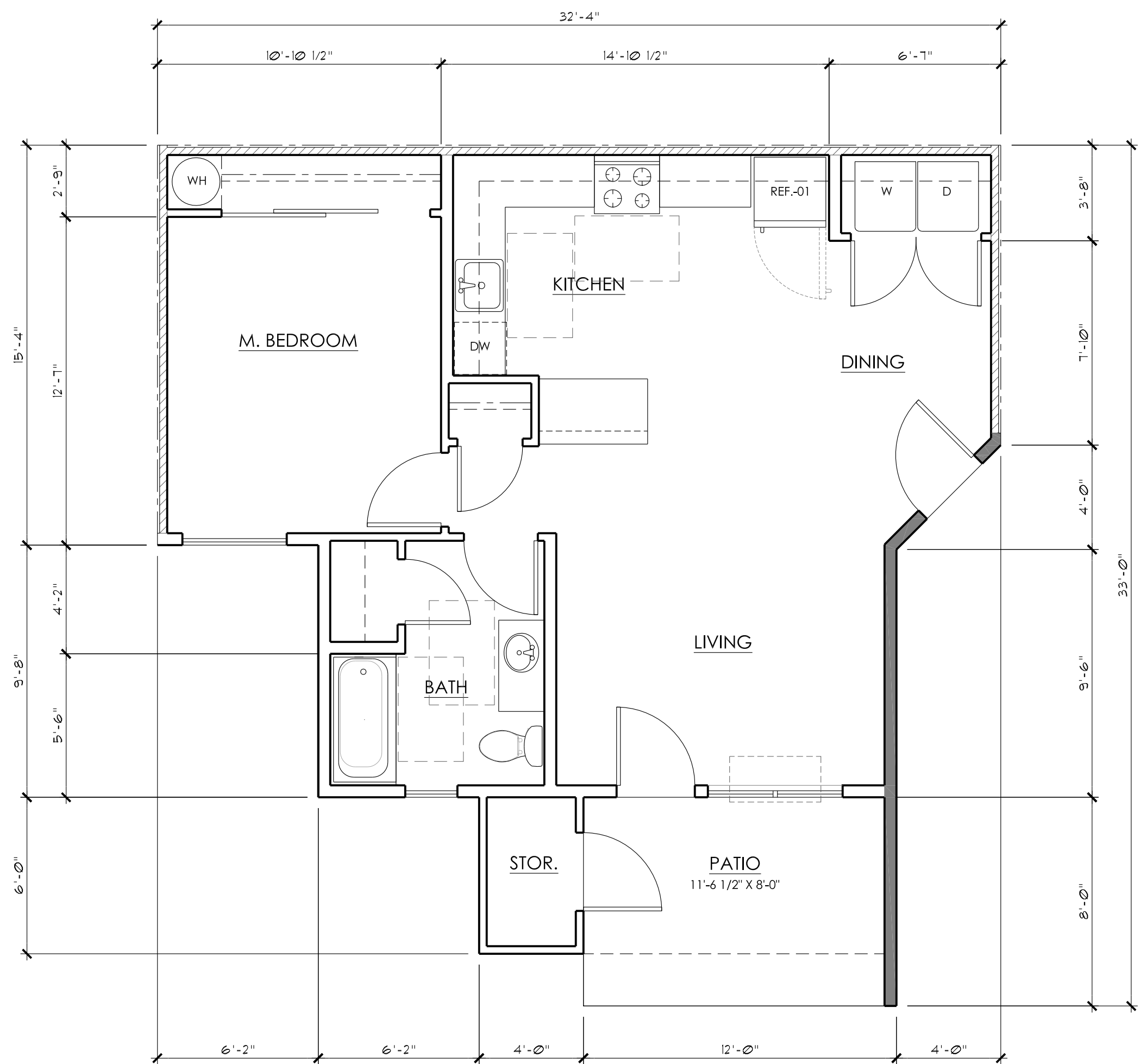
703 SF UNIT TYPE D - 1BR, 1 BATH - UPPER END UNIT 1  
Scale: 1/4" = 1'-0" A4.1



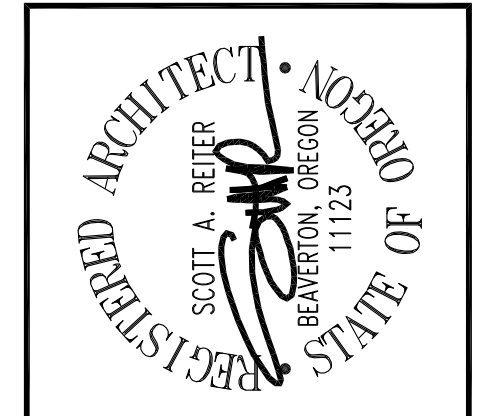
703 SF UNIT TYPE D - 1BR, 1 BATH UPPER MID-UNIT 2  
Scale: 1/4" = 1'-0" A4.1



703 SF UNIT TYPE D-B, 1BR, 1 BATH, ADA TYPE 'B' END UNIT 3  
Scale: 1/4" = 1'-0" A4.1



703 SF UNIT TYPE D-B, 1BR-1 BATH, ADA TYPE 'B' MID-UNIT 4  
Scale: 1/4" = 1'-0" A4.1



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
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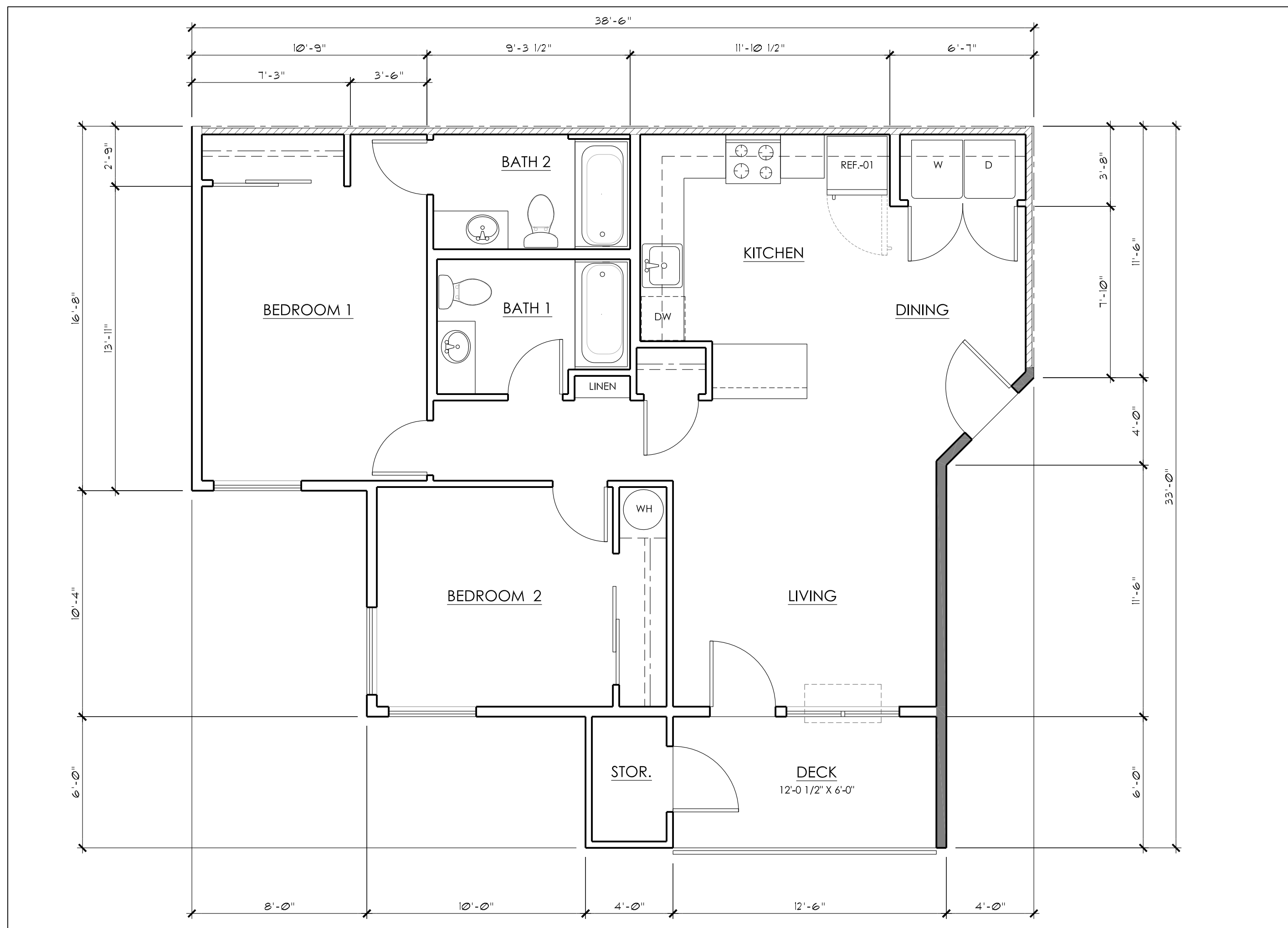
**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

CONCEPTUAL UNIT PLANS  
UNIT TYPES D, D-B

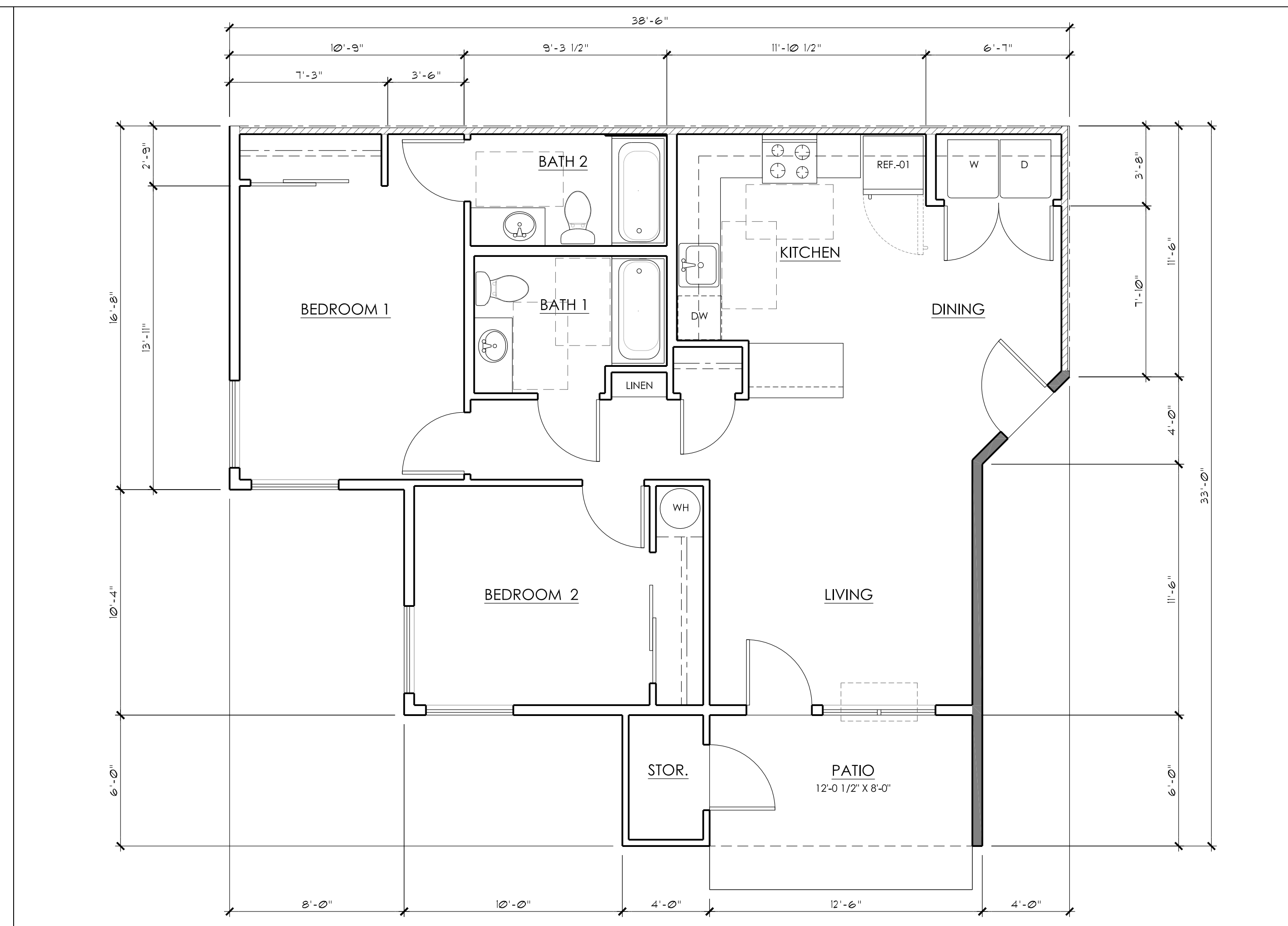
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| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

**A**  
**4.1**

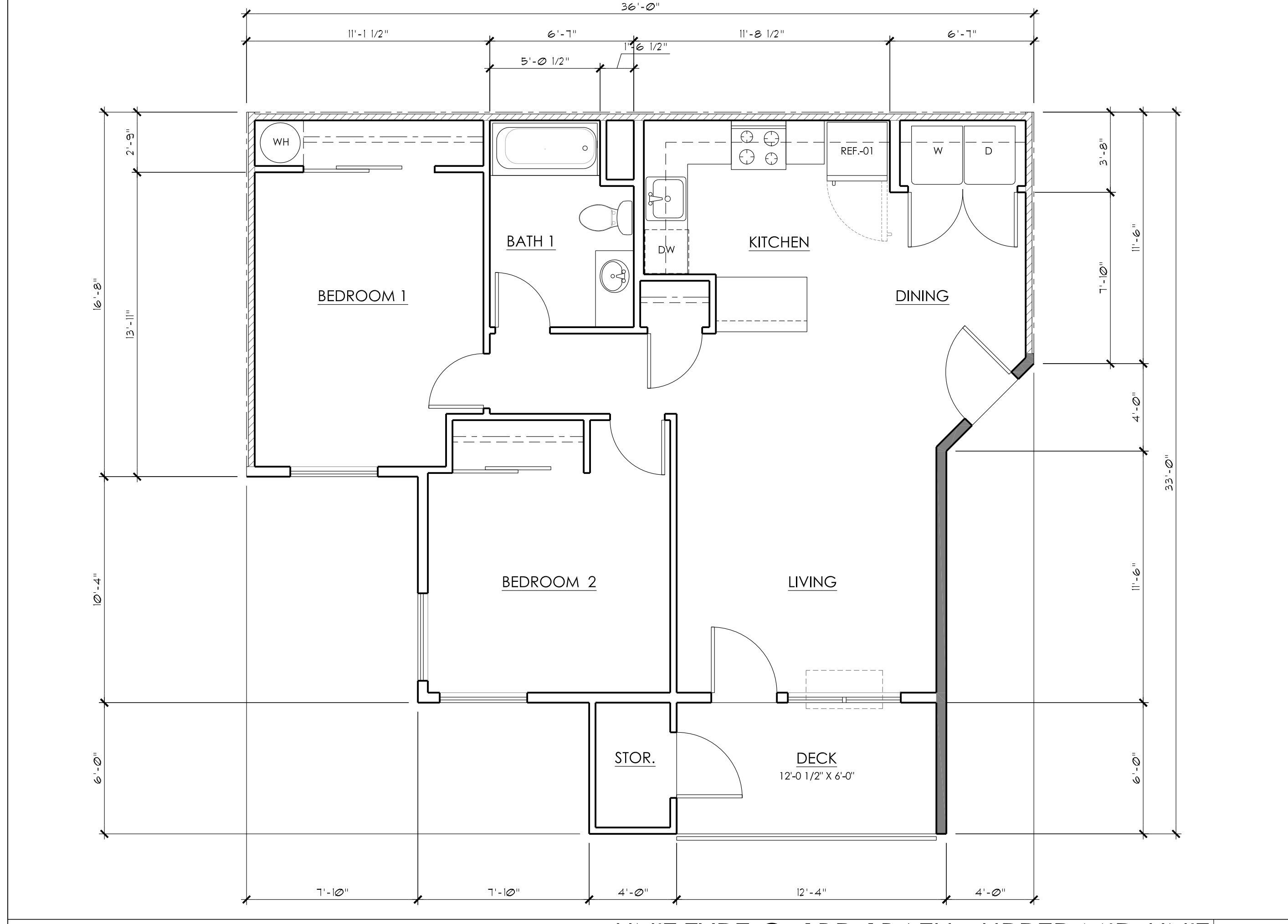
LAND-USE REVIEW



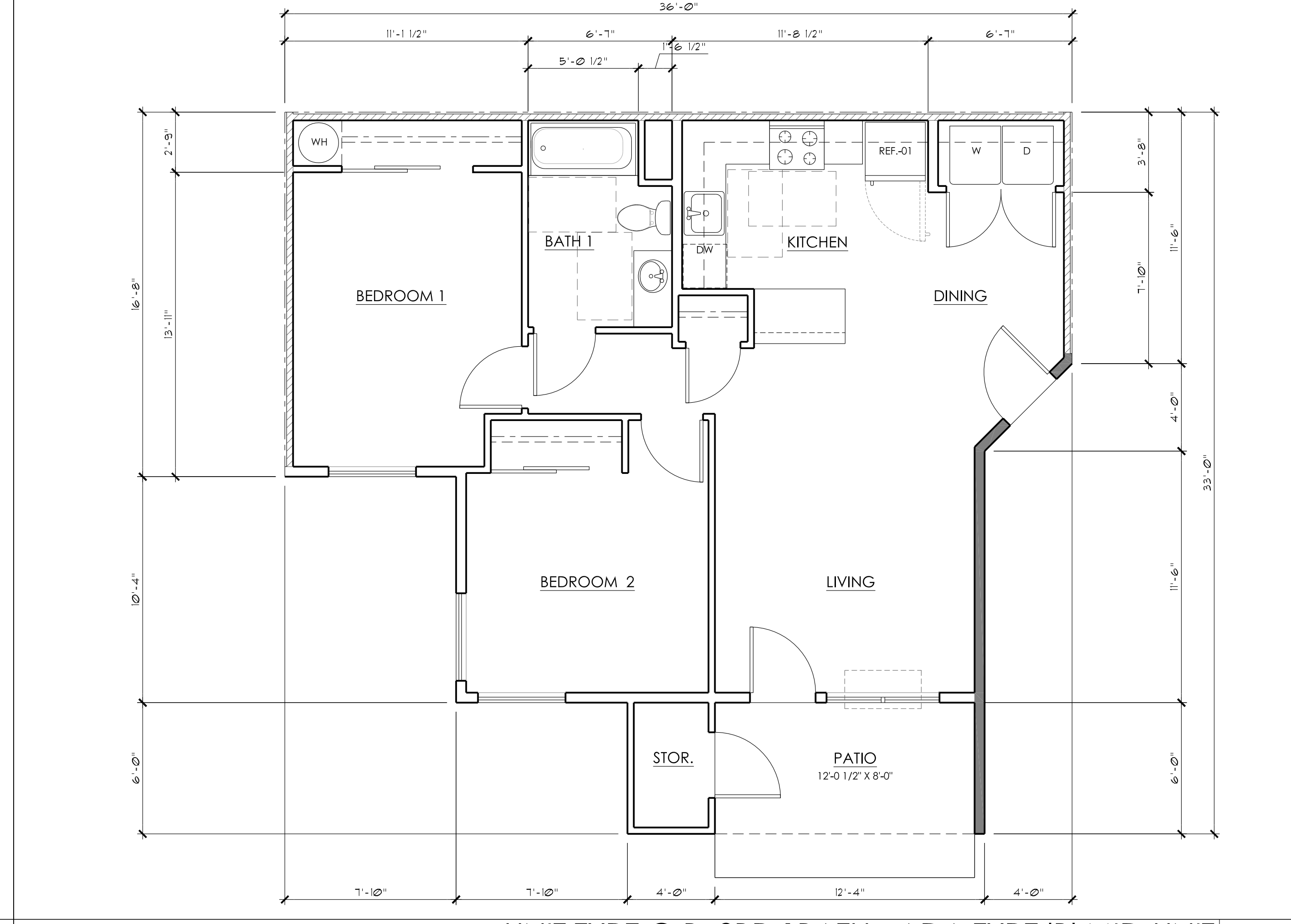
903 SF UNIT TYPE B, 2BR, 2BATH - UPPER END UNIT 1  
Scale: 1/4" = 1'-0" A4.2



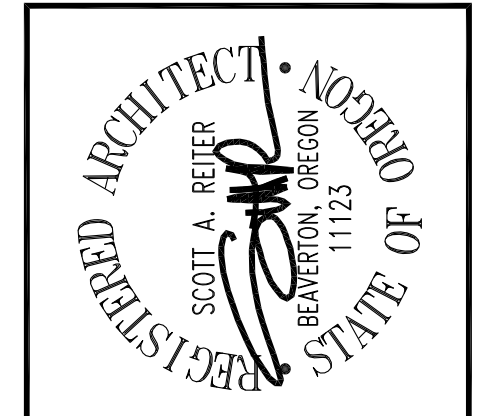
903 SF UNIT TYPE B-B, 2BR, 2 BATH - ADA TYPE 'B' END-UNIT 2  
Scale: 1/4" = 1'-0" A4.2



837 SF UNIT TYPE C, 1BR-1BATH - UPPER MID-UNIT 3  
Scale: 1/4" = 1'-0" A4.2



837 SF UNIT TYPE C-B, 2BR-1BATH - ADA TYPE 'B' MID-UNIT 4  
Scale: 1/4" = 1'-0" A4.2



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

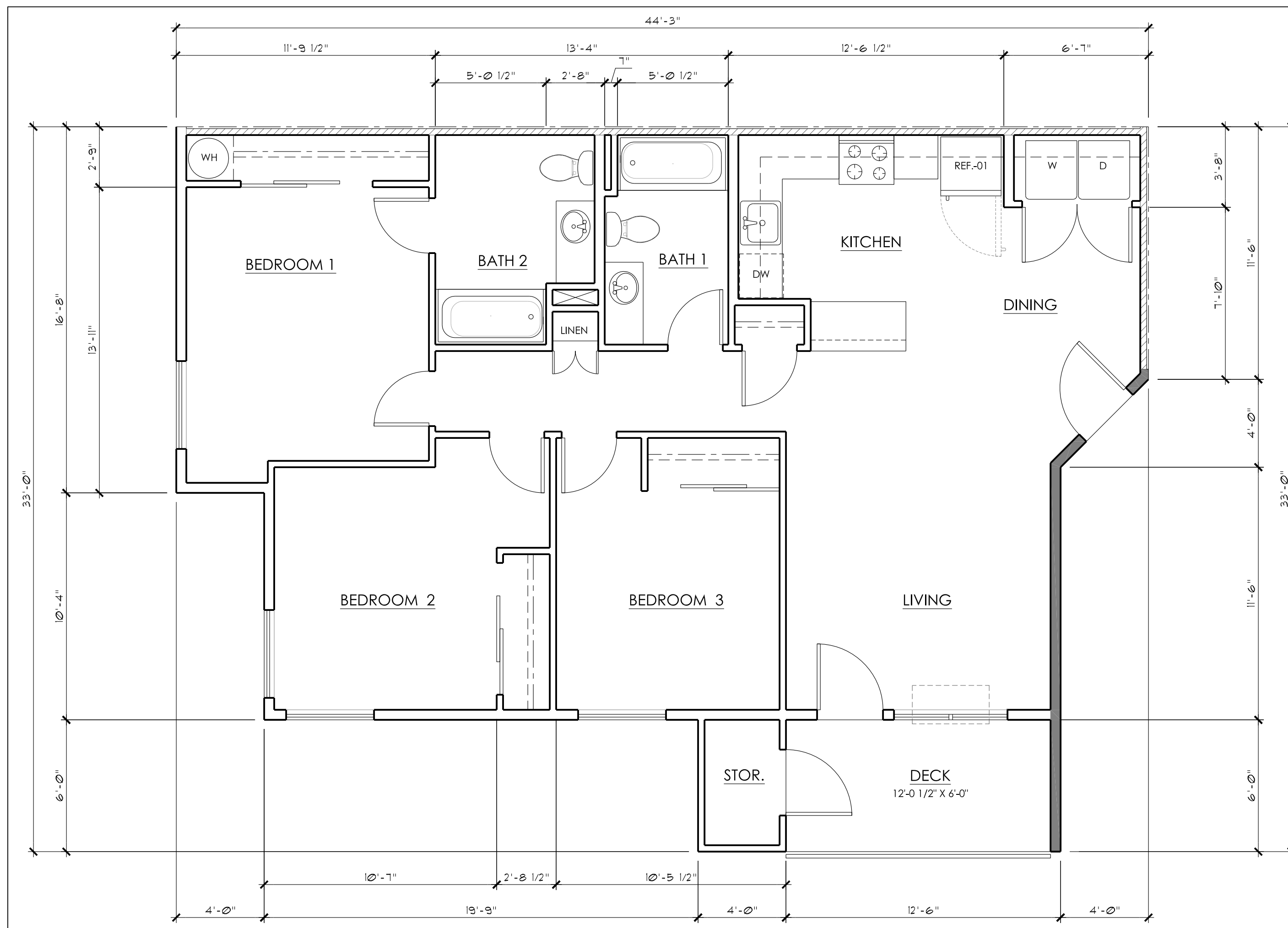
UNIT PLANS  
UNIT TYPES B, B-B, C, C-B

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

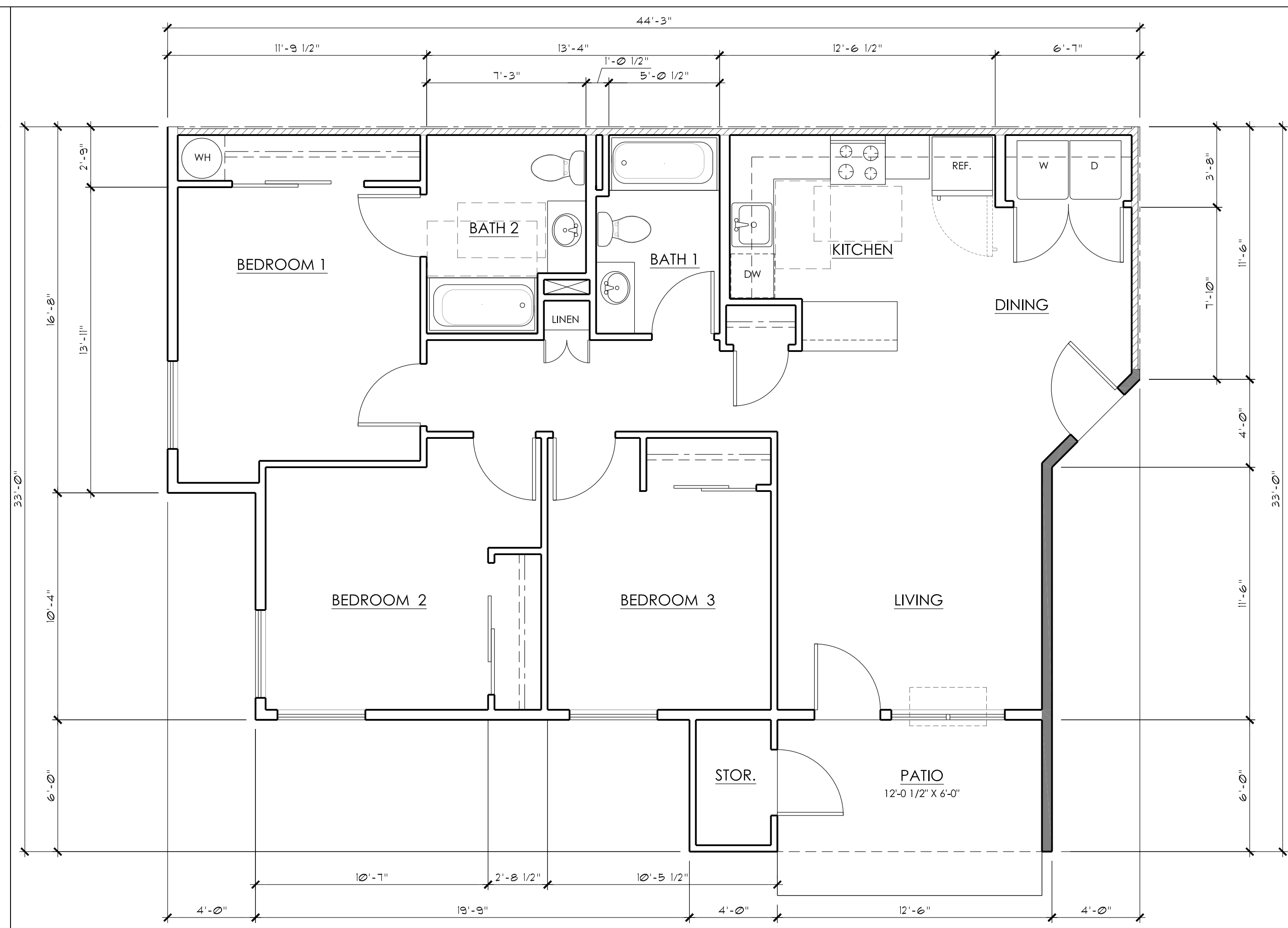
**A**  
**4.2**

LAND-USE REVIEW

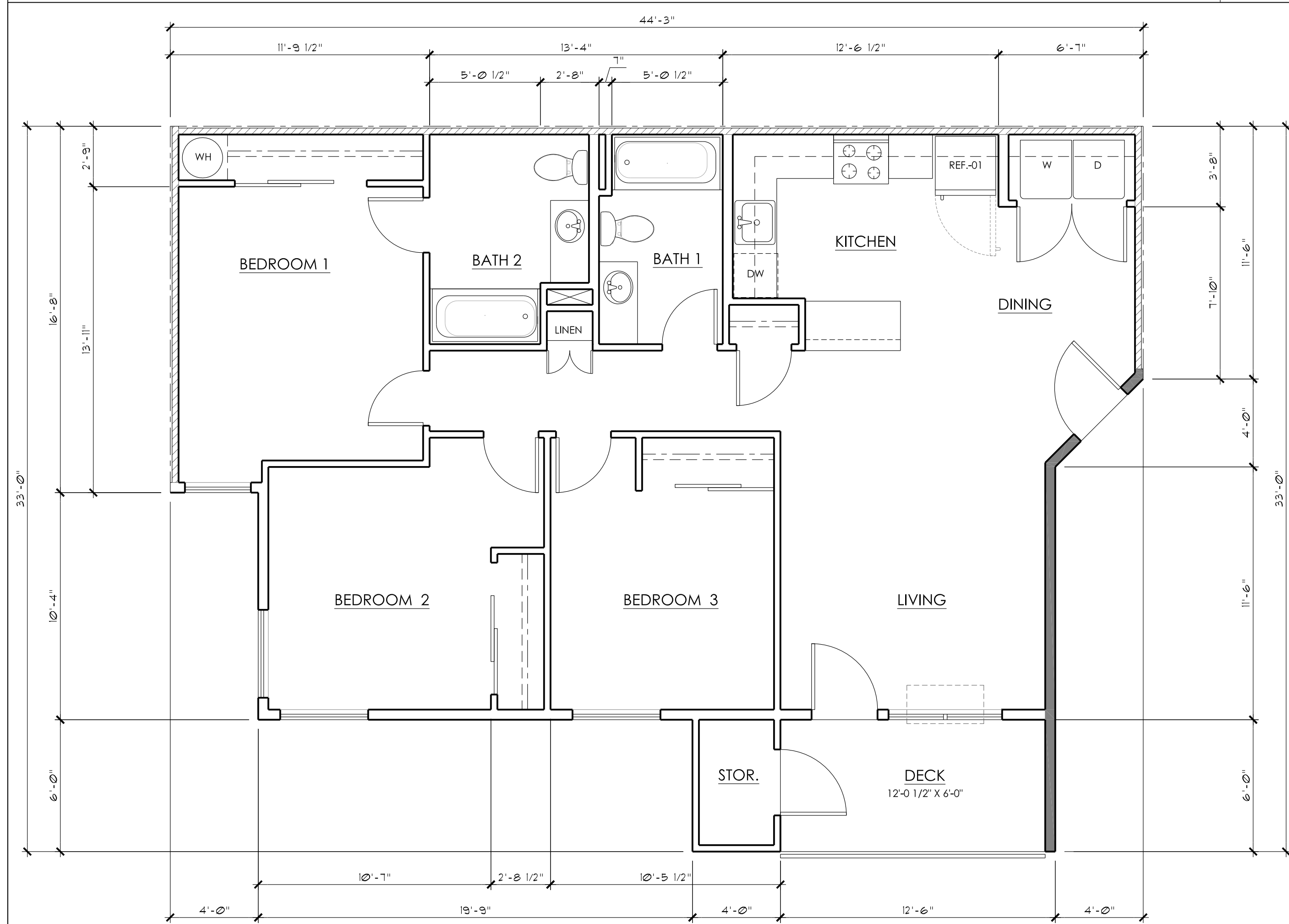




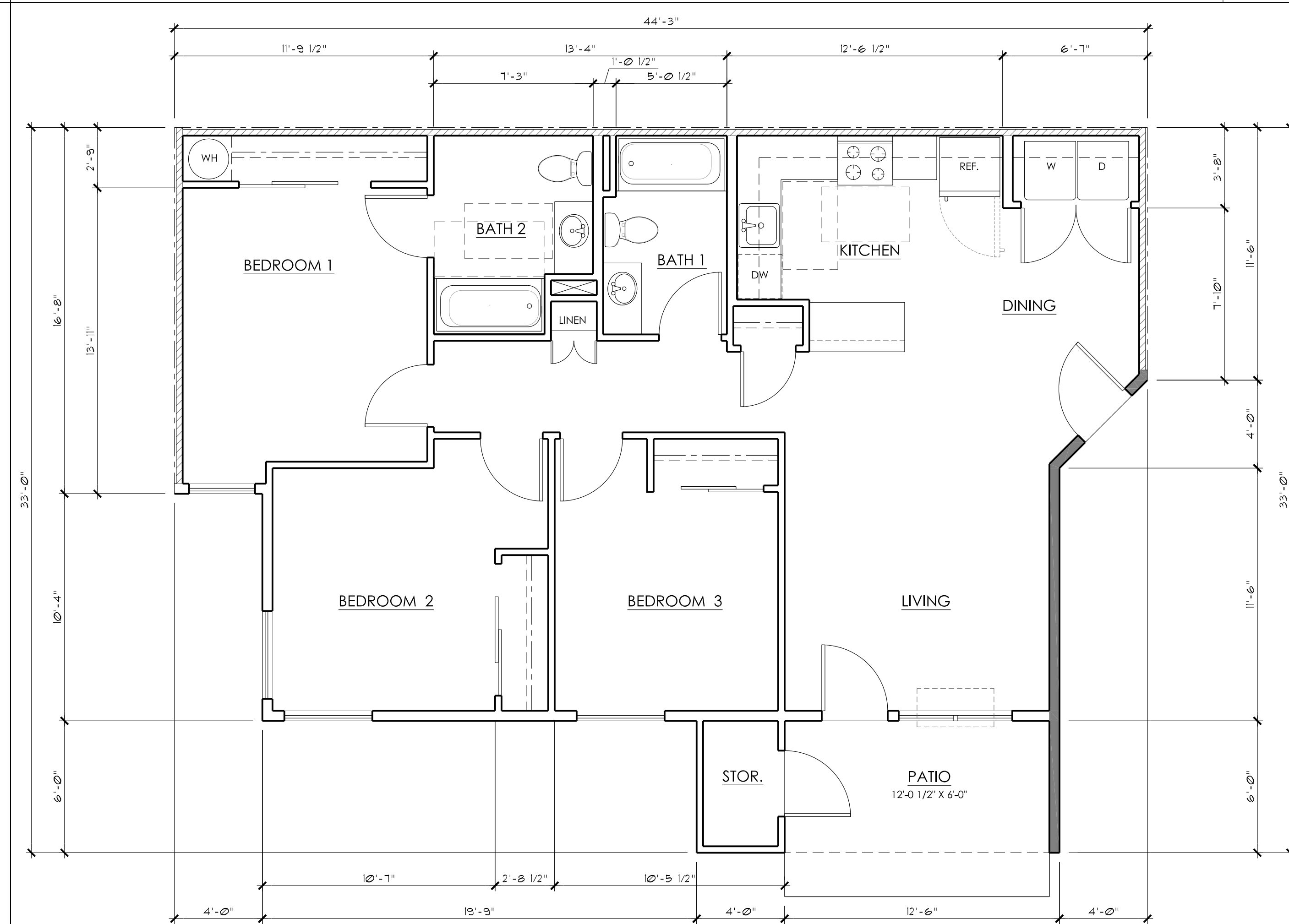
1,100 SF UNIT TYPE A, 3BR, 2BATH - UPPER END UNIT 1  
Scale: 1/4" = 1'-0" A4.3



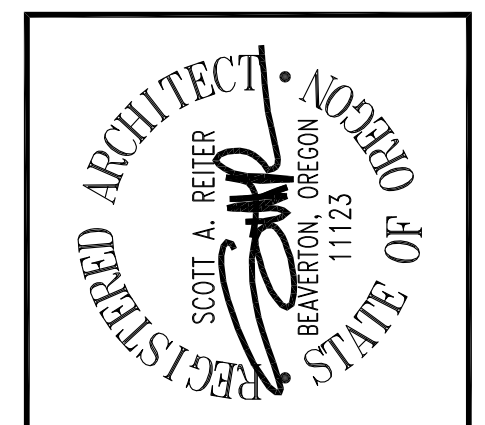
1,100 SF UNIT TYPE A-B, 3BR, 2 BATH - ADA TYPE 'B' END-UNIT 2  
Scale: 1/4" = 1'-0" A4.3



1,100 SF UNIT TYPE A - 3BR, 2 BATH, UPPER MID UNIT 3  
Scale: 1/4" = 1'-0" A4.3



1,100 SF UNIT TYPE A-B, 3BR, 2 BATH, ADA TYPE 'B' MID-UNIT 4  
Scale: 1/4" = 1'-0" A4.3



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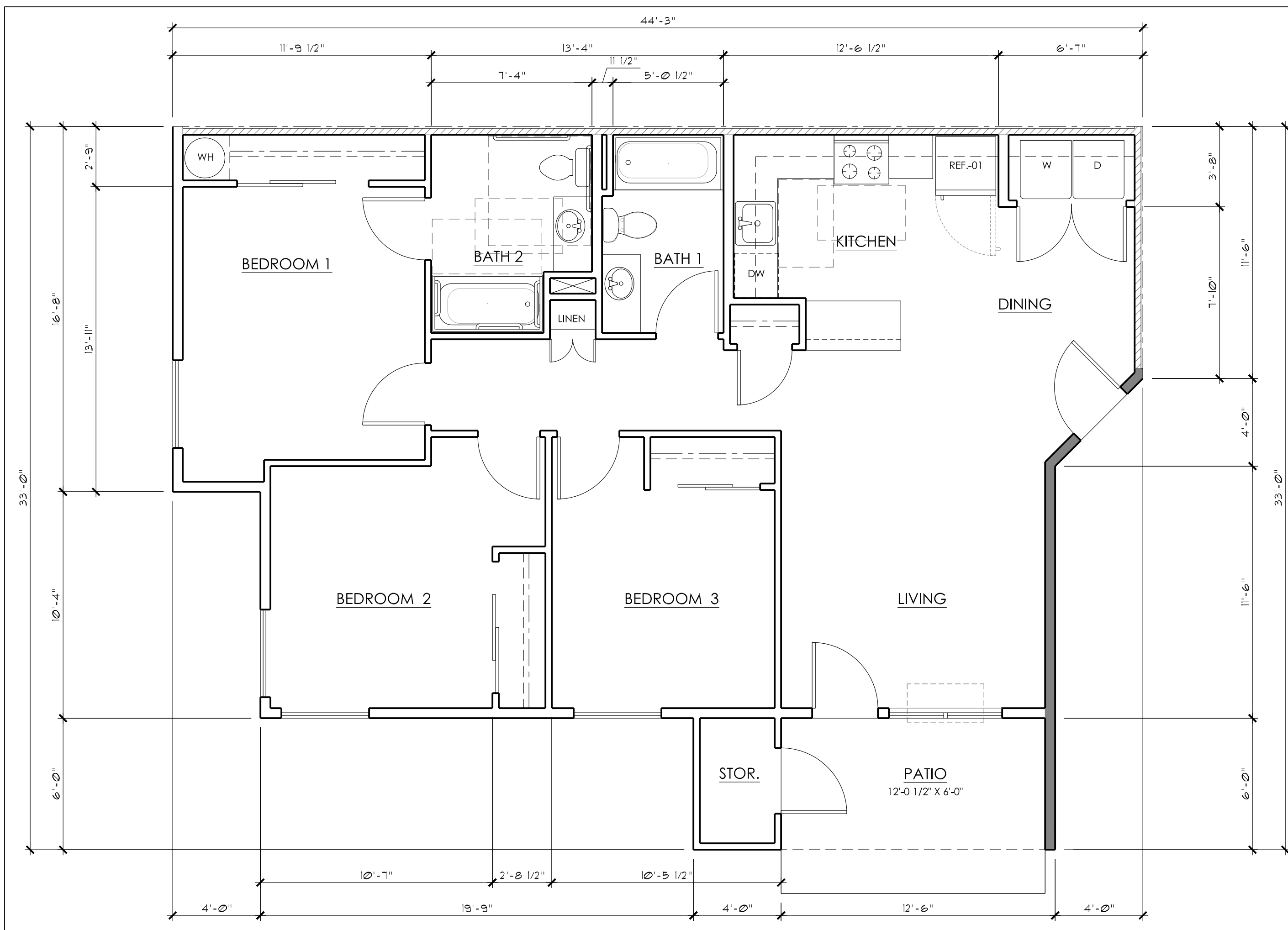
**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

UNIT PLANS  
UNIT TYPES A, A-B

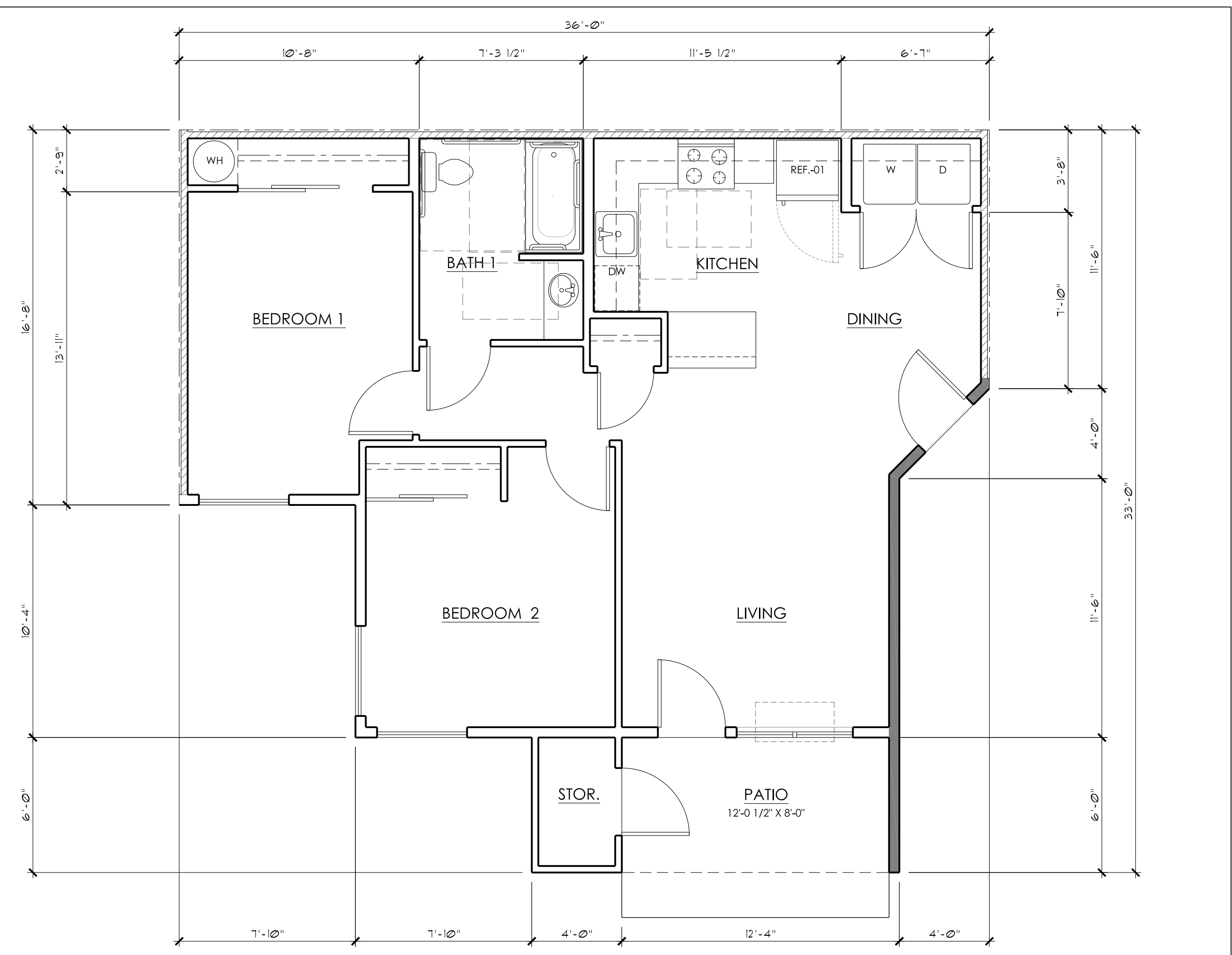
|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
| drawn:   | SAR           |
| job no.: | 2301          |

LAND-USE REVIEW

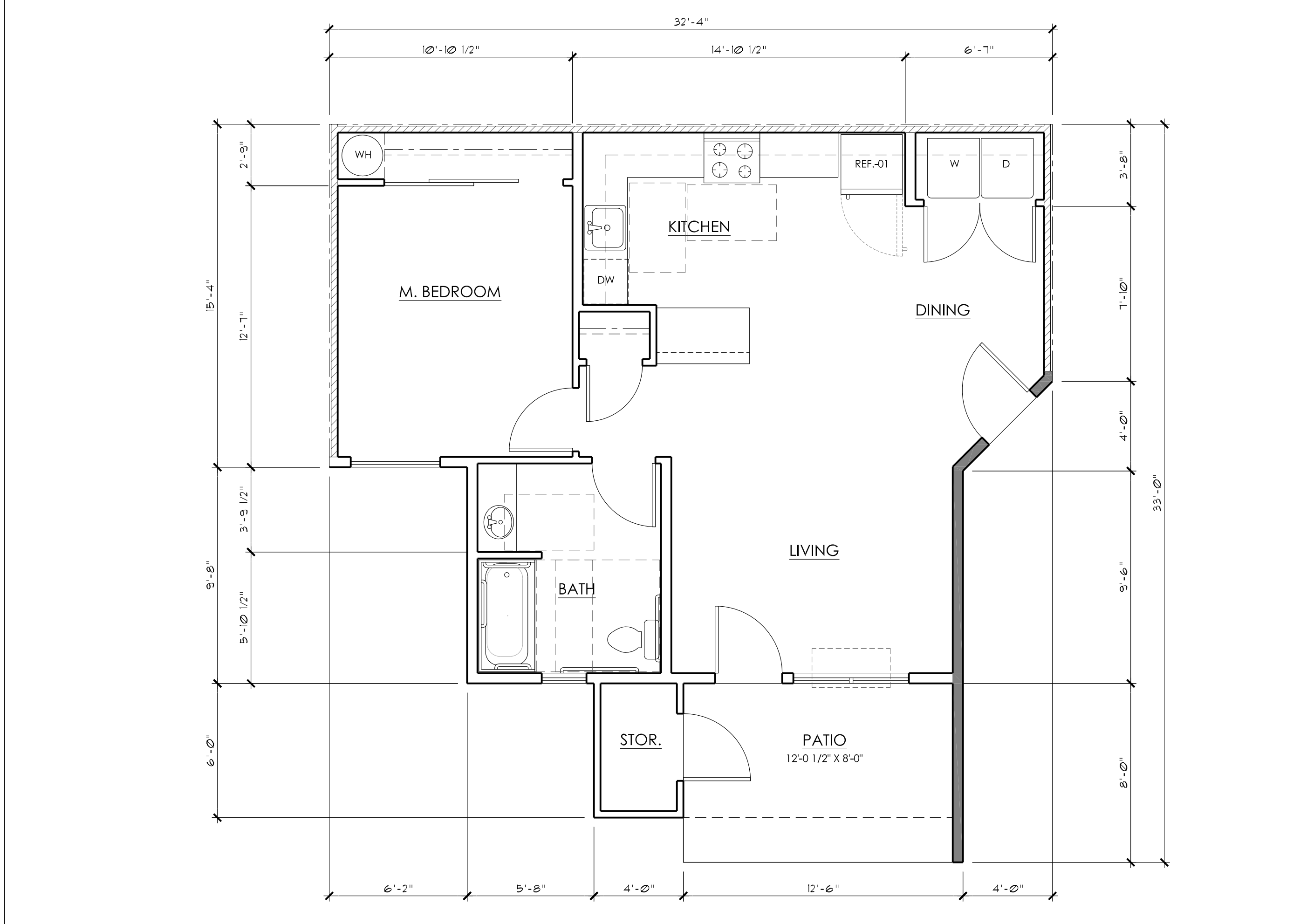
A  
4.3



1,100 SF UNIT TYPE A-A, 3BR, 2BATH - ADA TYPE 'A' END UNIT 1  
Scale: 1/4" = 1'-0" A4.4



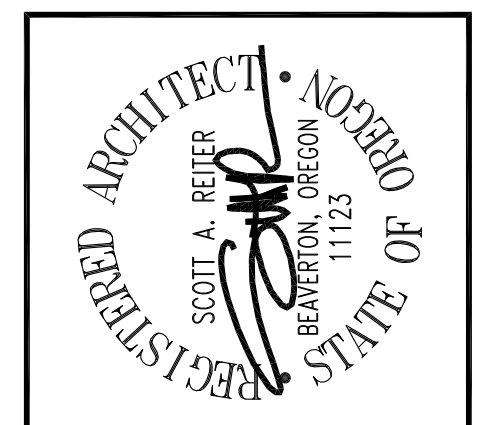
837 SF UNIT TYPE C-A, 2BR, 1 BATH - ADA TYPE 'A' MID-UNIT 2  
Scale: 1/4" = 1'-0" A4.4



703 SF UNIT TYPE D-A, 1BR-1BATH - ADA TYPE 'A' MID-UNIT 3  
Scale: 1/4" = 1'-0" A4.4



837 SF UNIT TYPE A-A, 3BR, 2BATH - ADA TYPE 'A' END UNIT 4  
Scale: 1/4" = 1'-0" A4.4



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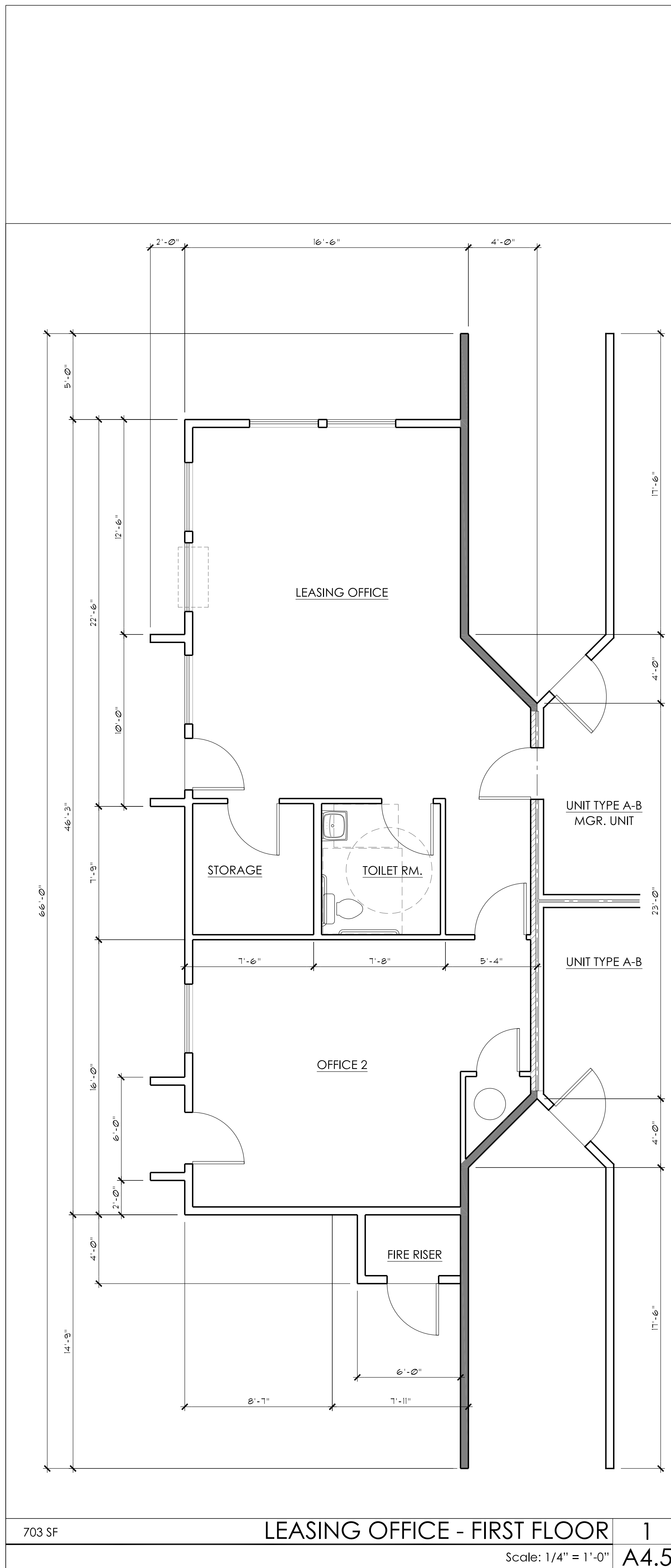
**NORTON LANDING APARTMENTS**  
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OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

UNIT PLANS, ADA TYPE 'A' UNITS  
UNIT TYPES A-A, C-A, D-A

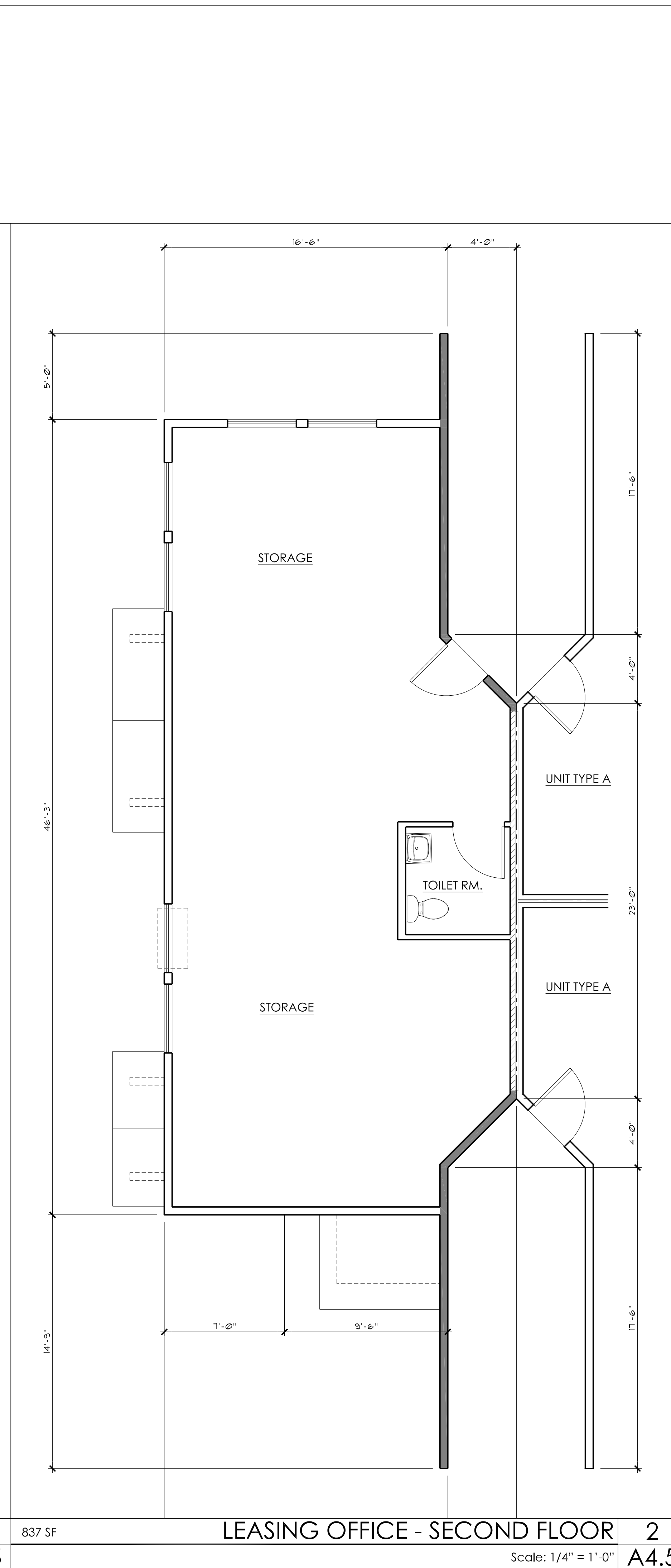
|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

LAND-USE REVIEW

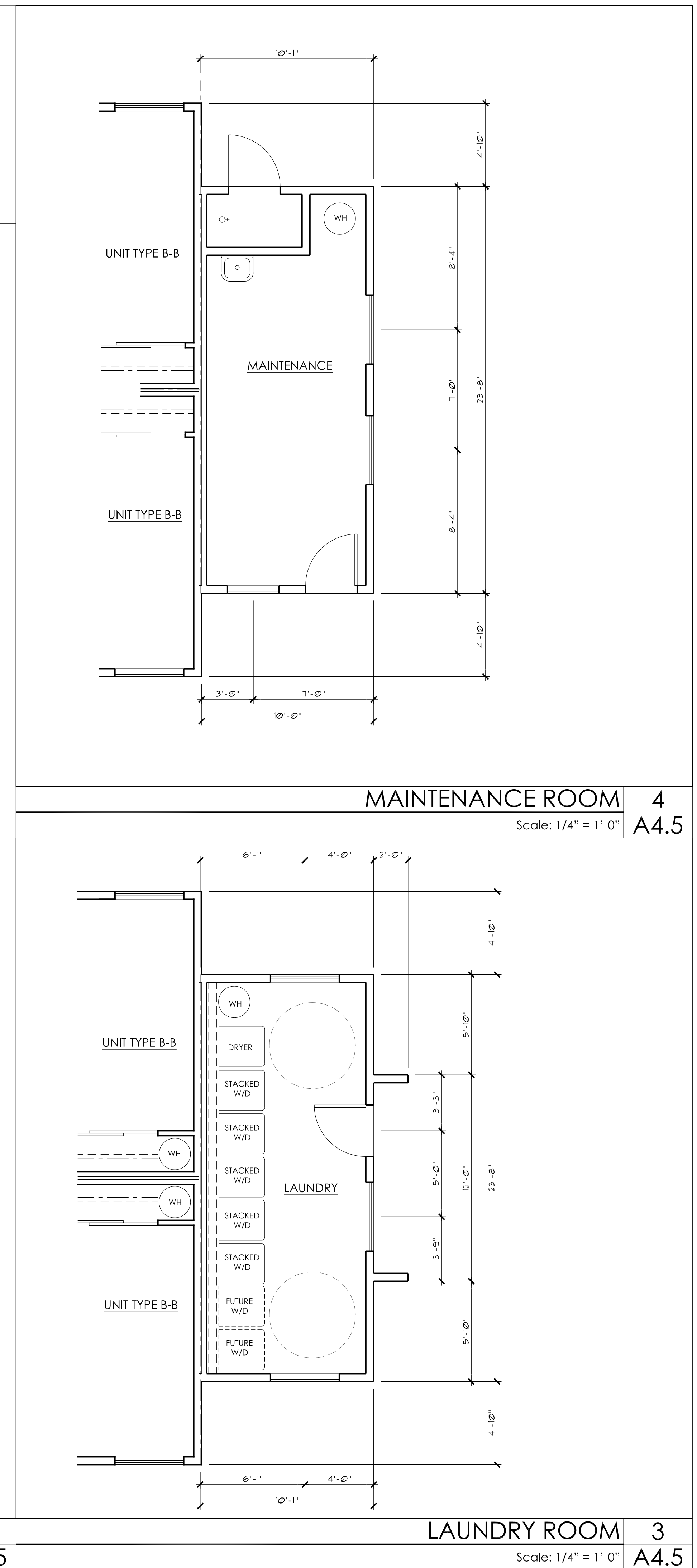
**A**  
**4.4**



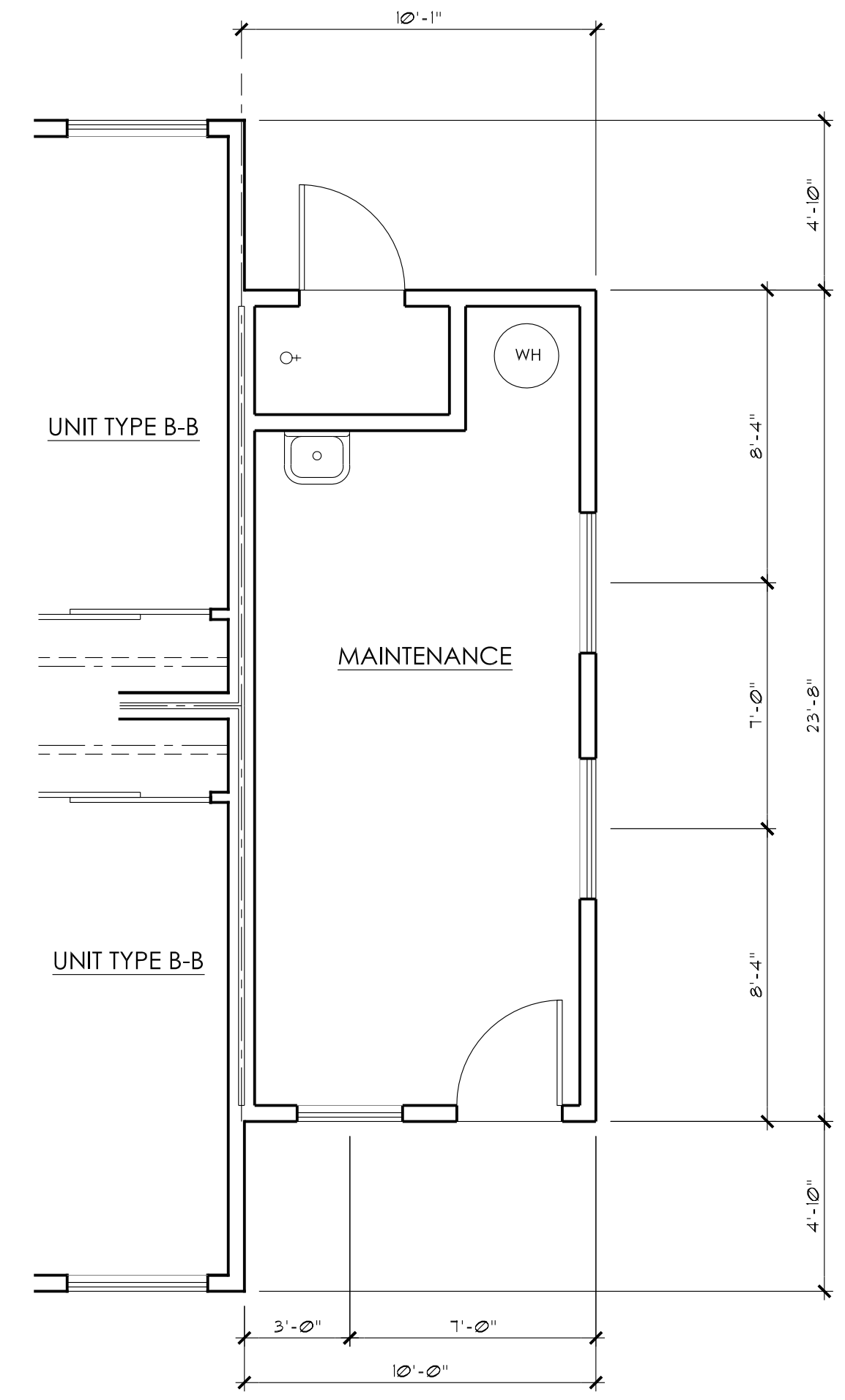
703 SF LEASING OFFICE - FIRST FLOOR 1  
Scale: 1/4" = 1'-0" A4.5



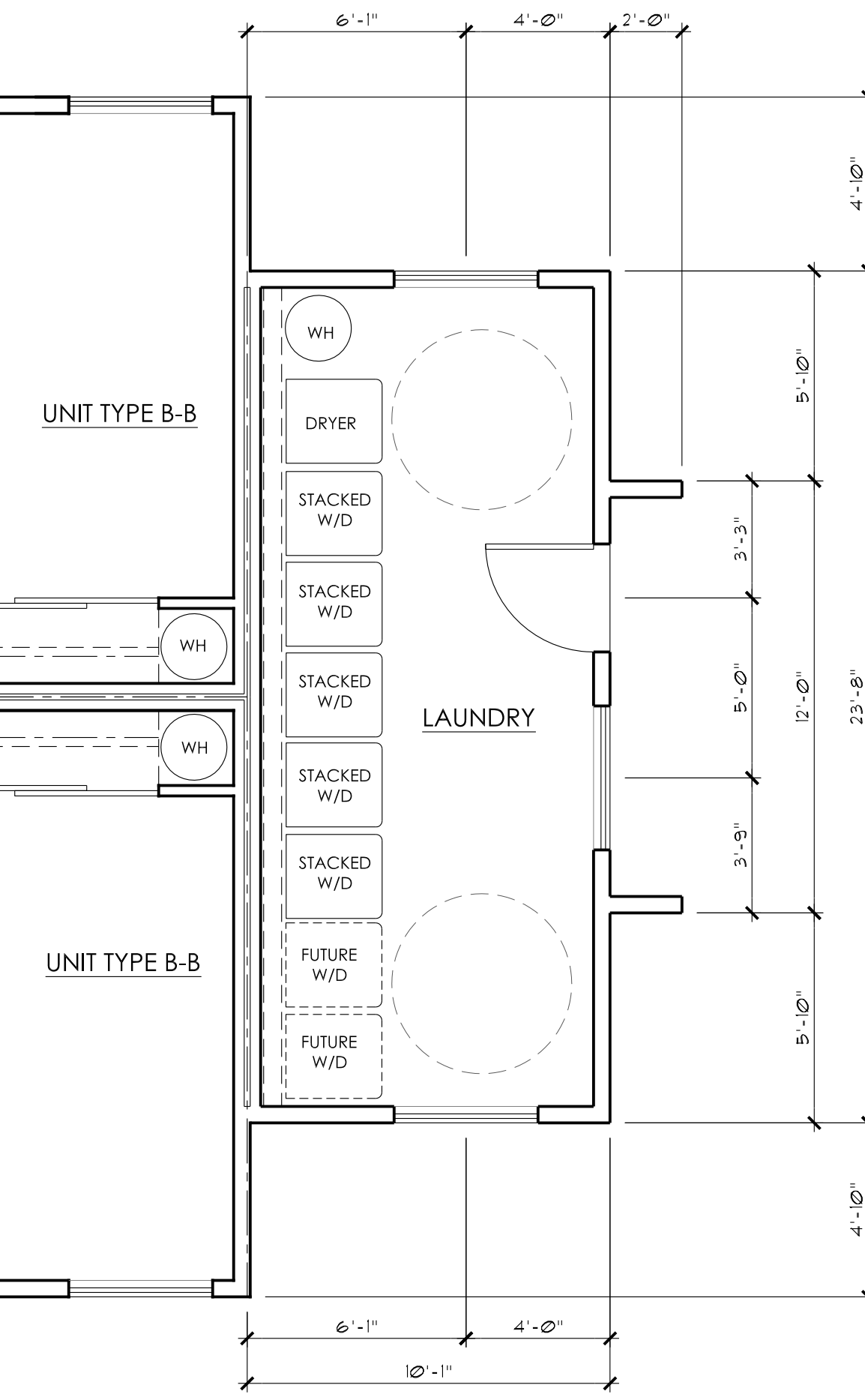
837 SF LEASING OFFICE - SECOND FLOOR 2  
Scale: 1/4" = 1'-0" A4.5



LAUNDRY ROOM 3  
Scale: 1/4" = 1'-0" A4.5



MAINTENANCE ROOM 4  
Scale: 1/4" = 1'-0" A4.5



LAND-USE REVIEW

LEASING OFFICE PLANS  
LAUNDRY ROOM PLAN  
MAINTENANCE PLAN

date: JULY 19, 2023  
scale: AS NOTED  
drawn: SAR  
job no: 2301

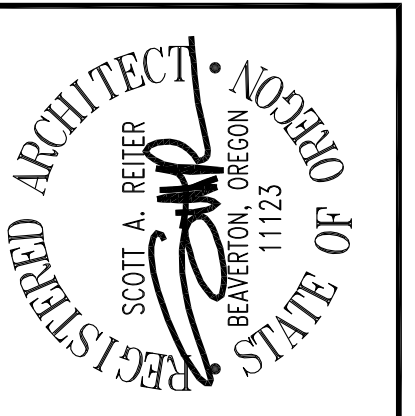
**A**  
**4.5**

**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

REGISTERED ARCHITECT  
SCOTT A. REITER  
BEAVERTON, OREGON  
11123  
STATE OF OREGON

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685



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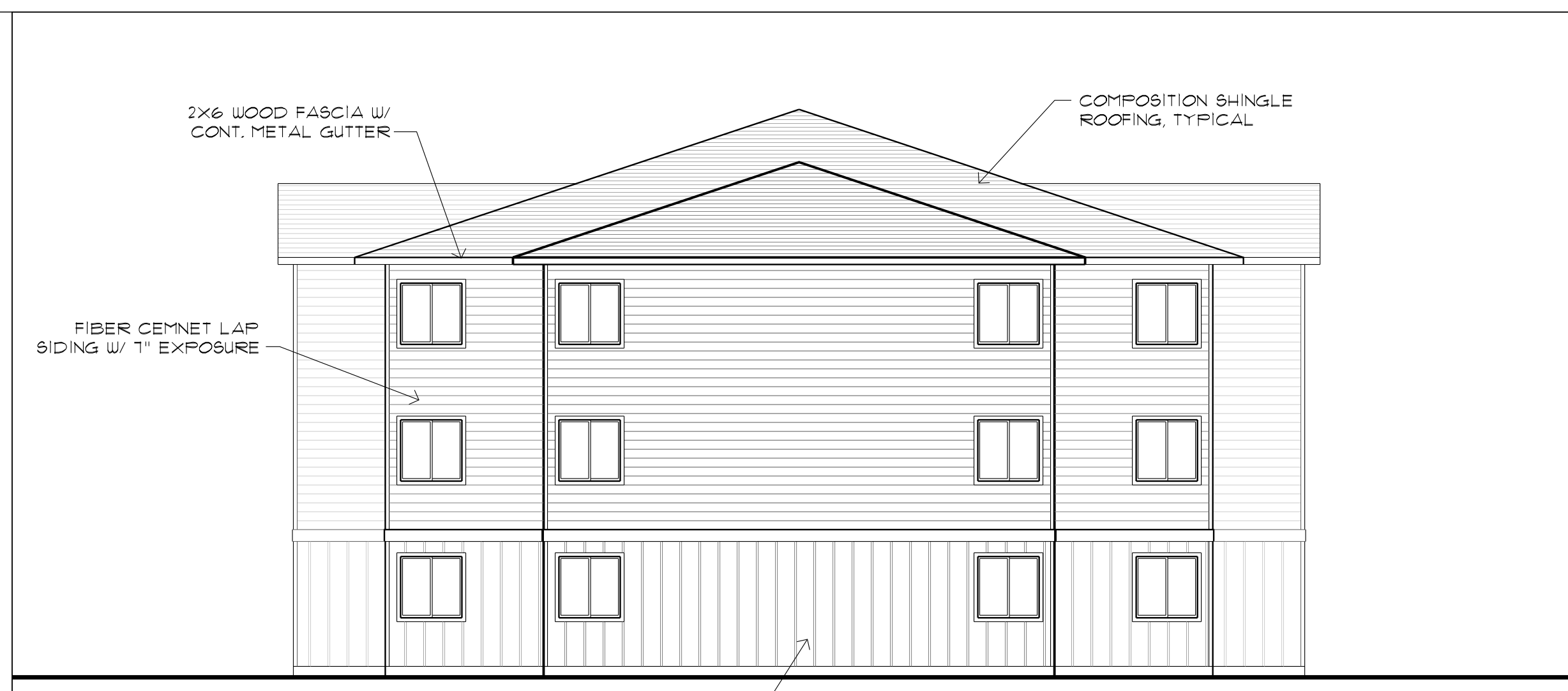
**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

EXTERIOR ELEVATIONS  
 BUILDING A

|          |               |
|----------|---------------|
| DATE:    | JULY 19, 2023 |
| SCALE:   | AS NOTED      |
| DRAWN:   | SAR           |
| JOB NO.: | 2301          |

**A**  
**7.1**

LAND-USE REVIEW



**BUILDING 'A' - NORTH ELEVATION** 3  
 Scale: 1/8" = 1'-0" **A7.1**



**BUILDING 'A' - WEST ELEVATION** 4  
 Scale: 1/8" = 1'-0" **A7.1**



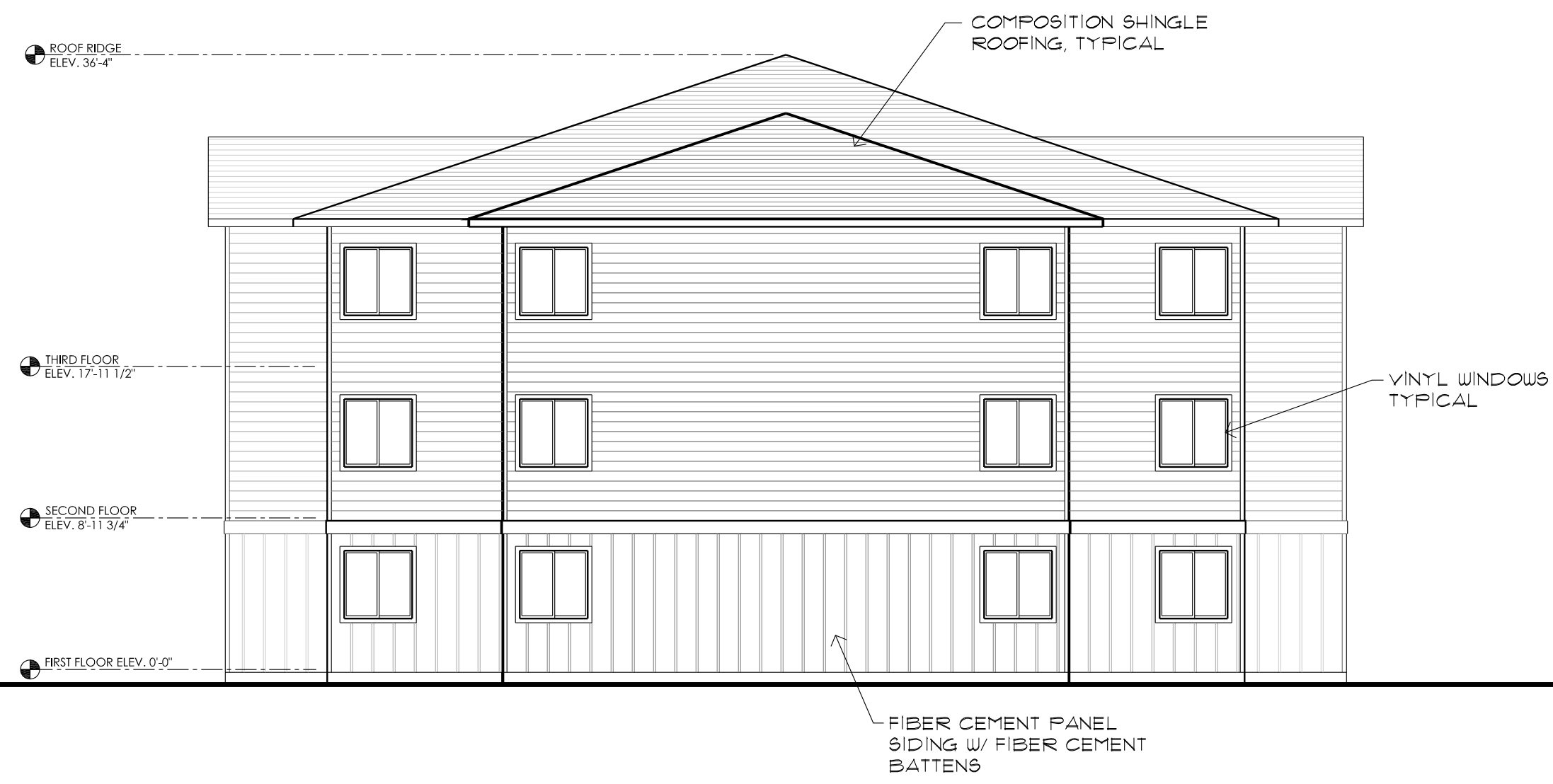
**BUILDING 'A' - SOUTH ELEVATION** 2  
 Scale: 1/8" = 1'-0" **A7.1**



**BUILDING 'A' - EAST ELEVATION** 1  
 Scale: 1/8" = 1'-0" **A7.1**



**BUILDING 'B' - WEST ELEVATION** 4  
 Scale: 1/8" = 1'-0" A7.2



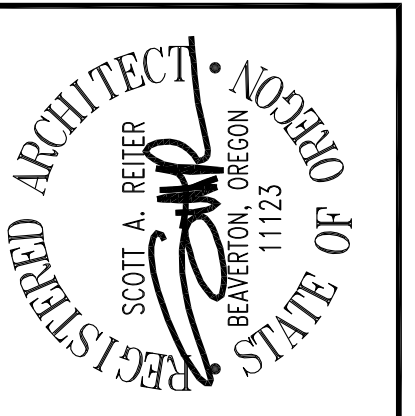
**BUILDING 'B' - SOUTH ELEVATION** 2  
 Scale: 1/8" = 1'-0" A7.2



**BUILDING 'B' - NORTH ELEVATION** 3  
 Scale: 1/8" = 1'-0" A7.2



**BUILDING 'B' - EAST ELEVATION** 1  
 Scale: 1/8" = 1'-0" A7.2



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**NORTON LANDING APARTMENTS**  
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 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

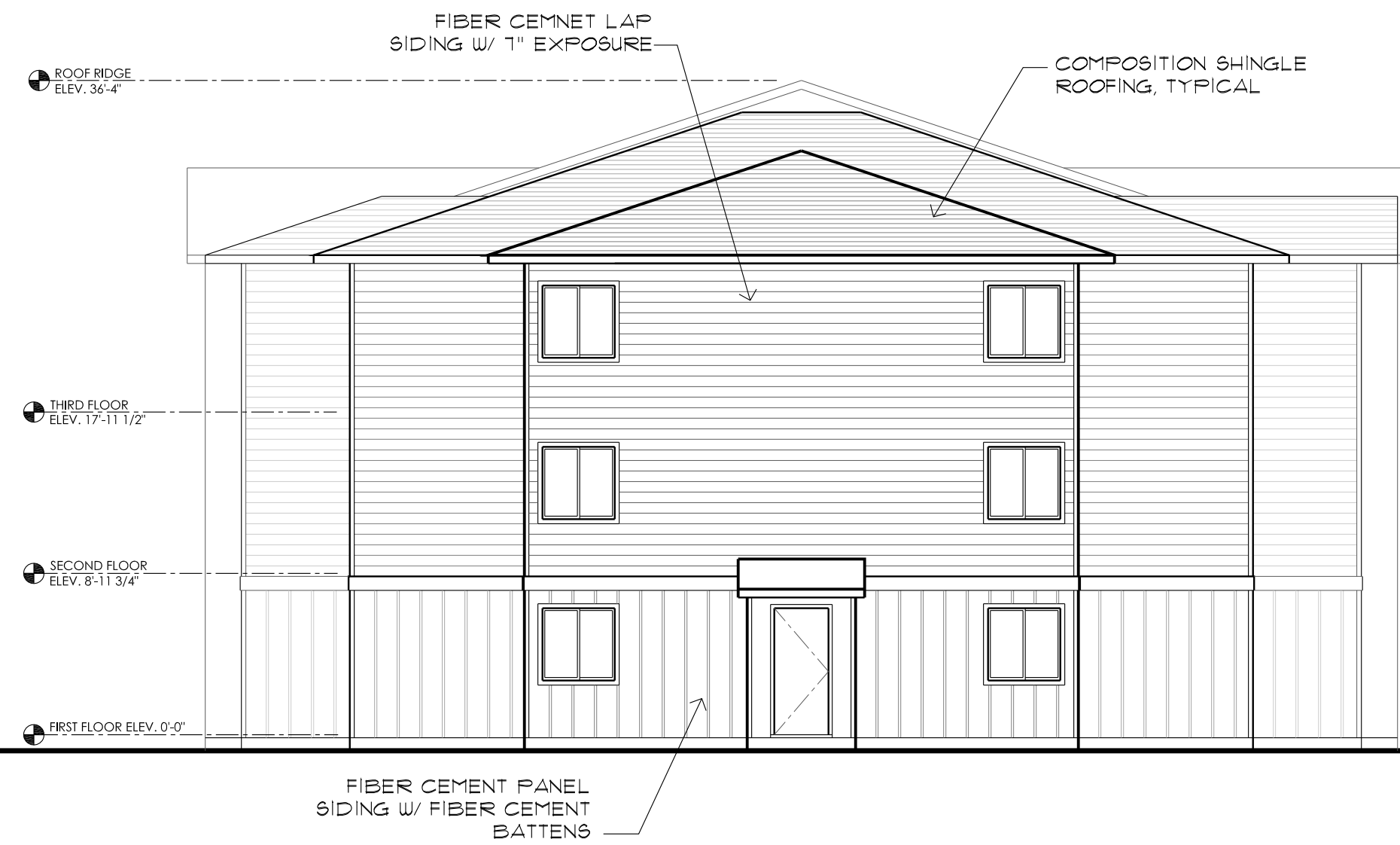
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|-----------------------------------|---------------------|------------|
| EXTERIOR ELEVATIONS<br>BUILDING B | date: JULY 19, 2023 | revisions: |
|                                   | scale: AS NOTED     |            |
|                                   | drawn: SAR          |            |
|                                   | job no: 2301        |            |

**A**  
**7.2**

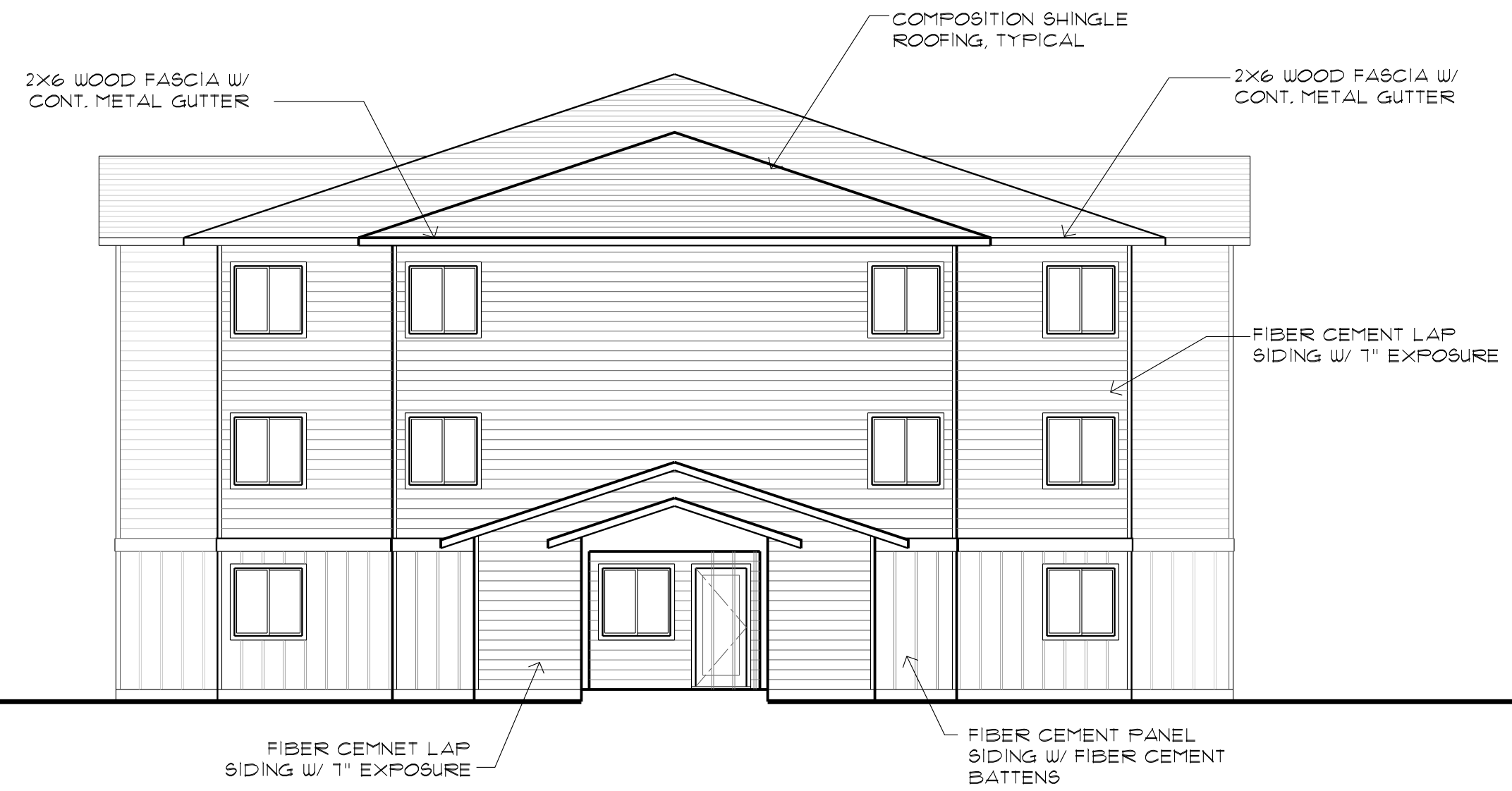




**BUILDING 'C' - NORTH ELEVATION** 4  
 Scale: 1/8" = 1'-0" A7.3



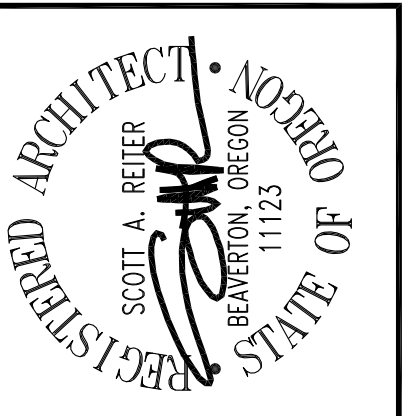
**BUILDING 'C' - WEST ELEVATION** 2  
 Scale: 1/8" = 1'-0" A7.3



**BUILDING 'C' - EAST ELEVATION** 3  
 Scale: 1/8" = 1'-0" A7.3



**BUILDING 'C' - SOUTH ELEVATION** 1  
 Scale: 1/8" = 1'-0" A7.3



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
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 BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

|                                   |                     |            |
|-----------------------------------|---------------------|------------|
| EXTERIOR ELEVATIONS<br>BUILDING C | date: JULY 19, 2023 | revisions: |
|                                   | scale: AS NOTED     |            |
|                                   | drawn: SAR          |            |
|                                   | job no: 2301        |            |

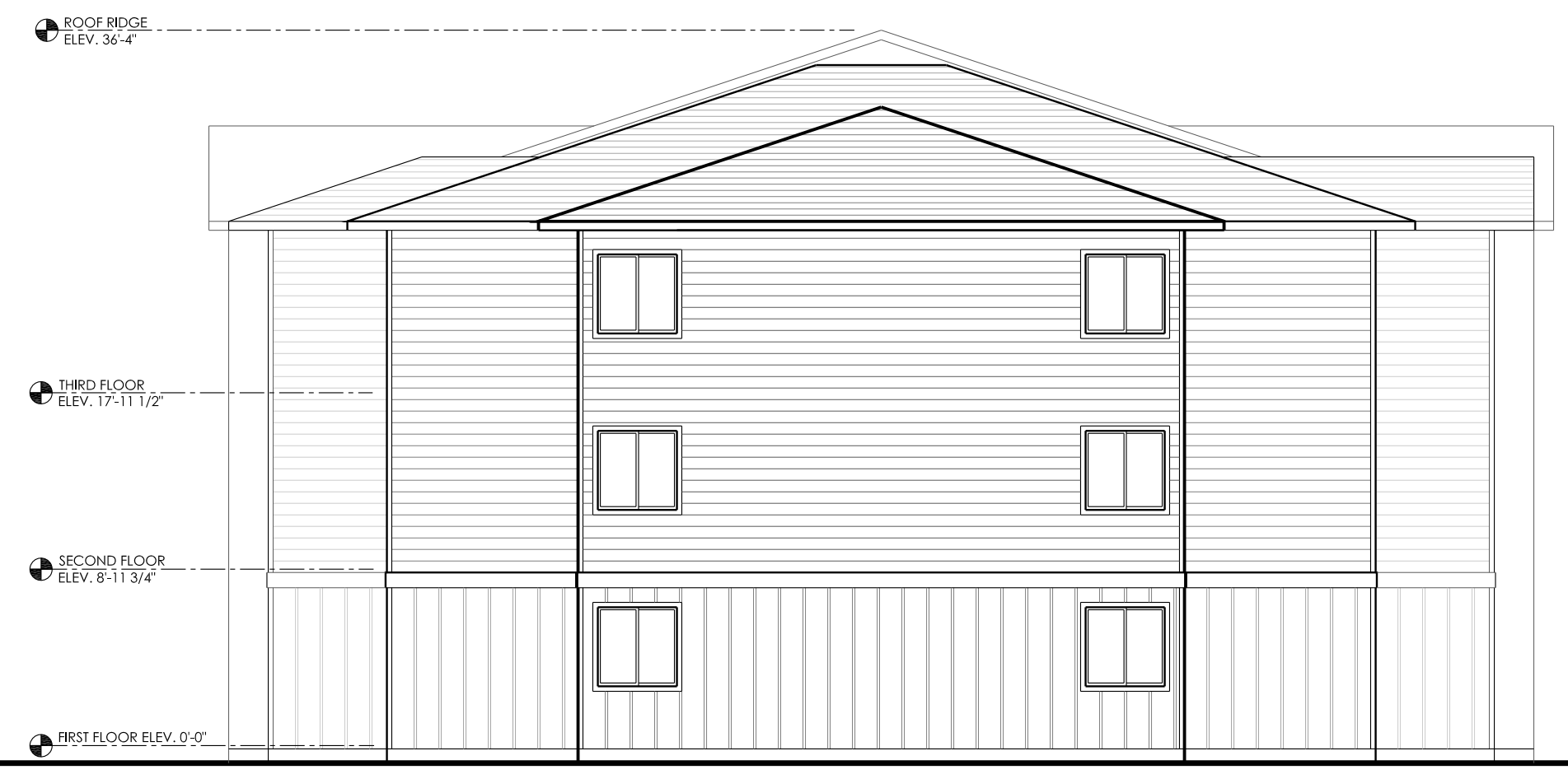
**A**  
**7.3**

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'D' - WEST ELEVATION 4  
Scale: 1/8" = 1'-0" A7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'D' - SOUTH ELEVATION 2  
Scale: 1/8" = 1'-0" A7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS

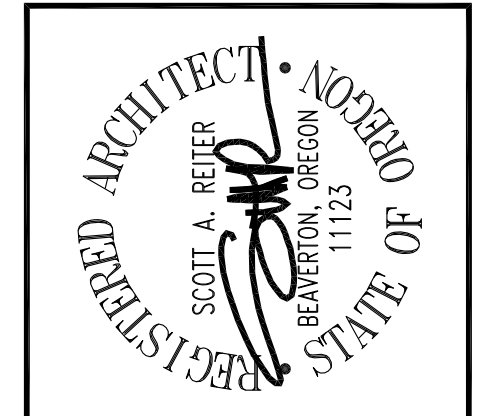


BUILDING 'D' - NORTH ELEVATION 3  
Scale: 1/8" = 1'-0" A7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'D' - EAST ELEVATION 1  
Scale: 1/8" = 1'-0" A7.4



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
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**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
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LAND-USE REVIEW

EXTERIOR ELEVATIONS  
BUILDING D

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

A  
7.4

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'E' - NORTH ELEVATION 4  
Scale: 1/8" = 1'-0" A7.5

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



ROOF RIDGE  
ELEV. 36'-4"

THIRD FLOOR  
ELEV. 17'-11 1/2"

SECOND FLOOR  
ELEV. 8'-11 3/4"

FIRST FLOOR ELEV. 0'-0"

BUILDING 'E' - WEST ELEVATION 2  
Scale: 1/8" = 1'-0" A7.5

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'E' - EAST ELEVATION 3  
Scale: 1/8" = 1'-0" A7.5

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



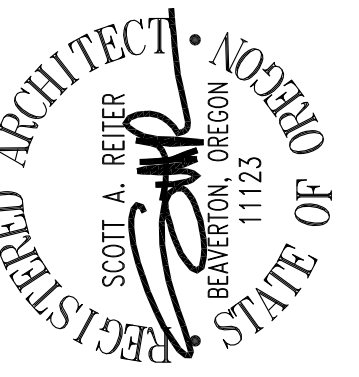
ROOF RIDGE  
ELEV. 36'-4"

THIRD FLOOR  
ELEV. 17'-11 1/2"

SECOND FLOOR  
ELEV. 8'-11 3/4"

FIRST FLOOR ELEV. 0'-0"

BUILDING 'E' - SOUTH ELEVATION 1  
Scale: 1/8" = 1'-0" A7.5



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**NORTON LANDING APARTMENTS**  
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EXTERIOR ELEVATIONS  
BUILDING E

date: JULY 19, 2023  
scale: AS NOTED  
drawn: SAR  
job no: 2301

**A**  
**7.5**

LAND-USE REVIEW

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - NORTH ELEVATION 4  
Scale: 1/8" = 1'-0" A7.6

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - WEST ELEVATION 2  
Scale: 1/8" = 1'-0" A7.6

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - EAST ELEVATION 3  
Scale: 1/8" = 1'-0" A7.6

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'F' - SOUTH ELEVATION 1  
Scale: 1/8" = 1'-0" A7.6



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OWNER:

**KWDS, LLC**

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LAND-USE REVIEW

EXTERIOR ELEVATIONS  
BUILDING F

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

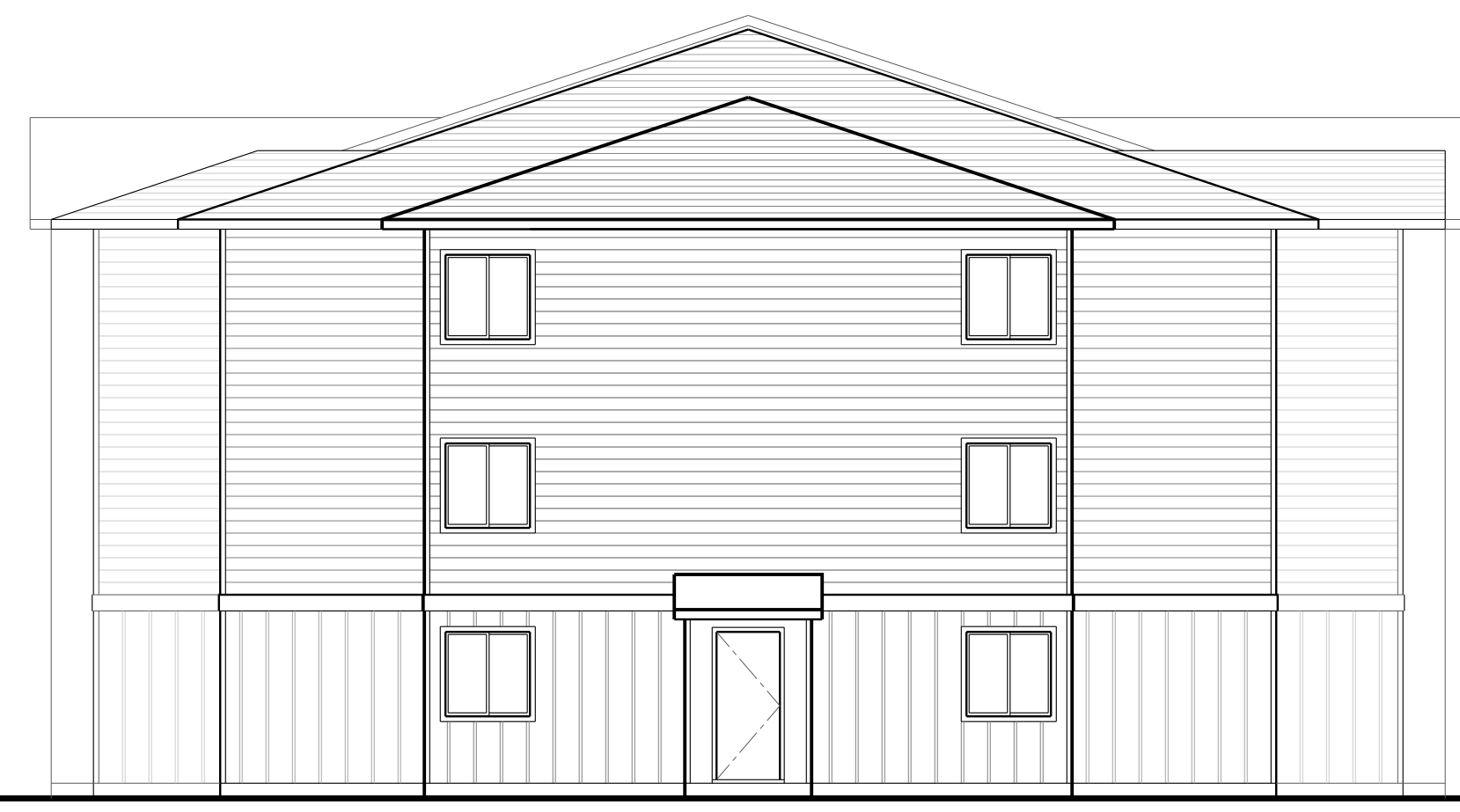
**A**  
**7.6**

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



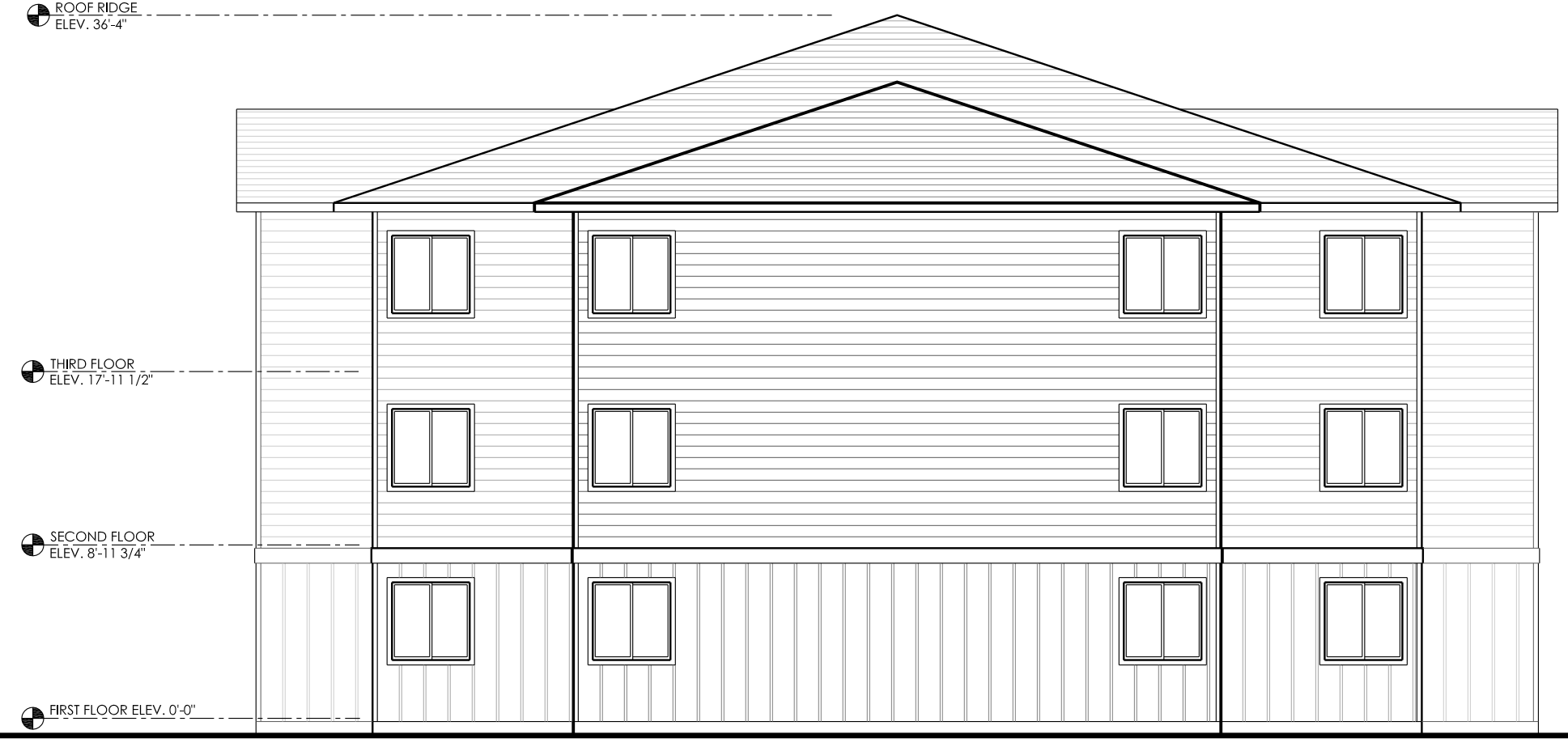
BUILDING 'G' - WEST ELEVATION 4  
Scale: 1/8" = 1'-0" A7.7

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'G' - SOUTH ELEVATION 2  
Scale: 1/8" = 1'-0" A7.7

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'G' - NORTH ELEVATION 3  
Scale: 1/8" = 1'-0" A7.7

NOTE:  
SEE DRAWINGS A7.1 THRU A7.3 FOR  
TYPICAL BUILDING ELEVATION MATERIALS



BUILDING 'G' - EAST ELEVATION 1  
Scale: 1/8" = 1'-0" A7.7



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REITER DESIGN ARCHITECT  
INCORPORATED

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LAND-USE REVIEW

EXTERIOR ELEVATIONS  
BUILDING G

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | AS NOTED      |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

**A**  
**7.7**



EVERGREEN MOBILE HOME PARK LLC  
2400 SE STRATUS AVE 38  
TAXLOT: 500

N0° 09' 43.69"E  
319.98'

HOUSING AUTHORITY OF YAMHILL COUNTY  
135 NE DUNN PL  
TAXLOT: 405

ROBERTI INVESTMENT COMPANY LLC  
375 SE NORTON LN  
TAXLOT: 402

KWDS LLC  
TAXLOT: 701  
GROSS AREA 4.93 AC

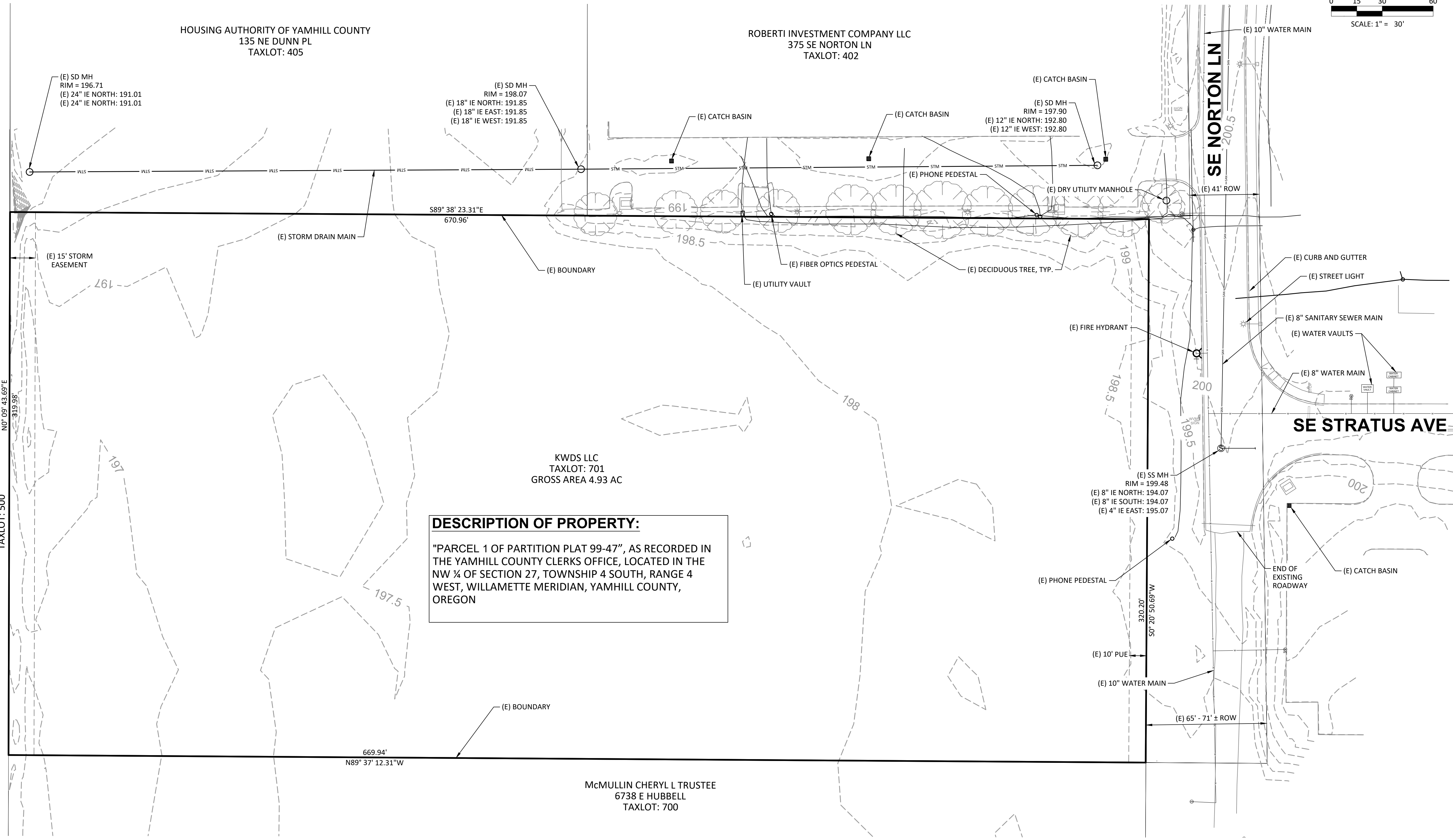
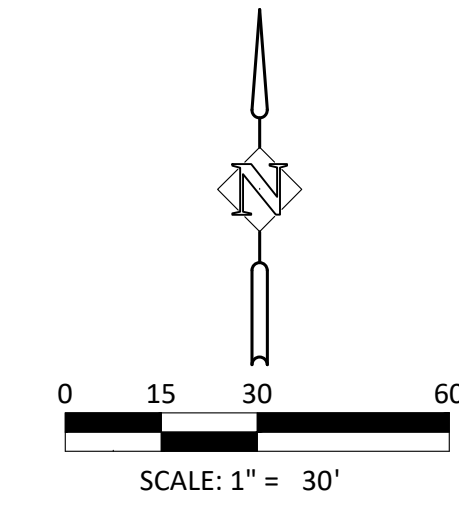
McMULLIN CHERYL L TRUSTEE  
6738 E HUBBELL  
TAXLOT: 700

**DESCRIPTION OF PROPERTY:**

"PARCEL 1 OF PARTITION PLAT 99-47", AS RECORDED IN THE YAMHILL COUNTY CLERKS OFFICE, LOCATED IN THE NW ¼ OF SECTION 27, TOWNSHIP 4 SOUTH, RANGE 4 WEST, WILLAMETTE MERIDIAN, YAMHILL COUNTY, OREGON

**EXISTING CONDITIONS**

1" = 30"



**EXISTING CONDITIONS**  
**NORTON LANDING**  
McMINNVILLE, OREGON

**Harper Houf Peterson Righellis Inc.**  
ENGINEERS \* PLANNERS  
LANDSCAPE ARCHITECTS \* SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhpr.com fax: 541.318.1141

**REGISTERED PROFESSIONAL ENGINEER**  
76,554  
**PRELIMINARY**  
OREGON  
DEC. 29, 2023  
JENNIFER L. YANCAMP

|           |        |          |            |
|-----------|--------|----------|------------|
| DESIGNED: | MD/JLV | DATE:    | 08.11.2023 |
| DRAWN:    | MD     | CHECKED: | JLV        |

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SHEET NO.  
**C0.1**  
JOB NO.  
KWD-11

**LAND USE**

**CONSTRUCTION NOTES:**

- 1 PROPOSED 6" CONCRETE CURB.
- 2 PROPOSED CONCRETE SIDEWALK WIDTH NOTED ON PLAN.
- 3 PROPOSED 26'-FT WIDE CONCRETE DRIVEWAY APRON.
- 4 PROPOSED SAWCUT LINE
- 5 PROPOSED AC PAVING SECTION - SEE TYPICAL SECTION
- 6 PROPOSED CURB AND GUTTER.
- 7 PROPOSED SWALE. SEE CROSS SECTION.

**LEGEND**

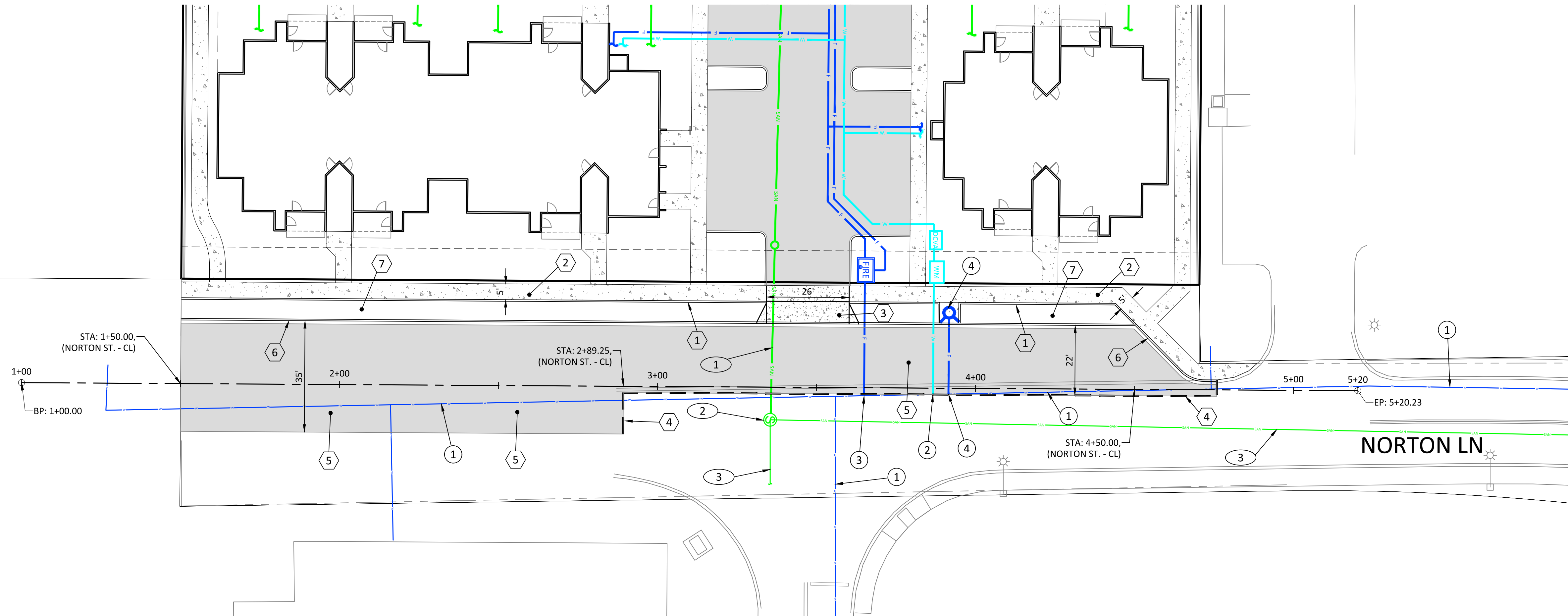
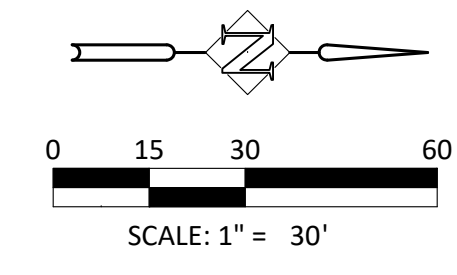
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**WATER MAIN NOTES**

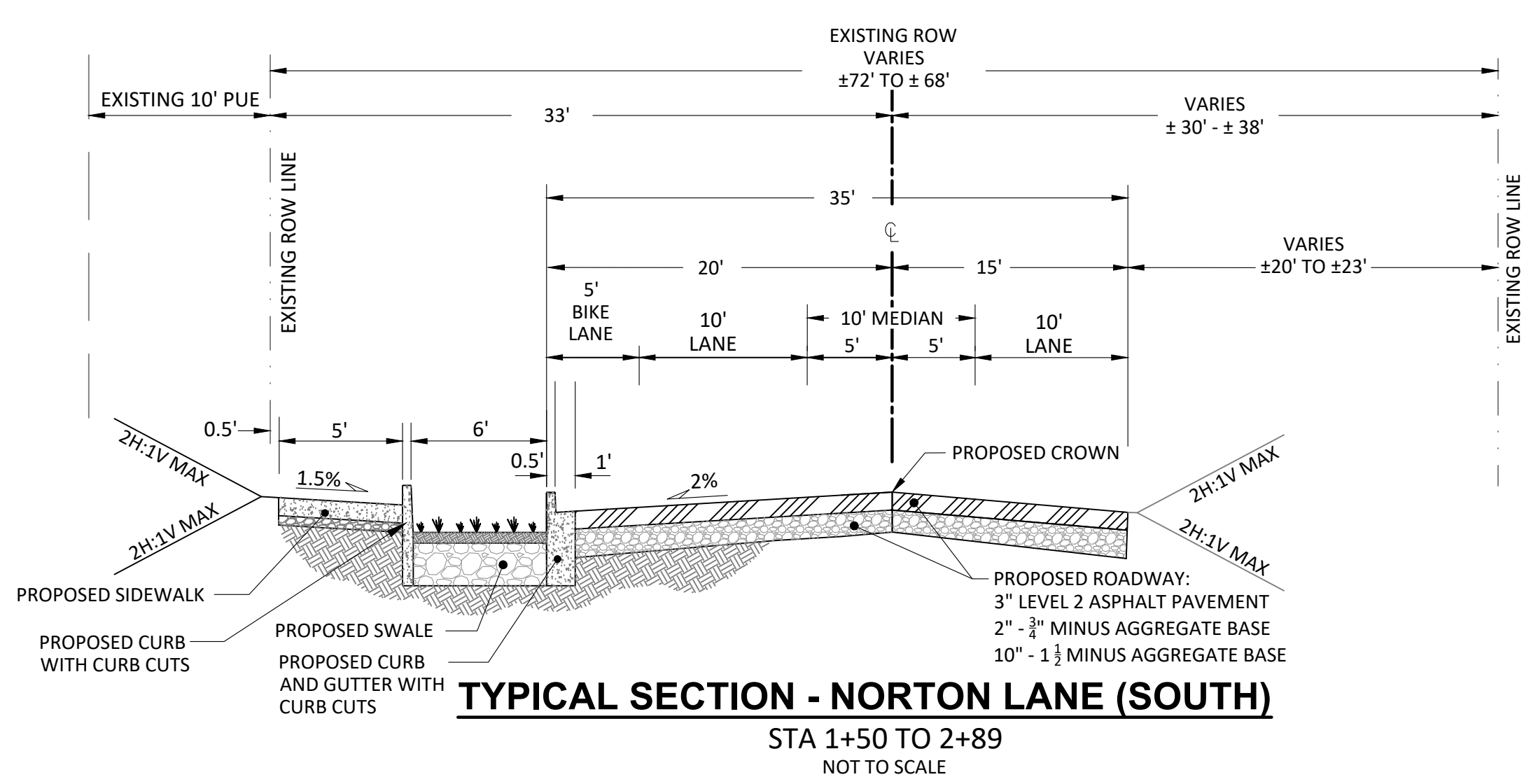
- 1 PROTECT EXISTING 10" WATER MAIN.
- 2 PROPOSED 4" DOMESTIC WATER SERVICE.
- 3 PROPOSED 6" FIRE SUPPRESSION SERVICE.
- 4 PROPOSED FIRE HYDRANT ASSEMBLY.

**SANITARY SEWER NOTES**

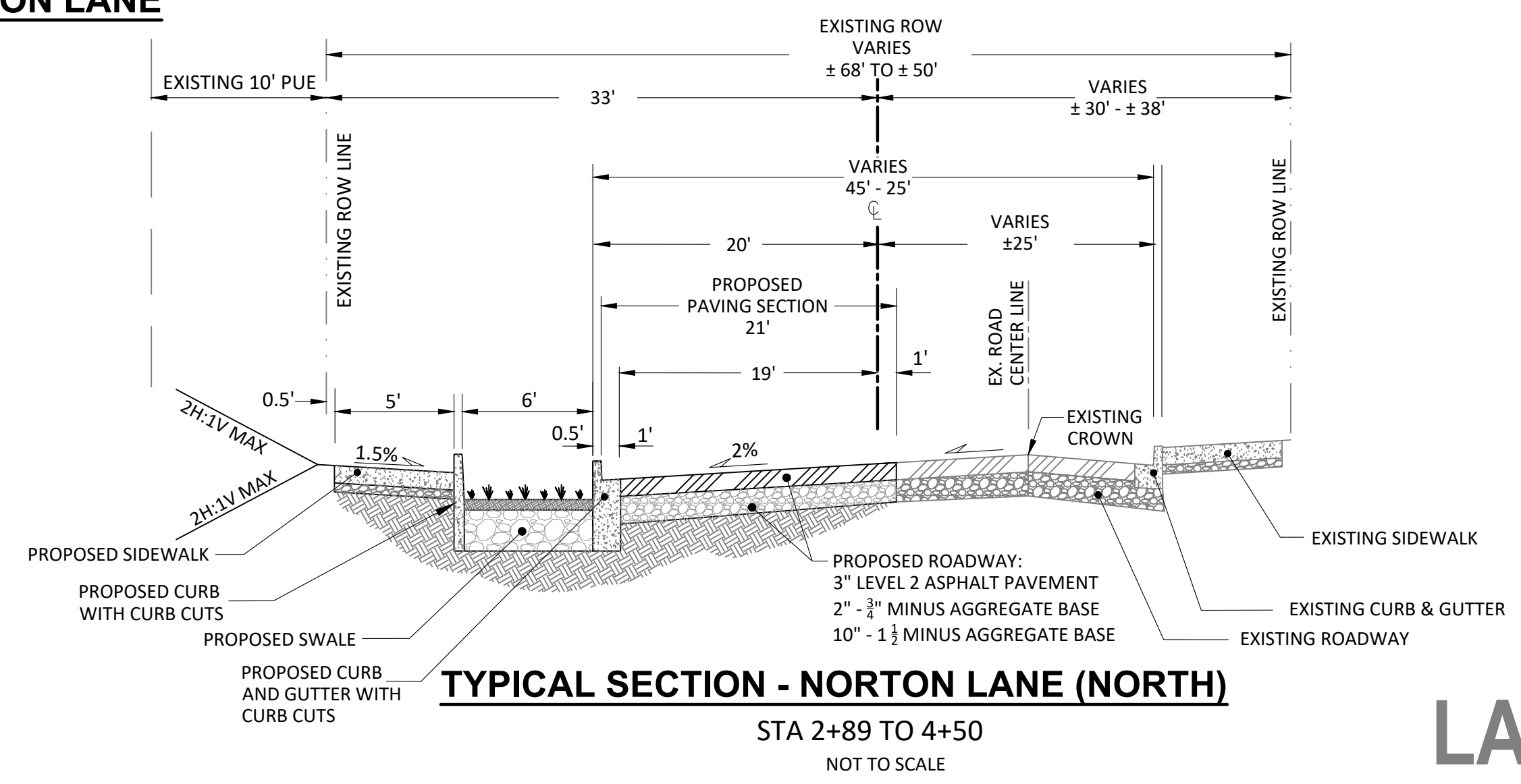
- 1 PROPOSED SANITARY SEWER SERVICE LINE
- 2 PROTECT EXISTING PUBLIC SEWER MANHOLE.
- 3 PROTECT EXISTING PUBLIC SEWER MAIN.



**PRELIMINARY PLAN - NORTON LANE**  
1" = 20"



**TYPICAL SECTION - NORTON LANE (SOUTH)**  
STA 1+50 TO 2+89  
NOT TO SCALE



**TYPICAL SECTION - NORTON LANE (NORTH)**  
STA 2+89 TO 4+50  
NOT TO SCALE

**LAND USE**

**SE NORTON LN - PRELIMINARY PLAN**  
**NORTON LANDING**  
McMINNVILLE, OREGON

**Harper Houf Peterson Righelisi Inc.**  
ENGINEERS + PLANNERS  
LANDSCAPE ARCHITECTS + SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhpr.com

**REGISTERED PROFESSIONAL ENGINEER**  
76,554  
**PRELIMINARY**  
OREGON  
DEC. 29, 2023  
JENNIFER L. YANCA, PE  
EXPIRES: 6/30/2024

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| DESIGNED: | MD/JLV     |
| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 08.11.2023 |

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SHEET NO. **C1.0**  
JOB NO. KWD-11

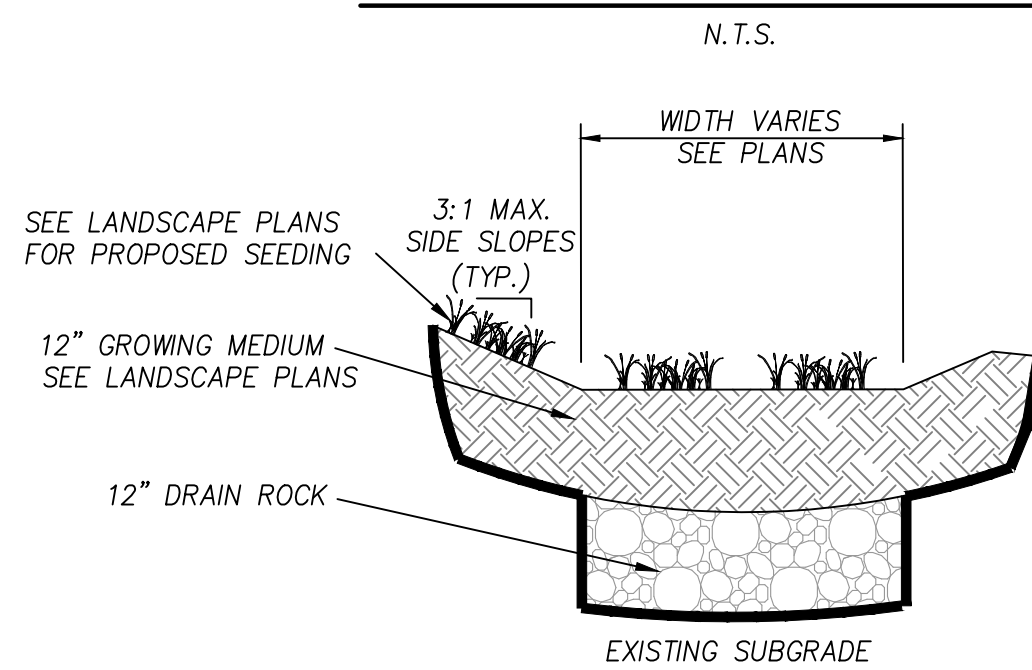
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**PRELIMINARY GRADING & DRAINAGE NOTES:**

1. ALL PROPOSED ELEVATIONS/GRADES SHOWN ON THIS PLAN ARE PRELIMINARY AND SUBJECT TO CHANGE DURING FINAL DESIGN.
2. STORMWATER FACILITY LOCATIONS ARE PRELIMINARY AND SUBJECT TO CHANGE DURING FINAL DESIGN.
3. FINAL DRAINAGE FACILITIES DESIGN WILL BE IN ACCORDANCE WITH CITY OF McMINNIVILLE STANDARDS AND OREGON LAW REQUIREMENTS.

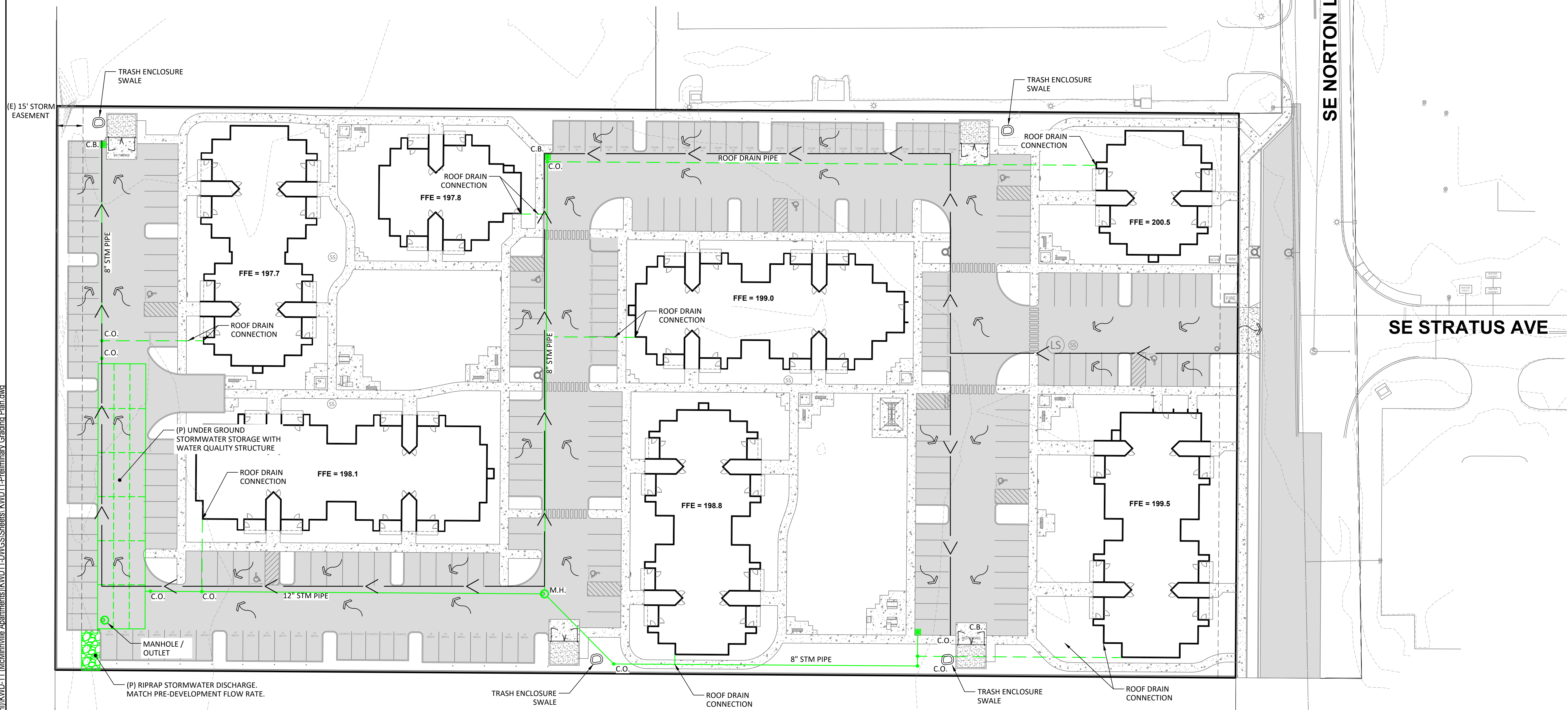
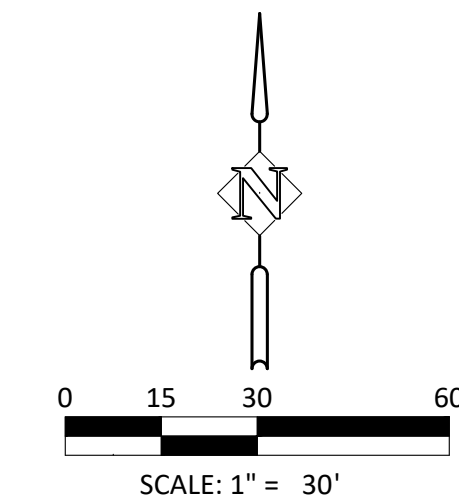
**TRASH ENCLOSURE SWALE**



1. PROVIDE PROTECTION FROM ALL VEHICLE TRAFFIC, EQUIPMENT STAGING, AND FOOT TRAFFIC IN PROPOSED INFILTRATION AREAS PRIOR TO, DURING, AND AFTER CONSTRUCTION.
2. DIMENSIONS:  
A. WIDTH OF BASIN VARIES, SEE PLANS.  
B. DEPTH OF BASIN VARIES, SEE TABLE THIS SHEET.
3. INSTALL 10' X 10' AREA OF WASHED PEA GRAVEL OR RIVER ROCK TO TRANSITION FROM PIPE OUTFALLS TO GROWING MEDIUM.

**LEGEND**

- 200 — (P) MAJOR CONTOUR
- 198 — (P) MINOR CONTOUR
- - - 200 - - - (E) MAJOR CONTOUR
- - - 198 - - - (E) MINOR CONTOUR
- ➔ (P) SLOPE ARROW
- V — (P) VALLEY GUTTER DRAINAGE
- - - (P) BUILDING / ROOF DRAIN CONNECTION



SE NORTON LN

SE STRATUS AVE

**PRELIMINARY GRADING & DRAINAGE**

1" = 30"

**PRELIMINARY GRADING & DRAINAGE PLAN**  
**NORTON LANDING**  
McMINNIVILLE, OREGON

**Harper Houf Peterson Righellis Inc.**  
ENGINEERS • PLANNERS  
LANDSCAPE ARCHITECTS • SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhpr.com

REGISTERED PROFESSIONAL ENGINEER  
OREGON  
PRELIMINARY  
JANUARY L. YANCAMP  
EXPIRES: 6/30/2024

|           |        |    |     |          |            |
|-----------|--------|----|-----|----------|------------|
| DESIGNED: | MD/JLV | MD | JLV | DATE:    | 08.11.2023 |
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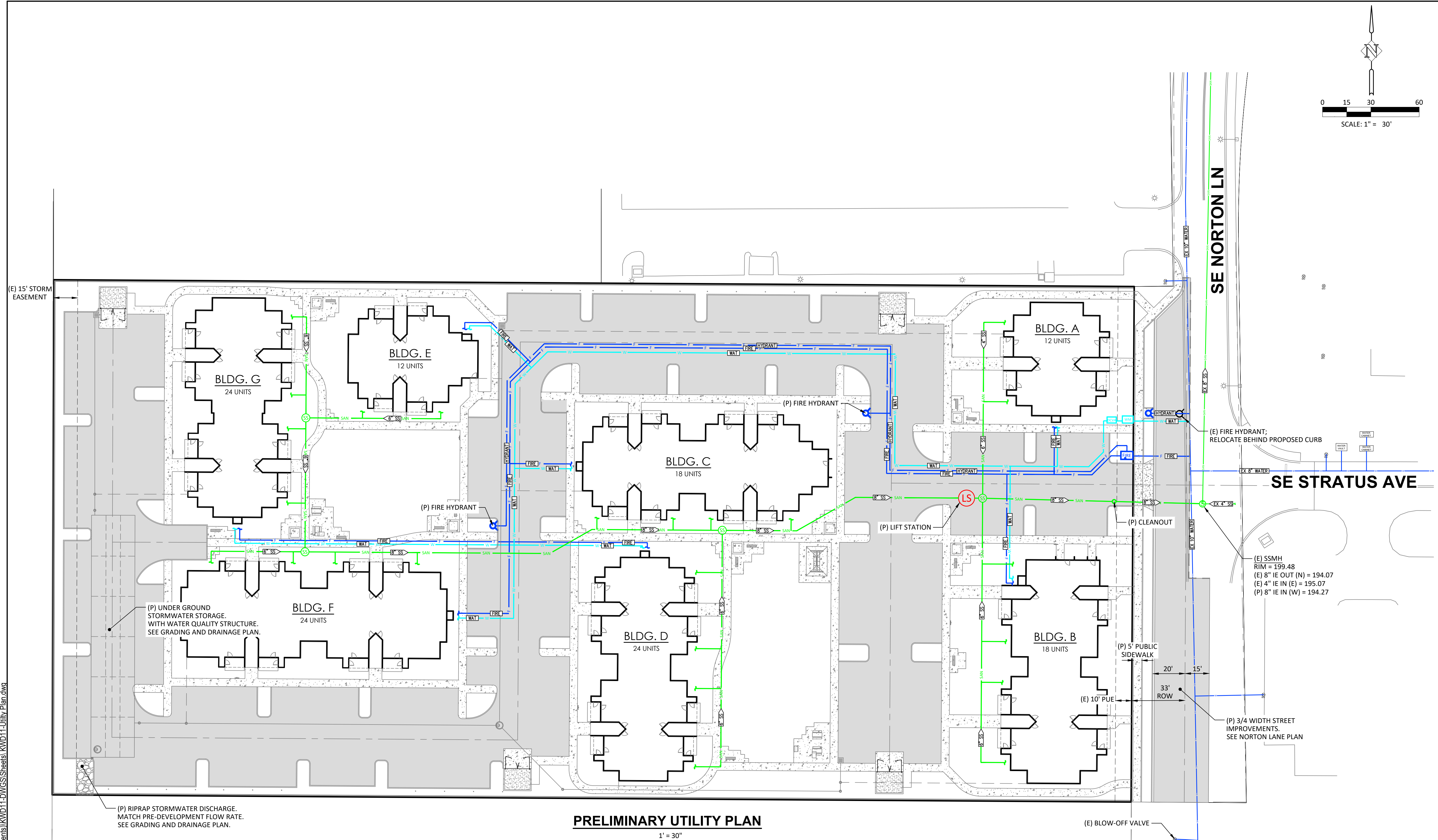
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JOB NO. KWD-11

**LAND USE**



P:\03-Bend\KVD-Kohli\KVD-11 (McMinnville Apartments)\KVD11-DWG\Sheets\KVD11-Utility Plan.dwg



**PRELIMINARY UTILITY PLAN  
NORTON LANDING  
MCMINNVILLE, OREGON**

**Harper Houf Peterson Righelis Inc.**  
ENGINEERS • PLANNERS  
LANDSCAPE ARCHITECTS • SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhp.com

**REGISTERED PROFESSIONAL ENGINEER**  
PRELIMINARY  
JENNIFER L. VANCE  
EXPIRES: 6/30/2024

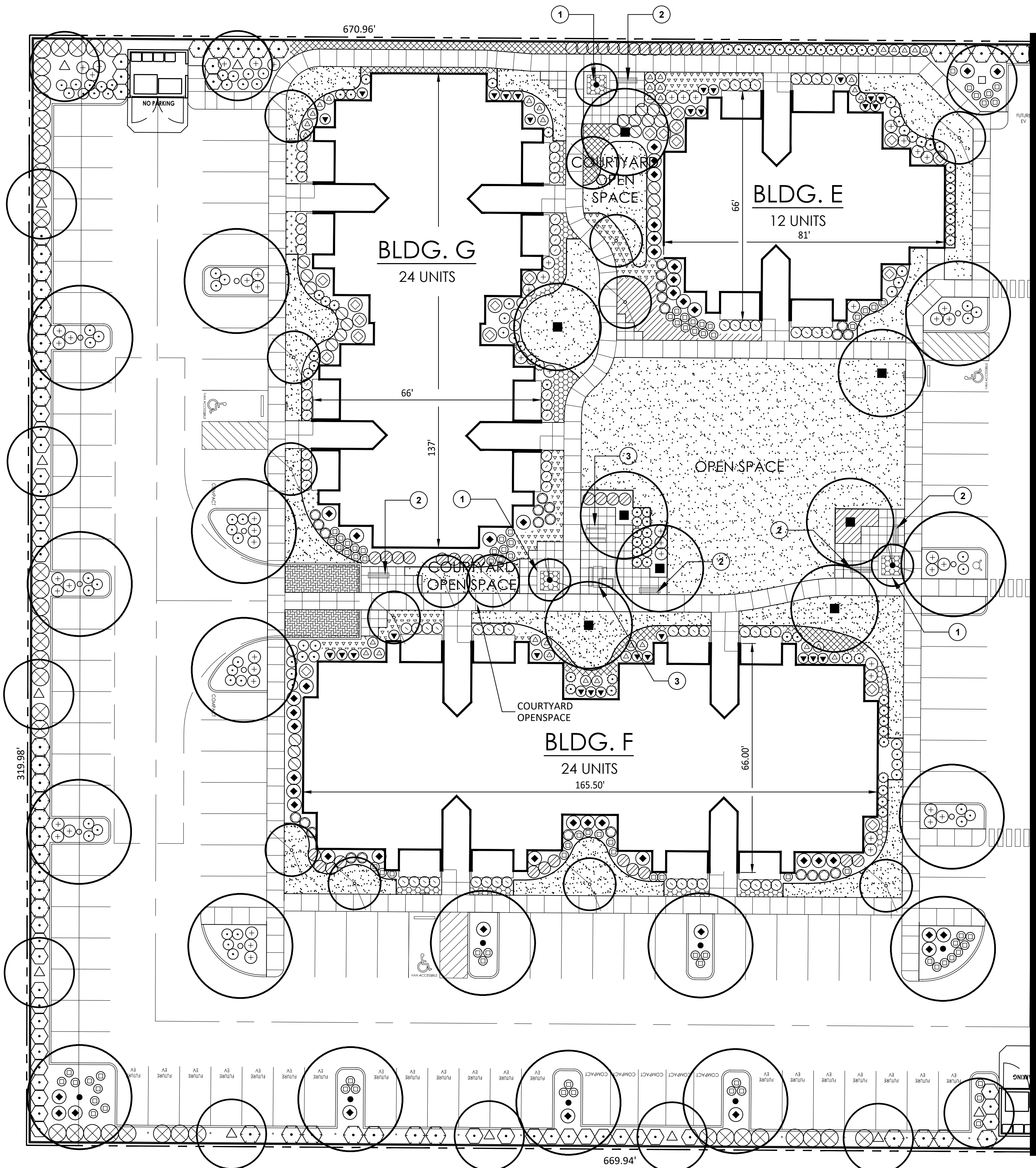
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| DESIGNED: | MD/JLV | DATE:    | 08.11.2023 |
| DRAWN:    | MD     | CHECKED: | JLV        |

| DATE | NO. | DESCRIPTION       |
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SHEET NO. **C3.0**  
JOB NO. KWD-11

**LAND USE**





**LANDSCAPE PLAN**  
1" = 20"

**KEY NOTES**

- 1 RAISED SEAT WALL PLANTER
- 2 BENCH
- 3 PICNIC TABLE

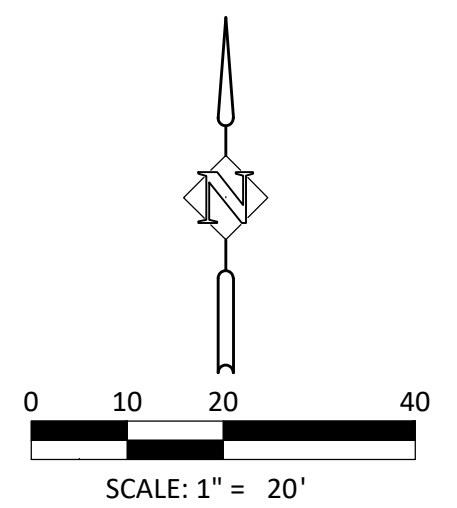
MATCHLINE - SEE SHEET L-12

**PLANT SCHEDULE**

| TREES        |  |              |              |
|--------------|--|--------------|--------------|
| 14           | Zelkova serrata 'Green Vase' - Green Vase Zelkova<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 40' W                  |              |              |
| 16           | Pyrus calleryana 'Aristocrat' - Callery Pear<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 35' H X 25' W                       |              |              |
| 4            | Acer griseum - Paperbark Maple<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 25' H X 20' W                                     |              |              |
| 16           | Acer rubrum 'Bowhall' - Bowhall Maple<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 25' H X 15' W                              |              |              |
| 7            | Lagerstroemia Plum Magic - Plum Magic Crape Myrtle<br>12" HGT. B&B, WELL BRANCHED, MULTI-TRUNKED<br>MATURE SIZE: 15' H X 15' W               |              |              |
| 19           | Carpinus betulus - European Hornbeam<br>X" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 35' W                               |              |              |
| 13           | Ginkgo biloba 'Autumn Gold' - Autumn Gold Ginkgo<br>X" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 35' W                   |              |              |
| 26           | Cornus 'Eddie's White Wonder' - Eddie's White Dogwood<br>X" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 35' H X 20' W              |              |              |
| SHRUBS       |  |              |              |
| 222          | Pennisetum orientale - Fountaingrass<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W                              |              |              |
| 102          | Abelia x grandiflora 'Kaleidoscope' - Kaleidoscope Abelia<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 3.5' W     |              |              |
| 220          | Helictotrichon sempervirens - Blue Oatgrass<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 2.5' W                   |              |              |
| 67           | Gardenia jasminoides 'Frostproof' - Frostproof Gardenia<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 4' H X 3' W           |              |              |
| 56           | Hydrangea macrophylla 'Mini Penny' - Mini Penny Hydrangea<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 3.5' W     |              |              |
| 66           | Deutzia gracilis 'Nikko' - Dwarf Nikko Deutzia<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2' H X 3' W                    |              |              |
| 74           | Berberis thunbergii 'Monomb' - Cherry Bomb Japanese Barberry<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 4' W      |              |              |
| 123          | Escallonia hybrid 'Compakta' - Compact Escallonia<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 4' W               |              |              |
| 104          | Spiraea x bumalda 'Goldflame' - Goldflame Spirea<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3.5' H X 3.5' W              |              |              |
| 85           | Thuja occidentalis 'Golden Globe' - Golden Globe Arborvitae<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 4' H X 4' W       |              |              |
| 68           | Choisya ternata 'Sundance' - Sundance Orange Blossom<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W              |              |              |
| 61           | Abelia gradiflora - Glossy Abelia<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W                                 |              |              |
| 61           | Viburnum tinus 'Spring Bouquet' - Laurustinus<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W (PRUNNED)           |              |              |
| 118          | Nandina domestica 'Gulf Stream' - Gulf Stream Nandina<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W             |              |              |
| 185          | Prunus laurocerasus 'Otto Luyken' - Dwarf Cherry Laurel<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W (PRUNNED) |              |              |
| GROUND COVER |  |              |              |
| 1            | Festuca glauca 'Elijah Blue' - Blue Fescue<br>CONT., FULL PLANTS, 12" O.C.   |              |              |
| 1            | Liriope muscari 'Big Blue' - Big Blue Liliyturf<br>CONT., FULL PLANTS, 18" O.C.  |              |              |
| 1            | Pennisetum alopecuroides 'Little Bunny' - Little Bunny Fountain Grass<br>CONT., FULL PLANTS, 12" O.C.  |              |              |
| 1            | Carex elata 'Aurea' - Bowles Golden Sedge<br>CONT., FULL PLANTS, 30" O.C.  |              |              |
| 1            | Carex oshimensis 'Carfit01' - Everest Variegated Sedge<br>CONT., FULL PLANTS, 24" O.C.   |              |              |
| SEED MIXES   |  |              |              |
|              | SEED MIX 1 (LAWN AREAS)  |              |              |
|              | Festuca rubra 'Gibraltar'  | 10           | 0.364        |
|              | Festuca rubra 'Silhouette'   | 10           | 0.364        |
|              | Lolium perenne 'Delaware Dwarf'  | 40           | 5.563        |
|              | Lolium perenne 'Amazing'   | 40           | 2.696        |
|              |  | <b>TOTAL</b> | <b>8.987</b> |

**GENERAL PLANTING NOTES**

1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT CITY OF MC MINNVILLE STANDARDS AND OREGON BUILDING AND SPECIALITY CODES.
2. INSTALL EROSION CONTROL SYSTEMS IN ACCORDANCE WITH CITY OF MC MINNVILLE STANDARDS PRIOR TO SITE WORK AND LANDSCAPE INSTALLATION.
3. CONTRACTOR SHALL MARK AND PROTECT ALL UTILITIES, SITE FEATURES, AND VEGETATION TO REMAIN IN PLACE.
4. CONTRACTOR SHALL REMOVE ALL WEEDS AND INVASIVE SPECIES PRIOR TO PLANTING OR SEEDING.
5. ALL DISTURBED AREAS SHALL BE SEEDED.
6. PRIOR TO PLANTING, CONTRACTOR SHALL TEST ON-SITE SOILS FOR SOIL FERTILITY BY CERTIFIED TESTING LAB. IF NECESSARY, BACKFILL SOILS FOR TREE PITS, SHRUB AND GROUND COVER AREAS SHALL BE AMENDED AS RECOMMENDED BY SOIL ANALYSIS REPORT.
7. ALL SEEDED AREAS SHALL BE STRIPPED OF VEGETATION, SCARIFIED AND RECEIVE 6" OF TOPSOIL PRIOR TO APPLICATION OF SEED.
8. ALL PLANTER BEDS SHALL BE SCARIFIED 12" BELOW FINISHED GRADE AND HAVE 12" OF TOPSOIL ADDED TO BRING BACK TO FINISHED GRADE PRIOR TO PLANTING.
9. CONTRACTOR TO INSTALL 3" LAYER OF BARK MULCH AT ALL TREE, SHRUB AND GROUND COVER AREAS.
10. LANDSCAPE INSTALLATION SHALL INCLUDE PROVISION OF AN AUTOMATIC IRRIGATION SYSTEM TO SUSTAIN LANDSCAPE PLANTINGS, MEETING LOCAL AND STATE BUILDING CODES.
11. PLANT MATERIAL INSTALLED SHALL CONFORM IN SIZE AND GRADE TO THE "AMERICAN STANDARD FOR NURSERY STOCK" CURRENT EDITION.
12. QUANTITIES OF PLANT MATERIALS SHALL BE AS DETERMINED BY CONTRACTOR IN ACCORDANCE WITH SPECIFIED SPACING OR LOCATION ON PLAN. MATERIAL QUANTITIES SHOWN ON PLAN ARE FOR CONTRACTOR CONVENIENCE ONLY AND SHALL BE VERIFIED BY CONTRACTOR PRIOR TO INSTALLATION. SURPLUS OR SHORTAGES OF PLANT QUANTITIES SHALL BE RESPONSIBILITY OF CONTRACTOR.
13. LANDSCAPE CONTRACTOR SHALL WATER PLANTINGS FOR DURATION OF 1-YEAR WARRANTY PERIOD AFTER INSTALLATION AND GUARANTEE ALL PLANTINGS TO BE IN SATISFACTORY HEALTH. LANDSCAPE CONTRACTOR SHALL REPLACE ALL DAMAGED, DEAD, OR DYING PLANTS COVERED BY WARRANTY WITHIN 30 DAYS OF INITIAL IDENTIFICATION OF CONDITION.



**LANDSCAPE PLAN**  
**NORTON LANDING**  
MCMINNVILLE, OREGON

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ENGINEERS + PLANNERS  
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250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhpr.com

REGISTERED  
747  
PRELIMINARY  
JEFFERY P. CREEEL  
OREGON  
05/13/11  
LANDSCAPE ARCHITECT

|           |        |    |     |       |            |
|-----------|--------|----|-----|-------|------------|
| DESIGNED: | MD/JLV | MD | JLV | DATE: | 07.21.2023 |
| DRAWN:    |        |    |     |       |            |
| CHECKED:  |        |    |     |       |            |

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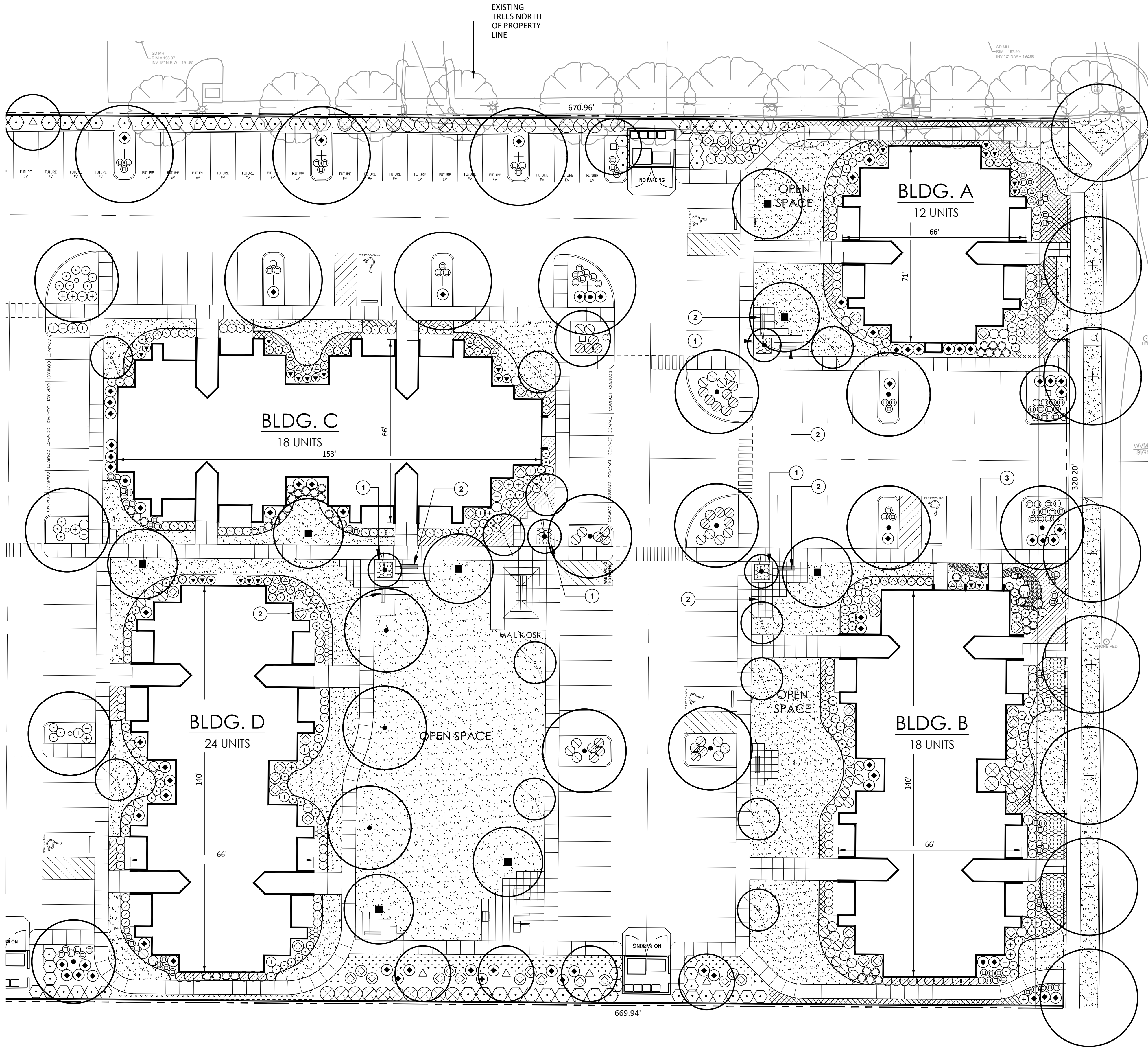
SHEET NO.

**L1.1**

JOB NO. KWD-11



N:\KWD\Kohn\KWD-11 (McMinnville Apartments)\KWD11-DWG\Steel\1.1 - Landscape Plans.dwg

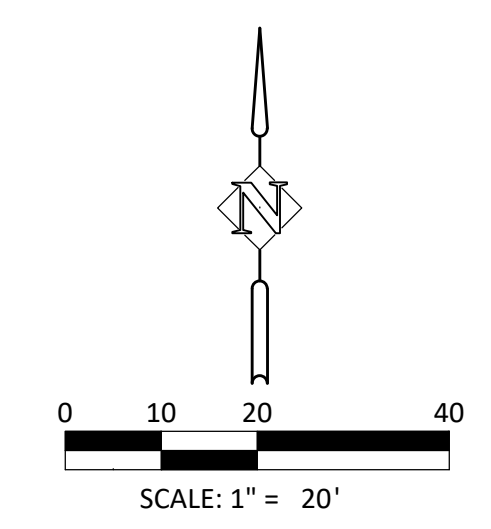


**LANDSCAPE PLAN**  
1" = 20"

| CITY OF MCMINNVILLE CODE COMPLIANCE |                              |   |   |   |
|-------------------------------------|------------------------------|---|---|---|
| SECTION #                           | SECTION                      | DESCRIPTION   | REQUIRED  | PROVIDED  |
| 17.11.090(D)                        | LARGE SITE DESIGN STANDARDS  | BUFFER PARKING AREAS WITH LANDSCAPING.  |   | LANDSCAPING AROUND PARKING LOT                    |
|                                     | LARGE SITE DESIGN STANDARDS  | PROVIDE DIFFERENT TYPE OF OPEN SPACE THROUGHOUT THE SITE  |   | DIFFERENT TYPES OF OPEN SPACE PROVIDED            |
|                                     | PARKING LOT DESIGN STANDARDS | >50,000 SF PARKING LOTS = 8% INTERIOR LANDSCAPING REQUIRED.   | PARKING LOT AREA = 75,075 SF X 8% = 6,006 SF        | 6,681 SF INT . PL LANDSCAPING                     |
|                                     | PARKING LOT DESIGN STANDARDS | 1 (NON-COLUMNAR) TREE PER ISLAND  | 37 PARKING LOT ISLANDS                              | 37 TREES  |
|                                     | PARKING LOT DESIGN STANDARDS | FENCE REQUIRED NEXT TO RESIDENTIAL ZONING   | FENCE ON WEST PL                                    | FENCE PROVIDED. SEE ARCHITECTURAL SITE PLANS      |
|                                     | PARKING LOT DESIGN STANDARDS | BUFFER BETWEEN PL AND BUILDINGS   | SW OR LANDSCAPE STRIP                               | 5' SW & LANDSCAPE STRIPS PROVIDED                 |
|                                     | PARKING LOT DESIGN STANDARDS | PLANTINGS ADJACENT TO SW (BETWEEN PL AND BLDGS)   | 6' LANDSCAPE STRIP NEXT TO SW                       | 6' PLANTING STRIP                                 |
|                                     | PARKING LOT DESIGN STANDARDS | TREES ALONG THE THROUGH SIDEWALK-WALKWAY CONNECTIONS  |   | TREES PROVIDED ALONG SIDEWALK CONNECTIONS         |
|                                     | OPEN SPACE                   | PROVIDE DECIDUOUS TREE FOR SHADE  | DEC. TREES IN OPEN SPACES                           | STEWARTIA PROVIDED                                |
|                                     | LANDSCAPING                  | A MIN. OF 20% OF THE SITE SHALL BE LANDSCAPED   | TOTAL SITE 214,606 SF X 20% = 42,921 SF             | 64,157 SF PROVIDED (30%)                          |
| 17.11.090(D). 10                    | FRONT YARD SETBACK           | FRONT YARD SETBACK BETWEEN ENTRANCE AND STREET GATEWAY ZONE   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
| 17.11.090(D). 6.B(3)                | PRIVATE OPEN SPACE           | PRIVATE OUTDOOR SPACE AT GROUND LEVEL REQUIRES FRONT YARD   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
|                                     | OPEN SPACE                   | COMMON OPEN SPACE 15% MIN.  | TOTAL SITE 214,606 SF X 15% = 32,191 SF             | 32,715 SF PROVIDED                                |
|                                     | OPEN SPACE                   | PASSIVE OPEN SPACE NO MORE THAN 5% OF SITE  | TOTAL SITE 214,606 SF X 5% = 10,730 SF MAX. PASSIVE | 21,985 SF ACTIVE OPEN SPACE                       |
| 17.57.070                           | EXISTING TREES               | EXISTING LOCATIONS OF TREES OVER 6" DBH   |   | NO EXISTING TREES ON SITE                         |
| 17.57.070                           | LANDSCAPE SCREENING          | SCREENING TO PROPOSED USE BY SIGHT OBSCURING, EVERGREEN PLANTINGS, SHADE TREES, FENCES OR COMBINATION |   | SIGHT OBSCURING FENCE & SCREENING SHRUBS PROVIDED |
| 17.57.090                           | STREET TREES                 | STREET TREES OVER 40' TALL AND WIDE   | 40' OC  | LARGE STREET TREES PROVIDED. SEE PLANT SCHEDULE   |

**KEY NOTES**

- 1 RAISED SEAT WALL PLANTER
- 2 BENCH



**LANDSCAPE PLAN**  
**NORTON LANDING**  
MCMINNVILLE, OREGON

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REGISTERED 747  
**PRELIMINARY**  
JEFFERY P. CREEL  
OREGON  
05/13/11  
LANDSCAPE ARCHITECT

DESIGNED: MD/JLV  
DRAWN: MD  
CHECKED: JLV  
DATE: 07.21.2023

| DATE | NO. | DESCRIPTION |
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R E V I S I O N S

SHEET NO.  
**L1.2**  
JOB NO.  
KWD-11

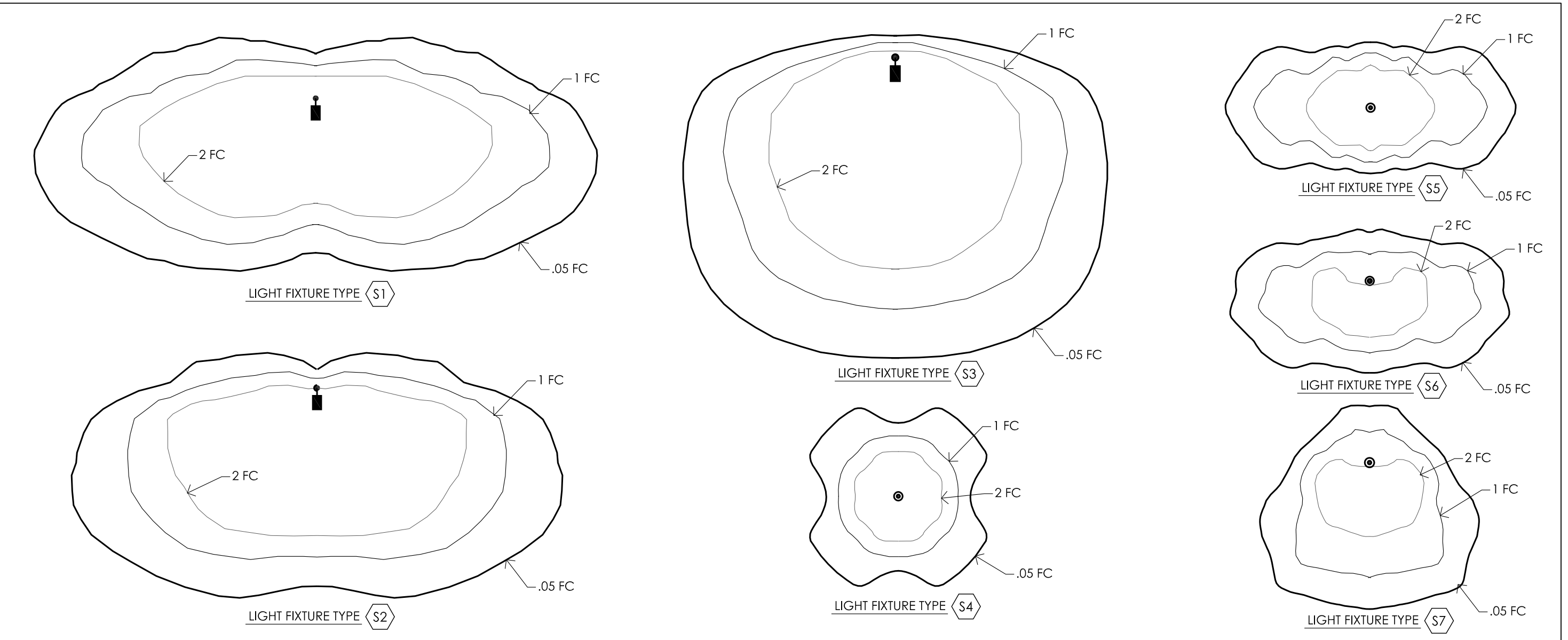
SEE L1.1 FOR PLANTING SCHEDULE

**LAND USE**

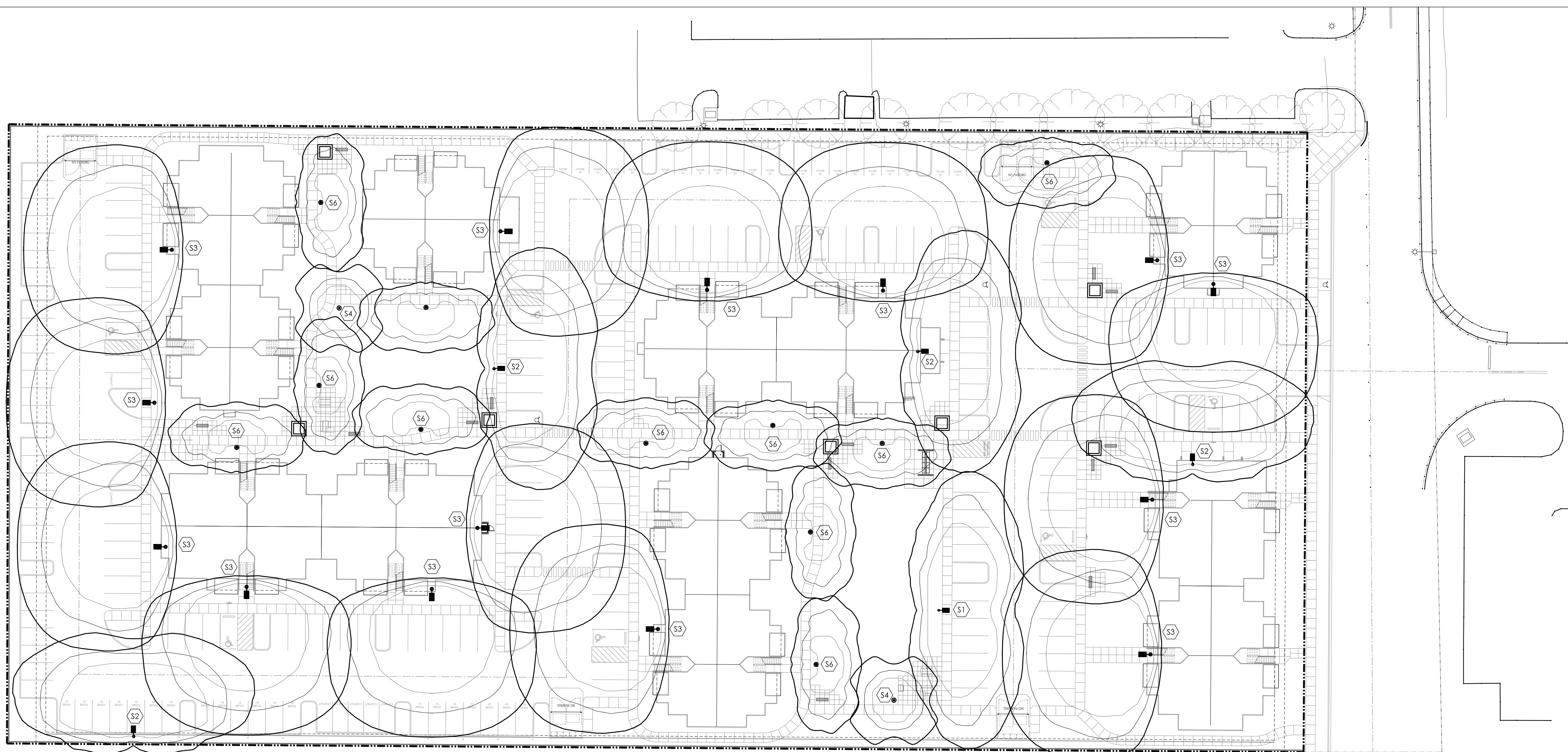


**SITE LIGHT FIXTURE SCHEDULE**

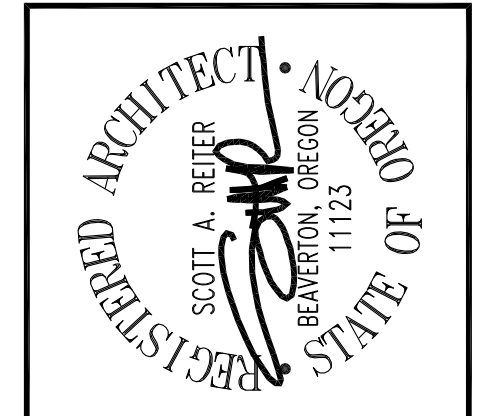
| ITEM | DESCRIPTION                   | MFR. & NUMBER                                 | LAMP | REMARKS  |
|------|-------------------------------|---|------|--|
| S1   | PARKING AREA LIGHT FIXTURE    | RAB ALED-105 W TYPE II LIGHT PATTERN          | LED  | POLE MOUNTED AT 20'-0"                             |
| S2   | PARKING AREA LIGHT FIXTURE    | RAB ALED-105 W TYPE III LIGHT PATTERN         | LED  | POLE MOUNTED AT 20'-0", OR BUILDING MTD. AT 20'-0" |
| S3   | PARKING AREA LIGHT FIXTURE    | RAB ALED-105 W TYPE IV LIGHT PATTERN          | LED  | POLE MOUNTED AT 20'-0", OR BUILDING MTD. AT 20'-0" |
| S4   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE I LIGHT PATTERN   | LED  | POLE MOUNTED AT 12'-0"                             |
| S5   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE II LIGHT PATTERN  | LED  | POLE MOUNTED AT 12'-0"                             |
| S6   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE III LIGHT PATTERN | LED  | POLE MOUNTED AT 12'-0"                             |
| S7   | PEDESTRIAN AREA LIGHT FIXTURE | KIM, BOUNSE - BNS1 LED TYPE IV LIGHT PATTERN  | LED  | POLE MOUNTED AT 12'-0"                             |



**SITE LIGHT FIXTURE LEGEND 2**  
Scale: NOT TO SCALE ME1.0



**SITE LIGHTING PLAN 1**  
Scale: 1" = 30'-0" ME1.0



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

OVERALL SITE LIGHTING PLAN

|          |               |            |  |
|----------|---------------|------------|--|
| date:    | JULY 19, 2023 | revisions: |  |
| scale:   | 1"=30'        |            |  |
| drawn:   | SAR           |            |  |
| job no.: | 2301          |            |  |

**ME 1.0**



**Planning Department**  
 231 NE Fifth Street ◦ McMinnville, OR 97128  
 (503) 434-7311 Office ◦ (503) 474-4955 Fax  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

|                         |                   |
|-------------------------|-------------------|
| <b>Office Use Only:</b> |                   |
| File No.                | <u>L 38-23</u>    |
| Date Received           | <u>8/14/23</u>    |
| Fee                     | <u>\$1,207.00</u> |
| Receipt No.             | <u>208599</u>     |
| Received by             | <u>AW</u>         |
| 569-23-000461-PLNG      |                   |

## Landscape Plan Review Application

### **Applicant Information**

**Applicant is:**    Property Owner    Contract Buyer    Option Holder    Agent    Other Architect/Planner

Applicant Name Reiter Design Architect Incorporated      Phone (503) 574-3036  
 Contact Name Scott Reiter      Phone (503) 789-6461  
*(If different than above)*  
 Address 6107 SW Murray Blvd., #480  
 City, State, Zip Beaverton, OR. 97008  
 Contact Email ScottRDG@aol.com

### **Property Owner Information**

Property Owner Name KWDS, LLC      Phone (503) 781-5685  
*(If different than above)*  
 Contact Name Chad Juranek      Phone \_\_\_\_\_  
 Address PO Box 145  
 City, State, Zip Wilsonville, OR. 97070  
 Contact Email Cjuranek@jkmanage.com

### **Site Location and Description**

*(If metes and bounds description, indicate on separate sheet)*

Property Address SE Norton Lane at SE Stratus Ave.  
 Assessor Map No. R4 4 - 4 - 27      Total Site Area 214,759 sf (4.98 acres)  
 Subdivision \_\_\_\_\_      Block \_\_\_\_\_      Lot 701  
 Comprehensive Plan Designation Commercial      Zoning Designation C-3 General Commercial



**Landscaping Information**

- 1. Total Landscaped Area: 64,157 sf
- 2. Percent Landscaped: 30%
- 3. Building Floor Area:  
New Structure: 138,476 sf Existing Structure: none Addition: none
- 4. Architect Name Harper Houf Peterson Righellis, Inc. Phone (503) 221-1131  
*(Landscape Architect; Engineer; or Other Designer)*  
Contact Name Jeffery Creel, RLA Phone (503) 221-1131  
Address 205 SE Spokane Street, Suite 200  
City, State, Zip Portland, OR. 97202  
Contact Email jeffc@hhpr.com

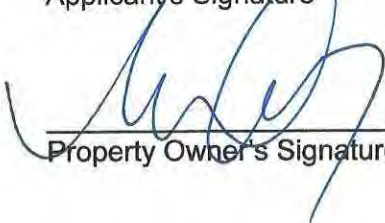
In addition to this completed application, the applicant must provide the following:

- Two (2) copies of the proposed landscape plan containing the information listed in the information sheet and Chapter 17.57 (Landscaping) of the Zoning Ordinance.*
- Payment of the applicable review fee, which can be found on the Planning Department web page.*

**I certify the statements contained herein, along with the evidence submitted, are in all respects true and are correct to the best of my knowledge and belief.**

  
Applicant's Signature

8/10/23  
Date

 manager KWDS, LLC  
Property Owner's Signature

8-11-23  
Date

NORTON LANDING  
APARTMENTS  
McMinnville, Oregon

**LAND-USE APPLICATION**

**LANDSCAPE PLAN REVIEW**

AUGUST 14, 2023

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070

**RDA**  

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REITER DESIGN ARCHITECT  
INCORPORATED



# NORTON LANDING APARTMENTS

## LANDSCAPE PLAN REVIEW

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# **LAND-USE APPLICATION**

Landscape Review

**1**

# Landscape Plan Review Information & Submittal Requirements



Planning Department  
231 NE Fifth Street • McMinnville, OR 97128  
(503) 434-7311 Office • (503) 474-4955 Fax  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

## Overview

It is recognized that quality landscaping not only enhances the appearance of the City, but also benefits the health of the general public. In McMinnville, landscaping is required for any development within commercial or industrial zoned areas, as well as for certain specific uses such as multiple-family, churches, schools, utility substations, and manufactured home parks. A landscape plan for such development is required at the time of building permit application, consistent with the requirements as defined in Section 17.57.040 (Plans – Information to be included) of the Zoning Ordinance. The submitted landscape plan is reviewed by the Landscape Review Committee (LRC), a five-member committee appointed by the City Council and comprised of qualified landscape professionals.

## Application Submittal

The following materials must be provided at the time of submittal, or the application will not be accepted for processing.

- A completed Landscape Review application form.
- Two (2) copies of the proposed landscape plan (drawn to scale, with a north arrow, legible, and of a reproducible size) containing the following information:
  - The location of existing trees over six inches in diameter, their variety (common or botanical name), and indication of whether they are to remain or be removed from the site;
  - The quantity, location, size, and variety (common or botanical name) of all new plantings and landscaping;
  - The percentage of the gross area to be landscaped;
  - Any equipment proposed for recreation areas;
  - All existing and proposed site features, including walkways, graveled areas, patios, courts, fences, decks, foundations, potted trees, or other open spaces;
  - Building location and dimensions and lot location and dimensions (all information on building permit plot plan); and
  - Irrigation plan.
- Payment of the applicable review fee.

## Review Process

A landscape plan, once determined to be complete, is then scheduled for review by the LRC as stated in Chapter 17.57 (Landscaping) of the Zoning Ordinance. The LRC may approve, approve with conditions, or deny a submitted landscape plan based upon the factors found in Section 17.57.050 (Area Determination – Planning factors). The decision made by the LRC may be appealed to the Planning Commission, as stated in Section 17.72.170 (Appeal from Ruling of Planning Director) of the Zoning Ordinance.

# **PROJECT DESIGN TEAM**

# **2**

# NORTON LANDING APARTMENTS

## PROJECT DESIGN TEAM

### **OWNER**

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070  
(503) 781-5685  
Owner's Representative: Chad Juranek

### **ARCHITECT / PLANNER**

REITER DESIGN ARCHITECT, INCORPORATED  
6107 SW Murray Blvd., #480  
Beaverton, Oregon 97008  
(503) 574-3036  
Project Architect, Project Planner: Scott A. Reiter

### **CIVIL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
250 NW Franklin Avenue, Suite 404  
Bend, Oregon 97703  
(541) 318-1161  
Project Civil Engineer: Jennifer VanCamp

### **LANDSCAPE ARCHITECT**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Landscape Architect: Jeffery Creel

### **STRUCTURAL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Engineer: Jeff Schwindt

### **TRANSPORATION ENGINEER**

LANCASTER-MOBLEY  
321 SW 4<sup>th</sup> Ave., Suite 400  
Portland, Oregon 97204  
(503) 248-0313  
Project Engineer: Daniel Stumpf



**APPLICATION NARRATIVE**  
Landscape Review

**3**

# NORTON LANDING APARTMENTS

SE Norton Lane at SE Stratus Avenue

## Landscape Plan Review Application Narrative

August 14, 2023

### **Project Description:**

The proposed project consists of the development of 138 apartment dwelling units located in seven three-story buildings on a 4.93 acre vacant site. The proposed development includes related site improvements and 3/4 public street improvements of SE Norton Lane along the subject site frontage.

The subject site has limited access to SE Norton Lane and has no other public frontage or access. The subject site has access to public utilities. There is an existing shallow 8" public sanitary sewer located in SE Norton Lane. There is an existing public water line located in SE Norton Lane. There is an existing 15-foot private storm drainage easement along the westerly property line. There is also a 10-foot public utility easement along the SE Norton Lane frontage.

The subject site lies within the Three Mile Lane Planned Development Overlay and the Three Mile Lane Area Plan. The subject site is subject to Planned Development Amendment Approval, Three Mile Lane Deign Review, and the conditions of Approval of the prior Ordinances affecting the subject Site, including Ordinance 4709 and Ordinance 5072.

### **ASSESSOR MAP/TAX LOT**

Section 27, T.4S, R4W, WM, Tax Lot 701

### **ZONING:**

C-3 PD: General Commercial, Planned Development (Three Mile Lane PD Overlay)  
Ordinance 4709  
Ordinance 5072 (PDA 6-18)

Adjacent Property Zoning:

**NORTH:** C-3 PD: General Commercial

**EAST:** M-L: Limited Light Industrial

**SOUTH:** Outside Urban Growth Boundary, Yamhill County

**WEST:** R-4 Medium, High Density Residential

**DEVELOPMENT APPLICATION APPROVALS REQUESTED:**

The following land-use applications are being submitted for approval:

- Planned Development Amendment to Ordinance 4709 and Ordinance 5072
- Three Mile Lane Design Review
- Landscape Plan Review
- Administrative Variances

**EXISTING CONDITIONS**

The Subject Site is currently undeveloped and used for farming. The site is generally flat, with a minor slope to the southwest. There are no significant or distinguishing natural features associated with this property.

Access to the property is from Highway 18 via the intersection with Norton Lane. There is a signalized intersection of Norton Lane (SE & NE) with Highway 18. Immediately south of the highway, there is a three-way stop intersection of SE Norton Lane and SE Stratus Avenue. Stratus Avenue loops around the Medical Center and intersects back with SE Norton Lane across from the subject site.

**ACCESS and UTILITIES**

The property fronts on west side of SE Norton Lane, just south of the Altimus Plaza medical offices. The existing right-of-way for SE Norton Lane is 60 feet down to and through the Stratus Avenue Loop intersection, south of the Medical Center. The existing improvements extend through this intersection. The street is currently improved with curb and gutter and 28-foot paved section.

SE Norton Lane is improved to City standard south to the intersection with the Stratus Avenue loop. The street improvements extend approximately 180 feet along the frontage of the Subject Site. There is an additional 280 feet of site frontage that is currently not improved.

There are existing urban services and utilities within SE Norton Lane. These facilities are all available and were adequately sized to serve development of the subject site, consistent with the allowed commercial uses identified within Ordinance 4709.

**SURROUNDING USES**

**North** - The property immediately north of the subject property is the Altimus Plaza Medical Offices. Further to the north at the Stratus Avenue intersection are the Comfort Inn & Suites and the Diner restaurant. The land west of the motel is vacant land, however there is a current application for development of multifamily residential.

**East** - The property immediately east of the subject property is developed with medical offices, specifically Willamette Valley Medical Center, including a Heliport. To the south of the medical center is vacant farm land, outside of the city limits and UGB. The Evergreen Aviation Museum, north of Hwy 18, and Municipal Airport, south of Hwy 18, are located just over a mile to the east.

**South** - The property immediately south of the subject property is vacant farm land, outside of the city limits and UGB.

**West** - The property immediately west of the subject property is the Evergreen Estates Mobile Home Park, which is zoned R-4.

## **McMinnville Zoning Code, Title 17: Compliance to Applicable Chapters**

There are numerous Landscape Design requirements identified throughout Chapter 17.11 Residential Design Standards. Therefore the following narrative from the concurrent Planned Development amendment Application is included below.

### **CHAPTER 17.11 RESIDENTIAL DESIGN AND DEVELOPMENT STANDARDS**

#### **17.11.090 Apartments.**

Apartments are a type of attached housing within single-story or multi-story buildings. Apartment dwelling units may share common walls, ceilings, or floors.

##### **A. Characteristics.**

1. Site Sizes: Single walk-ups, block apartments, and many courtyard apartments can fit on a 100 x 100 foot lot. Bigger developments with multiple walk-up buildings may be as large as 250,000 square feet, or 500 x 500 foot lots.

**Applicant Response:**

*The subject site is 214,759 sf and within the range identified for walk-up apartments and is therefore compliant.*

2. Height Range: Apartment heights vary depending on the type and the location.

**Applicant Response:**

*The proposed development is for three-story apartment buildings which is fully compatible with the existing and proposed adjacent development.*

3. Density Ranges: Apartment densities vary depending on building type and site design layout.

**Applicant Response:**

*The proposed development has a density of just under 28 units per acre which is typical of "Walk-up Apartments".*

##### **B. Types of Apartments.**

###### **2. Walk-Up Apartments.**

- a. Description: Buildings are limited to three stories, and consist of about four to 12 units each, accessible from a single open-air stairwell. Dwelling units are typically constructed in Type V frame construction with fire sprinklers. Individual apartment buildings are arranged around common open space and shared parking areas.

**Applicant Response:**

*The proposed development is 12-unit to 24-unit three-story buildings with open-air stairways that provide access to the living units. The buildings are proposed to be Type V-B wood-framed construction with fire sprinklers. The buildings are located adjacent to several common open spaces.*

- b. Appropriate Context: Walk-up apartments are appropriate adjacent to or within a single dwelling neighborhood depending on site design, orientation to the street, location of parking, and the massing and scale of buildings.

**Applicant Response:**

*The subject site and proposed development is located within a C-3 Commercial zoned area to the North, an R-4 Medium, High Density parcel adjacent on the West side and the M-L Limited Light Industrial zone to the East. The existing neighborhood has a variety of building scales from the multi-story Willamette Valley Medical Center to the East, single story Medical Offices and a three-story motel to the North.*

- c. Also Named: Woody Walk-Ups, Single Stair Walk-Ups

**Applicant Response:**

*The proposed development is also referred to as a “woody walkup” and provides open air stairways to the upper living units.*

- d. Variations: May have an internal stair. Generally, in this case, the maximum number of units per floor are four. They can be designed with front and back windows for cross ventilation. Buildings can be separated to offer access to light and air on three sides.

**Applicant Response:**

*The proposed building design provides open air stairways on each side of the building to serve the upper floor units. The stairways serve as access to two ground floor and four upper floor units per stairway.*

- e. Lot Sizes: Vary widely, from 10,000 to 250,000 square feet

**Applicant Response:**

*The subject site is 214,759 sf which is within the identified range.*

- f. Density Range: 15 - 30 units per acre. (Note, maximum density will be governed by McMinnville’s municipal code.)

**Applicant Response:**

*The proposed density is just under 28 units per acre which is within the identified allowable density range.*

- g. Building Height: Usually 3 stories; can be 2 stories. (Note, maximum height will be governed by McMinnville’s Municipal Code.)

**Applicant Response:**

*The proposed building height is three-stories.*

- h. Construction Type and Building Code Issues: Typically Type V frame construction. Sprinklers for fire suppression are required.

**Applicant Response:**

*The proposed Construction Type is Type V-b, non-rated with NFPA 13-R Fire sprinklers.*



C. Development Standards. The applicable development standards are as follows:

**Applicant Response:**

| <u>Required:</u>             | <u>Proposed:</u>          |
|------------------------------|---------------------------|
| Lot Width: 50 feet min.,     | Subject Site: 320.20 feet |
| Lot Depth: 100 feet min.     | Subject Site: 670.96 feet |
| Lot Size: 5,000 sf Min.,     | Subject Site: 214,759 sf. |
| Front Setback: 15 feet min., | Proposed: 15 feet         |
| Side Setback: 10 feet min,   | Proposed: 10 feet min.    |
| Rear Setback: 20 feet min.,  | Proposed: 79'-6"          |
| Building Height:             | Proposed: 36'-4"          |

D. Design Standards. The Apartment Design Standards for multi-dwelling housing are standards that apply to apartment housing types. These standards are related to site design and building frontage, parking, compatibility with neighboring homes, open space, and private space.

1. Context and Site Design. Site design standards are intended to facilitate the development of attractive multi-dwelling housing. They encourage good site and building design, which contributes to livability, safety, and sustainability; helps create a stronger community, and fosters a quality environment for residents and neighbors.
  - a. Mirror the scale of blocks and the block-like structure of the surrounding neighborhood.

**Applicant Response:**

*The existing surrounding neighborhood has a variety of building sizes, scales and heights that range from mobile homes to the west, large multi-story hospital to the east, large single story medical office building to the north and a three-story hotel to the north. The proposed Site Plan provides a variety of three-story buildings ranging from 12 living units to 24 living units. There are portions of three buildings that have single story and two story pop-outs providing additional scale and variety.*

- b. Connect the internal network of streets and paths to those of the surrounding area where possible.

**Applicant Response:**

*Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive aisles provide access to the parking areas and buildings. There is a network of pathways and sidewalks that connect the parking areas, opens spaces and building entrances.*

- c. Configure apartments, parking areas, and common open space in clusters that mirror the scale of blocks of the surrounding neighborhood or are no more than 10,000 square feet in area per cluster.

**Applicant Response:**

*The neighborhood immediately adjacent to the subject site consists of large blocks, large sites and large scale buildings. The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters. This proposed Site Plan is compatible with existing adjacent development and provide smaller "clusters" of development.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.*

*Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.*

*Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.*

- d. Residential units must be oriented to a common open space, including a common green, a plaza, or a pocket park.

**Applicant Response:**

*The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The buildings on the proposed Site Plan are all oriented to common open spaces. The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provide the areas of each.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. Buildings A and B are directly adjacent to common open space. Building A is adjacent Open Space 01, Building B is adjacent Open Space 02.*

*Buildings C and D are grouped in a cluster and are oriented around Common Open Space 03.*

*Buildings E, F and G are also grouped in a cluster oriented around Common Open Space 04. There is also a Courtyard Open Space 05 between Building E and Building G.*

- e. Orient all buildings around a shared open space that meets the requirements of a Common Open Space.

**Applicant Response:**

*The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provides the areas of each space. There are a variety of uses proposed for the Common Open Spaces. These include both passive and active Open Space.*

*There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.*

- f. Align buildings to surrounding streets.

**Applicant Response:**

*There is only one public street, SE Norton Lane, that abuts the subject site. There are two buildings that are adjacent to the SE Norton Lane frontage, Building A and Building B. These two buildings are aligned, on the long building axis, along the street frontage.*

- g. Connect to surrounding neighborhoods, schools, parks, and other neighborhood destinations.

**Applicant Response:**

*The subject site has limited street frontage and the adjacent properties do not have existing site circulation systems to connect to. The existing sidewalk along SE Norton Lane will be extended with the public road extension. The proposed Site Plan provides several connections from the internal site circulation system to the street frontage and new public sidewalk. There are sidewalks along the norther property boundary and the southern property boundary that will provide opportunities for connections to future development of the adjacent properties.*

2. Large Site Design Requirements.

- 1. Break up parking into smaller areas and access from side streets when possible.

**Applicant Response:**

*The Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive break up the parking areas into smaller sections preventing long rows of parking. The narrow portion of the site is on the street frontage preventing additional site access points.*

- 2. Connect parking areas, building entries, and open spaces with paved walkways.

**Applicant Response:**

*The proposed Site Plan provides a network of paved walkways that connect the building entries, common open spaces and parking areas.*

- 3. Buffer parking areas with landscaping.

**Applicant Response:**

*The proposed Site Plan provides landscaped areas to serve as buffers to the exterior perimeter and to buffer the buildings from the parking. There are landscape islands throughout all parking areas that provide shade trees, ground cover and shrubbery to buffer the parking area.*

- 4. Minimize the width and number of driveways and curb cuts.

**Applicant Response:**

*The proposed Site Plan has a single 26-foot-wide driveway/curb cut onto SE Norton Lane.*

5. Provide different types of open space throughout site, both active and passive, including playgrounds, trails, volleyball courts, bocce ball courts, community gardens, etc.

**Applicant Response:**

*The proposed Site Plan provides a variety of common open spaces. They vary in size, shape and their proposed uses. There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some areas are more confined courtyard spaces and can provide a more intimate quiet setting.*

*There is a summary of the proposed Common Open Space areas on the Open Space Analysis Plan, drawing A1.0A.*

6. Align buildings to surrounding streets.

**Applicant Response:**

*The subject site has street frontage along SE Norton Lane. The proposed Site Plan orients two buildings along the street frontage. The long axis of these buildings provides the required alignment to the street frontage.*

7. Centrally locate common buildings and spaces

**Applicant Response:**

*The proposed Site Plan has buildings oriented around the common open spaces. There are three areas of the site where buildings orient to the common open spaces.*

8. Group apartments, parking, and open space into smaller clusters.

**Applicant Response:**

*The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.*

*Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.*

*Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.*

3. Pedestrian Access. On-site pedestrian circulation system shall include the following:

- a. Continuous connections between the primary buildings, streets abutting the site, ground-level entrances, common buildings, common open space, and vehicle and bicycle parking areas.

**Applicant Response:**

*The proposed Site Plan provides a network of sidewalks and walkways that connect parking areas, building entries and common open spaces.*

- b. At least one pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.

**Applicant Response:**

*The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk. The buildings that are located along the street frontage provide a sidewalk connection from the building entries to the new public sidewalk along SE Norton Lane.*

- c. Pedestrian walkways shall be separated from vehicle parking and maneuvering areas by physical barriers such as planter strips, raised curbs, or bollards.

**Applicant Response:**

*Pedestrian walkways that are located adjacent vehicle parking are separated by concrete wheel stops and the walkways are of concrete which is contrasting to the asphaltic concrete paving.*

- d. Walkways shall be constructed with the hard surface material, shall be permeable for stormwater, and shall be no less than 3 feet to 5 feet wide. If adjacent to a parking area where vehicles will overhang the walkway, a 7-foot-wide walkway shall be provided. The walkways shall be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials.

**Applicant Response:**

*Pedestrian walkways that are located adjacent vehicle parking are proposed to be concrete which is contrasting to the asphaltic concrete paving. The walkways are all 5'-0" wide. Concrete wheel stops are located 2 feet from the sidewalk in the parking spaces and will provide the 5'-0" minimum clear walkway width.*

- e. Spacing requirement: No further than 200 feet apart, on center. At least 1 pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.

**Applicant Response:**

*The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk.*



- f. May be co-located with a common green.

**Applicant Response:**

*The proposed network of walkways on the Site Plan provides circulation to all parking areas, building entries and the common green open spaces.*

4. Parking Lot Location and Design.

a. Characteristics.

1. A parking lot is a storage space for cars and should provide secure storage.
2. It is also a place where everyone is a pedestrian while getting to or from their car. Therefore, it should be designed primarily for the ease, safety and comfort of a person rolling or on foot.
3. Clearly defined pathways through parking lots and garages to building entrances, surrounding sidewalks, and transit stops enhance pedestrian safety. These pathways also provide an opportunity to improve the appearance of parking lots.
4. Design parking lots and garages so that vehicles are not the dominant feature.
5. To encourage bicycling as a mode choice, bike parking areas should include bike repair, maintenance, and cleaning stations.

b. Universal Design Standards for Nine Parking Spaces or More.

1. Parking lot pathways should be designed as part of the seamless accessibility network described in Apartment Standards, particularly the required Through Connection.

**Applicant Response to (a) and (b):**

*The proposed Site Plan provides a network of parking area sidewalks and pathways that provide convenient access for the residents to all areas of the site include the common open spaces, common facilities and the public street frontage.*

2. Driveways to shared parking areas are:

1. Limited to one driveway per street frontage.
2. Parallel parking is permitted on a driveway that crosses a front, side or rear yard abutting a street, but not within the required yard setback.

**Applicant Response:**

*The subject site and proposed Site Plan does not have any shared parking areas or site access.*

c. Parking Lots For Small Multi-Dwelling Sites - Containing More Than Nine Parking Spaces but Fewer Than 16 Parking Spaces. Off-street parking may be arranged in clusters, subject to the following standards:

1. Residential developments with fewer than 16 dwellings are permitted parking clusters of not more than five contiguous spaces.
2. Residential developments with 16 dwellings or more are permitted parking clusters of not more than eight contiguous spaces.

3. Parking clusters must be separated from other spaces by at least four feet of landscaping.
4. Clustered parking areas may be covered.

**Applicant Response:**

*The subject site and proposed Site Plan provides a parking area greater than 16 spaces therefore this is not applicable.*

d. Parking Lots For Medium-to-Large Multi-Dwelling Sites - Containing More Than 16 Parking Spaces.

1. Interior landscaping, minimum area
2. Interior landscaping shall be required for off-street parking areas 5,000 square feet or greater in size.

**Applicant Response:**

*Parking area landscaping is proposed to be provided for all parking areas and is shown on the proposed Landscape Plan.*

3. For parking lots less than 50,000 square feet, the minimum landscaped area is 5 percent.

**Applicant Response:**

*Parking area landscaping proposed is greater than the 5% minimum and is shown on the proposed Landscape Plan.*

4. For parking lots 50,000 square feet and greater, the minimum landscaped area is 8 percent.

**Applicant Response:**

*The proposed Site Plan does not include parking areas over 50,000 sf.*

5. Planted areas may take the form of landscape areas and planter bays.

**Applicant Response:**

*This is understood and the landscape islands have been included in the overall parking lot landscape area.*

6. Landscaped areas along a through connection count toward required interior landscaping.

**Applicant Response:**

*This is understood and the landscape areas along through connections have been included in the overall interior landscape area.*

7. Landscaped islands and peninsulas shall be evenly distributed throughout all parking areas and separated no more than 60 feet from another. Such islands shall be provided with raised curbs, be a minimum of five feet in width, and shall each contain at least one deciduous tree. To achieve the maximum canopy coverage, all trees shall be non-columnar and have

root systems that form deep before spreading to decrease the episodes of buckled pavement.

**Applicant Response:**

*Landscape islands have been incorporated into the proposed Site Plan and parking area design. Parking area landscape islands and planting beds will be contained with raised concrete curbs. The spacing of the landscape islands is proposed to exceed the 60 foot maximum spacing in several locations primarily due to the parking stall width and spacing standards. An administrative variance is being requested to increase the spacing to 63 feet maximum.*

8. Trees may line the required Through Connection, and/or be clustered within landscape islands or planter bays, and/or shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.

**Applicant Response:**

*Trees are proposed within all of the landscape islands and planter bays throughout the parking areas providing the design tree canopy effect and shading.*

9. When a parking area abuts property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line.

**Applicant Response:**

*Six foot high Sight obscuring fences are proposed along the side and rear property lines. Buffer plantings and trees are also proposed along these property lines.*

- e. Parking Lot Setbacks Adjacent to Buildings and Structures. Where an off-street parking or vehicular use area is located adjacent to a building or structure, the off-street parking or vehicular use area shall be set back from the exterior wall of the building or structure by a minimum five-foot-wide landscape strip, or by a minimum five-foot-wide paved pedestrian walkway.

**Applicant Response:**

*The proposed Site Plan maintains a minimum 10-foot separation of parking areas from the closest point of any building or structure.*

- f. Parking Lot Location. Off-street parking spaces and vehicle maneuvering areas shall not be located:
  1. Within of 20 feet from any street property line, except alley property lines;

**Applicant Response:**

*The parking areas on the proposed Site Plan are located at a distance greater than 20 feet from the street frontage property line.*

2. Between a street property line and the front façade of cottages located closest to the street property line. This standard does not apply to alleys.

**Applicant Response:**

*This is not applicable to this proposed Site Plan and development.*

3. Off-street parking spaces shall not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.

**Applicant Response:**

*The parking areas on the proposed Site Plan are located at a distance of 6-feet from the North and South (side) property lines and 7 feet from the West (rear) property line. This is less than the 10 feet required from the adjacent side and rear property lines. This is compatible with existing adjacent development that also has parking located 6 feet from the adjacent property lines.*

*An Administrative Adjustment is being requested for the non-compliance of this issue.*

4. Landscaping, fencing, or walls at least three feet tall shall separate clustered parking areas and parking structures from common courtyards and public streets.

**Applicant Response:**

*The is not applicable to the proposed Site Plan as there are no parking areas proposed adjacent to common courtyards and public streets.*

5. Garages and carports (whether shared or individual) must not abut common courtyards.

**Applicant Response:**

*This is not applicable as no carports or garages are proposed.*

6. Individual attached garages up to 200 square feet shall be exempt from the calculation of maximum building footprint for cottages.

**Applicant Response:**

*This is not applicable as no garages are proposed.*

7. Individual detached garages must not exceed 400 square feet in floor area.

**Applicant Response:**

*This is not applicable as no garages are proposed*

8. Garage doors for attached and detached individual garages must not exceed 20 feet in width.

**Applicant Response:**

*This is not applicable as no garages are proposed*

- g. Parking Lot Required Through Connections. Through Connections may be multi-modal or used exclusively for bicycle and pedestrian access and need to meet the standards in Table 1.

**Applicant Response:**

*The proposed Site Plan and parking area layout does not create “through connections”.*

5. Common Open Space.

a. Characteristics.

- 1. Provide opportunities for formal and informal recreational use by residents of all ages. This could be a shared recreational facility including sports fields, play structures, bike tracks, courts, swimming pool, or other options.

**Applicant Response:**

*The proposed common open areas provide formal and informal recreations opportunities for the residents. There are formal seating areas and courtyards with benches and raised planters with seating walls, large open lawn areas for informal sports activities and formal picnic areas with picnic tables and barbeques.*

- 2. Provide tall deciduous trees for summer shade and winter solar access. When possible, preserve and incorporate large existing trees at least 9 inches in diameter as a focal point of open spaces.

**Applicant Response:**

*The landscape design includes deciduous shade trees adjacent the formal sitting areas and picnic areas and they are adjacent to the larger open lawn areas. These will provide the desired summer shading and will permit winter sun exposure. The existing subject site does not contain any existing trees.*

- 3. Enhance the usability of the space through the inclusion of elements including seating, outdoor lighting, weather protection and/or shade structures, and art, among other features.

**Applicant Response:**

*The proposed common open areas provide seating areas and courtyards with benches and raised planters with seating walls. The formal picnic areas include picnic tables and barbeques.*

- 4. Incorporate landscaping that receives at least 50 percent of its irrigation from harvested rainwater.

**Applicant Response:**

*As this is an identified characteristic it is not a requirement. The proposed design does not have the ability to store and harvest rain water.*

- 5. Provide opportunities for food cultivation including a community garden and/or incorporating cultivated species into the landscaping.

**Applicant Response:**



*Community gardens are not proposed.*

6. A maximum of 50 percent of common open space may be provided in a rooftop deck that includes shared amenities, weather protection, and landscaping, and is accessible to all residents.

**Applicant Response:**

*Rooftop deck areas are not proposed.*

7. A shared outdoor courtyard or shared street/woonerf that is fronted by individual entrances, windows, and balconies There should be a combination of hardscape and landscaped space and/or planters.

**Applicant Response:**

*The proposed Common Open Space Courtyards between Buildings E and G and between Buildings F and G have building entries and ground floor patios adjacent to them. There is also decorative hardscape concrete paving with an enhanced score joint pattern. Benches and raised planters with seat height walls help to define the courtyard spaces. Enhanced landscape plantings also define the courtyard spaces.*

b. Required Elements, General.

1. A common open space shall be provided that is centrally located and designed with a clear function that enhances the livability of residents. These functions shall include passive and active uses. The open space shall be accessible to all residents and if possible be fronted by clearly defined unit entrances. The common open space shall serve as the focus of surrounding buildings. Entries and windows shall face the common open space to provide informal surveillance. Common open spaces shall be accessible to all residents.

**Applicant Response:**

*The proposed Common Open Spaces are centrally located in three primary building cluster groups. The on-site walkway systems connects all of the open spaces, building entries and parking areas throughout the site making accessible to all residents of the development.*

*There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.*

2. Common open space shall be a minimum of 15 percent of the site.

**Applicant Response:**

*The amount of total proposed Common Open Space provided is greater than the 15% of the site area required. The total site area is 214,759 sf, thus*

32,214 sf of Common Open Space is required. 32,769 sf of Common Open Space is provided.

An Open Space Analysis Plan has been prepared that delineates each proposed Common Open Space and identifies and summarizes the areas of each. See drawing A1.0A included within this application.

6. Private Open Space.

a. Characteristics.

1. Every dwelling needs private open space for relief from indoors and to provide access to fresh air, light, and nature. Private open space may take many forms based on the size of unit.
2. They should translate into a perception of an increase in living space and the ability to invite the outdoors in. Additionally, these open spaces can provide environmental benefits with plants that consume carbon dioxide and help reduce stormwater runoff. Spaces should be adequate to be usable, allowing space for a chair to sit in, a place to barbecue or hang clothes to dry, or for a pet to curl up.
3. Private open space should enhance the residential function of the building while also improving the appearance of the building. They should be integrated into the overall architectural form and add detail to the façade.
4. Placement can vary based on privacy concerns. It can be combined across multiple floors.

**Applicant Response to (a) 1-4:**

*The proposed living unit and building design provides private open space for each unit. The ground floor living units have on-grade patios that are 96 sf each. The ground floor patios are directly adjacent the living rooms and provide a door onto the patio with adjacent living room windows. This blends the living room space and the patios into usable living areas. The ground floor patios are screened with landscaping to maintain privacy from the adjacent walkways and parking areas.*

*The upper-level living units each have 72 sf private decks that are directly adjacent the living rooms with the living room windows and a door opening directly onto the deck area. This also extends the living space from the living room onto the private deck.*

*The placement of the decks and patios provides privacy to adjacent living units and provide greater articulation on the building facades improving the overall appearance and character of the buildings.*

b. Required Elements.

1. All units shall have a minimum of 36 square feet of private open space that allows for personalization and private use of the space and contributes to the livability and function of the dwelling. Any exterior private open spaces shall be supplemented with operable windows to allow for cross-

ventilation, increase airflow and provide the ability to control access to the outdoors.

**Applicant Response:**

*The ground floor living unit patios are 96 sf each and the upper floor living unit decks are 72 sf each. Both exceed the minimum areas required.*

2. At least 50 percent of upper units shall have a balcony that is accessible from the interior of the unit that is a minimum of 60 square feet with no dimension less than 6 feet. These balconies can be designed to be up to the full width of the apartment in order to provide adequate space for use and allow greater indoor/outdoor flow. Balconies can be cantilevered, semi-recessed, or fully recessed. They should be located based on privacy and environmental concerns. If balconies are transparent, adequate storage should be provided within the unit or the larger building so that balconies do not become informal storage spaces.

**Applicant Response:**

*All of the upper floor living units have decks that are 72 sf each with a minimum dimension of 6 feet. This exceeds the minimum number of units and the minimum deck area required. The decks are placed to maintain privacy between adjacent living units.*

3. Private outdoor space at the ground level must meet the requirements of Universal Standards: Front Yard regardless of whether the private outdoor space is in the front, side, or rear of a building.

**Applicant Response:**

*The ground floor living unit patios are all screened with landscaping and are setback from walkways, street frontage and adjacent parking areas as required.*

- c. Supplemental Elements. In addition to meeting the required elements above, projects must provide private open space in the form of one of the options listed below.

1. A "Juliet-style" balcony of 12-inch dimension that allows residents to bring a sense of the outdoors into the unit. Must have doors that can open inwards or full height sliding glass doors to allow the introduction of fresh air and sunlight. If this item is selected, units must also include operable windows to increase airflow/ability to control access to the outdoors.
2. An upper story rooftop deck or terrace that may include space for outdoor seating, dining, and planters for cultivation. This terrace may be stepped back on structures over two stories so as to reduce the visual impact of upper floors.
3. Alternative option that meets the concept and guiding principles.

**Applicant Response:**

*All upper floor living units provide decks that are 72 sf each, thus meeting the supplementary requirements. All unit decks are directly adjacent to the living rooms and provide operable windows and a deck access door directly onto the deck.*

7. Alleys.

**Applicant Response:**

*The subject site does not have the opportunity for providing alleys and this is not applicable.*

8. Landscaping

a. Characteristics.

1. Use landscape elements, particularly plant materials, in an organized and harmonious manner that will enhance, protect and promote the economic, ecological, and aesthetic environment of McMinnville.

**Applicant Response:**

*The proposed landscape design provides a cohesive design that utilizes plant materials to enhance the overall site appearance, enhance the outdoor living environment and provide an overall aesthetically pleasing site character. The choice and selection of plant materials incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity.*

*The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Shade trees will soften the built environment, reduce heat island, and provide habitat for birds. Flowering Shrubs will soften the buildings and enhance the aesthetic value of the site and provide food for local pollinators.*

*Lawn areas will provide a pastoral aesthetic reminiscent of the Willamette Valley, promoting harmony with adjacent landscapes. These areas will provide opportunities outdoor activities that promote the mental and physical health of the residential community, and in turn, the greater community of McMinnville. Finally, the regular maintenance of the landscape will help provide jobs for the local economy.*

2. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:

- a. Reduce soil erosion and the volume and rate of discharge of stormwater runoff.

**Applicant Response:**

*Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.*

- b. Aid in energy conservation by shading structures from energy losses caused by weather and wind.

**Applicant Response:**

*Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.*

- c. Mitigate the loss of natural resources.

**Applicant Response:**

*Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.*

- d. Provide parking lot landscaping to reduce the harmful effects of heat, noise, and glare associated with motor vehicle use.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects. The combination of landscape plant materials and sight obscuring fences buffer noise from the properties.*

- e. Create safe, attractively landscaped areas adjacent to public streets.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.*

- f. Require the planting of street trees along the City's rights-of-way.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.*

- g. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character, and value of existing neighborhoods.

**Applicant Response:**

*The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adject to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.*

- h. Provide shade, and seasonal color.

**Applicant Response:**

*The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers*



and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.

- i. Reduce glare, noise, and heat.

**Applicant Response:**

The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.

Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.

- j. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.

**Applicant Response:**

Visual noise will be mitigated with the use of a site obscuring fence, trees, and evergreen shrubs.

Lighting impacts are mitigated by the selection and placement of light fixtures that provide full cut-off of light pollution to the abutting properties.

- k. Unify development and enhance and define public and private places.

**Applicant Response:**

The landscape design helps unify the development with the use of repetition of design elements in private and public areas. These elements include repetition of form, texture, and color of the plant materials.

Public spaces meant to be used by all residents are defined with lawn areas, walkways, benches, raise planters, and flowering trees.

Landscaping adjacent to private areas are defined by ornamental plantings of shrubs, grasses and groundcovers.

- l. Preserve existing mature trees.

**Applicant Response:**

There are no existing trees onsite.

- m. Enhance the urban forest and tree canopy.

**Applicant Response:**

*The urban forest is enhanced with the planting of trees throughout the site; including shade trees and flowering trees. In total, 115 trees are proposed, including street trees.*

- n. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.

**Applicant Response:**

*The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.*

- o. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.

**Applicant Response:**

*A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.*

*The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically branched and high canopy structure for the interphase between public and private space.*

*Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.*

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

- p. Support McMinnville as a community that cares about its appearance. It is further recognized that landscaping increases property values, attracts potential residents and businesses to McMinnville, and creates safer, more pleasant living and working environments for all residents and visitors to the city.

**Applicant Response:**

*The proposed landscape design will use a variety of trees, shrubs, grasses and groundcovers to provide an aesthetically pleasing appearance that will increase the value of the property, attract residents and create a safe and pleasing living environment.*

*The proposed trees throughout the site will provide structure, shade, pleasing fall color and a net increase to the urban forest canopy. The ornamental landscape elements will soften the built environment and offer a pleasing appearance for those living and visiting the community.*

b. Required Elements.

- 1. All areas of the site not occupied by the structures or paved areas shall be landscaped in an attractive and functional manner.

**Applicant Response:**

*The proposed landscape design provides landscape plant materials in all areas of the site not occupied by structures or parking and paved areas. The proposed landscape design provides an aesthetically pleasing design and use of a variety of plant materials.*

- 2. A minimum of 20 percent of the net site shall be landscaped. Paved pedestrian paths, when integrated within the landscaped area, may satisfy up to 5 percent of this requirement. Landscaped setback areas, landscaped common open spaces, eco-roofs, vegetated stormwater facilities, preserved natural areas, and planter areas can be credited toward the minimum landscape standard.

**Applicant Response:**

*The proposed Site Plan and landscape design provides 30 percent of the subject site area in landscaping. More specifically, the site area is 214,759 sf. 64,157 sf of landscape area is provided, which is 30 percent.*

9. Privacy and Screening.

- a. Characteristics. Low walls or fences are encouraged to provide separation between private open space and common open space, streets, or internal circulation paths. Fences should be designed to integrate with the architecture of the building and add visual interest through the use of materials, color, and detail.

**Applicant Response:**

*The proposed landscape design provides landscape screening and buffering to ground floor living unit patios (private open space). The plant materials used for screening these areas blends harmoniously with the adjacent landscape design providing an attractive and effective privacy screen.*

b. Required Elements.

1. All fences on the interior of the development shall be no more than 3 feet high. Fences along the rear or side property lines of the development may be up to 6 feet high. Chain-link fences are prohibited.

**Applicant Response:**

*There are no internal site fences proposed. There is a 6-foot sight obscuring fence proposed on the site side and rear property lines. The fence will not be chain-link fencing.*

2. Mechanical and communication equipment and outdoor garbage and recycling areas shall be screened so they are not visible from streets and common open spaces.

**Applicant Response:**

*The proposed trash and recycling enclosures provide a solid evergreen screen along the sides of the facilities, as illustrated on the Landscape Plan.*

3. Utilities such as transformers, heating and cooling, electric meters, and other utility equipment shall not be located within 5 feet of a front entrance and shall be screened with sight-obscuring materials.

**Applicant Response:**

*The proposed locations of any on-site transformers or other utility equipment has not yet been determined. The transformers and equipment will be screened as required.*

4. The placement of balconies above the first story shall not create a direct line of sight into the living spaces or backyards of adjacent residential properties.

**Applicant Response:**

*The proposed Site Plan does not locate buildings that would orient living unit balconies that would create a line of sight into adjacent properties.*

10. Front Yard.

a. Characteristics.

1. For all housing types the front setback—even when it is small or zero, should be designed to provide a transition from the public realm of the street to the private realm of the dwelling.
2. The front setback provides a vital transition between the public area of the street and the private spaces within the dwelling. The smaller the front setback is, the more important the concept of layering public to private spaces becomes. When multi-dwelling units are on the ground floor of the building and face the perimeter of the site and surrounding streets, they must meet the standards of this section.

b. Required Elements.

1. Dwelling units located on the ground floor of the building and facing the perimeter of the site and surrounding streets must meet the requirements of either:

- a. Front Yard Type 1: Neighborhood

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provides a landscape design that meets or exceeds the requirements for a Type 1 Front Yard (Neighborhood Type). The "Gateway" area is landscaped at the minimum depth with grasses, the "Front Yard" is fully landscaped and provides a hedge screen at 3 feet to provide privacy to the unit patios.*

11. Compatibility.

- a. Characteristics. New multi-dwelling housing should be compatible with its surrounding context while introducing new shape, size, and detail variation, enabling different housing styles and types to sit side-by-side harmoniously.

**Applicant Response:**

*The existing developed neighborhood contains a wide variety of uses, building styles and scales. The existing uses include a medical office building, a three-story hotel, the large multi-story hospital (Willamette Valley Medical Center) and single-story mobile homes in a mobile home park. The proposed three-story apartment buildings are well within the range and scale of existing adjacent developments. The proposed buildings range in size from twelve units to twenty-four units also providing a variety in the scale of buildings within the proposed development.*

b. Required Elements.

1. On a site with multiple buildings of varying scales (or that vary from the surrounding context), provide a gradual transition between scales. For example, locate dwellings that are similar in scale and density along the street frontage and transition to lower scale and density buildings toward the rear of the site. Use rear driveways and landscaping as a buffer backing up to adjacent properties of a different scale.

**Applicant Response:**

*The proposed Site Plan locates similar scale buildings adjacent the street frontage and along the northerly property line that are compatible and in context with the existing adjacent development. The buildings in the rear of the site are set back nearly 80 feet to provide separation and a buffer from the single-story mobile home park adjacent the rear of the site.*

2. Arrange building volumes and setbacks in a way that reflects neighborhood patterns along street frontages and contributes to the desired character.

**Applicant Response:**

*The existing neighborhood character is quite diverse and does not have any established patterns to follow. The proposed Site Plan provides a street frontage compatible with similar developments within the community and provides a pedestrian friendly street scape.*



3. Arrange courtyard apartments so that end units reflect a neighborhood context of detached units along the street frontage.

**Applicant Response:**

*This element is not applicable to the garden style apartments.*

4. Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.

**Applicant Response:**

*The existing neighborhood has a variety of height and scale buildings. The existing large medical office building adjacent the site to the north is single story. However, the proposed Stratus Village to the West of the medical Office Building and directly adjacent the subject site to the North is proposed to be a three-story building.*

5. Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.

**Applicant Response:**

*The proposed site and building design does not propose stepped back upper floors. The building design, the proposed roof design and building articulation provides the desired human scale elements without stepping upper floors.*

- c. Supplemental Elements. In addition to meeting the required elements above, projects must respond to the compatibility requirement in the form of three of the options listed below.

1. Use roof forms and bays to break up the overall mass of larger residential structures.

**Applicant Response:**

*The proposed building design includes a variety of roof forms that include hipped sloped roofs, intersecting gable roof forms and large breaks in the overall roof at building indentations. The proposed roof design breaks up the overall building mass into smaller sections thus reducing the overall appearance of the building mass and scale.*

2. Walls incorporate vertical wall offsets, projections, or recesses to reduce building façades into smaller volumes and define visually distinct living unit modules.

**Applicant Response:**

*The proposed building design includes offsets in the façade wall planes. The deck forms project from the primary building wall plane and the intersecting gable roof form serves to break-up the overall building mass. The larger buildings have central indentations that serve to further break-up the building mass.*

3. Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.

**Applicant Response:**

*This element is not utilized in the building design.*

4. Mark a distinct physical transition between the base and upper floors of a building through a change in brick pattern, change in materials and/or wall surface pattern, articulation of a floor line, or change in window types.

**Applicant Response:**

*The proposed building design includes a variety of building materials and textures. The building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building.*

5. Use horizontal elements the entire width of the front façade to mark a break between floors or along the roofline including band course, band molding, bellyband, or belt course.

**Applicant Response:**

*The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building.*

6. Use a variation in roof forms on all four elevations of a structure to visually break up monotony including pitched or sloping roof elements, variations in pitch and height of roof planes, dormers, eaves, gable, or dormer end brackets, corbels, or decorative wood timbers.

**Applicant Response:**

*This element is not utilized in the building design.*

7. Limit continuous ridgelines to less than 40 feet in length and continuous eaves to 25 feet in length.

**Applicant Response:**

*This element is not utilized in the building design.*

8. Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.

**Applicant Response:**

*This element is not utilized in the building design.*

## 12. Wall and Roof Design.

### A. Characteristics.

1. For buildings that front the street, avoid long, monotonous, uninterrupted walls. Modulate buildings walls and roofs to prevent large, uninterrupted walls and building mass.

**Applicant Response:**

*The proposed building design provides significant articulation and variety in walls planes. The longest wall plane is less than 20 feet before intersecting or recessed wall planes. Projecting or recessed building elements provide a great amount of articulation and variety of wall planes and roof forms.*

2. Differentiate between the base of the building and the top of the building to enhance the pedestrian realm. Make base treatment cohesive across façades and integrate with the architectural character of the building.

**Applicant Response:**

*The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building. This design concept is applied to all buildings on site.*

3. Multi-dwelling development must address the following design objectives:
  - A. Articulation – All street-facing buildings shall incorporate design elements that break up façades into smaller planes.
  - B. Eyes on the street – A certain percentage of the area of each street-facing façade must be windows or entrance doors.
  - C. Main entrance – On street-facing façades, at least one main entrance must meet standards for location, orientation, and visibility.
  - D. Detailed Design – All street-facing buildings shall include several features.

**Applicant Response:**

*The proposed site and building design address the four design objectives. The building design provides building and roof forms to break up the facades into smaller planes; the street facing units provide windows and glazed doors that face the street frontage; the open entry stairway to the units faces the street frontage; and the street facing building elevations provide a variety of design features.*

### B. Required Elements.

#### 1. Articulation.

- A. For multi-dwelling buildings with 30 to 60 feet of street frontage, a minimum of one of the following elements shall be provided along the street-facing façades.
- B. For buildings with over 60 feet of street frontage, at least one element below shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.
  1. A porch at least 5 feet deep.

2. A balcony that is at least 2 feet deep and is accessible from an interior room.
  3. A bay window that extends at least 2 feet.
  4. A section of the façade that is recessed by at least 2 feet deep and 6 feet long.
  5. A gabled dormer.
- C. Buildings under 30 feet in length are exempt from these requirements.

**Applicant Response:**

*The proposed street frontage buildings have over 60 feet of street frontage. The proposed building design does not have a wall plane over 20 feet between articulation or design features to break up the facades. These include the recessed front porch/patio and upper floor decks that are 6 feet in depth. These projecting elements also have gabled and hipped roof dormers.*

2. Eyes on The Street.

- A. At least 15 percent of the area of each street-facing façade must be windows or entrance doors. Windows used to meet this standard must be transparent and allow views from the building to the street. Glass blocks and privacy windows in bathrooms do not meet this standard.
- B. Window area is considered the entire area within the outer window frame, including any interior window grid.
- C. Doors used to meet this standard must face the street or be at an angle of no greater than 45 degrees from the street.

**Applicant Response:**

*The proposed building design provides 22% window area on the street facing facades. This includes the living room, bedroom and bath windows and the glazed door in the living room leading to the decks and ground floor patios.*

3. Main Entrances. Main entrances must meet both of the following standards.

- A. Be no further than 8 feet behind the longest street-facing wall of the building.
- B. Face the street, be at an angle of up to 45 degrees from the street, or open onto a porch. If the entrance opens up onto a porch, the porch must meet all of these additional standards.
  1. Be at least 25 square feet in area with a minimum 4 feet depth.
  2. Have at least one porch entry facing the street.
  3. Have a roof that is no more than 12 feet above the floor of the porch.
  4. Have a roof that covers at least 30 percent of the porch area.

**Applicant Response:**

*The proposed building provides open air enclosed stairways to serve the living units on that side of the building including the ground floor units. The stairway provides a roof covering.*

4. Detailed Design.

- A. For multi-dwelling buildings with up to 30 feet or more of street frontage, a minimum of two of the elements shall be provided along the street-facing façade or façades.

- B. For buildings with over 30 feet of street frontage, at least one element shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.
1. Covered porch at least 5 feet deep, as measured horizontally from the face of the main building façade to the edge of the deck, and at least 5 feet wide.

**Applicant Response:**

*The proposed design for Building 'A' has 70 feet of street frontage. The façade is broken up into three primary articulated sections. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. The longest wall plane between these sections is 20 feet. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.*

*The proposed design for Building 'B' has 140 feet of street frontage. The façade is broken up into six articulated sections and the longest wall plane between these sections is 20 feet. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.*

2. Recessed entry area at least 2 feet deep, as measured horizontally from the face of the main building façade, and at least 5 feet wide.

**Applicant Response:**

*The proposed building recessed entries and open-air stairways to the upper floor living units. These entries are 8 feet wide and 18 feet deep.*

3. Offset on the building face of at least 16 inches from one exterior wall surface to the other.

**Applicant Response:**

*The proposed street facing building designs provide multiple changes in wall planes. The depth of the wall plane variations are 6 feet in several locations and 10 feet in additional locations. These variations occur multiple times on all street frontage building façade elevations.*

4. Dormer that is at least 4 feet wide and integrated into the roof form.

**Applicant Response:**

*This design element is not utilized in the building design.*

5. Roof eaves with a minimum projection of 12 inches from the intersection of the roof and the exterior walls.

**Applicant Response:**

*The proposed building roof design provides 24 inch, 18 inch and 12 inch roof overhangs.*

6. Roofline offsets of at least 2 feet from the top surface of one roof to the top surface of the other.

**Applicant Response:**

*The design element is not utilized in the building design.*

7. Horizontal lap siding between 3 to 7 inches wide (the visible portion once installed). The siding material may be wood, fiber-cement, or vinyl.

**Applicant Response:**

*The proposed building elevation design includes fiber cement horizontal lap siding with a 6-inch exposure.*

8. Brick, cedar shingles, stucco, or other similar decorative materials covering at least 40 percent of the street-facing façade.

**Applicant Response:**

*This design element is not utilized in the building design.*

9. Gable roof, hip roof, or gambrel roof design.

**Applicant Response:**

*The proposed building roof design includes hip roofs and gable roof forms.*

10. Window trim around all windows at least 3 inches wide and 5/8 inches deep.

**Applicant Response:**

*This design element is not utilized in the building design.*

11. Window recesses, in all windows, of at least 3 inches as measured horizontally from the face of the building façade.

**Applicant Response:**

*This design element is not utilized in the building design.*

12. Balcony that is at least 3 feet deep, 5 feet wide, and accessible from an interior room.

**Applicant Response:**

*The proposed building and living unit design includes recessed upper floor decks that are 6 feet deep and 12 feet wide.*



13. Bay window at least 2 feet deep and 5 feet long.

**Applicant Response:**

*This design element is not utilized in the building design.*

14. One roof pitch of at least 500 square feet in area that is sloped to face the southern sky and has its eave line-oriented within 30 degrees of the true north/ south axis.

**Applicant Response:**

*This design element is not utilized in the building design.*

## CHAPTER 17.57 LANDSCAPING

### 17.57.010 Purpose and Intent:

The purpose and intent of this Chapter is to encourage and, where appropriate, require the use of landscape elements, particularly plant materials, in proposed developments in an organized and harmonious manner that will enhance, protect and promote the economic, ecological and aesthetic environment of McMinnville. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:

A. Provide guidelines and standards that will:

1. Reduce soil erosion and the volume and rate of discharge of storm water runoff.

**Applicant Response:**

*Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.*

2. Aid in energy conservation by shading structures from energy losses caused by weather and wind.

**Applicant Response:**

*Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.*

3. Mitigate the loss of natural resources.

**Applicant Response:**

*Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.*

4. Provide parking lot landscaping to reduce the harmful effects of heat, noise and glare associated with motor vehicle use.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.*

5. Create safe, attractively landscaped areas adjacent to public streets.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.*

6. Require the planting of street trees along the City's rights-of-way.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.*

7. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character and value of existing neighborhoods.

**Applicant Response:**

*The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adject to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.*

8. Provide shade, and seasonal color.

**Applicant Response:**

*The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.*

9. Reduce glare, noise and heat.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.*

*Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.*

- B. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.

**Applicant Response:**

*The proposed Landscape Plan and design provides trees, buffer and screening plantings along the site perimeter. This will buffer the effects of on-site parking area and building lighting from the adjacent abutting properties. The overall landscape plan and*

development also aids in buffering sound both from the subject site and from the adjacent abutting properties.

- C. Unify development and enhance and define public and private places.

**Applicant Response:**

The proposed Landscape Plan and design will provide street trees along the SE Norton Lane street frontage. The landscape design also provides screening of private spaces for the living units and the proposed open common areas on site.

- D. Preserve existing mature trees.

**Applicant Response:**

The existing undeveloped site was historically agricultural uses and there are no existing trees.

- E. Enhance the urban forest and tree canopy.

**Applicant Response:**

The proposed Landscape Plan and design provides a variety of trees that will enhance and contribute to the overall urban tree canopy.

- F. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.

**Applicant Response:**

The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.

- G. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.

**Applicant Response:**

A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.

The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically

branched and high canopy structure for the interphase between public and private space.

Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

H. Support McMinnville as a community that cares about its appearance.

**Applicant Response:**

*The proposed Landscape Plan design provides an esthetically pleasing and attractive landscape design. This overall site and landscape design enhances the character of the existing neighborhood and provides a livable community environment.*

**17.57.070 Area Determination—Planning factors.**

A. Landscaping shall be accomplished within the following ranges:

3. Multiple-dwelling, twenty-five percent of the gross area. This may be reduced to not less than fifteen percent upon approval of the review committee. (The gross area to be landscaped may only be reduced by the review committee if there is a showing by the applicant that the intent and purpose of this chapter and subsection B of this section are met.)

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provide 30 percent of the site area in landscaping.*

4. A parking lot or parking structure built in any zone providing parking spaces as required by the zoning ordinance shall be landscaped in accordance with the commercial requirements set forth above in subsection 2 of this section.

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provide parking lot landscaping as identified in 17.11.090 Residential Design Standards for Apartments.*

B. The following factors shall be considered by the applicant when planning the landscaping in order to accomplish the purpose set out in Section 17.57.010. The Landscape Review Committee shall have the authority to deny an application for failure to comply with any or all of these conditions:

1. Compatibility with the proposed project and the surrounding and abutting properties and the uses occurring thereon.

**Applicant Response:**

*The adjacent uses: residential, commercial and agriculture would not be adversely affected by the proposed multi-family development. Uses to the north, south and west of the site will be sufficiently screened with a site obscuring fence and evergreen hedge. Additionally, the residential use to the west will be buffered with more than 15' between the property line and any structure.*

2. Screening the proposed use by sight-obscuring, evergreen plantings, shade trees, fences, or combinations of plantings and screens.

**Applicant Response:**

*Screening is proposed on north, south and west perimeters of the site. Screening will be achieved with a site obscuring fence and evergreen hedge.*

3. The retention of existing trees and natural areas that may be incorporated in the development of the project. The existing grade should be preserved to the maximum practical degree. Existing trees shall be provided with a watering area equal to at least one-half the crown area.

**Applicant Response:**

*The site was previously open for agricultural use and there are no existing trees on site.*

4. The development and use of islands and plantings therein to break up parking areas.

**Applicant Response:**

*A total of 37 parking lot islands are used to break up parking areas. Each island has a shade tree and shrubs / grasses to help break up the parking areas.*

5. The use of suitable street trees in the development of new subdivisions, shopping centers and like developments. Certain trees shall be prohibited in parking areas: poplar, willow, fruit, nut, birch, conifer, and ailanthus.

**Applicant Response:**

*None of the proposed trees are prohibited. Many of the trees are listed on the City of McMinnville street tree list, including the Green Vase Zelkova proposed along SE Norton Lane.*

6. Suitable watering facilities or irrigation systems must be included in or near all planted areas;

**Applicant Response:**

*The proposed development will have an automatic irrigation system with a water efficient controller and rain sensor. The irrigation system will be commercial grade with separate zones for lawn and shrub areas.*

**APPLICATION DRAWINGS**  
ARCHITECTURAL,  
AND LANDSCAPE

**4**



**PROJECT DATA**

**SITE INFORMATION:**  
 ZONING: C3 - GENERAL COMMERCIAL, THREE-MILE LANE PLANNED DEVELOPMENT OVERLAY  
 MAP NO: T4SR4W-27, TAX LOT 701  
 SITE AREA: 214,759 SF, 4.93 ACRES

**BUILDING INFORMATION**  
 CONSTRUCTION TYPE: TYPE V-B  
 BUILDING HEIGHT: THREE-STORIES  
 OCCUPANCY GROUPS: R-2 RESIDENTIAL, APARTMENTS

**LIVING UNITS:**  
 ONE-BEDROOM, 1 BATH: 42 UNITS  
 TWO-BEDROOM, 1 BATH: 36 UNITS  
 TWO-BEDROOM, 2 BATH: 42 UNITS  
 THREE-BEDROOM, 2 BATH: 18 UNITS  
 TOTAL UNITS: 138 UNITS

**PARKING:**  
 REQUIRED:  
 1BR AND 2BR UNITS, 1.5 SPACES/UNIT= 180 SPACES  
 3BR UNITS, 2 SPACES/UNIT= 36 SPACES  
 TOTAL REQUIRED: 216 SPACES

**PROVIDED:**  
 STANDARD SPACES: 188 SPACES  
 COMPACT SPACES: 22 SPACES  
 ACCESSIBLE SPACES: 8 SPACES  
 TOTAL PARKING PROVIDED: 218 SPACES

**PROJECT TEAM**

**OWNER/DEVELOPER:**  
 KWDS, LLC  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**GENERAL CONTRACTOR:**  
 KOHL INC.  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**ARCHITECT/PLANNER:**  
 REITER DESIGN ARCHITECT, INC.  
 7965 SW CIRRUS DRIVE  
 BEAVERTON, OREGON 97008  
 (503) 574-3036  
 PROJECT ARCHITECT: SCOTT REITER

**CIVIL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 250 NW FRANKLIN AVE., SUITE 404  
 BEND, OREGON 97703  
 (541) 318-1161  
 PROJECT ENG.: JENNIFER VAN CAMP, PE

**STRUCTURAL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT ENGINEER: JEFF SCHWINDT, PE

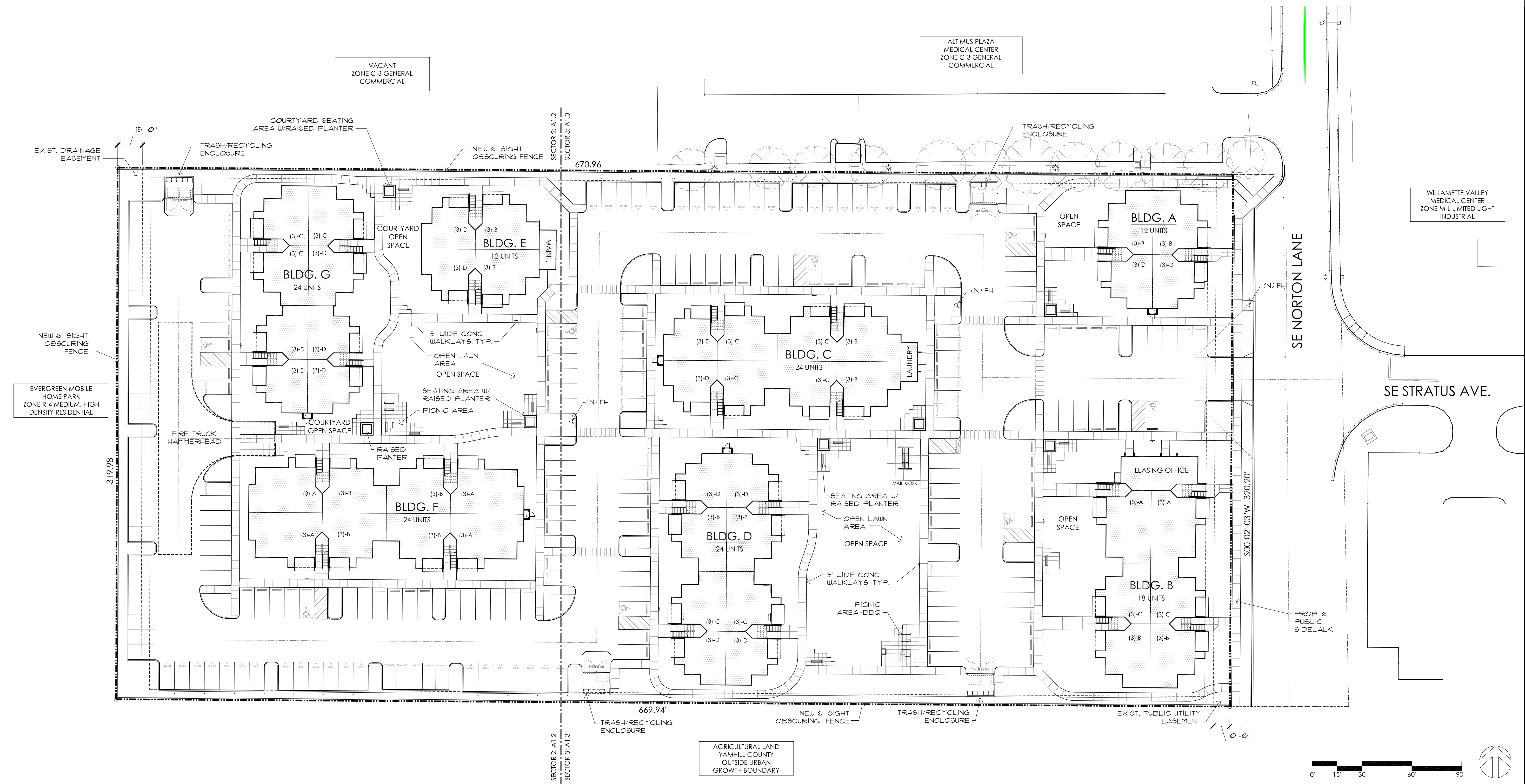
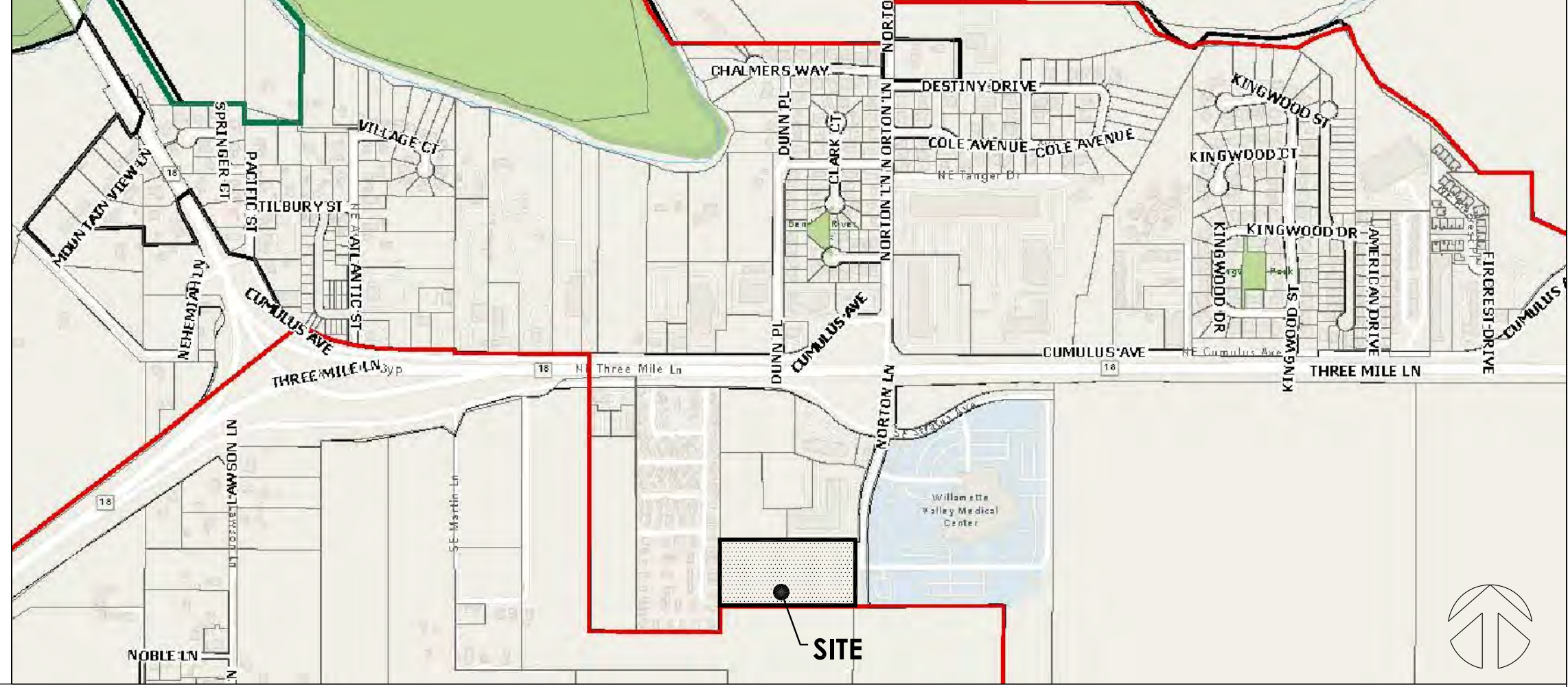
**TRANSPORTATION ENGINEER:**  
 LANCASTER-MOBY  
 321 SW 4TH, AVE, SUITE 400  
 PORTLAND, OREGON 97204  
 (503) 248-0313  
 CONTACT: DANIEL STUMPF

**LANDSCAPE ARCHITECT:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT LANDSCAPE ARCH.: JEFFERY CREEL

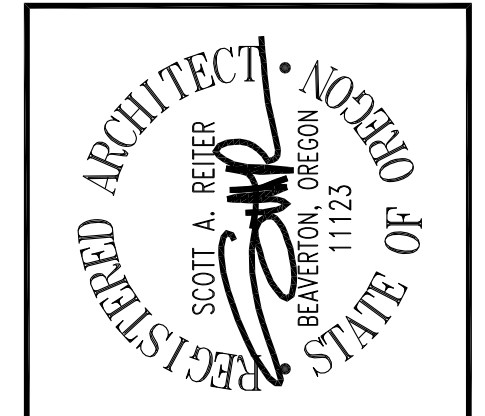
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**VICINITY MAP**



**CONCEPTUAL SITE PLAN** 1  
 Scale: 1" = 30'-0" A1.0



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

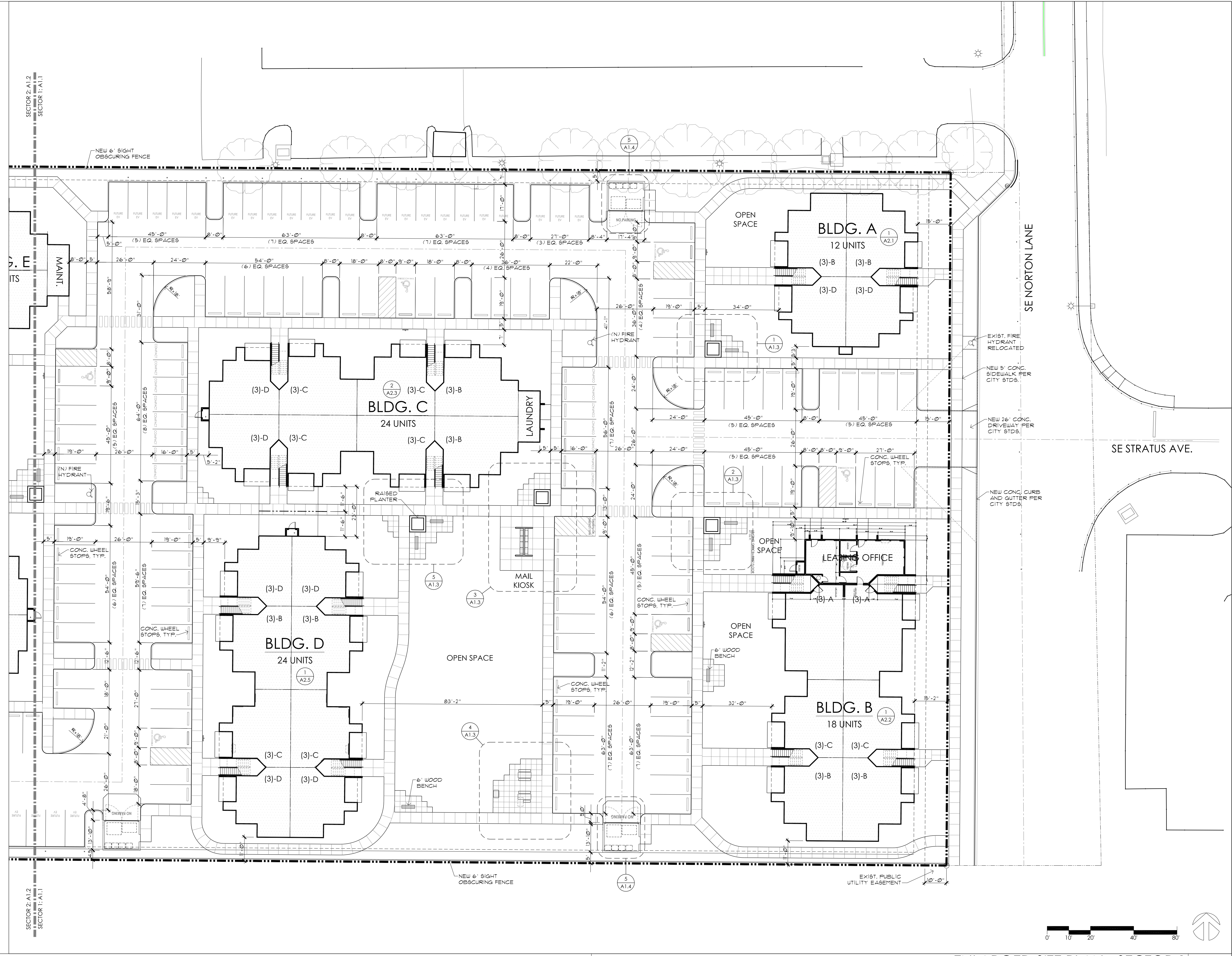
CONCEPTUAL SITE PLAN

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| JULY 19, 2023 | AS NOTED  |
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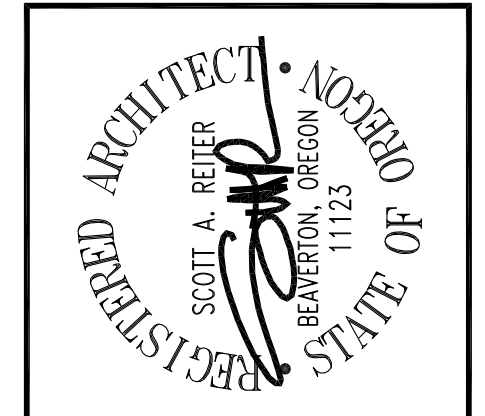




SECTOR 2: A1.2  
SECTOR 1: A1.1

SECTOR 2: A1.2  
SECTOR 1: A1.1

ENLARGED SITE PLAN - SECTOR 1 1  
Scale: 1" = 20'-0" A1.1



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

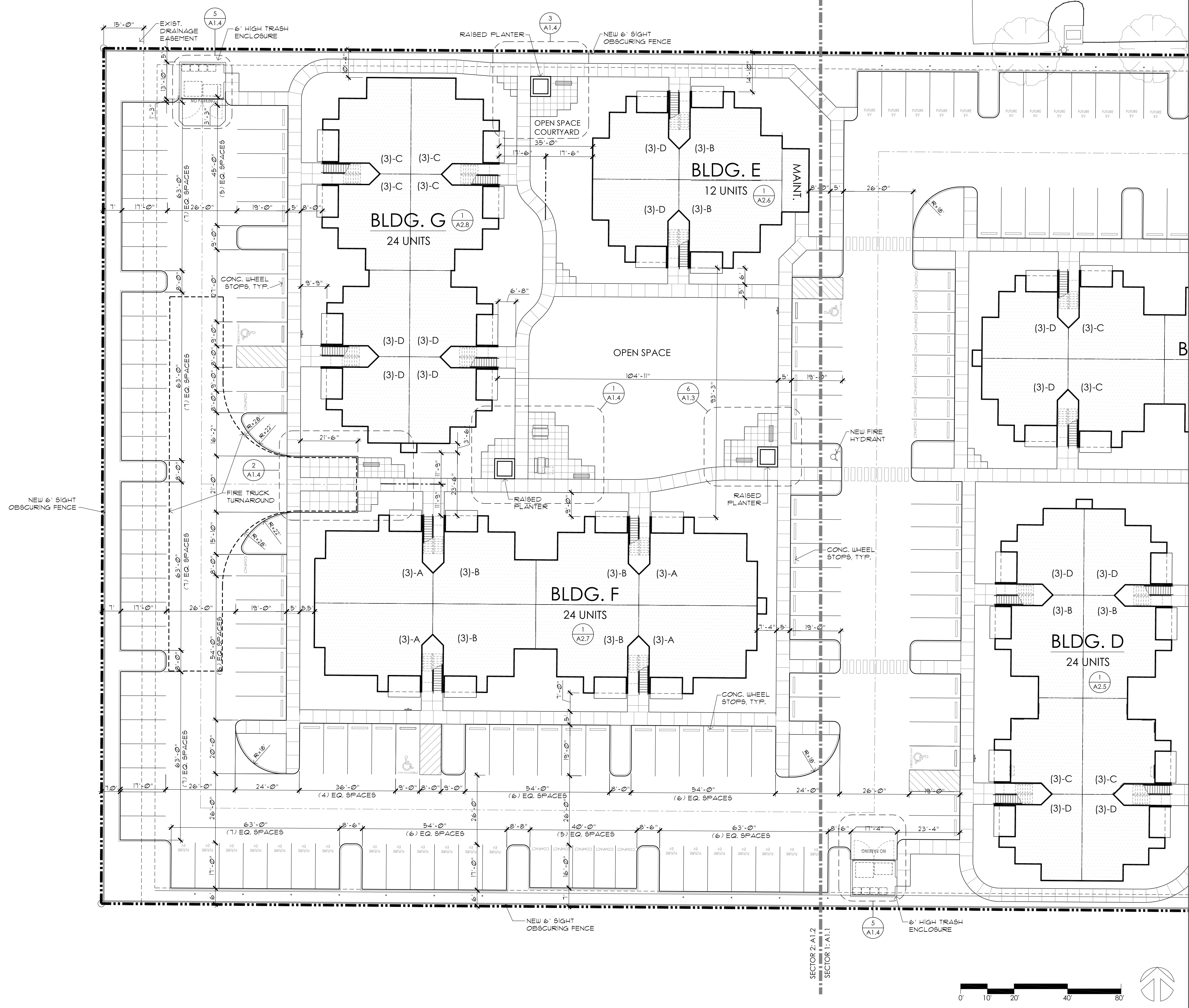
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|---------------|--------------|
| JULY 19, 2023 | AS NOTED     |
|               | DRAWN: 2301  |
|               | JOB NO. 2301 |

LAND-USE REVIEW

**A**  
**1.1**





ENLARGED SITE PLAN - SECTOR 2 1  
 Scale: 1" = 20'-0" A1.2

LAND-USE REVIEW

ENLARGED SITE PLAN - SECTOR 2

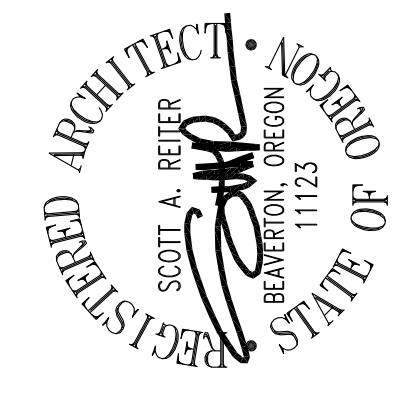
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| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
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| job no.: | 2301          |

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

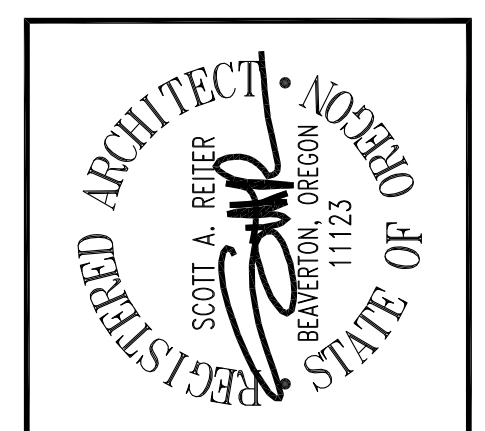
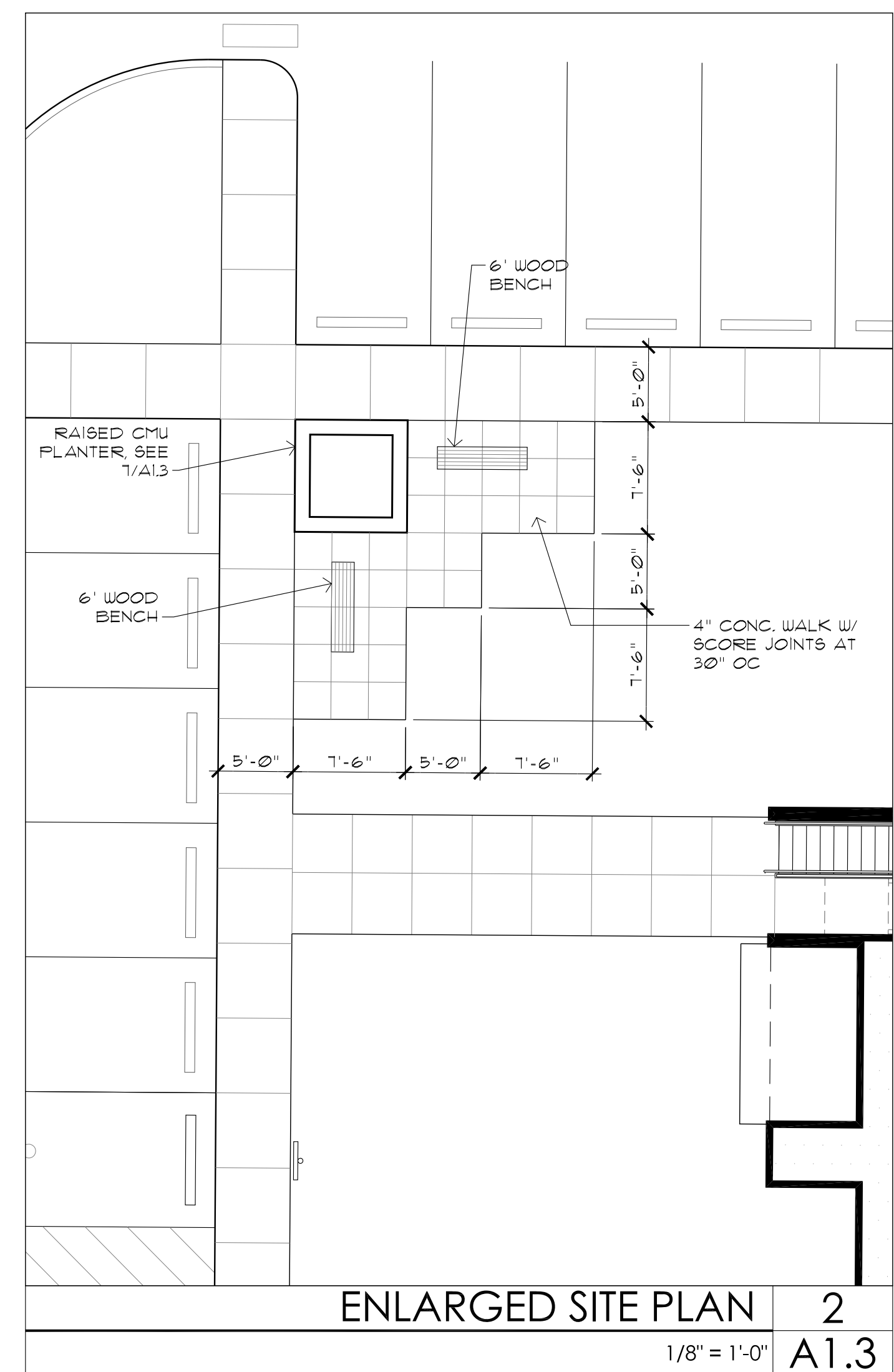
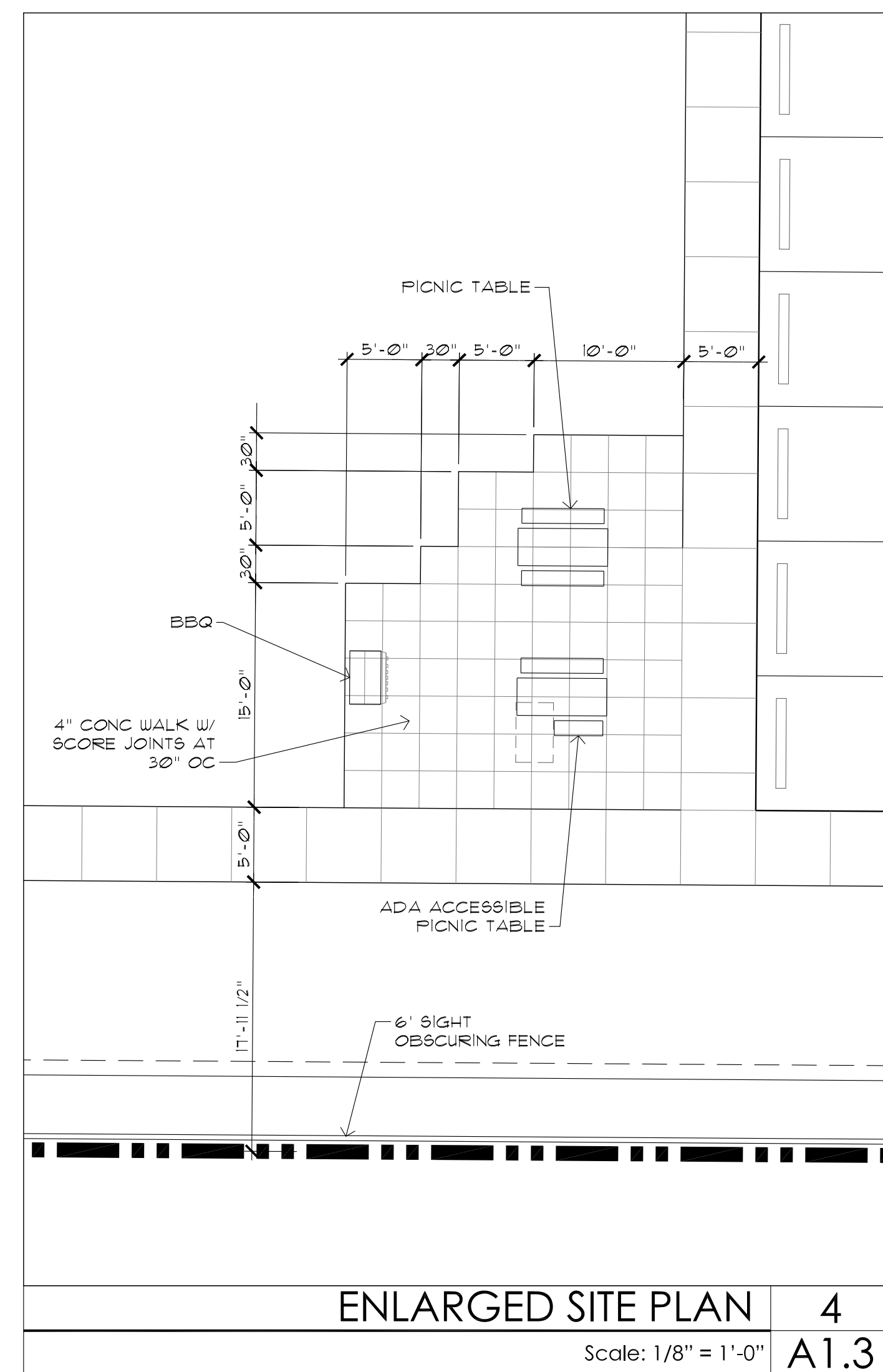
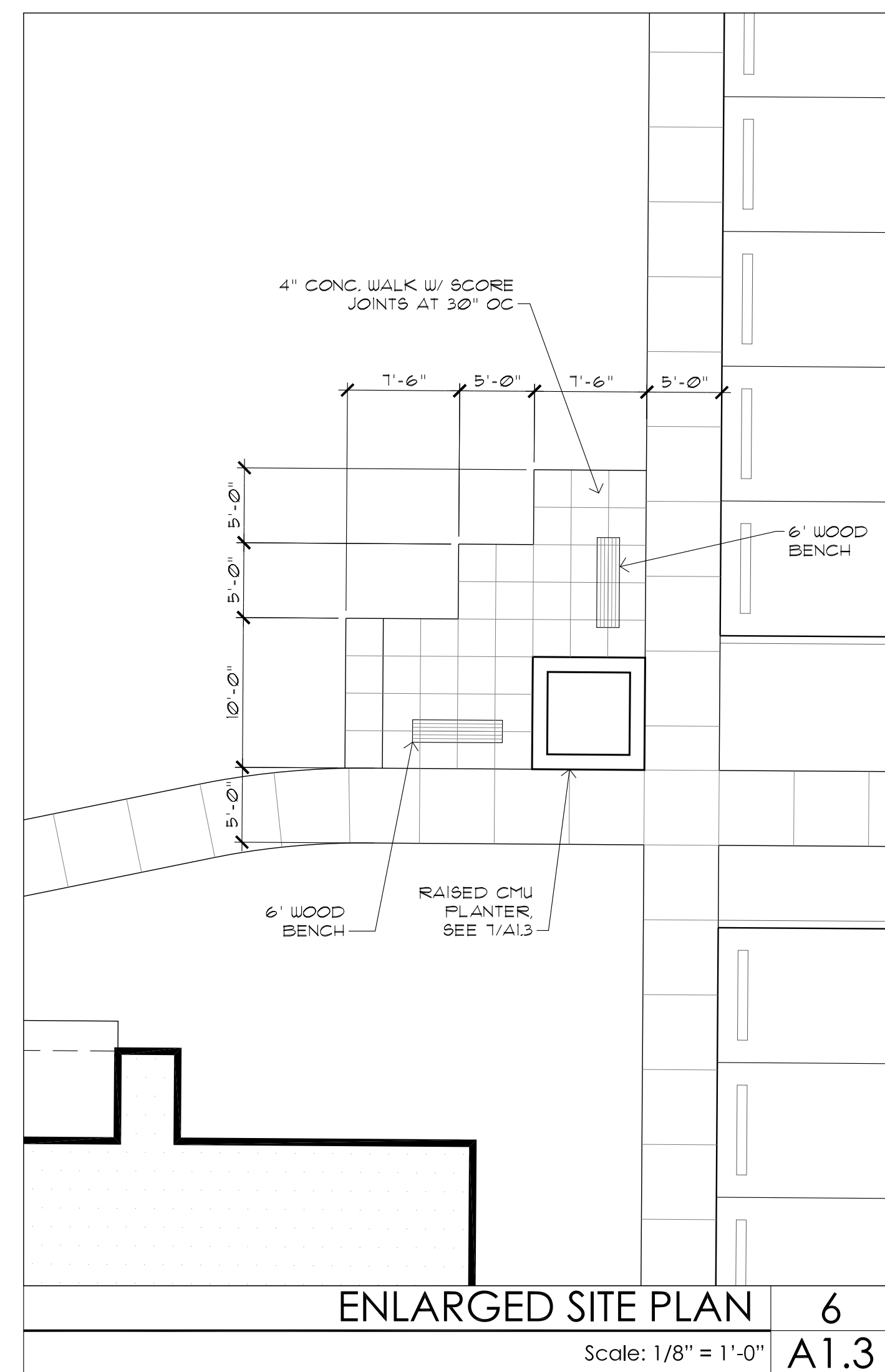
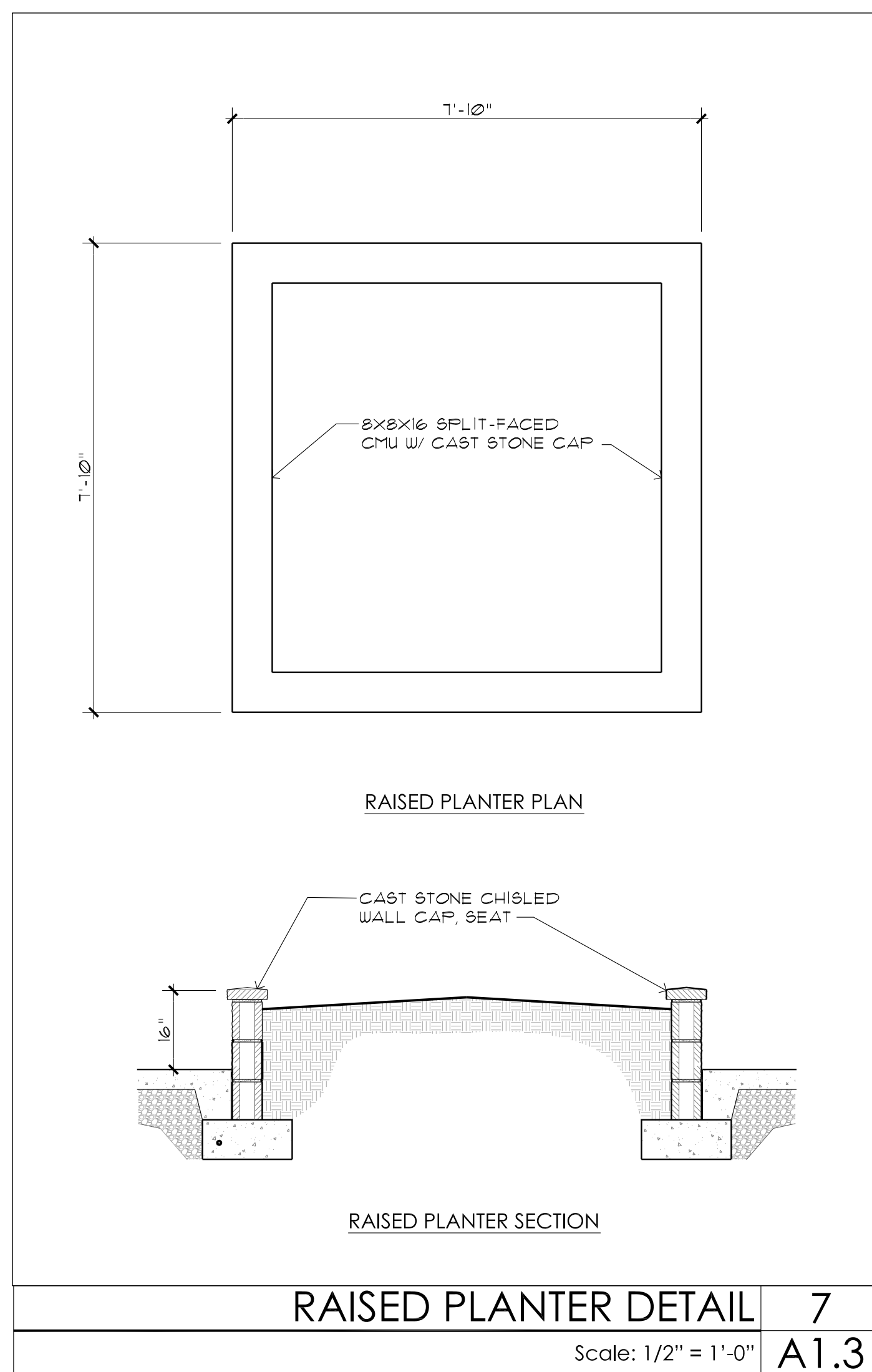
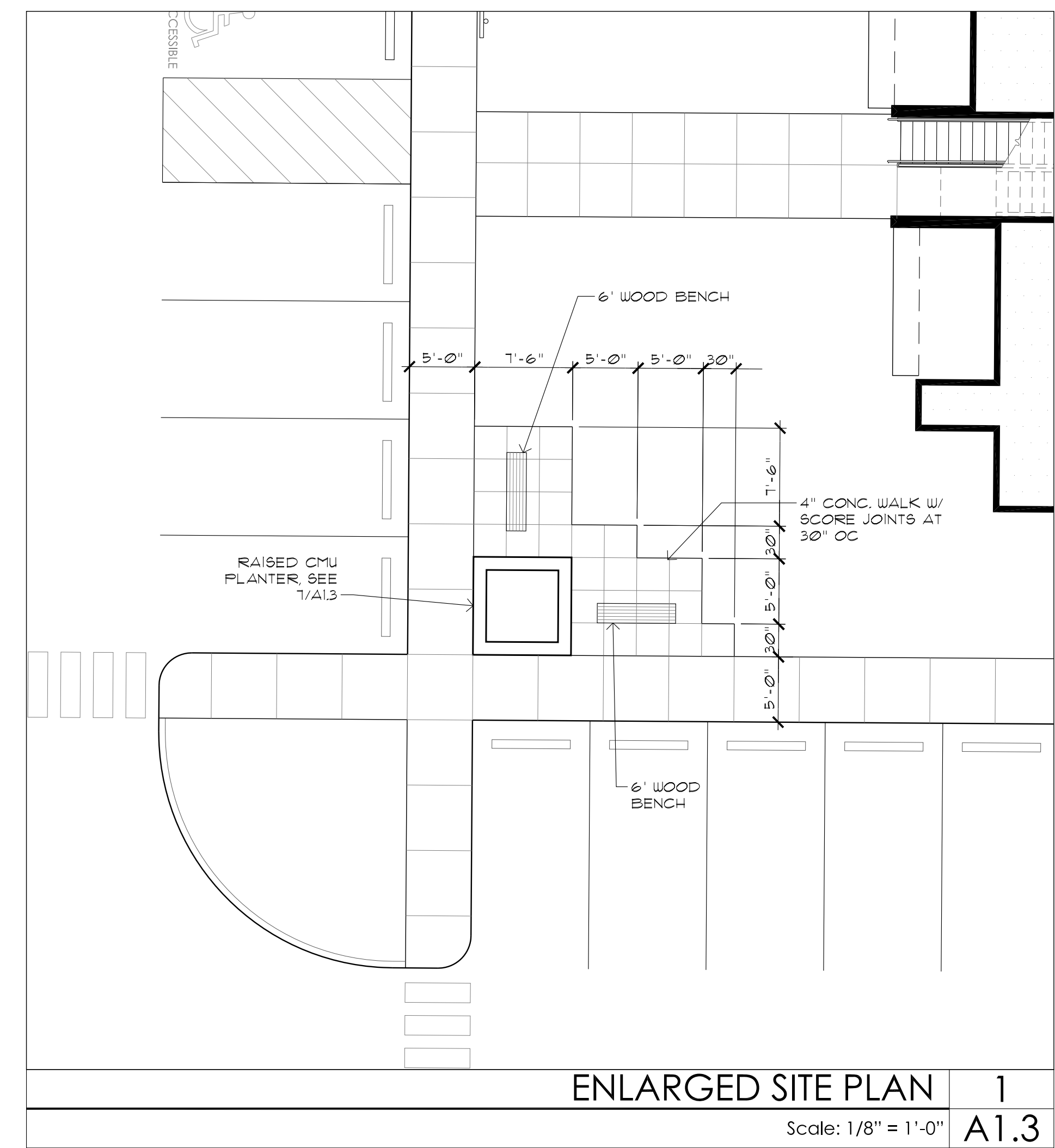
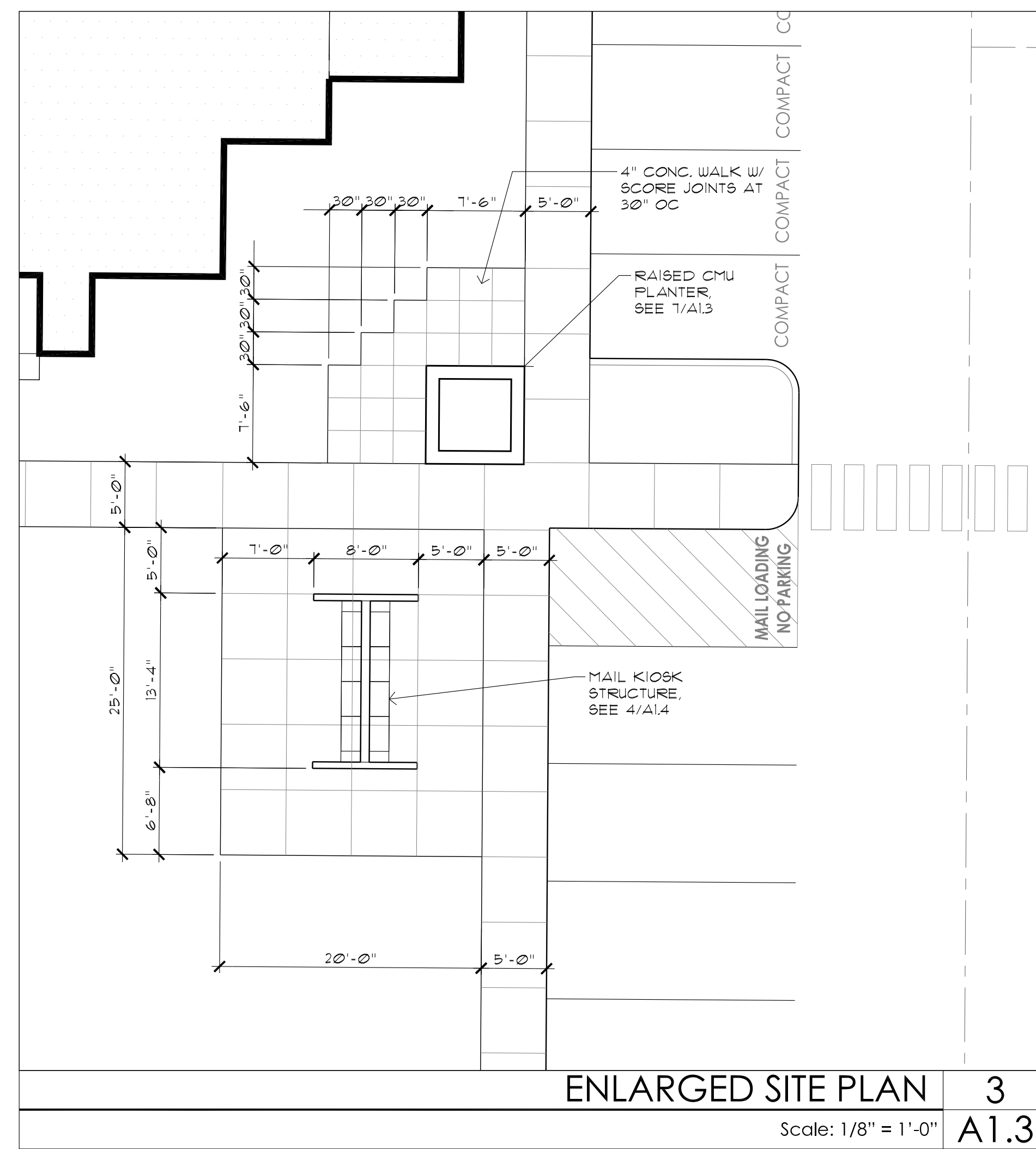
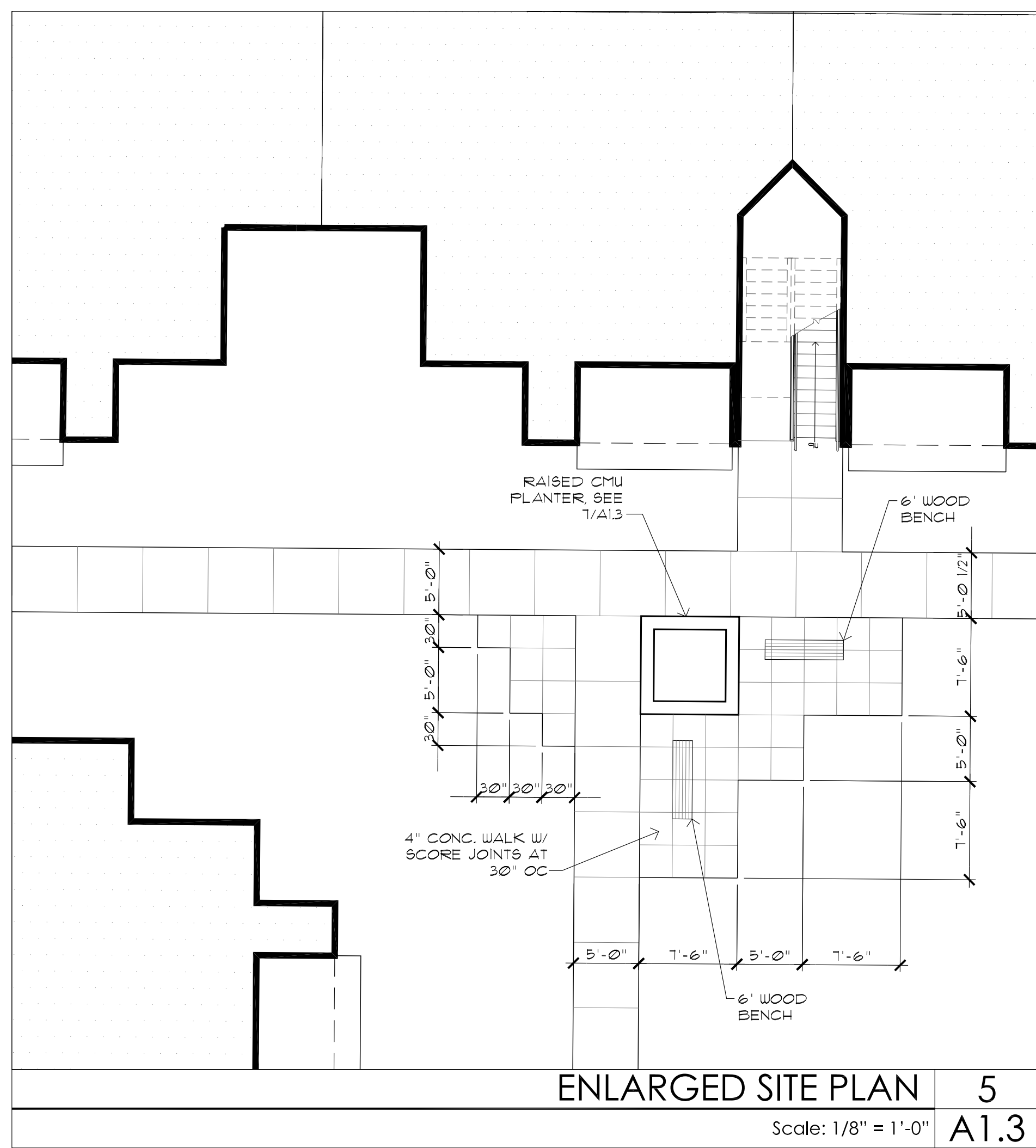
OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED

6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056



**A**  
**1.2**



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

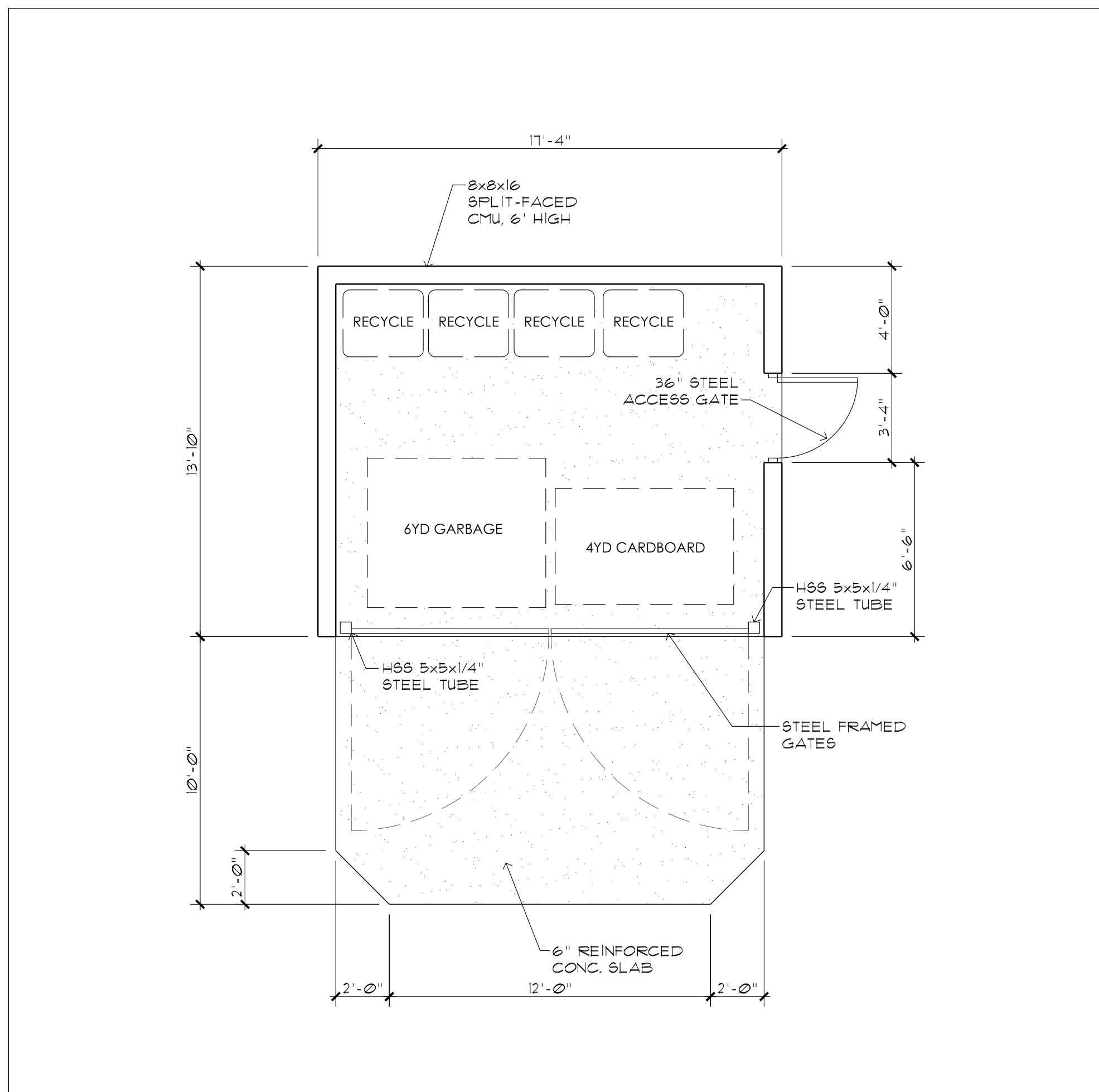
**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLANS

|          |               |
|----------|---------------|
| date:    | JULY 19, 2023 |
| scale:   | AS NOTED      |
| drawn:   |               |
| job no.: | 2301          |

**A**  
**1.3**

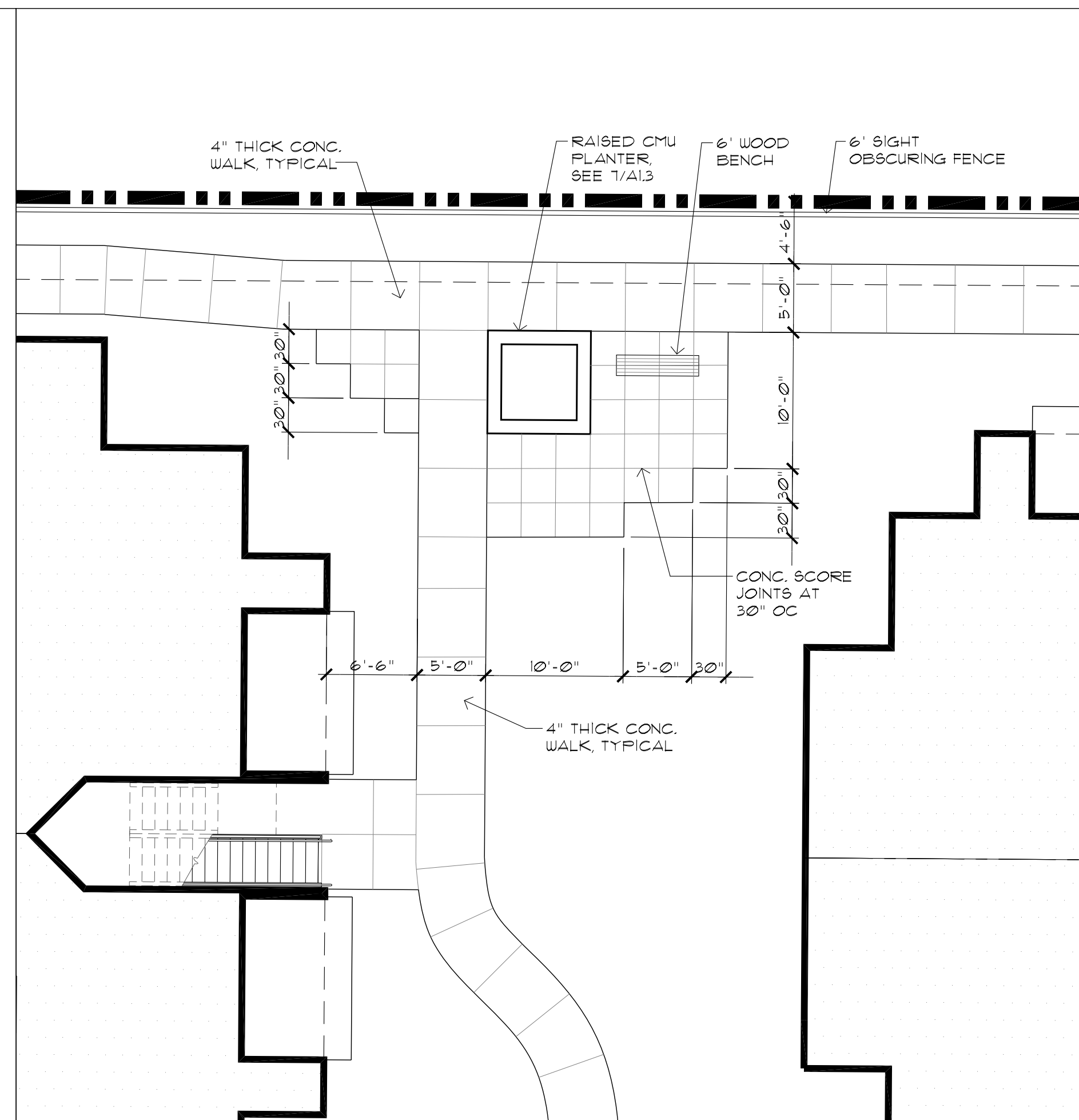




TRASH ENCLOSURE PLAN 5

Scale: 1/4" = 1'-0"

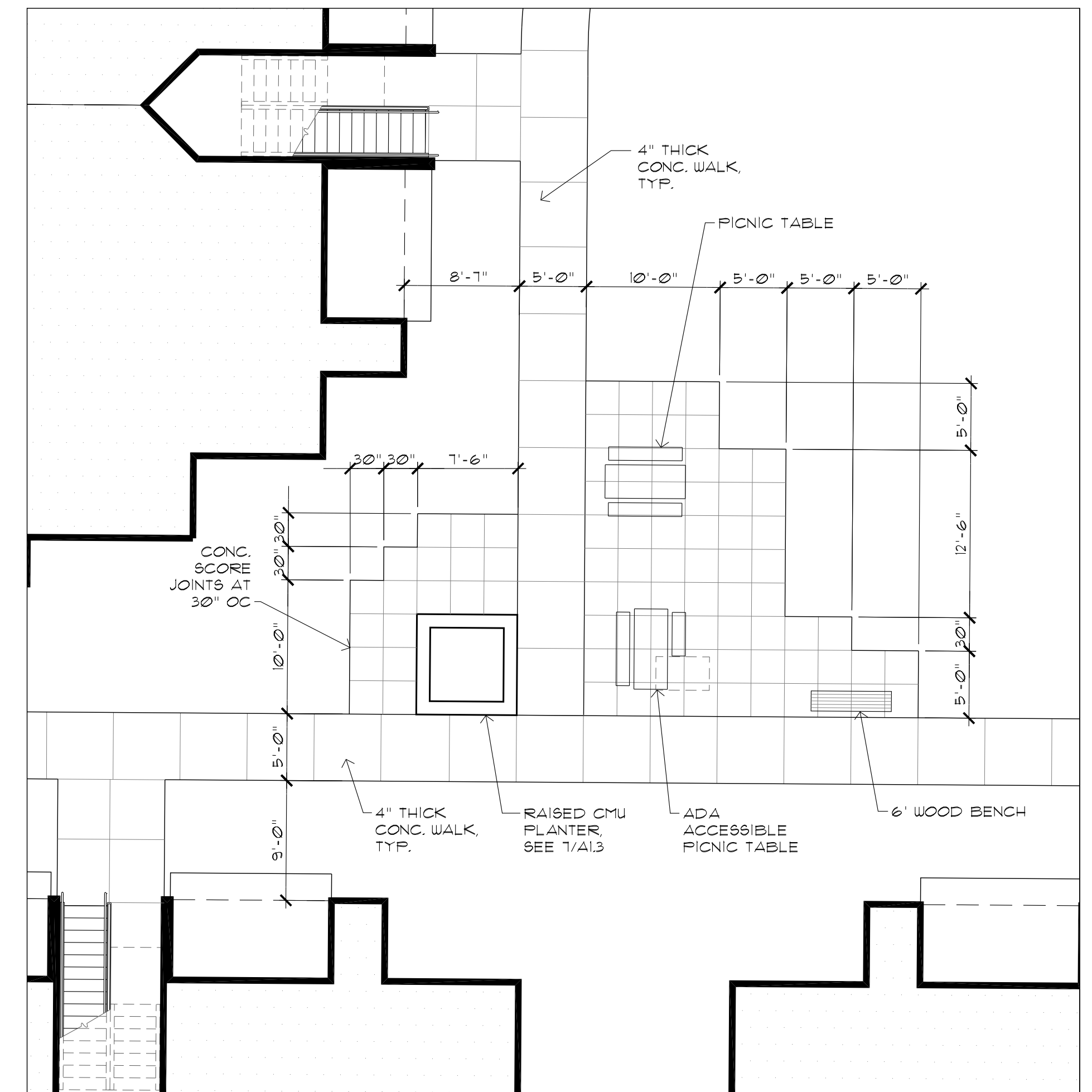
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ENLARGED SITE PLAN 3

Scale: 1/8" = 1'-0"

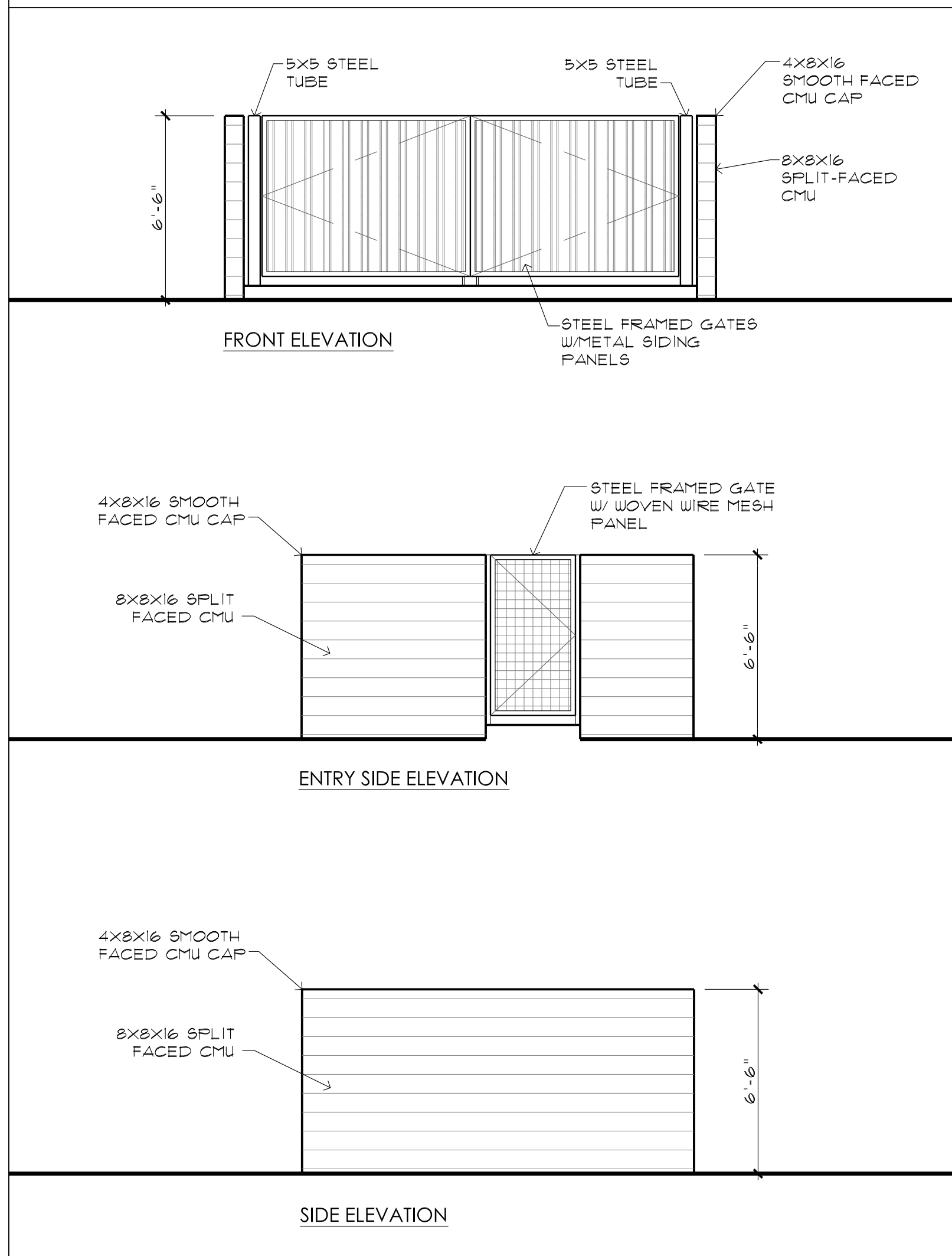
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ENLARGED SITE PLAN 1

Scale: 1/8" = 1'-0"

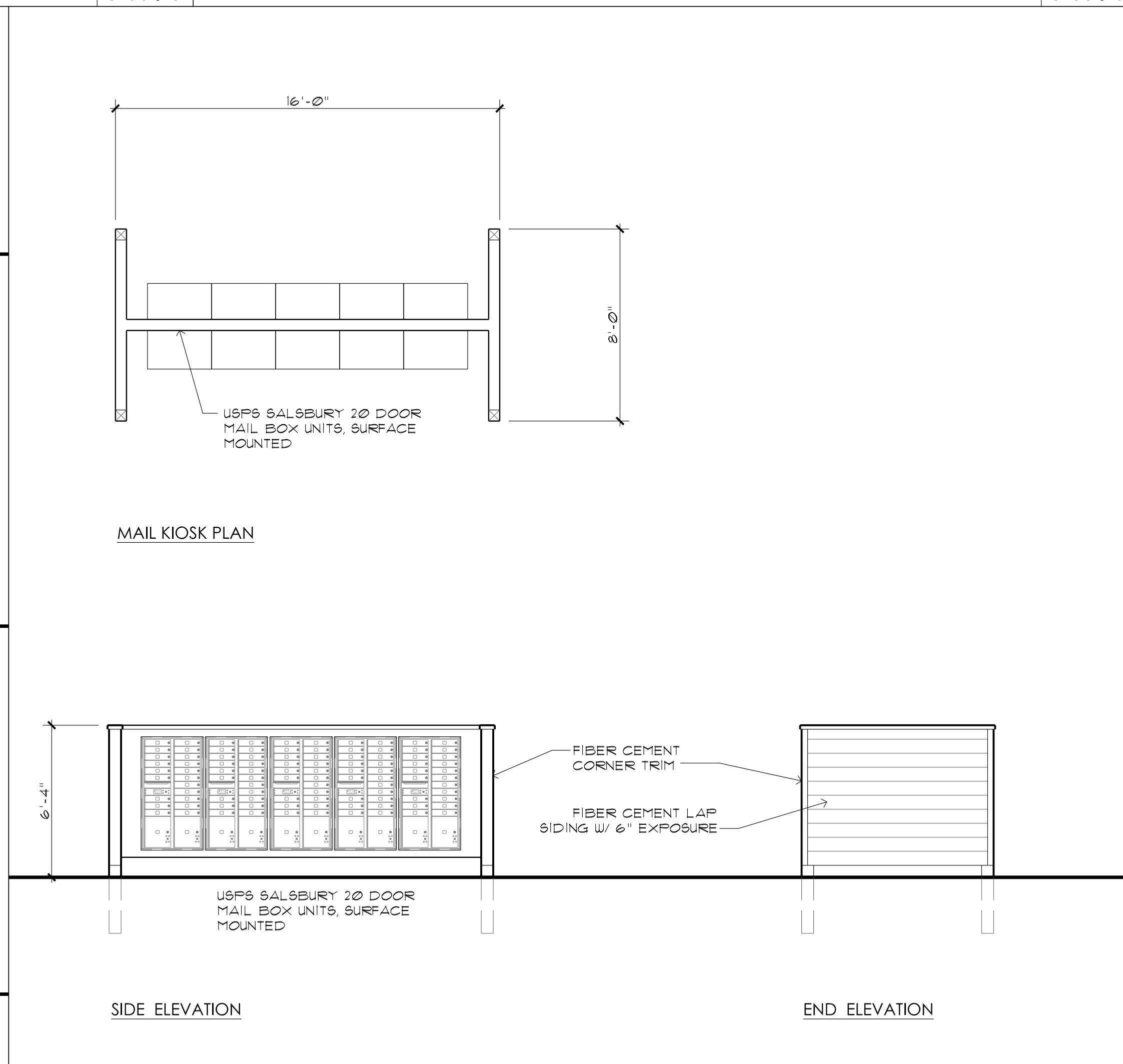
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TRASH ENCLOSURE ELEVATIONS 6

Scale: 1/4" = 1'-0"

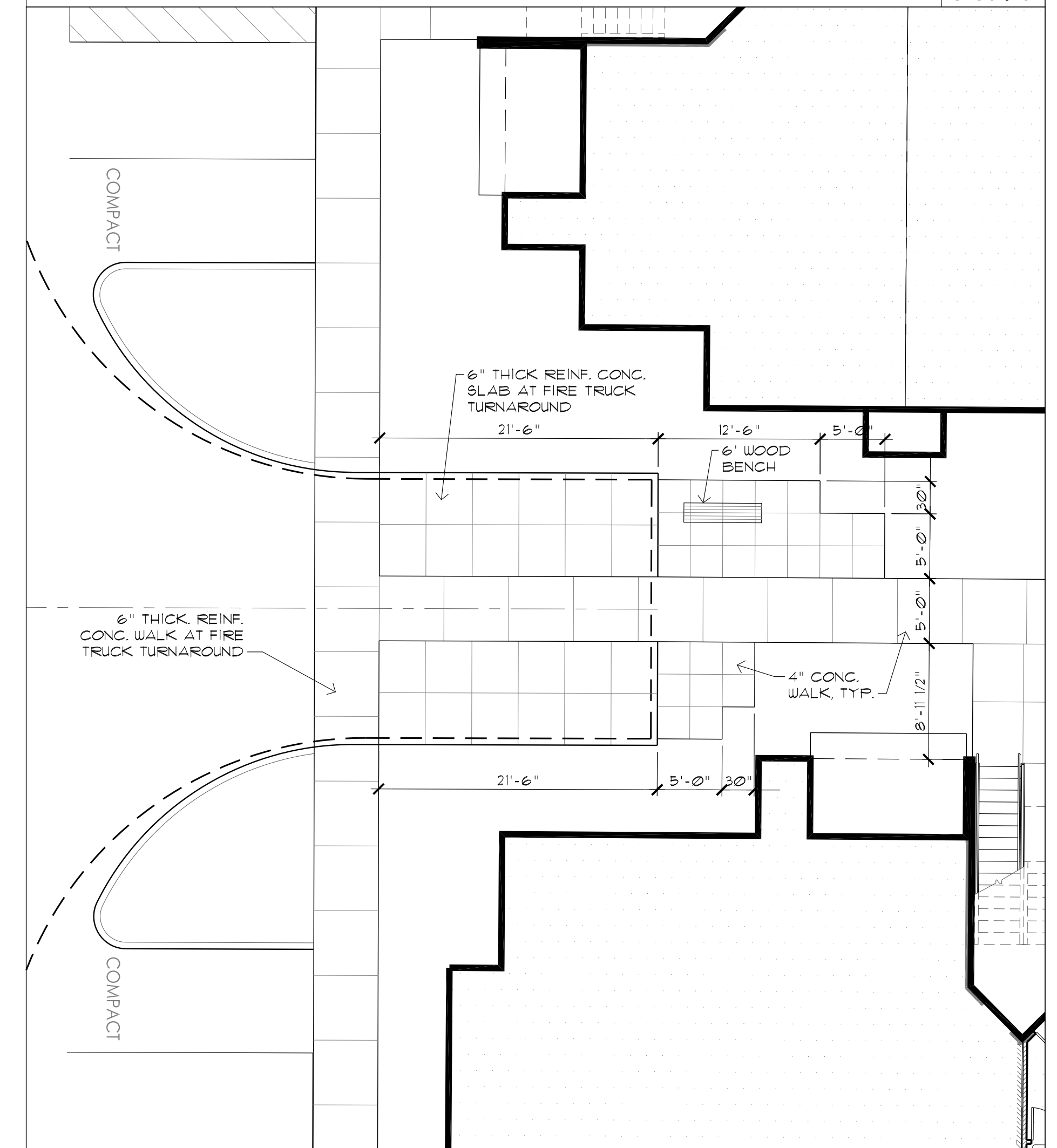
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MAIL KIOSK PLAN, ELEVATIONS 4

Scale: 1/4" = 1'-0"

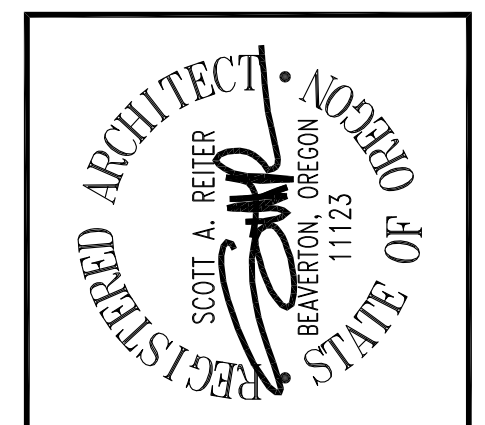
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ENLARGED SITE PLAN 2

Scale: 1/8" = 1'-0"

A1.4



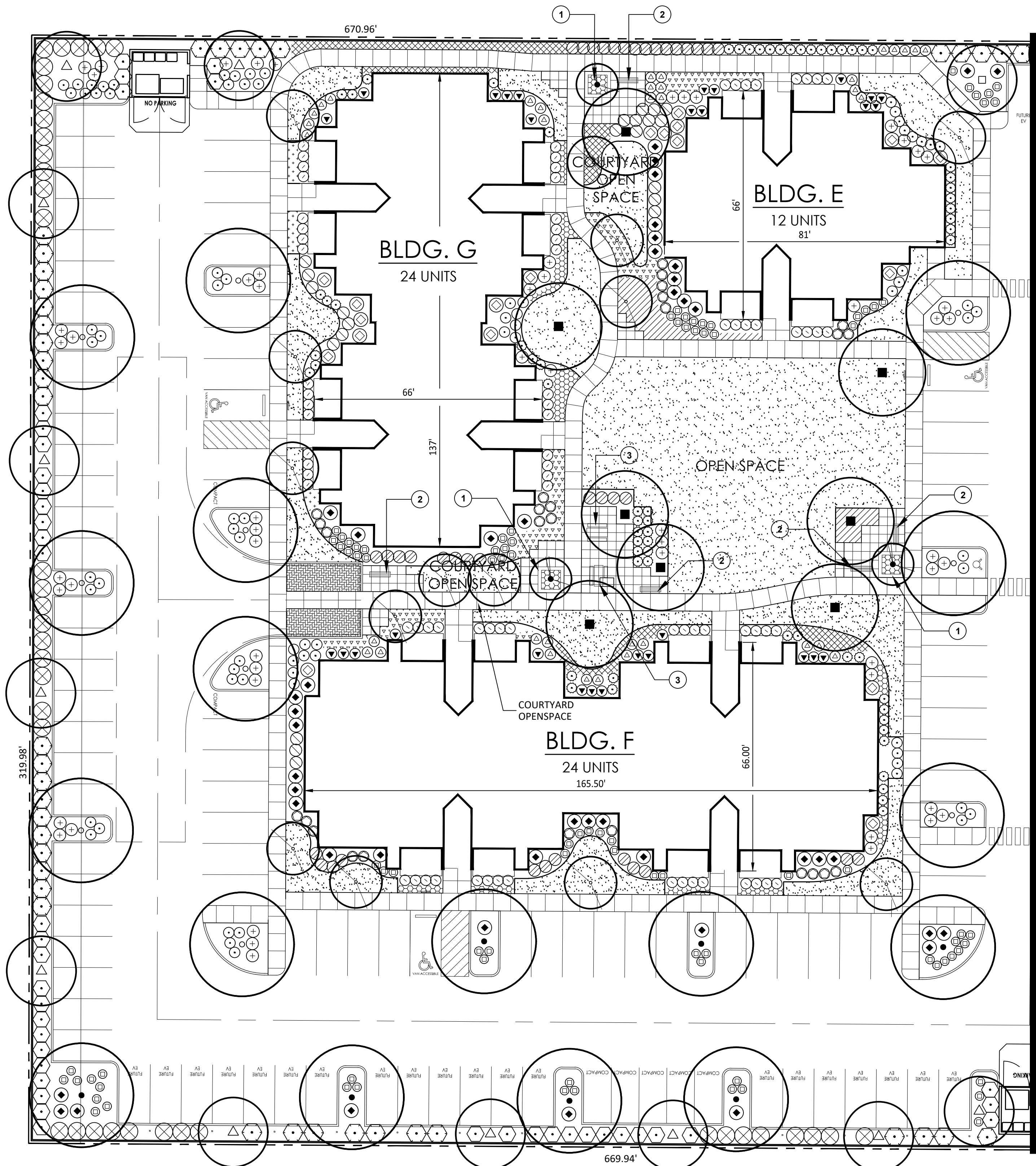
**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLANS  
MAIL KIOSK PLANS, ELEVATIONS  
TRASH ENCLOSURE PLAN, ELEVATIONS

date: JULY 19, 2023  
scale: AS NOTED  
drawn: [blank]  
job no: 2301

**A**  
**1.4**



**LANDSCAPE PLAN**  
1" = 20"

**KEY NOTES**

- 1 RAISED SEAT WALL PLANTER
- 2 BENCH
- 3 PICNIC TABLE

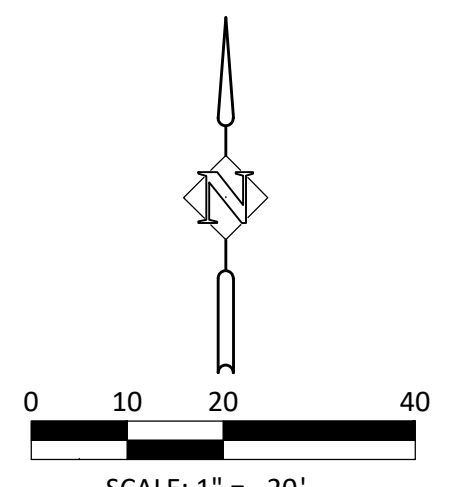
**PLANT SCHEDULE**

| TREES  |   |
|--------|---|
| 14     | <i>Zelkova serrata</i> 'Green Vase' - Green Vase Zelkova<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 40' W                  |
| 16     | <i>Pyrus calleryana</i> 'Aristocrat' - Callery Pear<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 35' H X 25' W                       |
| 4      | <i>Acer griseum</i> - Paperbark Maple<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 25' H X 20' W                                     |
| 16     | <i>Acer rubrum</i> 'Bowhall' - Bowhall Maple<br>2" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 25' H X 15' W                              |
| 7      | <i>Lagerstroemia Plum Magic</i> - Plum Magic Crape Myrtle<br>12" HGT. B&B, WELL BRANCHED, MULTI-TRUNKED<br>MATURE SIZE: 15' H X 15' W               |
| 19     | <i>Carpinus betulus</i> - European Hornbeam<br>X" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 35' W                               |
| 13     | <i>Ginkgo biloba</i> 'Autumn Gold' - Autumn Gold Ginkgo<br>X" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 50' H X 35' W                   |
| 26     | <i>Cornus</i> 'Eddie's White Wonder' - Eddie's White Dogwood<br>X" CAL. B&B, WELL BRANCHED, LIMBED TO 6'<br>MATURE SIZE: 35' H X 20' W              |
| SHRUBS |   |
| 222    | <i>Pennisetum orientale</i> - Fountaingrass<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W                              |
| 102    | <i>Abelia x grandiflora</i> 'Kaleidoscope' - Kaleidoscope Abelia<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 3.5' W     |
| 220    | <i>Helictotrichon sempervirens</i> - Blue Oatgrass<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 2.5' W                   |
| 67     | <i>Gardenia jasminoides</i> 'Frostproof' - Frostproof Gardenia<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 4' H X 3' W           |
| 56     | <i>Hydrangea macrophylla</i> 'Mini Penny' - Mini Penny Hydrangea<br>x GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 3.5' W     |
| 66     | <i>Deutzia gracilis</i> 'Nikko' - Dwarf Nikko Deutzia<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2' H X 3' W                    |
| 74     | <i>Berberis thunbergii</i> 'Monomb' - Cherry Bomb Japanese Barberry<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 4' W      |
| 123    | <i>Escallonia hybrid</i> 'Kompakta' - Compact Escallonia<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 2.5' H X 4' W               |
| 104    | <i>Spiraea x bumalda</i> 'Goldflame' - Goldflame Spirea<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3.5' H X 3.5' W              |
| 85     | <i>Thuja occidentalis</i> 'Golden Globe' - Golden Globe Arborvitae<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 4' H X 4' W       |
| 68     | <i>Choisya ternata</i> 'Sundance' - Sundance Orange Blossom<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W              |
| 61     | <i>Abelia gradiflora</i> - Glossy Abelia<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W                                 |
| 61     | <i>Viburnum tinus</i> 'Spring Bouquet' - Laurustinus<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 6' H X 5' W (PRUNNED)           |
| 118    | <i>Nandina domestica</i> 'Gulf Stream' - Gulf Stream Nandina<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W             |
| 185    | <i>Prunus laurocerasus</i> 'Otto Luyken' - Dwarf Cherry Laurel<br>5 GAL. CONT., FULL PLANTS, SPACING AS SHOWN<br>MATURE SIZE: 3' H X 3' W (PRUNNED) |

| GROUNDCOVER                            |  |
|--|--|
| 1 GAL.                                 | <i>Festuca glauca</i> 'Elijah Blue' - Blue Fescue<br>CONT., FULL PLANTS, 12" O.C.                            |
| 1 GAL.                                 | <i>Liriope muscari</i> 'Big Blue' - Big Blue Lillyturf<br>CONT., FULL PLANTS, 18" O.C.                       |
| 1 GAL.                                 | <i>Pennisetum alopecuroides</i> 'Little Bunny' - Little Bunny Fountain Grass<br>CONT., FULL PLANTS, 12" O.C. |
| 1 GAL.                                 | <i>Carex elata</i> 'Aurea' - Bowles Golden Sedge<br>CONT., FULL PLANTS, 30" O.C.                             |
| 1 GAL.                                 | <i>Carex oshimensis</i> 'Carfit01' - Everest Variegated Sedge<br>CONT., FULL PLANTS, 24" O.C.                |
| SEED MIXES                             |  |
| SEED MIX 1 (LAWN AREAS)                | % PLS LBS OF PLA/1000 SF   |
| <i>Festuca rubra</i> 'Gibraltar'       | 10 0.364   |
| <i>Festuca rubra</i> 'Silhouette'      | 10 0.364   |
| <i>Lolium perenne</i> 'Delaware Dwarf' | 40 5.563   |
| <i>Lolium perenne</i> 'Amazing'        | 40 2.696   |
| <b>TOTAL</b>                           | <b>8.987</b>   |

**GENERAL PLANTING NOTES**

- ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT CITY OF McMINNVILLE STANDARDS AND OREGON BUILDING AND SPECIALTY CODES.
- INSTALL EROSION CONTROL SYSTEMS IN ACCORDANCE WITH CITY OF McMINNVILLE STANDARDS PRIOR TO SITE WORK AND LANDSCAPE INSTALLATION.
- CONTRACTOR SHALL MARK AND PROTECT ALL UTILITIES, SITE FEATURES, AND VEGETATION TO REMAIN IN PLACE.
- CONTRACTOR SHALL REMOVE ALL WEEDS AND INVASIVE SPECIES PRIOR TO PLANTING OR SEEDING.
- ALL DISTURBED AREAS SHALL BE SEEDED.
- PRIOR TO PLANTING, CONTRACTOR SHALL TEST ON-SITE SOILS FOR SOIL FERTILITY BY CERTIFIED TESTING LAB. IF NECESSARY, BACKFILL SOILS FOR TREE PITS, SHRUB AND GROUNDCOVER AREAS SHALL BE AMENDED AS RECOMMENDED BY SOIL ANALYSIS REPORT.
- ALL SEEDED AREAS SHALL BE STRIPPED OF VEGETATION, SCARIFIED AND RECEIVE 6" OF TOPSOIL PRIOR TO APPLICATION OF SEED.
- ALL PLANTER BEDS SHALL BE SCARIFIED 12" BELOW FINISHED GRADE AND HAVE 12" OF TOPSOIL ADDED TO BRING BACK TO FINISHED GRADE PRIOR TO PLANTING.
- CONTRACTOR TO INSTALL 3" LAYER OF BARK MULCH AT ALL TREE, SHRUB AND GROUNDCOVER AREAS.
- LANDSCAPE INSTALLATION SHALL INCLUDE PROVISION OF AN AUTOMATIC IRRIGATION SYSTEM TO SUSTAIN LANDSCAPE PLANTINGS, MEETING LOCAL AND STATE BUILDING CODES.
- PLANT MATERIAL INSTALLED SHALL CONFORM IN SIZE AND GRADE TO THE "AMERICAN STANDARD FOR NURSERY STOCK" CURRENT EDITION.
- QUANTITIES OF PLANT MATERIALS SHALL BE AS DETERMINED BY CONTRACTOR IN ACCORDANCE WITH SPECIFIED SPACING OR LOCATION ON PLAN. MATERIAL QUANTITIES SHOWN ON PLAN ARE FOR CONTRACTOR CONVENIENCE ONLY AND SHALL BE VERIFIED BY CONTRACTOR PRIOR TO INSTALLATION. SURPLUS OR SHORTAGES OF PLANT QUANTITIES SHALL BE RESPONSIBILITY OF CONTRACTOR.
- LANDSCAPE CONTRACTOR SHALL WATER PLANTINGS FOR DURATION OF 1-YEAR WARRANTY PERIOD AFTER INSTALLATION AND GUARANTEE ALL PLANTINGS TO BE IN SATISFACTORY HEALTH. LANDSCAPE CONTRACTOR SHALL REPLACE ALL DAMAGED, DEAD, OR DYING PLANTS COVERED BY WARRANTY WITHIN 30 DAYS OF INITIAL IDENTIFICATION OF CONDITION.



**LANDSCAPE PLAN  
NORTON LANDING  
McMINNVILLE, OREGON**

**Harper Houf Peterson Righellis Inc.**  
ENGINEERS & PLANNERS  
LANDSCAPE ARCHITECTS & SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhpr.com

REGISTERED 747  
**PRELIMINARY**  
JEFFERY P. CREEL  
OREGON  
06/13/11  
LANDSCAPE ARCHITECT

|           |            |
|-----------|------------|
| DESIGNED: | MD/JLV     |
| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 07.21.2023 |

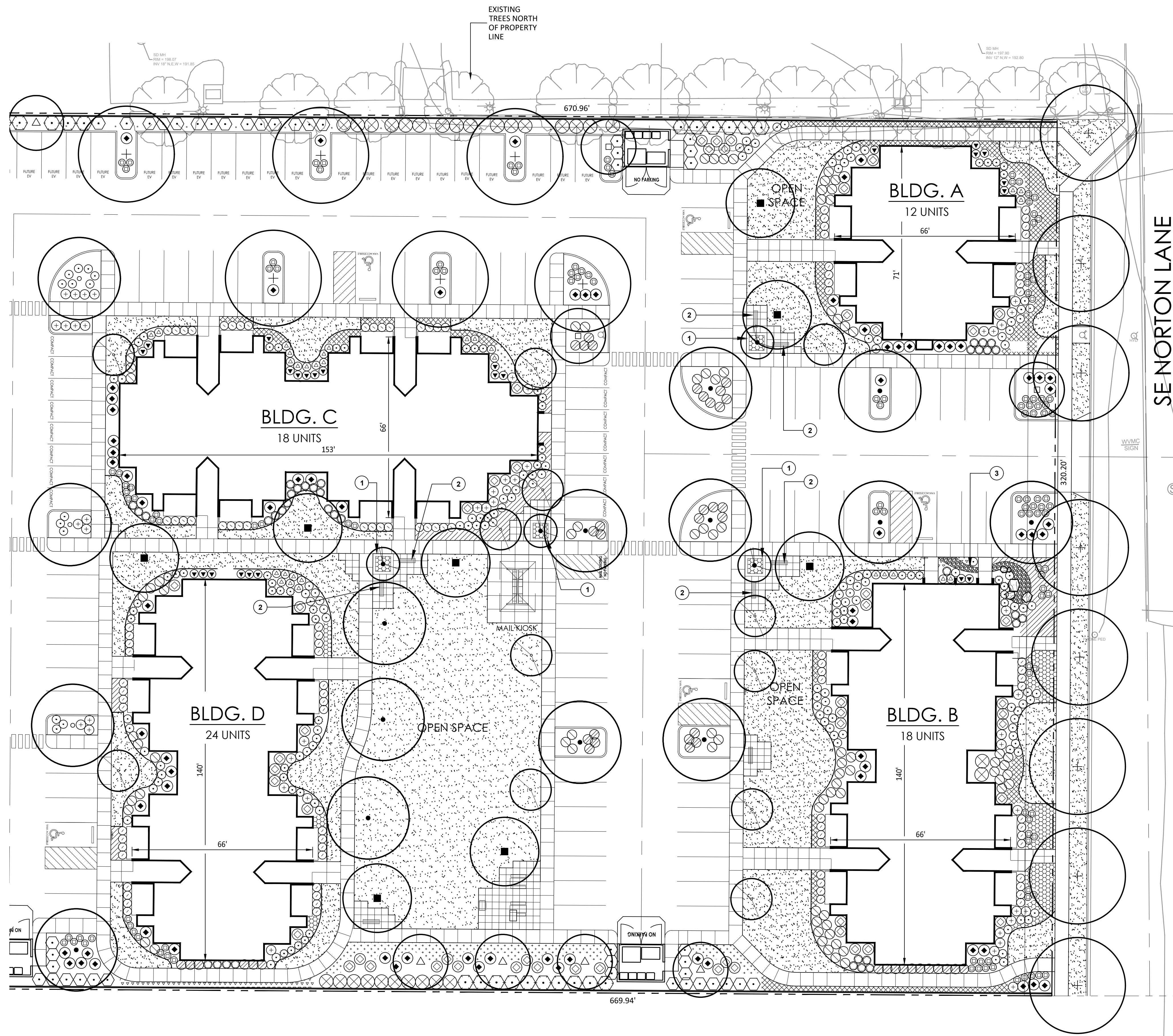
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SHEET NO. **L1.1**  
JOB NO. KWD-11

**LAND USE**



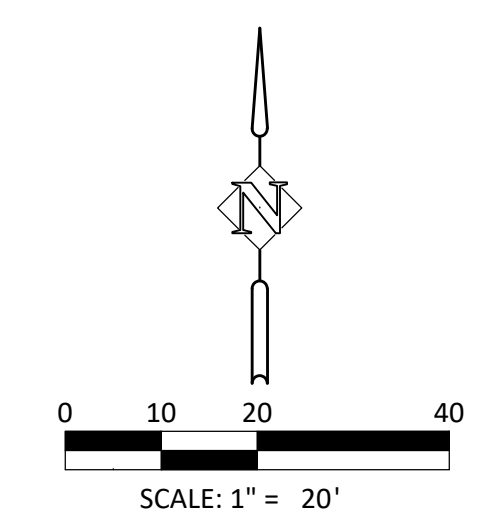
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**LANDSCAPE PLAN**  
1" = 20"

| CITY OF MCMINNVILLE CODE COMPLIANCE |                              |   |   |   |
|-------------------------------------|------------------------------|---|---|---|
| SECTION #                           | SECTION                      | DESCRIPTION   | REQUIRED  | PROVIDED  |
| 17.11.090(D)                        | LARGE SITE DESIGN STANDARDS  | BUFFER PARKING AREAS WITH LANDSCAPING.  |   | LANDSCAPING AROUND PAVING LOT                     |
|                                     | LARGE SITE DESIGN STANDARDS  | PROVIDE DIFFERENT TYPE OF OPEN SPACE THROUGHOUT THE SITE  |   | DIFFERENT TYPES OF OPEN SPACE PROVIDED            |
|                                     | PARKING LOT DESIGN STANDARDS | >50,000 SF PARKING LOTS = 8% INTERIOR LANDSCAPING REQUIRED.   | PARKING LOT AREA = 75,075 SF X 8% = 6,006 SF        | 6,681 SF INT. PL. LANDSCAPING                     |
|                                     | PARKING LOT DESIGN STANDARDS | 1 (NON-COLUMNAR) TREE PER ISLAND  | 37 PARKING LOT ISLANDS                              | 37 TREES  |
|                                     | PARKING LOT DESIGN STANDARDS | FENCE REQUIRED NEXT TO RESIDENTIAL ZONING   | FENCE ON WEST PL                                    | FENCE PROVIDED. SEE ARCHITECTURAL SITE PLANS      |
|                                     | PARKING LOT DESIGN STANDARDS | BUFFER BETWEEN PL AND BUILDINGS   | SW OR LANDSCAPE STRIP                               | 5' SW & LANDSCAPE STRIPS PROVIDED                 |
|                                     | PARKING LOT DESIGN STANDARDS | PLANTINGS ADJACENT TO SW (BETWEEN PL AND BLDGS)   | 6' LANDSCAPE STRIP NEXT TO SW                       | 6' PLANTING STRIP                                 |
|                                     | PARKING LOT DESIGN STANDARDS | TREES ALONG THE THROUGH SIDEWALK-WALKWAY CONNECTIONS  |   | TREES PROVIDED ALONG SIDEWALK CONNECTIONS         |
|                                     | OPEN SPACE                   | PROVIDE DECIDUOUS TREE FOR SHADE  | DEC. TREES IN OPEN SPACES                           | STEWARTIA PROVIDED                                |
|                                     | LANDSCAPING                  | A MIN. OF 20% OF THE SITE SHALL BE LANDSCAPED   | TOTAL SITE 214,606 SF X 20% = 42,921 SF             | 64,157 SF PROVIDED (30%)                          |
| 17.11.090(D). 10                    | FRONT YARD SETBACK           | FRONT YARD SETBACK BETWEEN ENTRANCE AND STREET GATEWAY ZONE   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
| 17.11.090(D). 6.B(3)                | PRIVATE OPEN SPACE           | PRIVATE OUTDOOR SPACE AT GROUND LEVEL REQUIRES FRONT YARD   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
|                                     | OPEN SPACE                   | COMMON OPEN SPACE 15% MIN.  | TOTAL SITE 214,606 SF X 15% = 32,191 SF             | 32,715 SF PROVIDED                                |
|                                     | OPEN SPACE                   | PASSIVE OPEN SPACE NO MORE THAN 5% OF SITE  | TOTAL SITE 214,606 SF X 5% = 10,730 SF MAX. PASSIVE | 21,985 SF ACTIVE OPEN SPACE                       |
| 17.57.070                           | EXISTING TREES               | EXISTING LOCATIONS OF TREES OVER 6" DBH   |   | NO EXISTING TREES ON SITE                         |
| 17.57.070                           | LANDSCAPE SCREENING          | SCREENING TO PROPOSED USE BY SIGHT OBSCURING, EVERGREEN PLANTINGS, SHADE TREES, FENCES OR COMBINATION |   | SIGHT OBSCURING FENCE & SCREENING SHRUBS PROVIDED |
| 17.57.090                           | STREET TREES                 | STREET TREES OVER 40' TALL AND WIDE   | 40' OC  | LARGE STREET TREES PROVIDED. SEE PLANT SCHEDULE   |

- KEY NOTES**
- 1 RAISED SEAT WALL PLANTER
  - 2 BENCH



SEE L1.1 FOR PLANTING SCHEDULE

**LAND USE**

**LANDSCAPE PLAN**  
**NORTON LANDING**  
MCMINNVILLE, OREGON

**Harper Houf Peterson Righellis Inc.**  
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**PRELIMINARY**  
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05/13/11  
LANDSCAPE ARCHITECT

|           |            |
|-----------|------------|
| DESIGNED: | MD/JLV     |
| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 07.21.2023 |

| DATE | NO. | DESCRIPTION |
|------|-----|-------------|
|      |     |             |
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SHEET NO.  
**L1.2**  
JOB NO. KWD-11

**Planning Department**

231 NE Fifth Street ◦ McMinnville, OR 97128

(503) 434-7311 Office ◦ (503) 474-4955 Fax

[www.ci.mcminnville.or.us](http://www.ci.mcminnville.or.us)**Office Use Only:**File No. VR 3-23Date Received 8/14/23Fee \$1,190.00Receipt No. 208598Received by AW

569-23-000460-PLNG

## Administrative Variance Application

### Applicant Information

**Applicant is:**  Property Owner  Contract Buyer  Option Holder  Agent  Other Architect/Planner

Applicant Name Reiter Design Architect Incorporated Phone (503) 574-3036Contact Name Scott Retier Phone (503) 789-6461  
(If different than above)Address 6107 SW Murray Blvd., #480City, State, Zip Beaverton, OR. 97008Contact Email ScottRDG@aol.com

### Property Owner Information

Property Owner Name KWDS, LLC Phone (503) 781-5686  
(If different than above)Contact Name Chad Juranek Phone \_\_\_\_\_Address PO Box 145City, State, Zip Wilsonville, OR. 97070Contact Email Cjuranek@jkmanage.com

### Site Location and Description

(If metes and bounds description, indicate on separate sheet)

Property Address SE Norton Lane at SE Stratus Ave.Assessor Map No. R4 4 - 4 - 27 Total Site Area 214,759sf, 4.93 acresSubdivision \_\_\_\_\_ Block \_\_\_\_\_ Lot 701Comprehensive Plan Designation Commercial Zoning Designation C-3 General Commercial

Please indicate the type of administrative variance requested:

- Lot Size Requirement \_\_\_\_\_ Reduction to \_\_\_\_\_
- Road Frontage Requirement \_\_\_\_\_ Reduction to \_\_\_\_\_
- Setback--front, rear, side Requirement \_\_\_\_\_ Reduction to \_\_\_\_\_
- Other \_\_\_\_\_ Requirement \_\_\_\_\_ Reduction to \_\_\_\_\_

1. Describe, in detail, the nature of the request. \_\_\_\_\_  
See attached Narrative

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2. Is the variance request for this property due to unique circumstances (i.e. shape or topography of the site)? If yes, please describe nature of circumstance. \_\_\_\_\_  
See Attached Narrative

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3. What exceptional or extraordinary circumstances apply to the property which do not apply generally to other property in the same zone or vicinity. \_\_\_\_\_  
See Attached Narrative

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4. What property right would be preserved substantially the same as owners of the other property in the same zone or vicinity, by granting the variance. \_\_\_\_\_  
See Attached Narrative

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5. What unnecessary hardship would be avoided by granting the variance. \_\_\_\_\_  
See Attached Narrative

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

6. Why won't this request be materially detrimental to the surrounding area. \_\_\_\_\_  
See Attached Narrative

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_


In addition to this completed application, the applicant must provide the following:

- A site plan (drawn to scale, with a north arrow, legible, and of a reproducible size), indicating existing and proposed buildings, dimensions, adjacent street(s), and distances from property lines, and any other information that would help substantiate or clarify your request.
- Payment of the applicable review fee, which can be found on the Planning Department web page.

**I certify the statements contained herein, along with the evidence submitted, are in all respects true and are correct to the best of my knowledge and belief.**

  
\_\_\_\_\_  
Applicant's Signature

8/10/23  
\_\_\_\_\_  
Date

  
\_\_\_\_\_  
Property Owner's Signature

manager KWDS, LLC

8-11-23  
\_\_\_\_\_  
Date

NORTON LANDING  
APARTMENTS  
McMinnville, Oregon

**LAND-USE APPLICATION**

**ADMINISTRATIVE VARIANCE**

AUGUST 14, 2023

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070

**RDA**  

---

REITER DESIGN ARCHITECT  
INCORPORATED

# NORTON LANDING APARTMENTS

## ADMINISTRATIVE VARIANCE

### TABLE OF CONTENTS

1. LAND-USE APPLICATION: Administrative Variance.
2. PROJECT DESIGN TEAM
3. APPLICATION NARRATIVE: Administrative Variance  
Project Description, Information: page 01  
  
McMinnville Zoning Code Compliance  
17.74 Review Criteria page 03
4. APPLICATION DRAWINGS: Architectural and Landscape



**LAND-USE APPLICATION**  
Administrative Variance

**1**

# Administrative Variance Information & Submittal Requirements



## Planning Department

231 NE Fifth Street • McMinnville, OR 97128  
(503) 434-7311 Office • (503) 474-4955 Fax  
[www.mcminnvilleoregon.gov](http://www.mcminnvilleoregon.gov)

## Overview

An administrative variance is the process under which the Planning Director may grant certain adjustments to specific requirements in the Zoning Ordinance when the adjustment is within the limitations and conditions stated in Section 17.74.090 (Administrative Variance-Review Criteria) of the McMinnville Zoning Ordinance and listed below. These provisions are to be used sparingly. A request for a variance beyond these limitations is processed as a full variance application, which is subject to Planning Commission review at a public hearing.

### *Limits for adjustment are as follows:*

- A. Lot area: Maximum possible adjustment of one percent of the minimum lot area but not more than 90 square feet.
- B. Setbacks: Maximum adjustment of 10 percent of the required setback.

*Variations are not allowed for standards pertaining to the number of dwelling units permitted, parking requirements, height of building, vision clearance area, density, or use of property.*

## Submittal Requirements

The following materials must be provided at the time of submittal, or the application will not be accepted for processing.

- A completed Administrative Variance application form.
- A site plan (drawn to scale, with a north arrow, legible, and of a reproducible size), indicating existing and proposed buildings, dimensions, adjacent street(s), distances from property lines, and any other information that would help substantiate or clarify your request.
- Payment of applicable review fee.

## Review Process

Upon receipt of a complete application, the Planning Director will send notice of the request to owners of property within 100 feet of the site for which the application is made for their review and comment. Following the allowed 14 (fourteen) day comment period, the Planning Director will advise the applicant of his decision by mail. The decision of the Planning Director to grant or deny the requested variance may be appealed to the Planning Commission under the provisions contained in Section 17.72.170 (Appeal from Ruling of Planning Director) of the Zoning Ordinance.

For further information regarding an Administrative Variance request and the process for review, please see Chapters 17.72 (Applications and Review Process) and 17.74 (Review Criteria) of the Zoning Ordinance, or contact the McMinnville Planning Department.

# **PROJECT DESIGN TEAM**

# **2**

# NORTON LANDING APARTMENTS

## PROJECT DESIGN TEAM

### **OWNER**

KWDS, LLC  
PO Box 145  
Wilsonville, Oregon 97070  
(503) 781-5685  
Owner's Representative: Chad Juranek

### **ARCHITECT / PLANNER**

REITER DESIGN ARCHITECT, INCORPORATED  
6107 SW Murray Blvd., #480  
Beaverton, Oregon 97008  
(503) 574-3036  
Project Architect, Project Planner: Scott A. Reiter

### **CIVIL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
250 NW Franklin Avenue, Suite 404  
Bend, Oregon 97703  
(541) 318-1161  
Project Civil Engineer: Jennifer VanCamp

### **LANDSCAPE ARCHITECT**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Landscape Architect: Jeffery Creel

### **STRUCTURAL ENGINEER**

HARPER HOUF PETERSON RIGHELLIS, INC.  
205 SE Spokane St., Suite 200  
Portland, Oregon 97202  
(503) 221-1131  
Project Engineer: Jeff Schwindt

### **TRANSPORATION ENGINEER**

LANCASTER-MOBLEY  
321 SW 4<sup>th</sup> Ave., Suite 400  
Portland, Oregon 97204  
(503) 248-0313  
Project Engineer: Daniel Stumpf

**SEE REVISED NARRATIVE**

**APPLICATION NARRATIVE**  
Administrative Variance

**3**

**APPLICATION DRAWINGS**  
ARCHITECTURAL,  
AND LANDSCAPE

**4**



**PROJECT DATA**

**SITE INFORMATION:**  
 ZONING: C3 - GENERAL COMMERCIAL, THREE-MILE LANE PLANNED DEVELOPMENT OVERLAY  
 MAP NO: T4SR4W-27, TAX LOT 701  
 SITE AREA: 214,759 SF, 4.93 ACRES

**BUILDING INFORMATION**  
 CONSTRUCTION TYPE: TYPE V-B  
 BUILDING HEIGHT: THREE-STORIES  
 OCCUPANCY GROUPS: R-2 RESIDENTIAL, APARTMENTS

**LIVING UNITS:**  
 ONE-BEDROOM, 1 BATH: 42 UNITS  
 TWO-BEDROOM, 1 BATH: 36 UNITS  
 TWO-BEDROOM, 2 BATH: 42 UNITS  
 THREE-BEDROOM, 2 BATH: 18 UNITS  
 TOTAL UNITS: 138 UNITS

**PARKING:**  
 REQUIRED:  
 1BR AND 2BR UNITS, 1.5 SPACES/UNIT= 180 SPACES  
 3BR UNITS, 2 SPACES/UNIT= 36 SPACES  
 TOTAL REQUIRED: 216 SPACES

**PROVIDED:**  
 STANDARD SPACES: 188 SPACES  
 COMPACT SPACES: 22 SPACES  
 ACCESSIBLE SPACES: 8 SPACES  
 TOTAL PARKING PROVIDED: 218 SPACES

**PROJECT TEAM**

**OWNER/DEVELOPER:**  
 KWDS, LLC  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**GENERAL CONTRACTOR:**  
 KOHL INC.  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**ARCHITECT/PLANNER:**  
 REITER DESIGN ARCHITECT, INC.  
 7965 SW CIRRIUS DRIVE  
 BEAVERTON, OREGON 97008  
 (503) 574-3036  
 PROJECT ARCHITECT: SCOTT REITER

**CIVIL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 250 NW FRANKLIN AVE., SUITE 404  
 BEND, OREGON 97703  
 (541) 318-1161  
 PROJECT ENG.: JENNIFER VAN CAMP, PE

**STRUCTURAL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT ENGINEER: JEFF SCHWINDT, PE

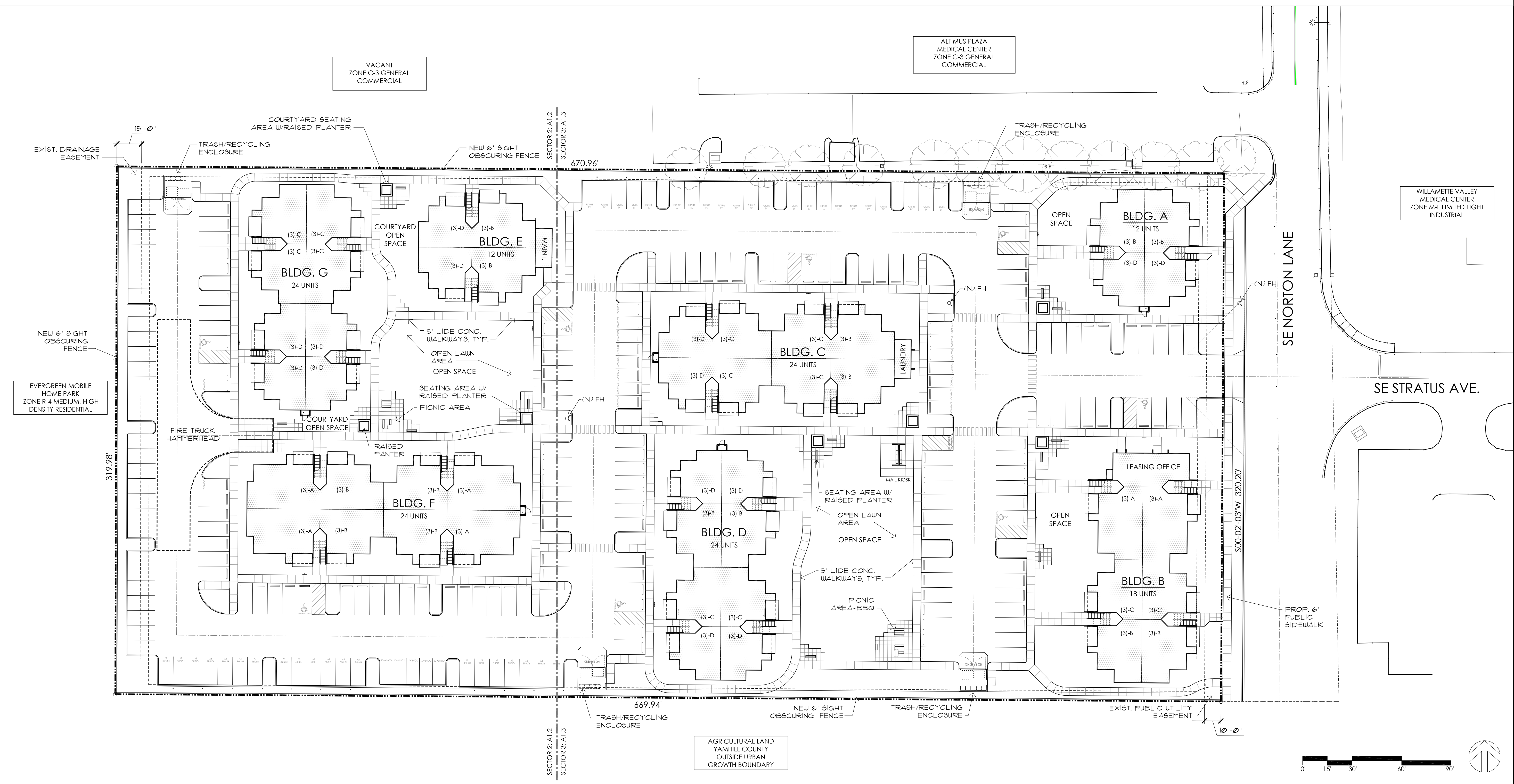
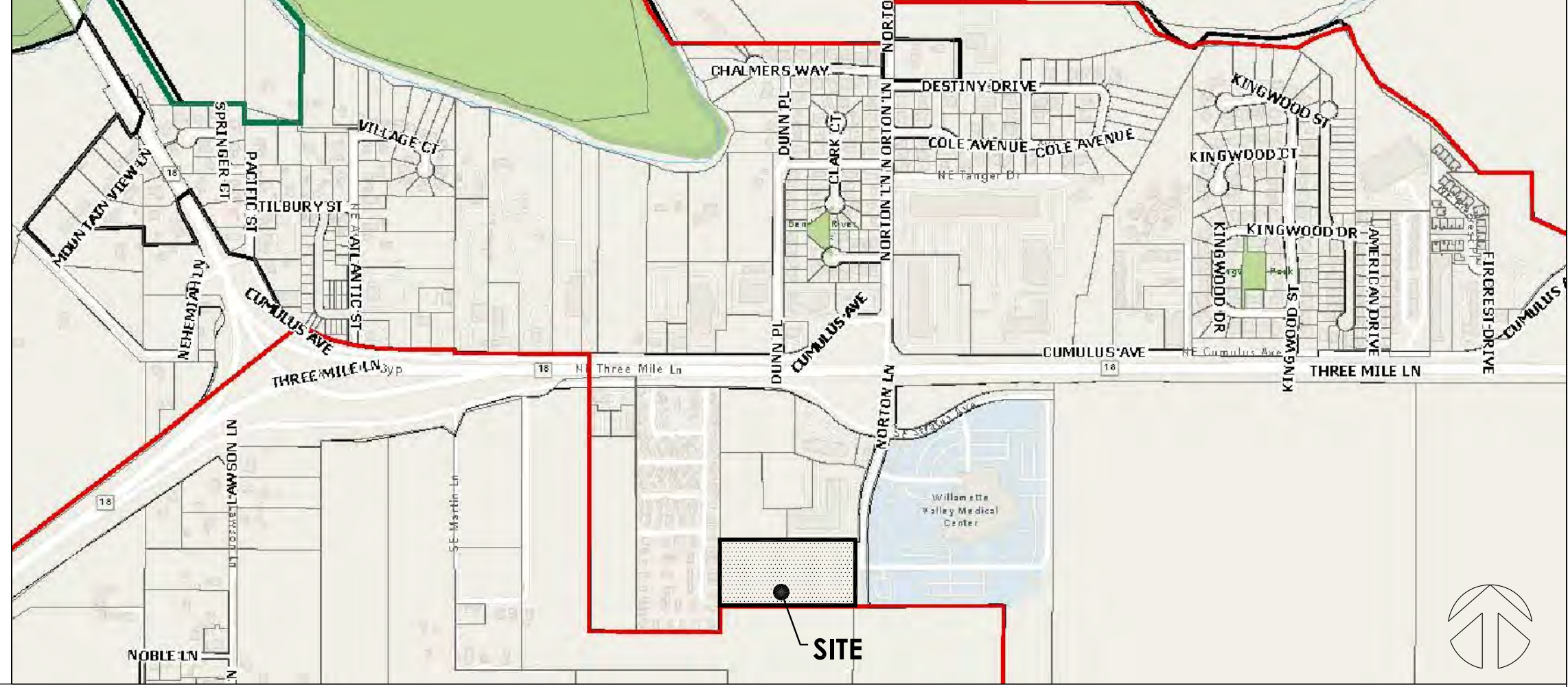
**TRANSPORTATION ENGINEER:**  
 LANCASTER-MOBY  
 321 SW 4TH, AVE, SUITE 400  
 PORTLAND, OREGON 97204  
 (503) 248-0313  
 CONTACT: DANIEL STUMPF

**LANDSCAPE ARCHITECT:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT LANDSCAPE ARCH.: JEFFERY CREEL

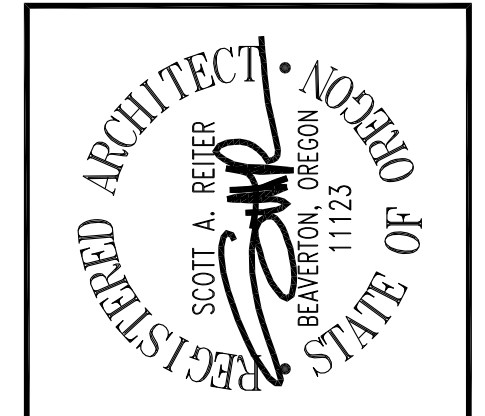
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**CONCEPTUAL SITE PLAN** 1  
 Scale: 1" = 30'-0" A1.0



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3036

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

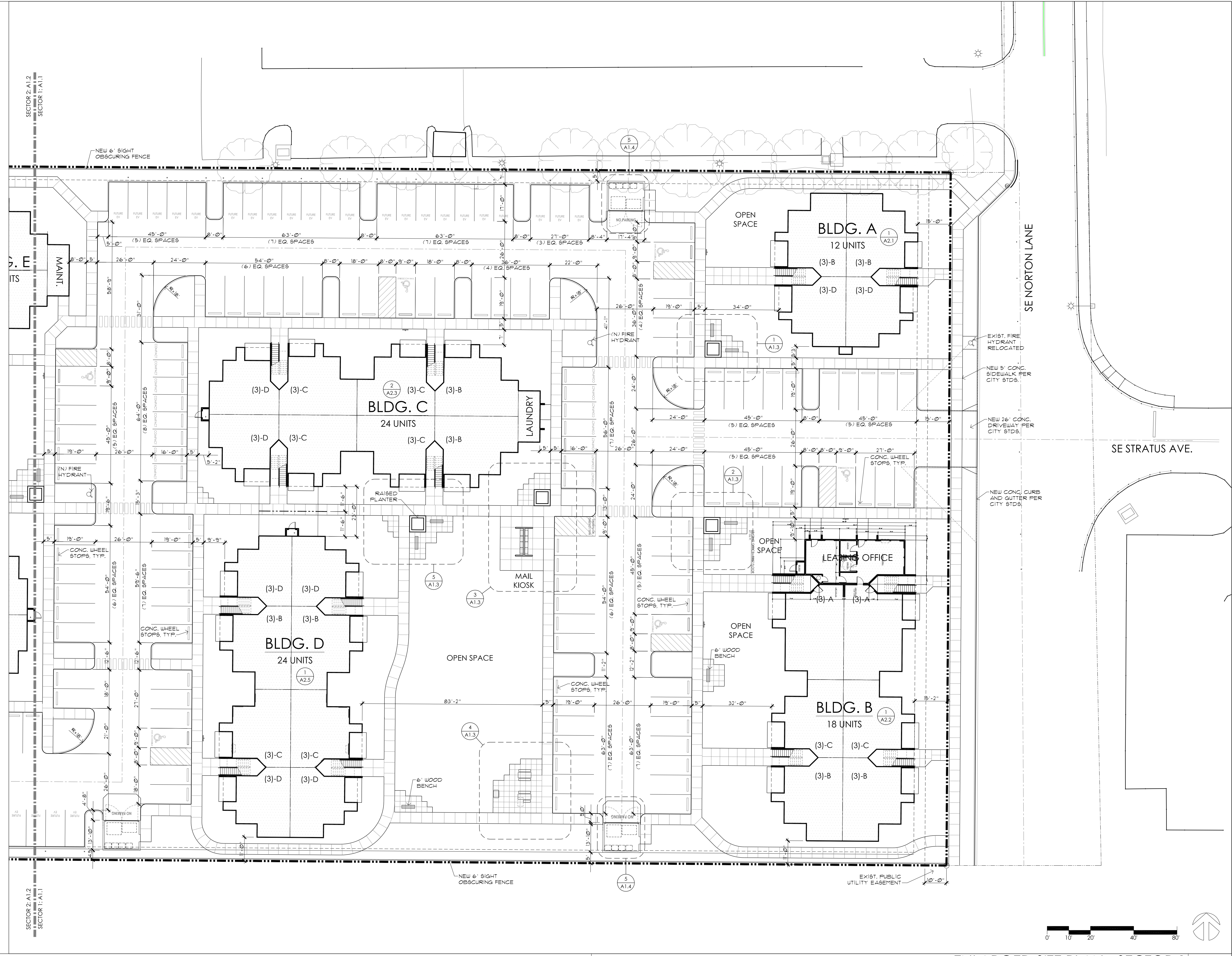
CONCEPTUAL SITE PLAN

| date:         | revisions: |
|---------------|------------|
| JULY 19, 2023 | AS NOTED   |
| scale:        |            |
| drawn:        |            |
| job no.:      | 2301       |

LAND-USE REVIEW

**A**  
**1.0**



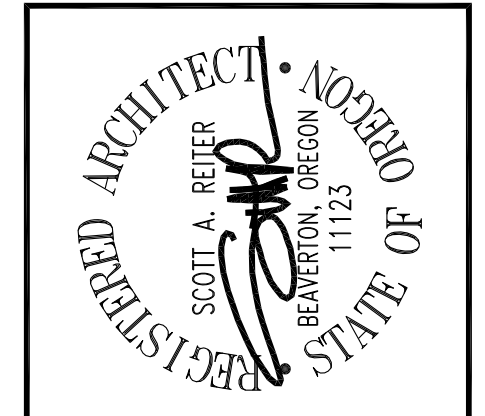


SECTOR 2: A1.2  
SECTOR 1: A1.1

SECTOR 2: A1.2  
SECTOR 1: A1.1

ENLARGED SITE PLAN - SECTOR 1 1  
Scale: 1" = 20'-0" A1.1

LAND-USE REVIEW



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED

6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

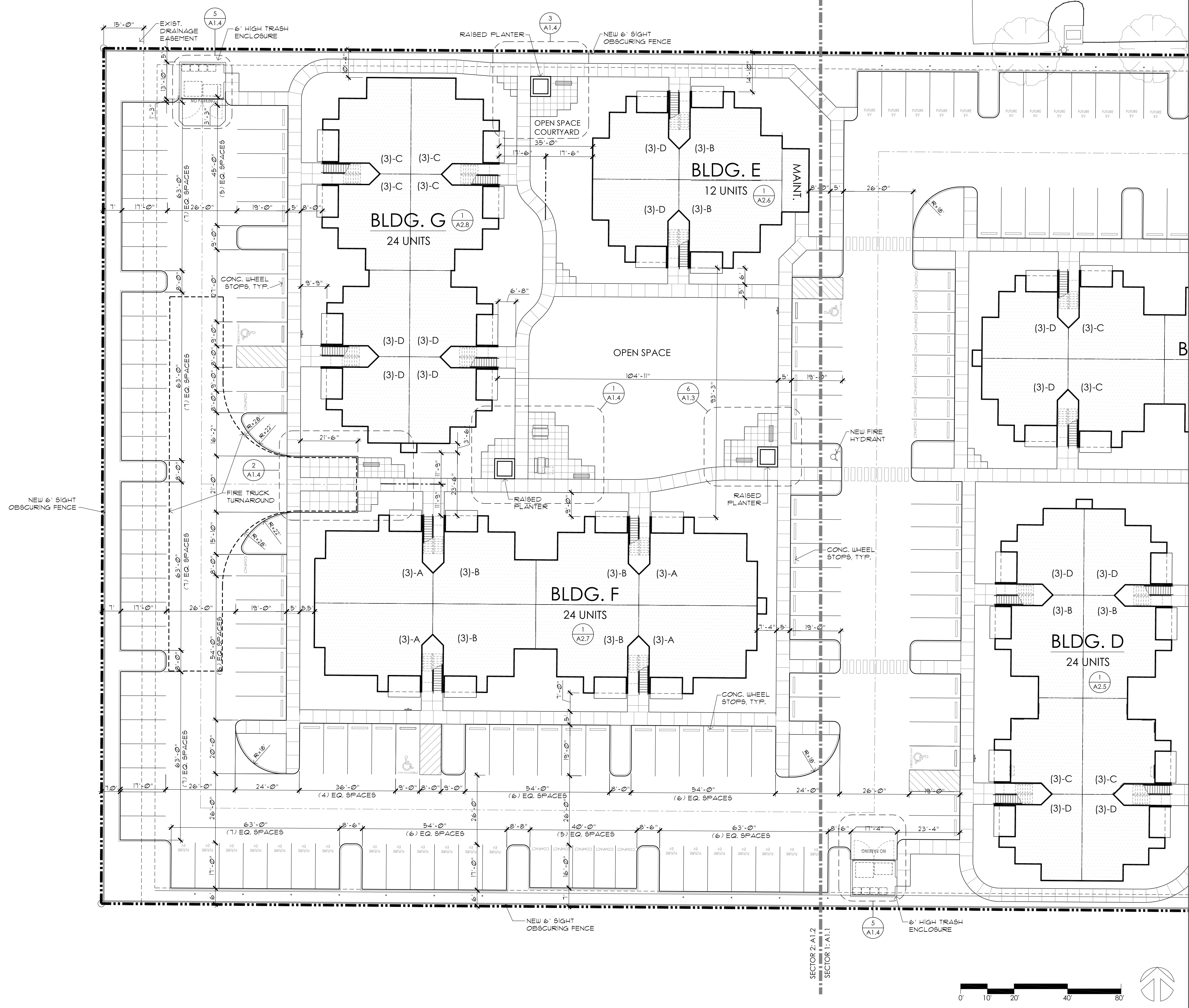
OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLAN - SECTOR 1

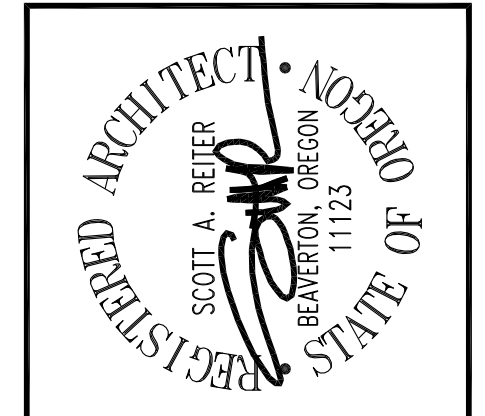
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| drawn:   |               |
| job no.: | 2301          |

**A**  
**1.1**





ENLARGED SITE PLAN - SECTOR 2 1  
 Scale: 1" = 20'-0" A1.2



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056

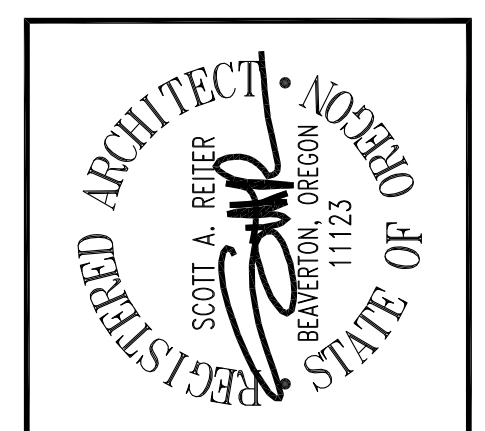
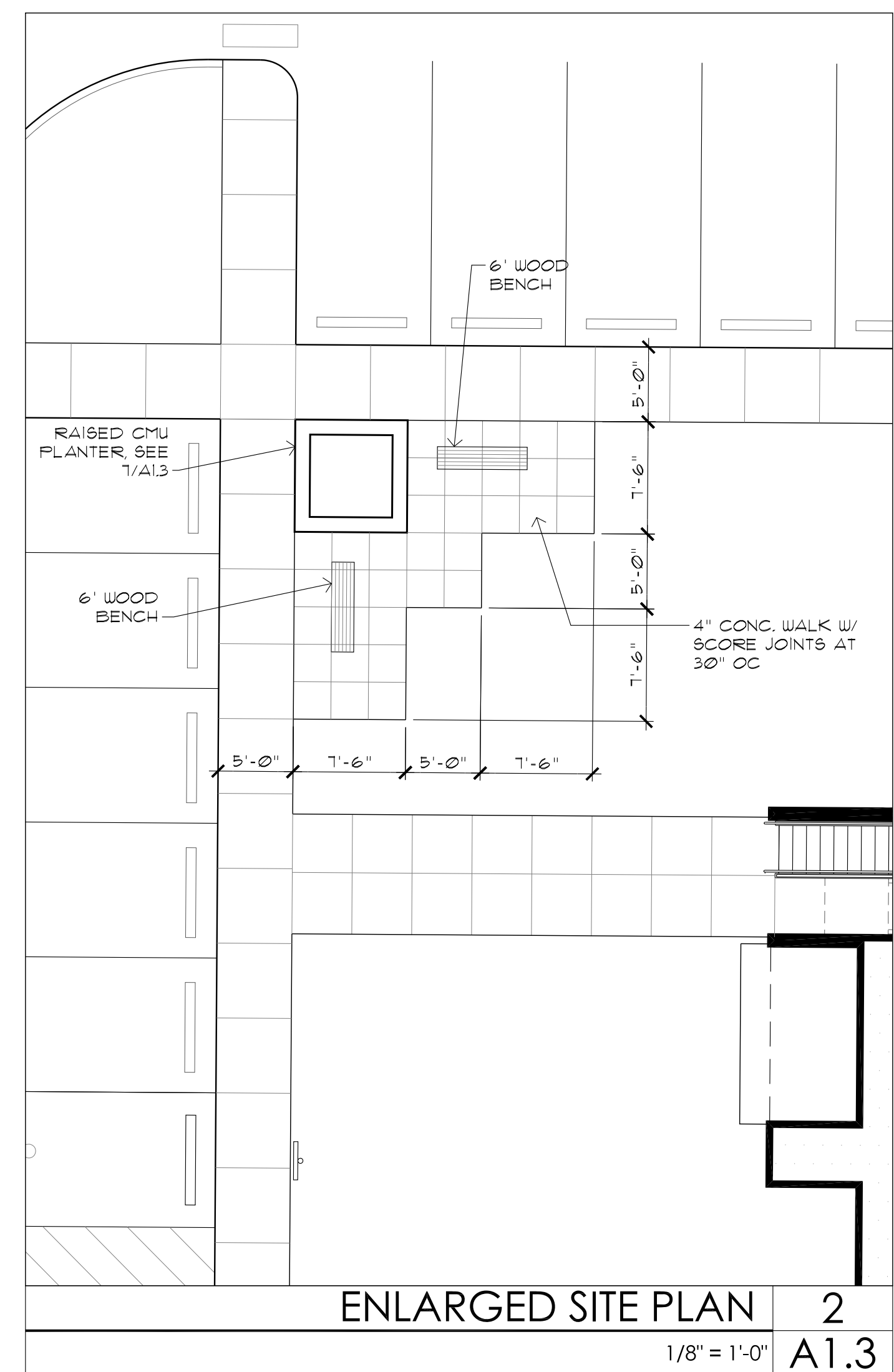
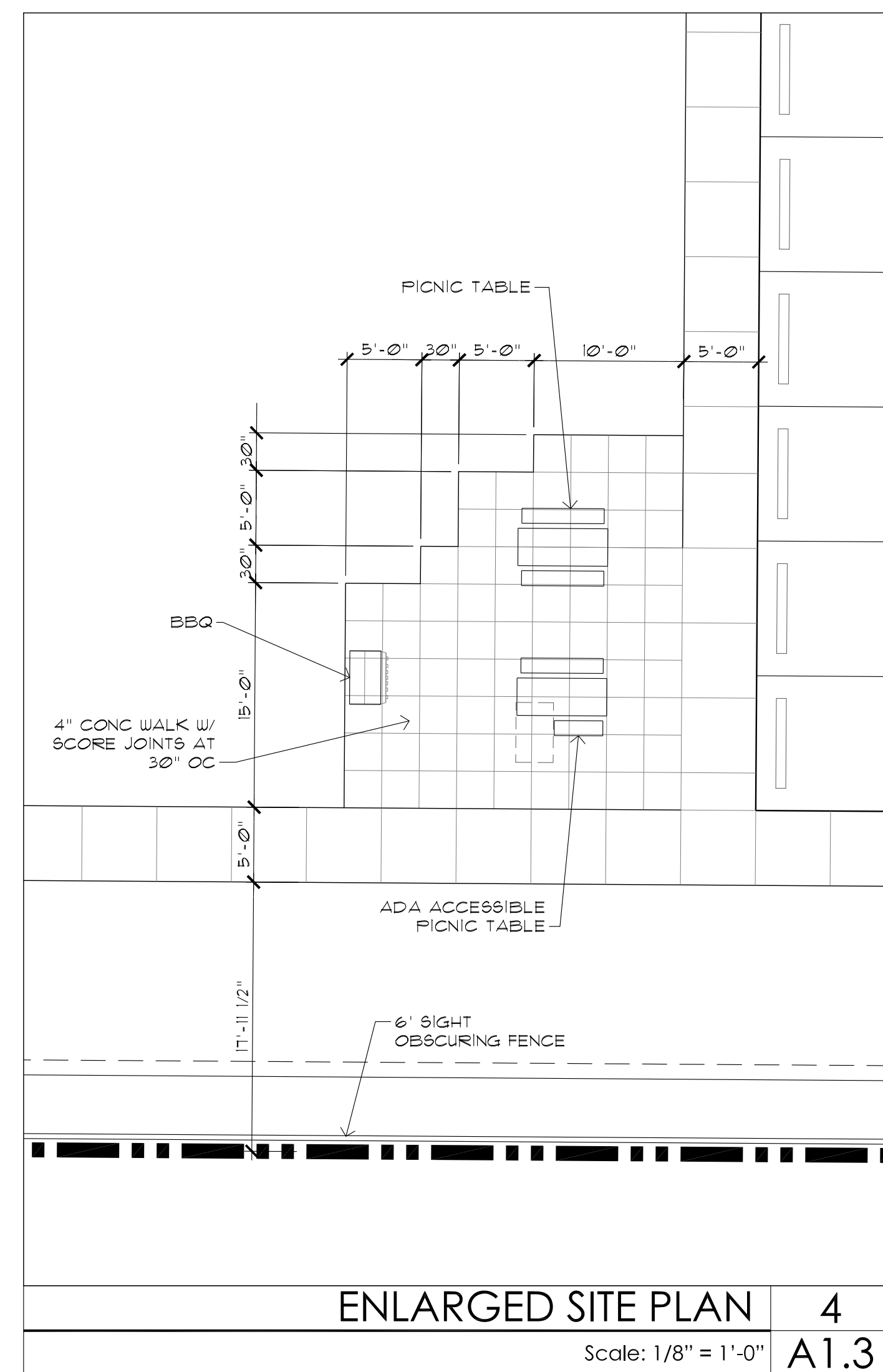
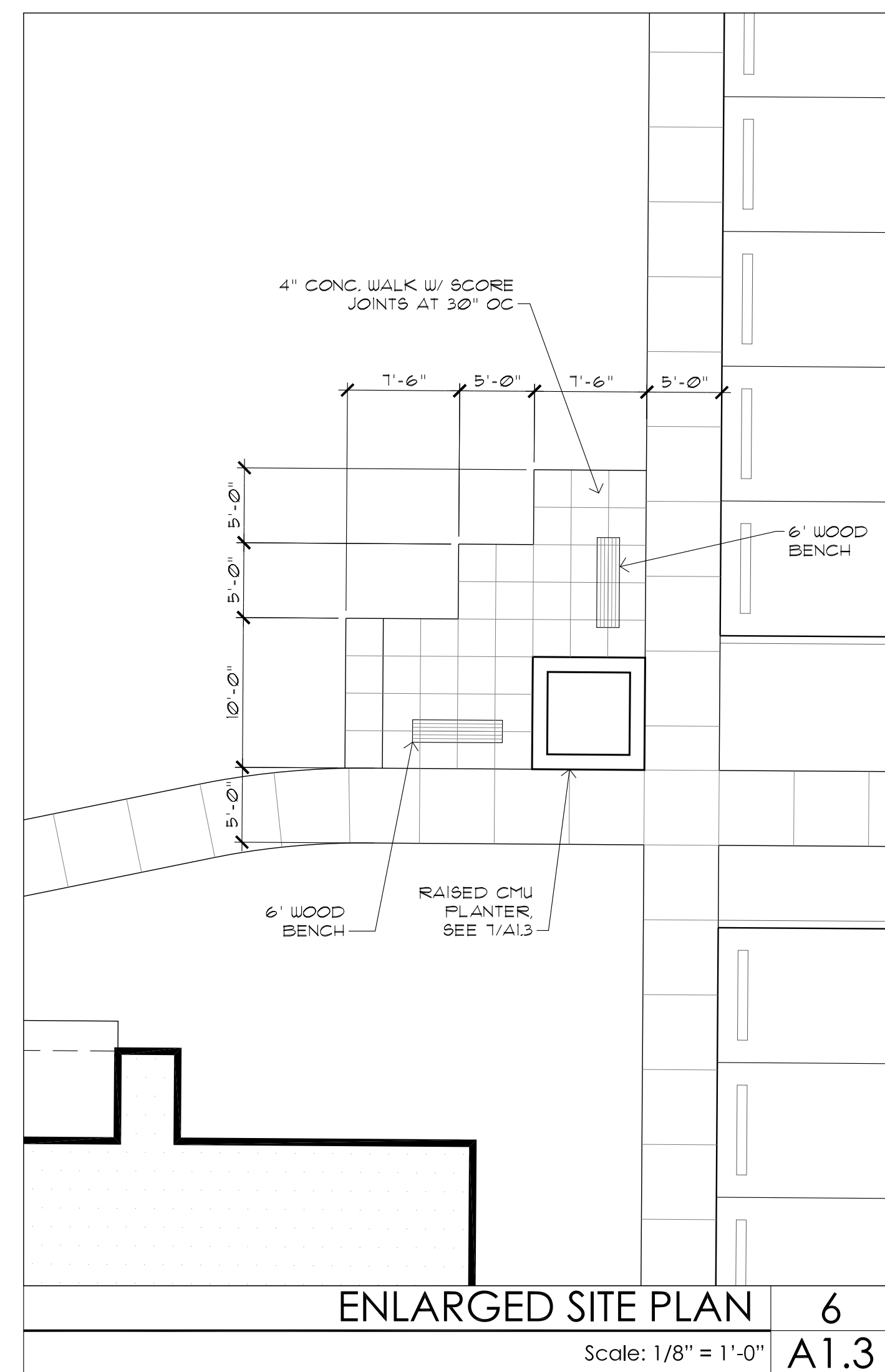
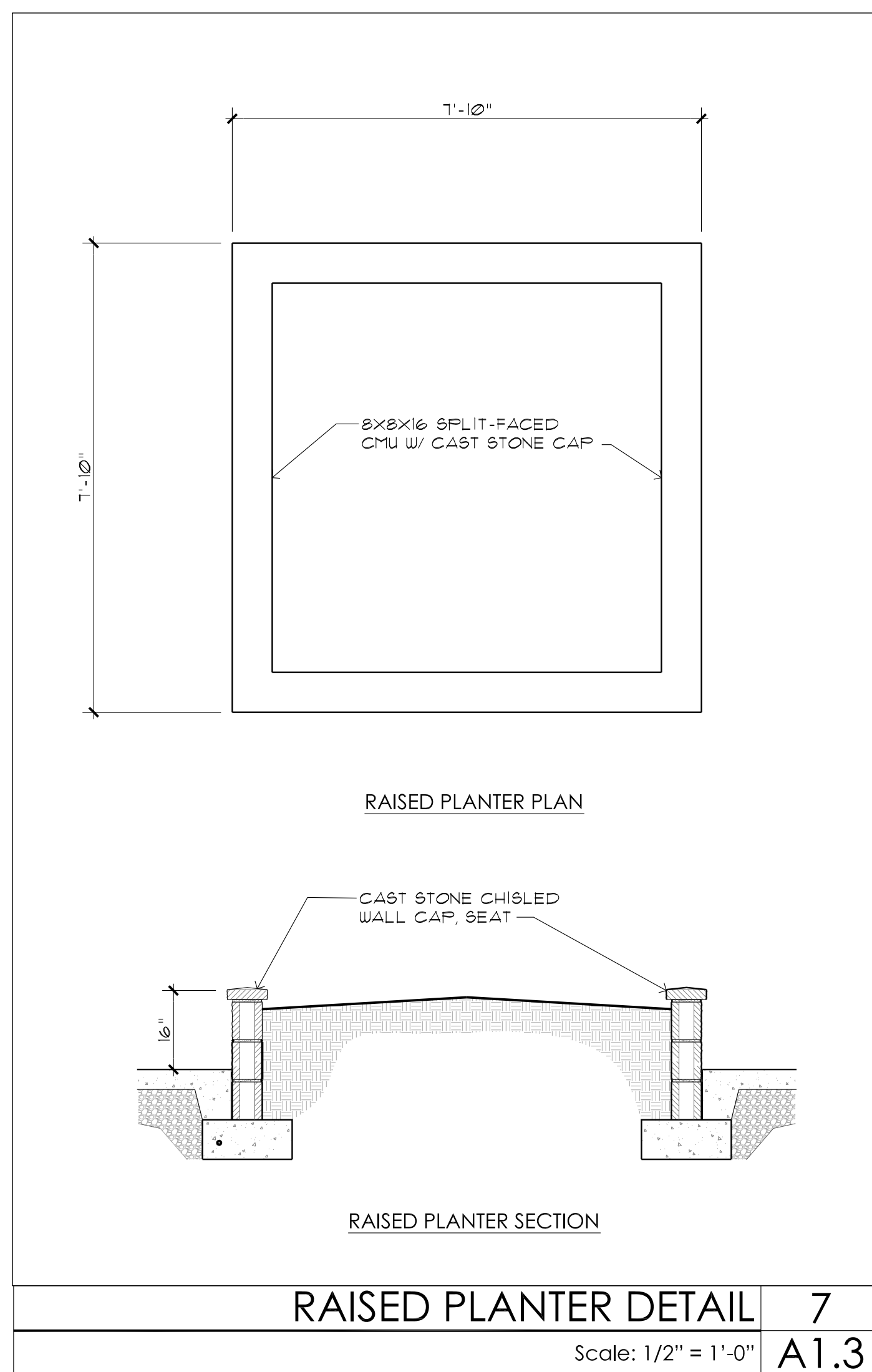
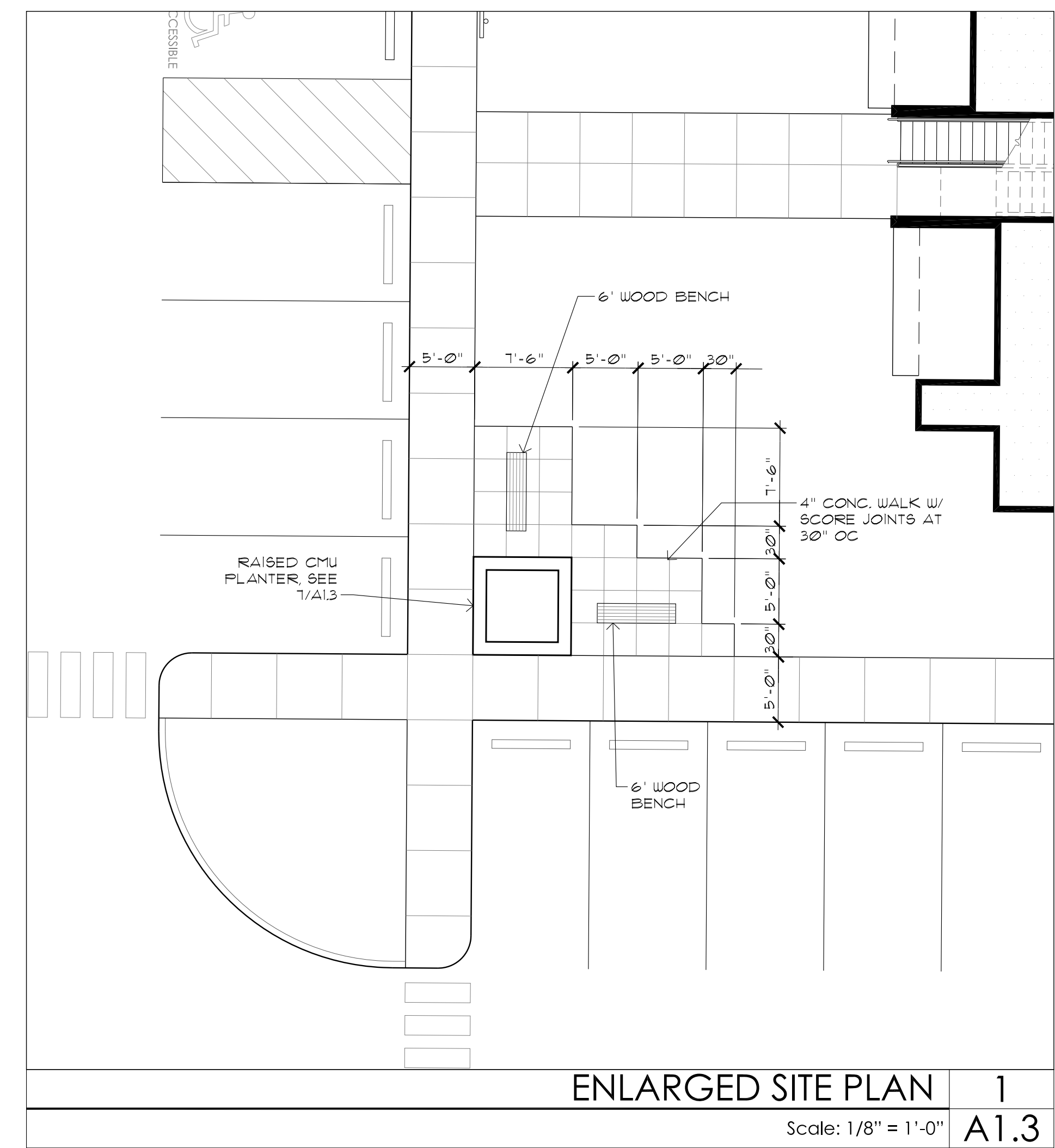
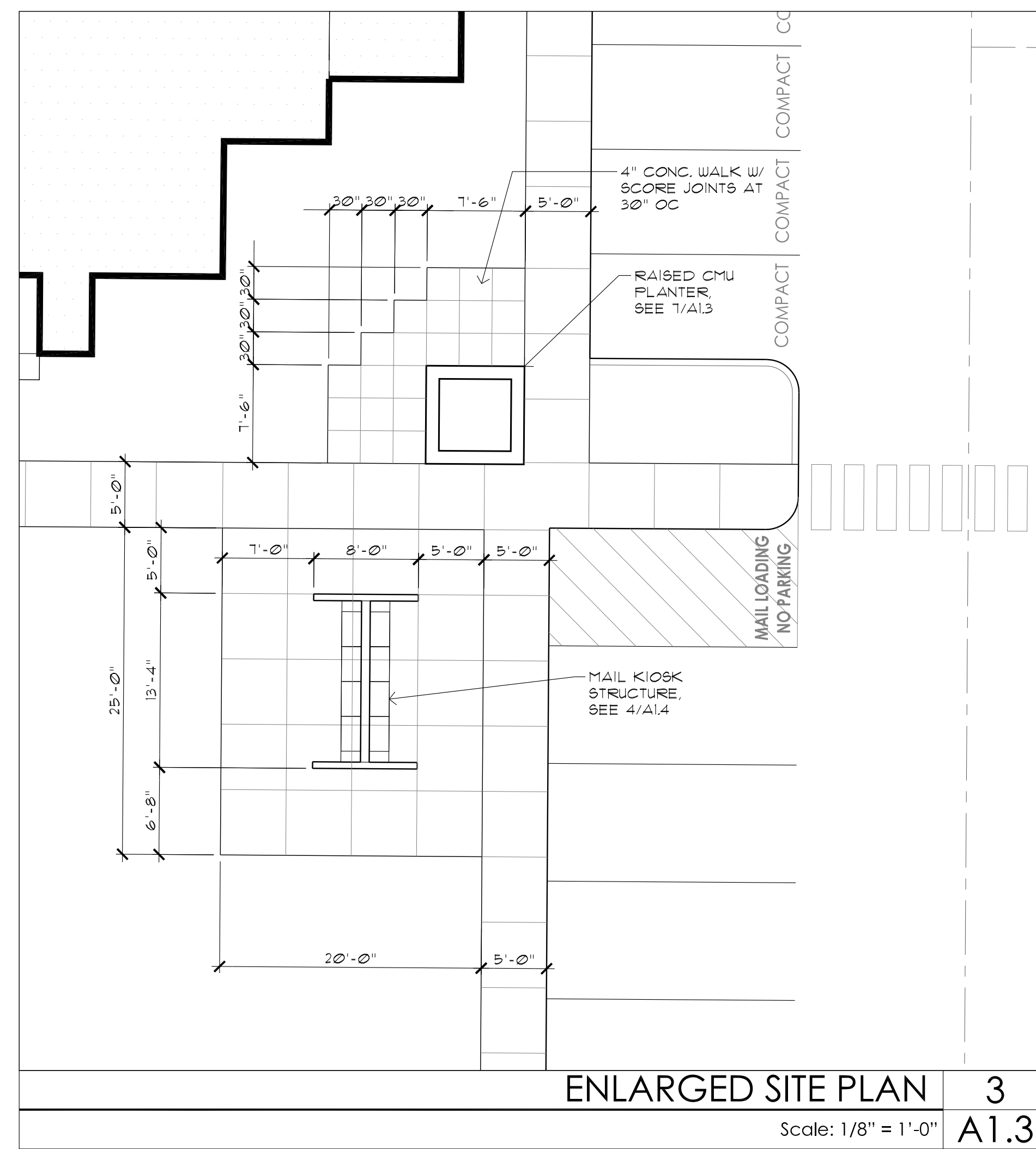
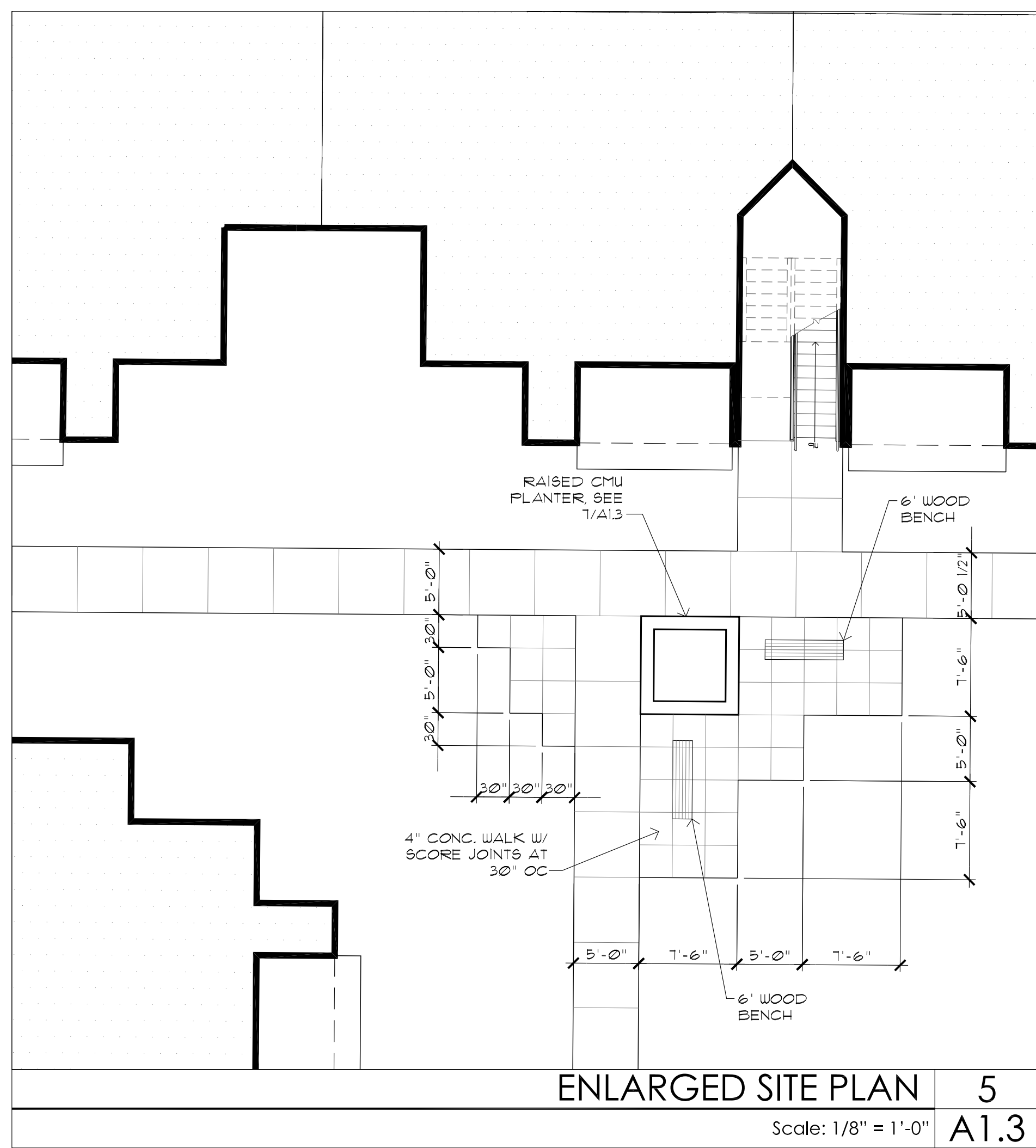
**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLAN - SECTOR 2

| DATE          | REVISIONS |
|---------------|-----------|
| JULY 19, 2023 | AS NOTED  |
| SCALE:        |           |
| DRAWN:        |           |
| JOB NO.:      | 2301      |

LAND-USE REVIEW

**A**  
**1.2**



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED  
6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
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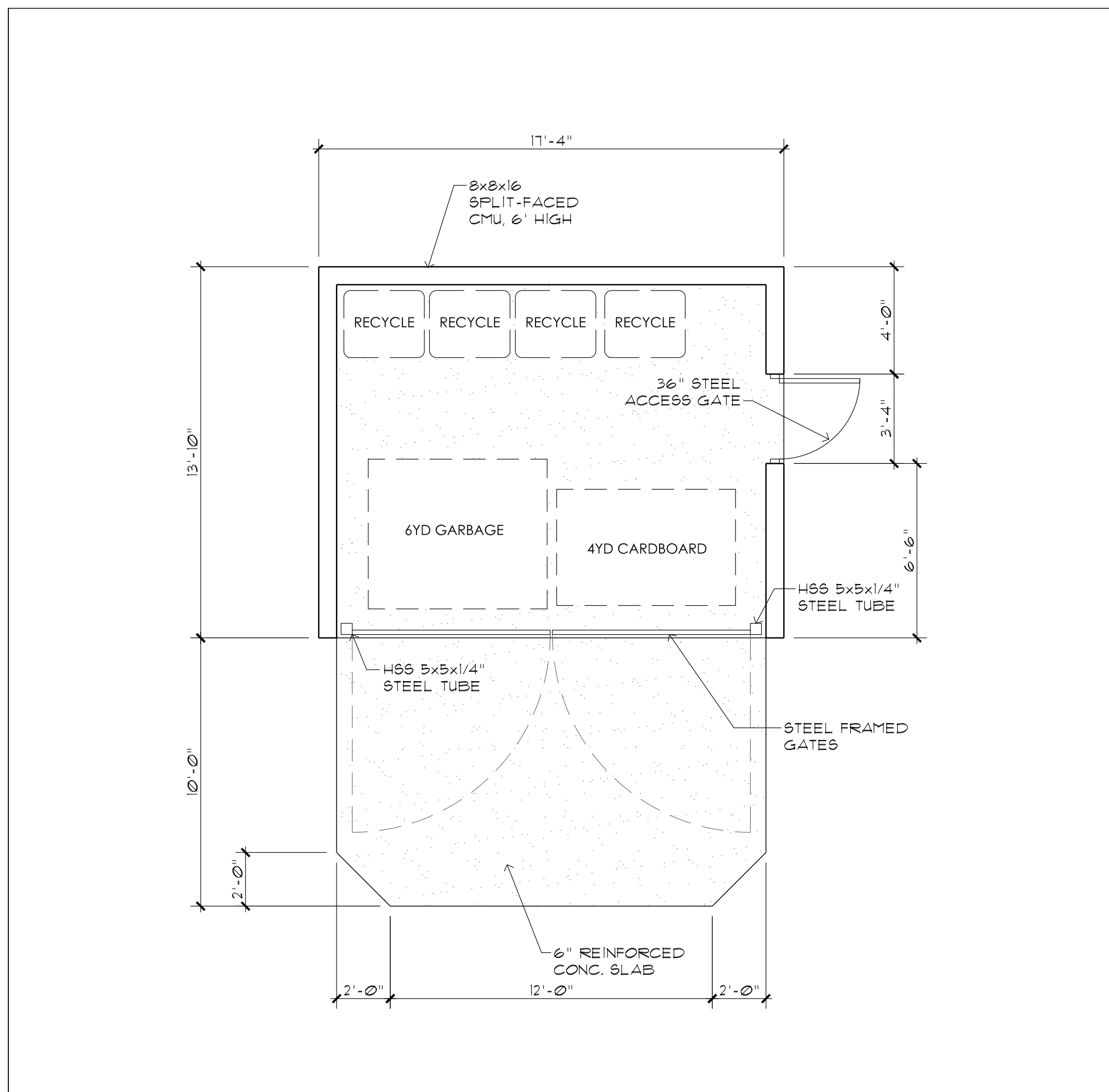
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|----------|---------------|
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| drawn:   |               |
| job no.: | 2301          |

**A**  
**1.3**

LAND-USE REVIEW

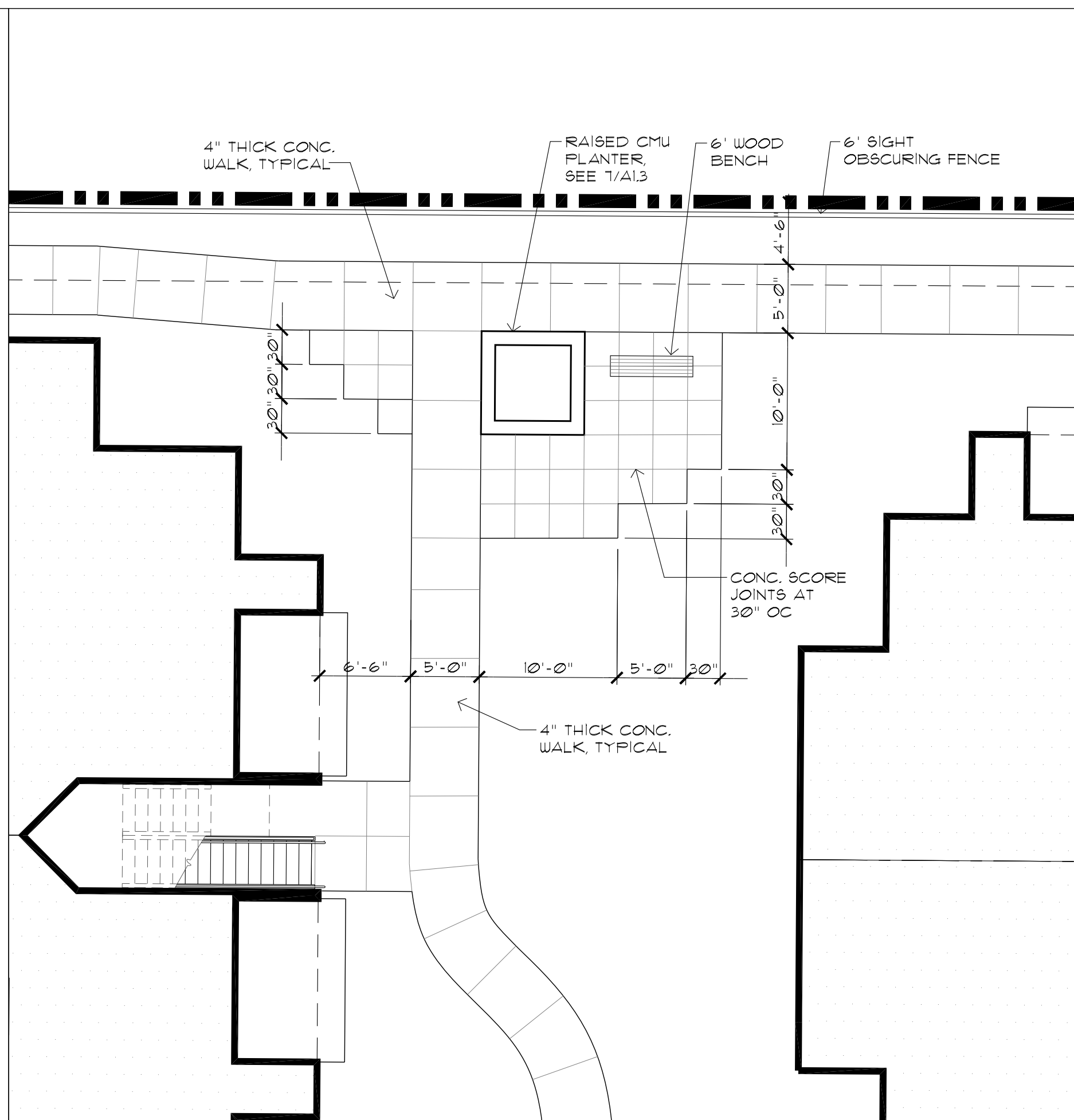




**TRASH ENCLOSURE PLAN** 5

Scale: 1/4" = 1'-0"

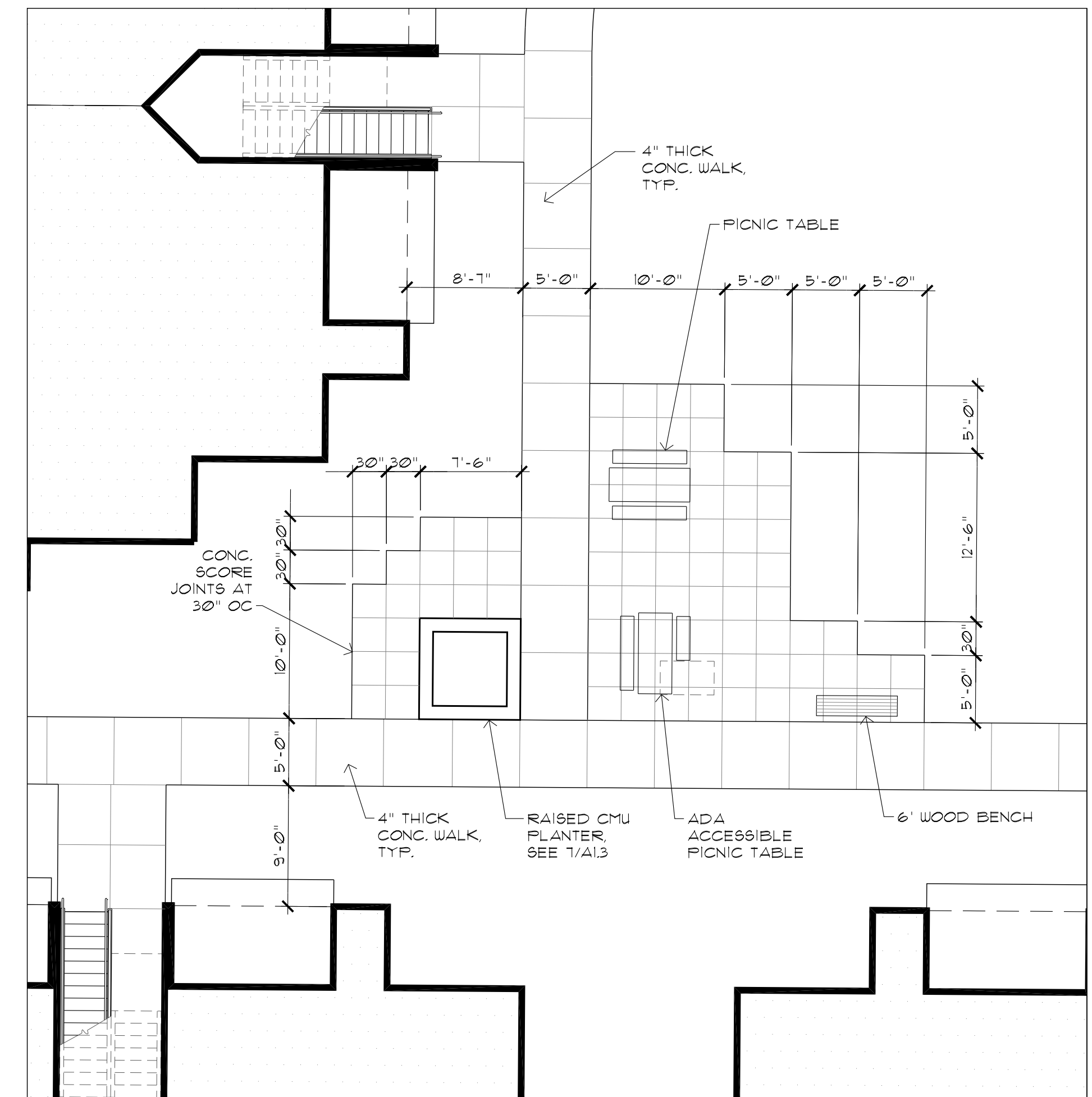
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**ENLARGED SITE PLAN** 3

Scale: 1/8" = 1'-0"

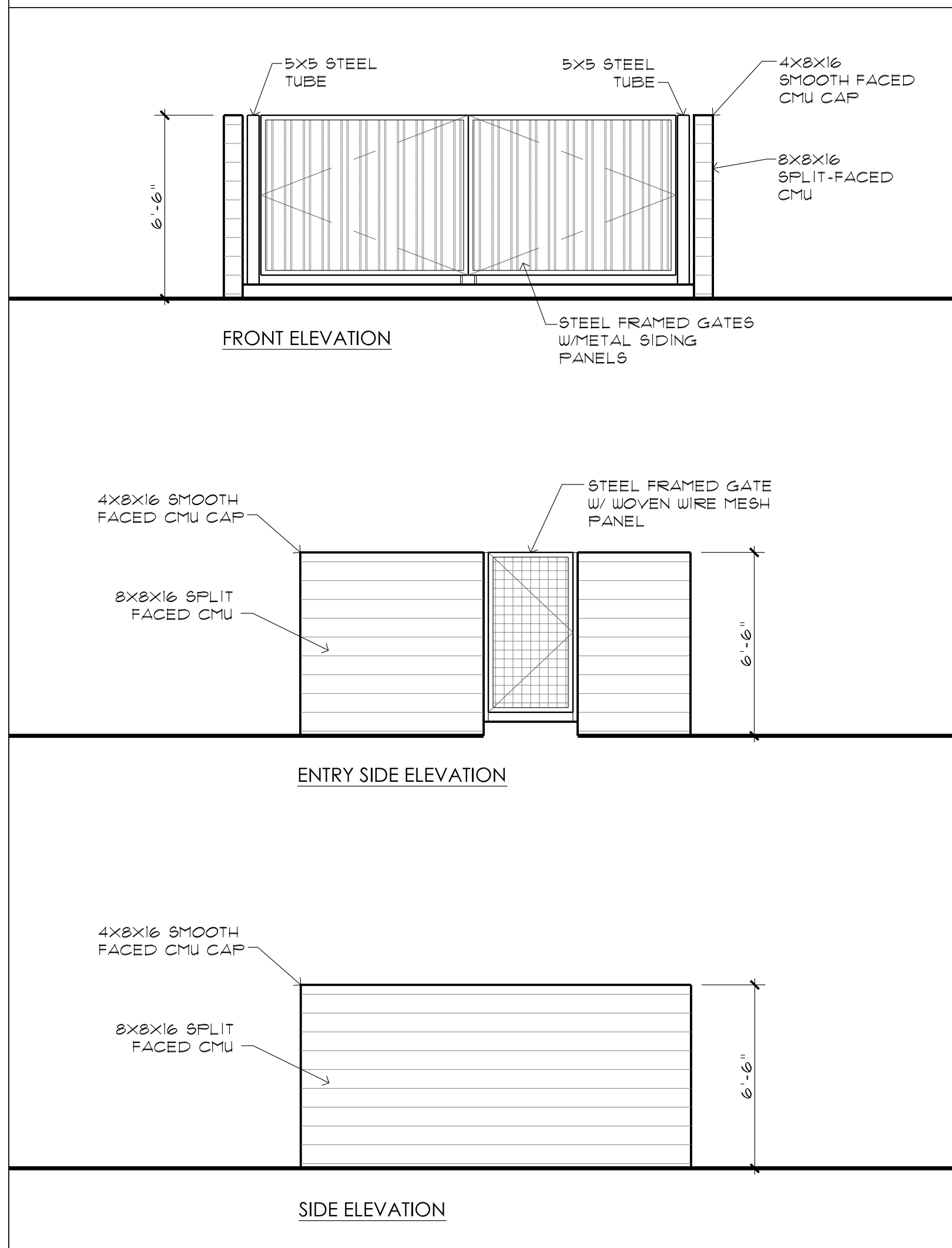
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**ENLARGED SITE PLAN** 1

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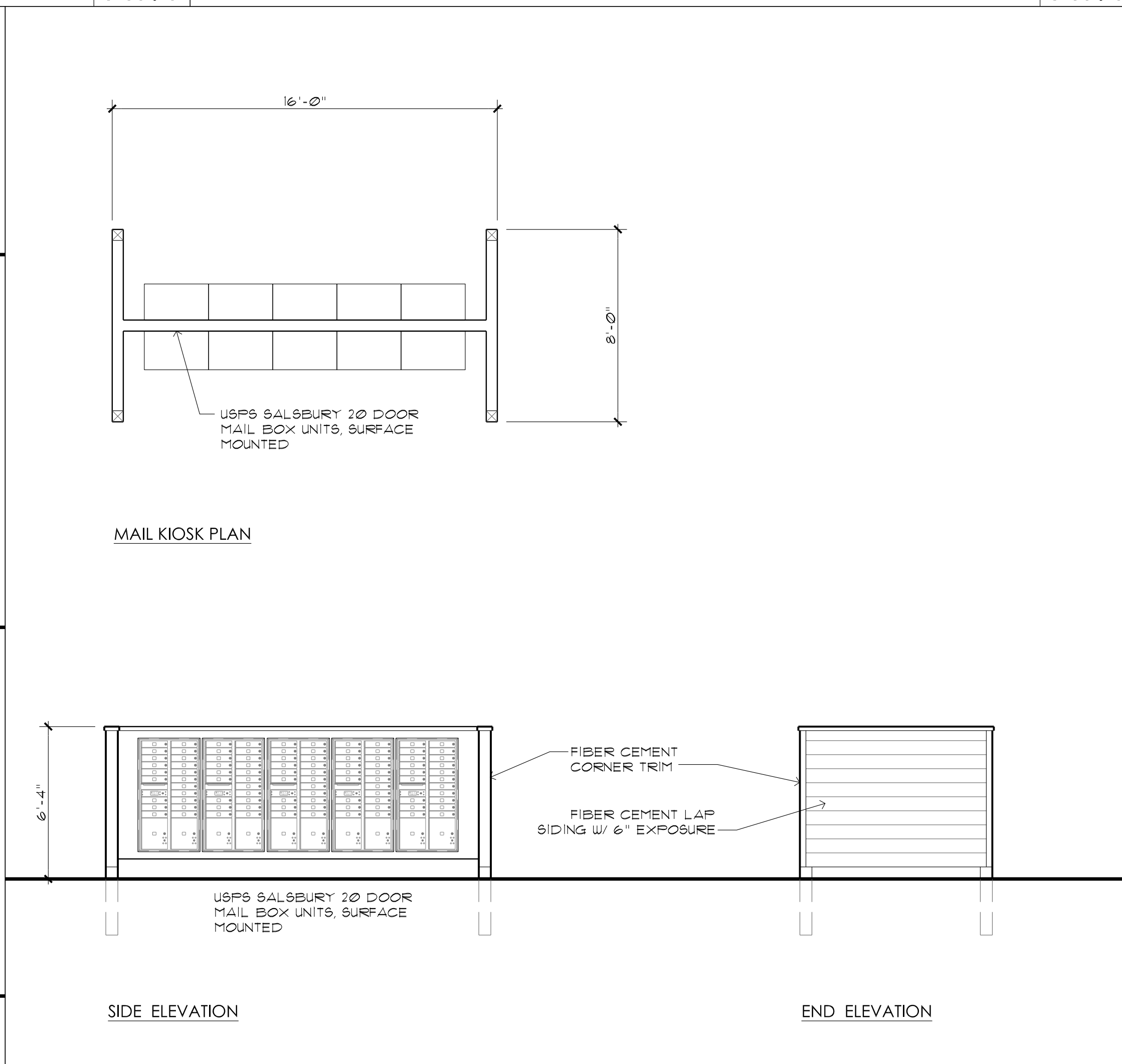
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**TRASH ENCLOSURE ELEVATIONS** 6

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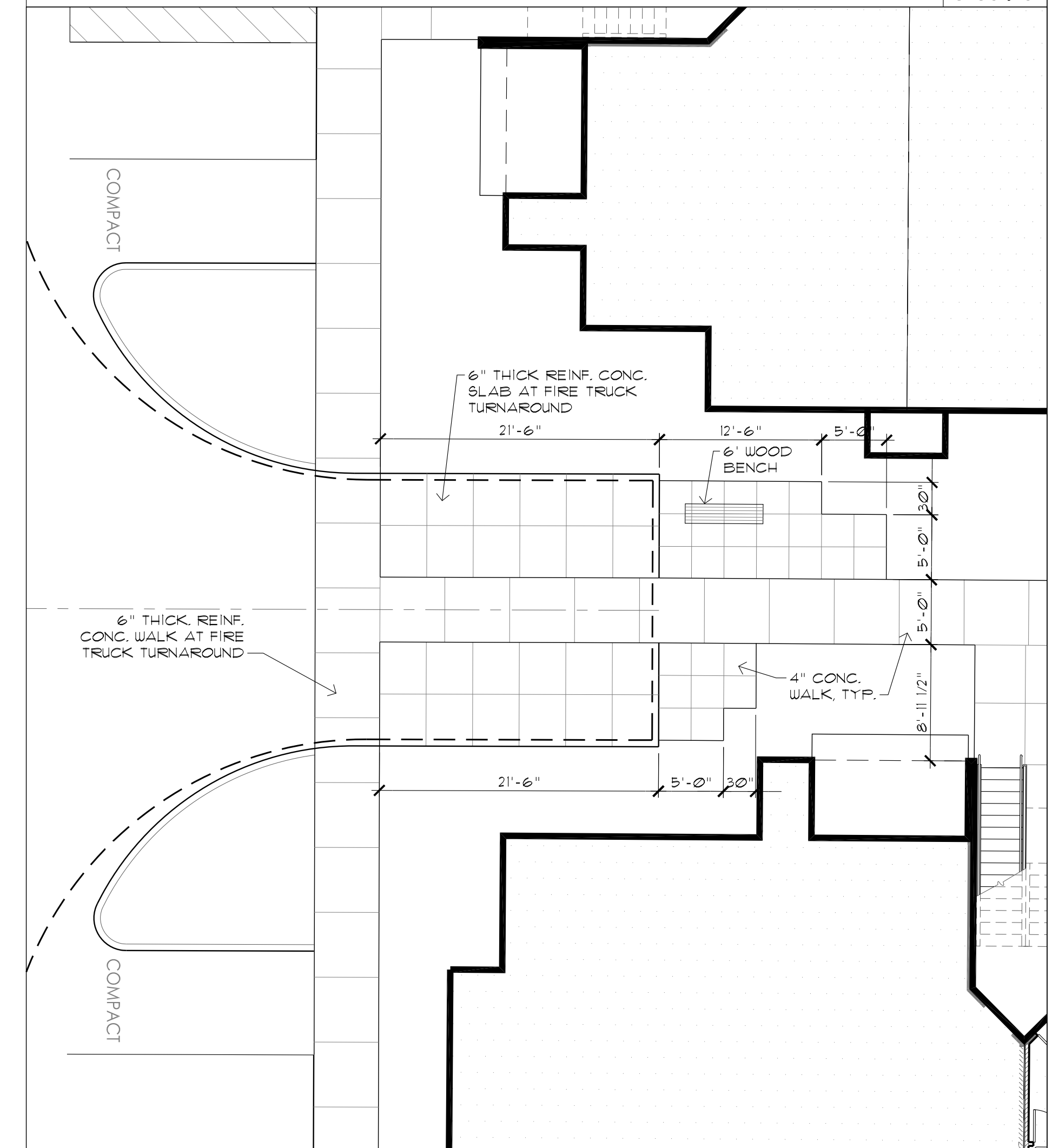
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**MAIL KIOSK PLAN, ELEVATIONS** 4

Scale: 1/4" = 1'-0"

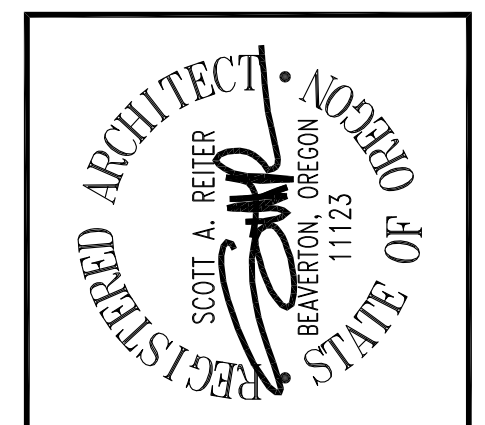
**A1.4**



**ENLARGED SITE PLAN** 2

Scale: 1/8" = 1'-0"

**A1.4**



**RDA**  
REITER DESIGN ARCHITECT  
INCORPORATED

6107 SW MURRAY BLVD. #480  
BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON

OWNER:  
**KWDS, LLC**  
PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLANS  
MAIL KIOSK PLANS, ELEVATIONS  
TRASH ENCLOSURE PLAN, ELEVATIONS

date: JULY 19, 2023  
scale: AS NOTED  
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job no: 2301

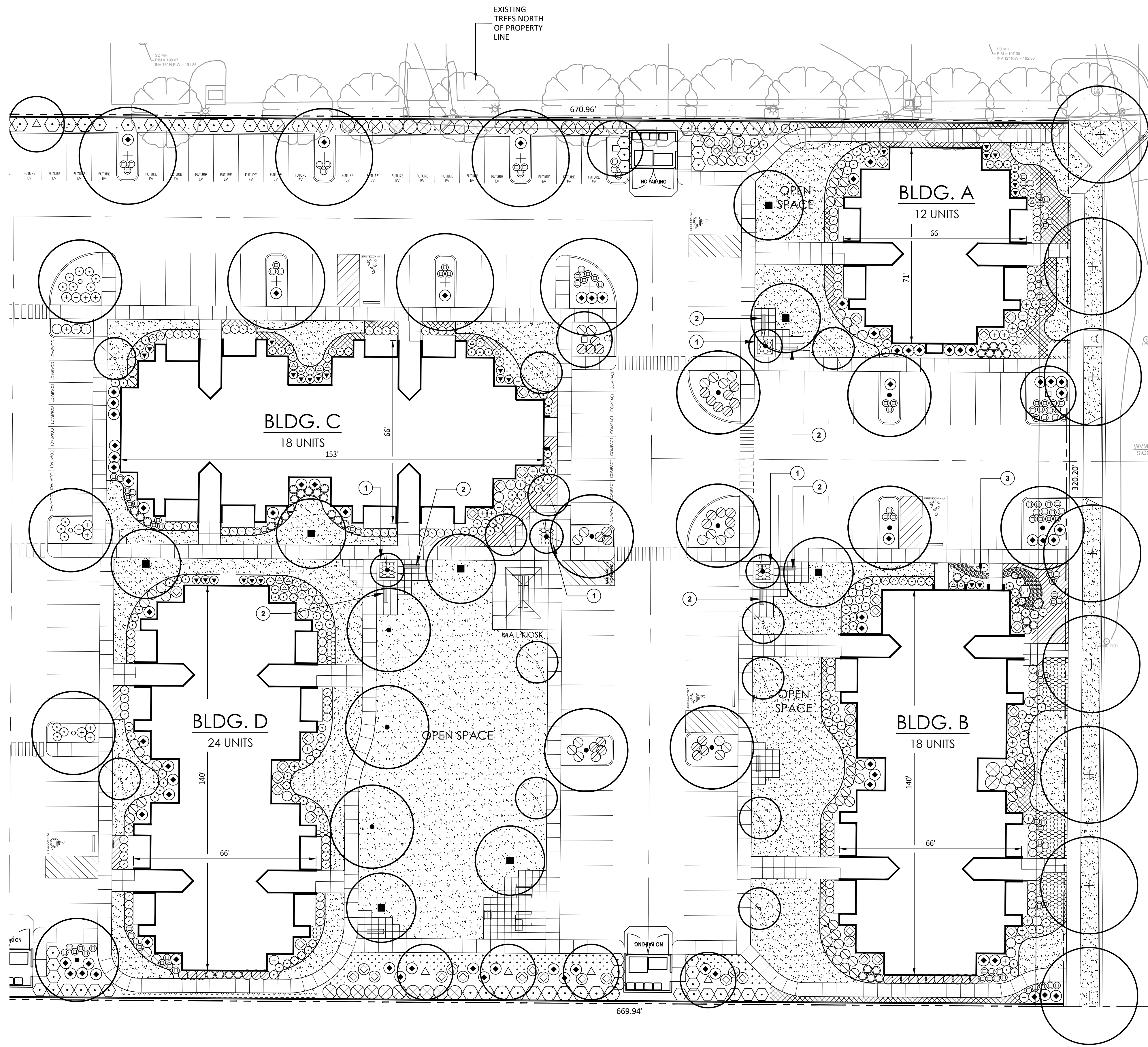
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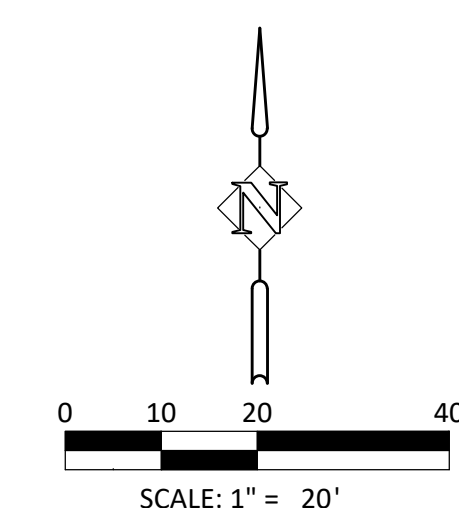
N:\KWD\KWD-11 (McMinnville Apartments)\KWD-11-DWG\Sheets\L1.1 - Landscape Plans.dwg



**LANDSCAPE PLAN**  
1" = 20'

| CITY OF MCMINNVILLE CODE COMPLIANCE |                              |   |   |   |
|-------------------------------------|------------------------------|---|---|---|
| SECTION #                           | SECTION                      | DESCRIPTION   | REQUIRED  | PROVIDED  |
| 17.11.090(D)                        | LARGE SITE DESIGN STANDARDS  | BUFFER PARKING AREAS WITH LANDSCAPING.  |   | LANDSCAPING AROUND PAVING LOT                     |
|                                     | LARGE SITE DESIGN STANDARDS  | PROVIDE DIFFERENT TYPE OF OPEN SPACE THROUGHOUT THE SITE  |   | DIFFERENT TYPES OF OPEN SPACE PROVIDED            |
|                                     | PARKING LOT DESIGN STANDARDS | >50,000 SF PARKING LOTS = 8% INTERIOR LANDSCAPING REQUIRED.   | PARKING LOT AREA = 75,075 SF X 8% = 6,006 SF        | 6,681 SF INT. PL LANDSCAPING                      |
|                                     | PARKING LOT DESIGN STANDARDS | 1 (NON-COLUMNAR) TREE PER ISLAND  | 37 PARKING LOT ISLANDS                              | 37 TREES  |
|                                     | PARKING LOT DESIGN STANDARDS | FENCE REQUIRED NEXT TO RESIDENTIAL ZONING   | FENCE ON WEST PL                                    | FENCE PROVIDED. SEE ARCHITECTURAL SITE PLANS      |
|                                     | PARKING LOT DESIGN STANDARDS | BUFFER BETWEEN PL AND BUILDINGS   | SW OR LANDSCAPE STRIP                               | 5' SW & LANDSCAPE STRIPS PROVIDED                 |
|                                     | PARKING LOT DESIGN STANDARDS | PLANTINGS ADJACENT TO SW (BETWEEN PL AND BLDGS)   | 6' LANDSCAPE STRIP NEXT TO SW                       | 6' PLANTING STRIP                                 |
|                                     | PARKING LOT DESIGN STANDARDS | TREES ALONG THE THROUGH SIDEWALK-WALKWAY CONNECTIONS  |   | TREES PROVIDED ALONG SIDEWALK CONNECTIONS         |
|                                     | OPEN SPACE                   | PROVIDE DECIDUOUS TREE FOR SHADE  | DEC. TREES IN OPEN SPACES                           | STEWARTIA PROVIDED                                |
|                                     | LANDSCAPING                  | A MIN. OF 20% OF THE SITE SHALL BE LANDSCAPED   | TOTAL SITE 214,606 SF X 20% = 42,921 SF             | 64,157 SF PROVIDED (30%)                          |
| 17.11.090(D). 10                    | FRONT YARD SETBACK           | FRONT YARD SETBACK BETWEEN ENTRANCE AND STREET GATEWAY ZONE   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
| 17.11.090(D). 6.B(3)                | PRIVATE OPEN SPACE           | PRIVATE OUTDOOR SPACE AT GROUND LEVEL REQUIRES FRONT YARD   | MUST CONTAIN LOW SHRUBS OR FENCE                    | URBAN FRONT YARD TYPE USED. LOW SHRUBS PROVIDED   |
|                                     | OPEN SPACE                   | COMMON OPEN SPACE 15% MIN.  | TOTAL SITE 214,606 SF X 15% = 32,191 SF             | 32,715 SF PROVIDED                                |
|                                     | OPEN SPACE                   | PASSIVE OPEN SPACE NO MORE THAN 5% OF SITE  | TOTAL SITE 214,606 SF X 5% = 10,730 SF MAX. PASSIVE | 21,985 SF ACTIVE OPEN SPACE                       |
| 17.57.070                           | EXISTING TREES               | EXISTING LOCATIONS OF TREES OVER 6" DBH   |   | NO EXISTING TREES ON SITE                         |
| 17.57.070                           | LANDSCAPE SCREENING          | SCREENING TO PROPOSED USE BY SIGHT OBSCURING, EVERGREEN PLANTINGS, SHADE TREES, FENCES OR COMBINATION |   | SIGHT OBSCURING FENCE & SCREENING SHRUBS PROVIDED |
| 17.57.090                           | STREET TREES                 | STREET TREES OVER 40' TALL AND WIDE   | 40' OC  | LARGE STREET TREES PROVIDED. SEE PLANT SCHEDULE   |

- KEY NOTES**
- 1 RAISED SEAT WALL PLANTER
  - 2 BENCH



SEE L1.1 FOR PLANTING SCHEDULE

**LAND USE**

**LANDSCAPE PLAN**  
**NORTON LANDING**  
MCMINNVILLE, OREGON

**Harper Houf Peterson Righellis Inc.**  
ENGINEERS + PLANNERS  
LANDSCAPE ARCHITECTS + SURVEYORS  
250 NW Franklin Avenue, Suite 404, Bend, OR 97703  
phone: 541.318.1161 www.hhp.com

REGISTERED 747  
**PRELIMINARY**  
JEFFERY P. CREEL  
OREGON  
05/13/11  
LANDSCAPE ARCHITECT

|           |            |
|-----------|------------|
| DESIGNED: | MD/JLV     |
| DRAWN:    | MD         |
| CHECKED:  | JLV        |
| DATE:     | 07.21.2023 |

| DATE | NO. | DESCRIPTION |
|------|-----|-------------|
|      |     |             |
|      |     |             |
|      |     |             |
|      |     |             |
|      |     |             |

SHEET NO.  
**L1.2**  
JOB NO. KWD-11

# NORTON LANDING APARTMENTS

SE Norton Lane at SE Stratus Avenue

Planned Development Amendment,  
Application Narrative

August 20, 2023

## **Project Description:**

The proposed project consists of the development of 138 apartment dwelling units located in seven three-story buildings on a 4.93 acre vacant site. The proposed development includes related site improvements and 3/4 public street improvements of SE Norton Lane along the subject site frontage.

The subject site has limited access to SE Norton Lane and has no other public frontage or access. The subject site has access to public utilities. There is an existing shallow 8" public sanitary sewer located in SE Norton Lane. There is an existing public water line located in SE Norton Lane. There is an existing 15-foot private storm drainage easement along the westerly property line. There is also a 10-foot public utility easement along the SE Norton Lane frontage.

The subject site lies within the Three Mile Lane Planned Development Overlay and the Three Mile Lane Area Plan. The subject site is subject to Planned Development Amendment Approval, Three Mile Lane Deign Review, and the conditions of Approval of the prior Ordinances affecting the subject Site, Including Ordinance 4709 and Ordinance 5072.

## **ASSESSOR MAP/TAX LOT**

Section 27, T.4S, R4W, WM, Tax Lot 701

## **ZONING:**

C-3 PD: General Commercial, Planned Development (Three Mile Lane PD Overlay)  
Ordinance 4709  
Ordinance 5072 (PDA 6-18)

Adjacent Property Zoning:

**NORTH:** C-3 PD: General Commercial

**EAST:** M-L: Limited Light Industrial

**SOUTH:** Outside Urban Growth Boundary, Yamhill County

**WEST:** R-4 Medium, High Density Residential

**DEVELOPMENT APPLICATION APPROVALS REQUESTED:**

The following land-use applications are being submitted concurrently for approval:

- Planned Development Amendment to Ordinance 4709 and Ordinance 5072
- Three Mile Lane Design Review
- Landscape Plan Review
- Administrative and Planning Commission Reviewed Variances

**EXISTING CONDITIONS**

The Subject Site is currently undeveloped and used for farming. The site is generally flat, with a minor slope to the southwest. There are no significant or distinguishing natural features associated with this property.

Access to the property is from Highway 18 via the intersection with Norton Lane. There is a signalized intersection of Norton Lane (SE & NE) with Highway 18. Immediately south of the highway, there is a three-way stop intersection of SE Norton Lane and SE Stratus Avenue. Stratus Avenue loops around the Medical Center and intersects back with SE Norton Lane across from the subject site.

**ACCESS and UTILITIES**

The property fronts on west side of SE Norton Lane, just south of the Altimus Plaza medical offices. The existing right-of-way for SE Norton Lane is 60 feet down to and through the Stratus Avenue Loop intersection, south of the Medical Center. The existing improvements extend through this intersection. The street is currently improved with curb and gutter and 28-foot paved section.

SE Norton Lane is improved to City standard south to the intersection with the Stratus Avenue loop. The street improvements extend approximately 180 feet along the frontage of the Subject Site. There is an additional 280 feet of site frontage that is currently not improved.

There are existing urban services and utilities within SE Norton Lane. These facilities are all available and were adequately sized to serve development of the subject site, consistent with the allowed commercial uses identified within Ordinance 4709.

**SURROUNDING USES**

**North** - The property immediately north of the subject property is the Altimus Plaza Medical Offices. Further to the north at the Stratus Avenue intersection are the Comfort Inn & Suites and the Diner restaurant. The land west of the motel is vacant land, however there is a current application for development of multifamily residential.

**East** - The property immediately east of the subject property is developed with medical offices, specifically Willamette Valley Medical Center, including a Heliport. To the south of the medical center is vacant farm land, outside of the city limits and UGB. The Evergreen Aviation Museum, north of Hwy 18, and Municipal Airport, south of Hwy 18, are located just over a mile to the east.

**South** - The property immediately south of the subject property is vacant farm land, outside of the city limits and UGB.

**West** - The property immediately west of the subject property is the Evergreen Estates Mobile Home Park, which is zoned R-4.

**NEIGHBORHOOD MEETING**

A Neighborhood Meeting was held on June 28, 2023. The Neighborhood Meeting Notice was mailed, posted and held in compliance with the requirements of McMinnville Zoning Ordinance, Chapter 17.72.095 Neighborhood Meetings.

The Neighborhood Meeting materials, notes and documentation of compliance with McMinnville Zoning Ordinance, Chapter 17.72.095 Neighborhood Meetings, are included within this application.

**RELATED ORDINANCES, PLANS AND POLICES:**

In addition to compliance of the applicable requirements of the City of McMinnville Zoning Ordinance, there are other related ordinances, plans and policies that the proposed development must demonstrate compliance of their applicable conditions of approval and related policies.

The following narrative provides the applicants response to the following:

- Ordinance 4709, Conditions of Approval
- Ordinance 5072, Conditions of Approval
- McMinnville Comprehensive Plan, Volume II, applicable Goals and Policies
- Three Mile Lane Area Plan, Ordinance 5126, Policies

**Prior Land-Use Decisions Affecting Subject Site:**

**ORDINANCE 4131: THREE MILE LANE**

By Ordinance 4131, the City of McMinnville established Planned Development requirements for the Three Mile Lane area (Oregon Highway 18 Corridor). The area covered by this requirement extends north and south of Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18.

This ordinance established the Three-Mile Lane Planned Development overlay and identified specific conditions and policies for development. The ordinance also identified procedures for review and approval of proposed developments within the subject area.

**ORDINANCE 4709: (CPA 6-99/ZC 11-00)**

By Ordinance 4709, the City of McMinnville amended the Comprehensive Plan Map and rezoned the subject site to C-3 PD (General Commercial Planned Development), subject to 20 Conditions of Approval.

**ORDINANCE 5072: (PDA 6-18)**

By Ordinance 5072, the City of McMinnville amended and revised Condition 15 of Ordinance 4709 to allow multiple family dwellings. The original Condition 15 of Ordinance 4709 specifically limited the allowable uses which did not allow multiple family dwellings.

The revised Condition 15 applies specifically to Tax Lot 701. All other requirements and conditions of approval from Planned Development Ordinance 4709 remain in effect.

**Conditions of Approval – Prior Ordinances:**

**ORDINANCE 4709: CONDITIONS OF APPROVAL:**

1. That the zone change request shall not take effect until and unless CPA 6-99 is approved by City Council.



**Applicant Response:**

*This condition of approval is no longer applicable and Ordinance 4709 was approved by City Council.*

2. That the development and use of the site is subject to the provisions of the Three Mile Lane Planned Development Ordinance, as amended. Further, that a detailed Master Plan for the subject site shall be submitted to the McMinnville Planning Commission for review and approval, pursuant to the procedures of McMinnville Zoning Ordinance 17.51, Planned Development Overlay, prior to any development occurring on the site. The plan shall include, at a minimum, proposed land uses and their locations, building locations, proposed circulation patterns, proposed open spaces, grading and drainage information, location and size of public utilities and services, off-street parking areas, direct pedestrian access, and other information deemed necessary to convey the details of the proposed development plans to the Planning Commission.

**Applicant Response:**

*This current application is for a Planned Development Amendment to include the proposed Master Plan for the site. The application includes the detailed drawings and design as outlined in the above condition of approval. This application also includes the application for the Three Mile Lane Design Review.*

3. That detailed plans for the proposed commercial development showing site layout, proposed circulation pattern, signage, building elevations, landscaping, parking, and lighting must be submitted to and approved by the Three Mile Lane Design Review Committee prior to issuance of any building permits for said development. Approval or denial of such plans shall be based upon findings that, to the extent possible, the building and site design employs principles that will ensure compatibility with adjacent development, and provide an architectural style appropriate to a "gateway" or entrance to the City of McMinnville. In addition, approval or denial of the exterior building design shall be based on a finding that, to the extent possible, the building design incorporates design and architectural features that would serve to break up the building's horizontal plane and provide visual interest. This may include, but is not limited to, the use of vertical columns, gables, variety of compatible and complementary building materials, providing openings in the building façade, and landscaping at the building perimeter. Submitted plans should include detail as regard to building colors and materials (provide texture and relief), building height, planting design, window treatment, vertical and horizontal articulation, massing, voids to solids relationships, and other elements appropriate to ensure that the building and site design complies with the objectives and requirements of this planned development approval.

The provisions of Chapter 17.51 of the McMinnville Zoning Ordinance may be used to place conditions of any development and to determine whether or not specific uses are permissible. The applicant may appeal the decisions of the Three Mile Lane Design Review Committee to the Planning Commission if notice of the appeal is filed in the Planning Commission office within 15 days of the Committee's decision.

**Applicant Response:**

*This application is for multifamily housing and is not a commercial proposal. Therefore this condition of approval is not applicable.*

4. That final development plans include landscape plans submitted to and approved by the McMinnville Landscape Review Committee and Three Mile Lane Design Review Committee. A minimum of 14 percent of the site must be landscaped. An arborvitae hedge or similar type of planted visual screen shall be required along the property lines adjacent to the residentially zoned land to the west. Landscaping emphasis shall exist along the site's eastern edge, adjacent to Norton lane, including required street trees, with particular emphasis at the site's eastern driveway intersections at Norton Lane. In addition, landscape islands are required to be located throughout the proposed off-street parking areas.

Street trees within a curbside planting strip along the Norton Lane frontage are required to have a two-inch minimum caliper, exhibit size and growing characteristics appropriate for the particular planting strip and be spaced as appropriate for the selected species and as may be required for the locations of above ground utility vaults, transformers, light poles, and hydrants. All street trees shall be of good quality and shall conform to American Standards for Nursery Stock (ANSI Z60.1). The Planning Director reserves the right to reject any plant material which does not meet this standard.

**Applicant Response:**

*This proposed development and application includes the Landscape Plan Review application. The landscape design for the site includes the required screening and buffering to the adjacent properties along the North, West and South property lines. There are also enhanced plantings along the SE Norton Lane frontage.*

*The proposed development application also includes the SE Norton Lane street frontage improvements. These include the required street trees. The proposed street trees meet the specified requirements.*

5. That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private, rather than public, and private maintenance agreements must be approved by the City for them.

**Applicant Response:**

*This application includes a Preliminary Grading and Drainage Plan, drawing C2.0. It is understood that complete drainage design must be provided in accordance with the City of McMinnville Storm Drainage Master Plan. Detailed drainage design and stormwater analysis will be provided for permit submittal to the City Engineering Department.*

6. The final development plans for the subject site include a detailed sanitary sewerage collection plan which incorporates the requirements of City's Collection Systems Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the structures located within the subject site are required to connect to the sanitary sewer systems as soon as service is available.

**Applicant Response:**

*This application includes a Preliminary Utilities Plan, drawing C3.0. It is understood that complete sanitary sewerage design must be provided in accordance with the City of McMinnville Collection Systems Plan. Detailed sanitary sewerage design and analysis will be provided for permit submittal to the City Engineering Department.*

7. That the developer secure from the Department of Environmental Quality (DEQ) applicable storm runoff and site development permits prior to construction of the required site improvements. Evidence of such permit shall be submitted to the City Engineer.

**Applicant Response:**

*It is understood that the owner/developer must secure the applicable Department of Environmental Quality (DEQ) stormwater runoff permits. Evidence of the permits will be submitted to the City Engineer as required.*

8. That the developer enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and grading permit for a lot fill agreement from the City Building Division. All fill placed in the areas where construction is expected shall be engineered and shall meet with approval of the City Building Division and the City Engineering Department.

**Applicant Response:**

*It is understood that the owner/developer must enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and grading permit for a lot fill agreement from the City Building Division. It is also understood that all fill placed in the areas where construction is expected shall be engineered and shall meet with approval of the City Building Division and the City Engineering Department.*

9. That the developer extend water and power services to the subject site in accordance with McMinnville Water and Light requirements, including any necessary contracts and/or easements. Said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.

**Applicant Response:**

*It is understood that the owner/developer extend water and power services to the subject site, if required by McMinnville Water and Light. The required improvements, if any, will be designed and installed to the requirements of McMinnville Water and Light. Although, there is an existing public water main located within the NE Norton Lane right-of-way and power currently serves the adjacent property to the north.*

*It is further understood that said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.*

10. That utilities shall be extended to the property boundaries by the applicant, as may be required by the City Engineer or McMinnville Water and Light.

**Applicant Response:**

*It is understood that McMinnville Water and Light may require utility extensions to the subject site, if not currently provided. The required improvements, if any, will be designed and installed to the requirements of McMinnville Water and Light. Although, there is an existing public water main located within the NE Norton Lane right-of-way in front of the subject site and power currently serves the adjacent property to the north.*

11. That no building shall exceed the height of 35 feet.

**Applicant Response:**

*The Applicant is proposing a Variance through the Planning Commission Authority to request approval to allow a roof height of 37 feet to the highest ridge line. The detailed response to the requested Variance is included within the Variance Application Narrative.*

12. That if outside lighting is provided, it must be directed down and away from residential areas and public streets.

**Applicant Response:**

*The proposed site lighting design is included in this application, drawing ME1.0. The proposed lighting design provides full cut-off light fixture that prevent light from entering the adjacent properties and the lighting design does not shine light onto the SE Norton Lane right-of-way.*

13. That signs located on the site shall be subject to the requirements of McMinnville Ordinance 4572 (B).

**Applicant Response:**

*It is understood that any proposed signage must be designed to be in compliance with the City of McMinnville Ordinance 4572 (B). A small site identification sign is proposed and the location is noted on the Site Plan, drawing A1.0 and A1.1.*

14. That all business, storage, or displays shall be conducted wholly within an enclosed building; except for off-street parking and loading.

**Applicant Response:**

*The proposed development is for multifamily housing and this condition of approval is not applicable.*

15. That the subject site is limited to professional office use or medical office use, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use or medical office use may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site."

**Applicant Response:**

*This condition of approval has been amended to include multifamily housing. The revised condition was included in Ordinance 5072 (PDA 6-18). A respond to the amended condition of approval follows in the response to address Ordinance 5072 below.*

16. That if restrictive covenants are proposed for the development they must meet with the approval of the Planning Director.

**Applicant Response:**

*There are no restrictive covenants proposed with this development application. This condition of approval is not applicable.*

17. That the approved Master Plan as approved by the Planning Commission shall be placed on file with the Planning Department and become part of the zone and binding to the owner and developer. The developer will be responsible for requesting permission of the Planning Commission for any major changes of the details of the final development plans. Minor changes to the details of the adopted plan may be approved by the City Planning Director. It shall be the Planning Director's decision as to what constitutes a minor or major change. An appeal from a ruling by him may be made only to the Commission. Review of the Planning Director's decision by the Planning Commission may be initiated at the request of any one of the commissioners.

**Applicant Response:**

*The requirements of this condition of approval are understood by the developer/owner.*

18. That improvements to the Norton Lane frontage, to the southern most entry drive, or as may otherwise be required by the City of McMinnville, shall be done at the developer's expense and be finalized prior to release of any occupancy permits. Plans for improvement to Norton Lane shall be submitted to the City Engineer for review and approval prior to its construction.

**Applicant Response:**

*It is understood that SE Norton Lane frontage improvements are required for the proposed development and that those improvements will be at the owner/developers expense. It is further understood that the design of the proposed SE Norton Lane improvements shall be submitted to the City engineer for review and approval. It is also understood that the frontage improvements must be completed before any occupancy permits will be issued.*

19. The developer/owner of the subject site must grant to the Willamette Valley Medical Center and the City of McMinnville the right to cause all airspace above the surface of the subject site such as noise, vibrations, fumes, dust, fuel particles and all other effects as may be caused by the operations of aircraft landing at or taking off or operating at or on the Medical Center property and the McMinnville Municipal Airport. The owner must fully waive any right or cause action which he may now or in the future arise against the Willamette Valley Medical Center and the City of McMinnville due to such circumstances.

**Applicant Response:**

*The owner/developer understands the requirements of this condition of approval.*



20. That prior to the development of the site, the applicant shall sign a waiver of remonstrance against the future development of Norton Lane. The waiver shall be prepared by the City.

**Applicant Response:**

*The owner/developer understands the requirements of this condition of approval.*

**ORDINANCE 5072: CONDITIONS OF APPROVAL:**

1. That Condition 15 of Ordinance 4709, as amended by Ordinance 4863, be amended as follows:

That the subject site is limited to professional office use or medical office use, or multiple family dwellings, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, multiple family dwellings, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.

If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped."

**Applicant Response:**

*The proposed development is for 138 units of multiple family dwellings (apartments) and is therefore a complying use.*

*The subject site is 214, 759 sf. The proposed Site Plan provides 32,769 sf of common open space, which is 15% of the gross site area and is therefore compliant.*

2. All other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect.

**Applicant Response:**

*The response to the remaining Conditions of Approval of Ordinance 4709 has been addressed above in the response to Ordinance 4709.*

## **McMinnville Comprehensive Plan: VOLUME II GOALS AND POLICIES:**

Volume II, Goals and Policies, contains the goal, policy, and proposal statements which shall be applied to all land use decisions. Goal, policy, and proposal statements each have different purposes: goal statements are the most general principles; policy statements are directed to specific areas to further define the goal statements; and proposals are possible courses of action open to the City which shall be examined to further implement the goal and policy requirements. Each of these statement types is further defined below:

**GOALS:** are the broadly-based statements intended to set forth the general principles on which all future land use decisions will be made. Goals carry the full force of the authority of the City of McMinnville and are therefore mandated.

**POLICIES:** are the more precise and limited statements intended to further define the goals. These statements also carry the full force of the authority of the City of McMinnville and are therefore mandated.

**PROPOSALS:** are the possible courses of action available to the City to implement the goals and policies. These proposals are not mandated; however, examination of the proposals shall be undertaken in relation to all applicable land use requests.

The implementation of these goals, policy, and proposal statements shall occur in one of two ways. First, the specific goal, policy, or proposal shall be applied to a land use decision as a criterion for approval, denial, or modification of the proposed request. In this case the goal, the policy, or the proposal is directly applied. The second method for implementing these statements is through the application of provisions and regulations in ordinances and measures created to carry out the goals and policies. This method involves the indirect application of the statements. These ordinances and measures are included in Volume III of the McMinnville Comprehensive Plan.

## **CHAPTER V: HOUSING AND RESIDENTIAL DEVELOPMENT**

### **GOAL V 1: TO PROMOTE DEVELOPMENT OF AFFORDABLE, QUALITY HOUSING FOR ALL CITY RESIDENTS.**

#### General Housing Policies:

- 58.00 City land development ordinances shall provide opportunities for development of a variety of housing types and densities.

***Applicant Response:***

*The proposed development provides multifamily housing that has been designed to be in compliance with the applicable sections of the City of McMinnville Zoning Ordinance and applicable ordinances within the planned development area of the subject site.*

- 59.00 Opportunities for multiple dwelling and mobile home developments shall be provided in McMinnville to encourage lower-cost renter and owner-occupied housing. Such housing shall be located and developed according to the residential policies in this plan and the land development regulations of the City.

**Applicant Response:**

*The proposed multifamily development has been designed to be in compliance with the applicable sections of the City of McMinnville Zoning Ordinance, including the Residential Design Standards for apartments, 17.11.090 and the applicable ordinances within the planned development area of the subject site.*

Low-Cost Housing Policies:

- 64.00 The City of McMinnville shall work in cooperation with other governmental agencies, including the Mid-Willamette Valley Council of Governments and the Yamhill County Housing Authority, and private groups to determine housing needs, provide better housing opportunities and improve housing conditions for low- and moderate-income families.
- 65.00 The City of McMinnville shall coordinate with the Mid-Willamette Valley Council of Governments to develop a "fair share" plan to allocate low-cost housing throughout the tri-county region.
- 66.00 The City of McMinnville shall continue to allow development of its fair share of the region's low-cost housing. The share accepted will be based on quantifiable studies which take into account the amount of the low-cost housing already in the community and the overall housing opportunities in the city and region.
- 67.00 Subsidized low-cost housing shall be dispersed throughout the McMinnville urban area. Dispersal plans shall be coordinated with appropriate agencies.

**Applicant Response:**

*The proposed development is for market-rate multifamily housing and is not proposed to be a low-income or subsidized development.*

**GOAL V 2: TO PROMOTE A RESIDENTIAL DEVELOPMENT PATTERN THAT IS LAND INTENSIVE AND ENERGY-EFFICIENT, THAT PROVIDES FOR AN URBAN LEVEL OF PUBLIC AND PRIVATE SERVICES, AND THAT ALLOWS UNIQUE AND INNOVATIVE DEVELOPMENT TECHNIQUES TO BE EMPLOYED IN RESIDENTIAL DESIGNS.**

Policies:

- 68.00 The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center, to designated neighborhood activity centers, and to those areas where urban services are already available before committing alternate areas to residential use. (Ord. 5098, December 8, 2020)

**Applicant Response:**

*The subject site is designated C-3 Commercial and has been identified for Medium-High Density Residential under the Three Mile Lane Planned Development Amendment to Ordinance 4709.*

*The subject site has existing urban services available to the site.*

**Planned Development Policies:**

72.00 Planned developments shall be encouraged as a favored form of residential development as long as social, economic, and environmental savings will accrue to the residents of the development and the city.

**Applicant Response:**

*The subject site is located within the Three Miler Lane Planned Development Overlay. The proposed development has been designed to be in compliance with the applicable ordinances within the Three Mile Lane PD overly including Ordinance 4709 and Ordinance 5072.*

74.00 Distinctive natural, topographic, and aesthetic features within planned developments shall be retained in all development designs.

**Applicant Response:**

*The subject site does not contain any distinctive natural or topographic features.*

75.00 Common open space in residential planned developments shall be designed to directly benefit the future residents of the developments. When the open space is not dedicated to or accepted by the City, a mechanism such as a homeowners association, assessment district, or escrow fund will be required to maintain the common area.

**Applicant Response:**

*The proposed Site Plan provides Common Open Spaces in compliance with 17.11.090. The proposed Common Open Spaces occur within the private market-rate apartment development and will be maintained by the development ownership.*

76.00 Parks, recreation facilities, and community centers within planned developments shall be located in areas readily accessible to all occupants.

**Applicant Response:**

*The proposed Site Plan provides Common Open for recreational use by the apartment residents. There are no other recreation facilities or community centers proposed within this development.*

77.00 The internal traffic system in planned developments shall be designed to promote safe and efficient traffic flow and give full consideration to providing pedestrian and bicycle pathways.

**Applicant Response:**

*The proposed development and Site Plan is limited to a single 4.93 acre site and does not include any internal public streets. The proposed Site Plan does include internal drive aisles and parking areas that will provide safe and efficient access to parking and the buildings throughout the site. There is a network of internal sidewalks and pathways that connect the parking, buildings and common open spaces to the public street frontage.*

78.00 Traffic systems within planned developments shall be designed to be compatible with the circulation patterns of adjoining properties.

**Applicant Response:**

*The subject site has limited public street frontage and is limited to a single driveway onto SE Norton Lane.*

**Multiple Dwelling Development Policies:**

- 86.00 Dispersal of new multi-dwelling housing development will be encouraged throughout the City in areas designated for residential and mixed-use development to encourage a variety of housing types throughout the community and to avoid an undue concentration of multi dwelling development in specific areas of the community leading to a segregation of multi dwelling development in McMinnville from residential neighborhoods. Dispersal policies will be consistent with the Great Neighborhood Principles.

In areas where there are the amenities, services, infrastructure and public facilities to support a higher density of multi-dwelling development, and the area is commensurate with a higher concentration of multi-dwelling development without creating an unintended segregation of multi-dwelling development, such as McMinnville's downtown, the area surrounding Linfield University and Neighborhood Activity Centers, a higher concentration of multi-dwelling development will be encouraged. (Ord. 5098, December 8, 2020)

**Applicant Response:**

*As provided for by the adoption of Ordinance 4709, amended by Ordinance 5072, the subject site is located in an area of McMinnville that has the existing infrastructure, public facilities to support the density of the proposed development. The proposed development will provide needed housing for the support staff of the adjacent medical facilities and the Willamette Valley medical Center Hospital, as well as other nearby employment centers.*

- 87.00 Residential developments at densities beyond that normally allowed in the multiple-dwelling zone shall be allowed in the core area subject to review by the City. These developments will be encouraged for (but not limited to) the provision of housing for the elderly.

**Applicant Response:**

*The proposed development is within the allowable density for the subject site. The subject site is not located within the core area and this does not apply.*

- 88.00 Deleted as per Ord. 4796, October 14, 2003.

- 89.00 Zoning standards shall require that all multiple-dwelling housing developments provide landscaped grounds. (Ord. 4796, October 14, 2003)

**Applicant Response:**

*The proposed development includes detailed Landscape Plans that have been designed for review in conjunction with all aspects of the McMinnville Zoning Codes. Multiple locations with Zoning Code require landscaping, including the 17.11 the Residential Design Standards. The proposed landscape design provides landscaping throughout all areas of the site not covered by buildings, parking or drive aisles.*



- 90.00 Greater residential densities shall be encouraged to locate along major and minor arterials, within one-quarter mile from neighborhood and general commercial shopping centers or within neighborhood activity centers, and within a one-half mile wide corridor centered on existing or planned public transit routes. (Ord. 5098, December 8, 2020; Ord. 4840, January 11, 2006; Ord. 4796, October 14, 2003)

**Applicant Response:**

*The subject site is located on SE Norton Lane at the intersection of SE Stratus Avenue. SE Norton Lane, which is classified as a minor collector, provides direct access to Highway 18, which is classified as a major arterial. This network of roadways provides adequate access for vehicular traffic serving the proposed development.*

*Yamhill County Transit, Route 2, serves SE Norton Lane with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave.*

- 91.00 Multiple dwelling housing developments, including condominiums, but excluding campus living quarters, shall be required to access off of arterials or collectors or streets determined by the City to have sufficient traffic carrying capacities to accommodate the proposed development. (Ord. 4573, November 8, 1994)

**Applicant Response:**

*The subject site and proposed development has access from SE Norton Lane which is a minor collector. Norton Lane connects directly to Highway 18 that is a major arterial. The existing network of public roadways has sufficient carry capacity for the traffic generated by the proposed development.*

*A Transportation Impact Study has been prepared and is included in this application.*

- 92.00 High-density housing developments shall be encouraged to locate along existing or potential public transit routes.

**Applicant Response:**

*The subject site is located along an existing public transit route, Yamhill County Transit, Route 2, with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave.*

- 92.01 High-density housing shall not be located in undesirable places such as near railroad lines, heavy industrial uses, or other potential nuisance areas unless design factors are included to buffer the development from the incompatible use.

**Applicant Response:**

*The subject site is not located near railroad lines or heavy industrial uses.*

- 92.02 High-density housing developments shall, as far as possible, locate within reasonable walking distance to shopping, schools, and parks, or have access, if possible, to public transportation. (Ord. 4796, October 14, 2003)

**Applicant Response:**

*The subject site is located along an existing public transit route, Yamhill County Transit, Route 2, with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave. The subject site is not located near schools, shopping or parks.*

- 92.03 Housing developments for the elderly shall, as far as possible, locate near community centers, parks, and shopping areas, or where transportation services can be provided to enable access to these activity areas. (Ord. 4796, October 14, 2003)

**Applicant Response:**

*The proposed project is not an Elderly Housing development.*

**Urban Policies:**

- 99.00 An adequate level of urban services shall be provided prior to or concurrent with all proposed residential development, as specified in the acknowledged Public Facilities Plan. Services shall include, but not be limited to:
1. Sanitary sewer collection and disposal lines. Adequate municipal waste treatment plant capacities must be available.
  2. Storm sewer and drainage facilities (as required).
  3. Streets within the development and providing access to the development, improved to city standards (as required).
  4. Municipal water distribution facilities and adequate water supplies (as determined by City Water and Light). (as amended by Ord. 4796, October 14, 2003)

**Applicant Response:**

*The subject site has existing water and sanitary sewer lines located within the SE Norton Lane right-of-way. The existing sanitary sewer is an 8" main, which has adequate capacity for the proposed development.*

*There is an existing 10" water main located in the SE Norton Lane right-of-way. This line has adequate capacity for domestic and fire line services for the proposed development and additional new fire hydrants.*

*The subject site is accessed by SE Norton Lane, which currently terminates just south of the intersections with SE Stratus Avenue. The proposed development will provide public street frontage improvements along the SE Norton Lane right-of-way to the southern boundary of the site and the current McMinnville City limits.*

*There are no public streets proposed or required within the proposed development. The Site Plan provides a network of drive aisles and parking areas serving the buildings and living units. A Fire Truck turnaround has been provided at the rear of the site.*

**Urbanization, Great Neighborhood Principles:**

- 187.10 The City of McMinnville shall establish Great Neighborhood Principles to guide the land use patterns, design, and development of the places that McMinnville citizens live, work, and play. The Great Neighborhood Principles will ensure that all developed places include characteristics and elements that create a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood with enduring value, whether that place is a completely new development or a redevelopment or infill project within an existing built area.
- 187.20 The Great Neighborhood Principles shall encompass a wide range of characteristics and elements, but those characteristics and elements will not function independently. The Great Neighborhood Principles shall be applied together as an integrated and assembled approach to neighborhood design and development to create a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood, and to create a neighborhood that supports today's technology and infrastructure, and can accommodate future technology and infrastructure.
- 187.30 The Great Neighborhood Principles shall be applied in all areas of the city to ensure equitable access to a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood for all McMinnville citizens.
- 187.40 The Great Neighborhood Principles shall guide long range planning efforts including, but not limited to, master plans, small area plans, and annexation requests. The Great Neighborhood Principles shall also guide applicable current land use and development applications.
- 187.50 The McMinnville Great Neighborhood Principles are provided below. Each Great Neighborhood Principle is identified by number below (numbers 1 – 13), and is followed by more specific direction on how to achieve each individual principle.

1. Natural Feature Preservation. Great Neighborhoods are sensitive to the natural conditions and features of the land.
  - a. Neighborhoods shall be designed to preserve significant natural features including, but not limited to, watercourses, sensitive lands, steep slopes, wetlands, wooded areas, and landmark trees.

***Applicant Response:***

*The existing subject site is vacant agricultural land and the no existing natural features or trees present.*

2. Scenic Views. Great Neighborhoods preserve scenic views in areas that everyone can access.
  - a. Public and private open spaces and streets shall be located and oriented to capture and preserve scenic views, including, but not limited to, views of significant natural features, landscapes, vistas, skylines, and other important features.

**Applicant Response:**

*The existing subject site does not have any scenic views or existing natural features. The proposed site design does create large open space areas within the subject site for residents to recreate.*

3. Parks and Open Spaces. Great Neighborhoods have open and recreational spaces to walk, play, gather, and commune as a neighborhood.
  - a. Parks, trails, and open spaces shall be provided at a size and scale that is variable based on the size of the proposed development and the number of dwelling units.
  - b. Central parks and plazas shall be used to create public gathering spaces where appropriate.
  - c. Neighborhood and community parks shall be developed in appropriate locations consistent with the policies in the Parks Master Plan.

**Applicant Response:**

*The proposed site plan creates a variety of open space types for the residents. There are large open spaces, space courtyard style gathering and sitting areas and picnic and barbeque areas. There is a network of onsite pathways that connect the building entries, parking areas and the public street frontage to the open spaces throughout the site.*

4. Pedestrian Friendly. Great Neighborhoods are pedestrian friendly for people of all ages and abilities.
  - a. Neighborhoods shall include a pedestrian network that provides for a safe and enjoyable pedestrian experience, and that encourages walking for a variety of reasons including, but not limited to, health, transportation, recreation, and social interaction.
  - b. Pedestrian connections shall be provided to commercial areas, schools, community facilities, parks, trails, and open spaces, and shall also be provided between streets that are disconnected (such as cul-de-sacs or blocks with lengths greater than 400 feet).

**Applicant Response:**

*The subject site provides a network of walkways and pathways throughout the site connecting the living units to the open spaces, parking areas and the public street frontage. The subject site is also served by Yamhill County Transit with a transit stop at the corner of SE Norton Land and SE Stratus Avenue.*

*The network of pedestrian walkways and pathways connect to all existing adjacent public sidewalks and street frontage. There are not existing adjacent trails.*

5. Bike Friendly. Great Neighborhoods are bike friendly for people of all ages and abilities.

- a. Neighborhoods shall include a bike network that provides for a safe and enjoyable biking experience, and that encourages an increased use of bikes by people of all abilities for a variety of reasons, including, but not limited to, health, transportation, and recreation.
- b. Bike connections shall be provided to commercial areas, schools, community facilities, parks, trails, and open spaces.

**Applicant Response:**

*The proposed street frontage improvements along the SE Norton Lane Frontage includes a bike lane.*

- 6. Connected Streets. Great Neighborhoods have interconnected streets that provide safe travel route options, increased connectivity between places and destinations, and easy pedestrian and bike use.
  - a. Streets shall be designed to function and connect with the surrounding built environment and the existing and future street network, and shall incorporate human scale elements including, but not limited to, Complete Streets features as defined in the Comprehensive Plan, grid street networks, neighborhood traffic management techniques, traffic calming, and safety enhancements.
  - b. Streets shall be designed to encourage more bicycle, pedestrian and transit mobility with a goal of less reliance on vehicular mobility.

**Applicant Response:**

*The subject site has limited street frontage. There are no additional current or future rights of way adjacent the site.*

- 7. Accessibility. Great Neighborhoods are designed to be accessible and allow for ease of use for people of all ages and abilities.
  - a. To the best extent possible all features within a neighborhood shall be designed to be accessible and feature elements and principles of Universal Design.
  - b. Design practices should strive for best practices and not minimum practices.

**Applicant Response:**

*The proposed on-site network of walkways and pathways will be designed and built to be fully accessible to all residents of the proposed development and the surrounding community.*

- 8. Human Scale Design. Great Neighborhoods have buildings and spaces that are designed to be comfortable at a human scale and that foster human interaction within the built environment.
  - a. The size, form, and proportionality of development is designed to function and be balanced with the existing built environment.
  - b. Buildings include design elements that promote inclusion and interaction with the right-of-way and public spaces, including, but not limited to, building orientation towards the street or a public space and placement of vehicle-oriented uses in less prominent locations.



- c. Public spaces include design elements that promote comfortability and ease of use at a human scale, including, but not limited to, street trees, landscaping, lighted public areas, and principles of Crime Prevention through Environmental Design (CPTED).

**Applicant Response:**

*The proposed site and building design provides a variety of scales for the buildings and usable open spaces. The building forms are heavily articulated to reduce mass and scale. The street frontage building are oriented along the street frontage and provide direct access to the building entries. The on-site parking areas are screened from the public street frontage and the buildings are oriented around the common open spaces within the site.*

- 9. Mix of Activities. Great Neighborhoods provide easy and convenient access to many of the destinations, activities, and local services that residents use on a daily basis.
  - a. Neighborhood destinations including, but not limited to, neighborhood-serving commercial uses, schools, parks, and other community services, shall be provided in locations that are easily accessible to surrounding residential uses.
  - b. Neighborhood-serving commercial uses are integrated into the built environment at a scale that is appropriate with the surrounding area.
  - c. Neighborhoods are designed such that owning a vehicle can be optional.

**Applicant Response:**

*The existing subject site is vacant is located along the existing of the current urban growth boundary and the McMinnville city limits. The subject site is near the Highway 18 and Norton Lane intersections, and adjacent to the Yamhill County Transit bus line ant stop. A large employment center, Willamette Valley Medical Center across SE Norton Lane from the subject site.*

- 10. Urban-Rural Interface. Great Neighborhoods complement adjacent rural areas and transition between urban and rural uses.
  - a. Buffers or transitions in the scale of uses, buildings, or lots shall be provided on urban lands adjacent to rural lands to ensure compatibility.

**Applicant Response:**

*The subject site is located adjacent the current McMinnville City Limits and the Urban Growth Boundary. The scale and mass of the proposed development serves as a transition from the higher density larger scale buildings adjacent to the site, to the agricultural lands outside the Urban Growth Boundary that is adjacent to the site.*

*The proposed site plan does not place buildings adjacent the southerly property line to the south. This "steps down" the massing of the site to the UGB. The proposed site plan places the parking area adjacent to the UGB to soften the transition. Parking is proposed*

*within 6 feet of this property line. A six-foot sight obscuring fence and landscape screening and buffering with continuous shrubs and trees are proposed along this boundary to mitigate the proximity of the parking to the UGB.*

11. Housing for Diverse Incomes and Generations. Great Neighborhoods provide housing opportunities for people and families with a wide range of incomes, and for people and families in all stages of life.
  - a. A range of housing forms and types shall be provided and integrated into neighborhoods to provide for housing choice at different income levels and for different generations.

**Applicant Response:**

*The proposed development is for market rate housing and will be located adjacent to the proposed low-income housing project, Status Village. It is anticipated that the residents of both projects will represent a wide diversity of residents from young people just leaving home to live on their own, young families with small children, middle class working families and senior citizens.*

12. Housing Variety. Great Neighborhoods have a variety of building forms and architectural variety to avoid monoculture design.
  - a. Neighborhoods shall have several different housing types.
  - b. Similar housing types, when immediately adjacent to one another, shall provide variety in building form and design.

**Applicant Response:**

*The subject site is located adjacent to a variety of uses and subsequent building scales and building forms. The proposed development is for needed multi-family housing apartment units. The proposed development is for market rate housing and will be located adjacent to the proposed low-income housing project, Status Village. This accomplishes the Great Neighborhood Principle of providing a variety of housing types within the subject neighborhood.*

13. Unique and Integrated Design Elements. Great Neighborhoods have unique features, designs, and focal points to create neighborhood character and identity. Neighborhoods shall be encouraged to have:
  - a. Environmentally friendly construction techniques, green infrastructure systems, and energy efficiency incorporated into the built environment.
  - b. Opportunities for public art provided in private and public spaces.
  - c. Neighborhood elements and features including, but not limited to, signs, benches, park shelters, street lights, bike racks, banners, landscaping, paved surfaces, and fences, with a consistent and integrated design that are unique to and define the neighborhood.

# McMinnville Zoning Code, Title 17: Compliance to Applicable Chapters

## CHAPTER 17.11 RESIDENTIAL DESIGN AND DEVELOPMENT STANDARDS

### 17.11.090 Apartments.

Apartments are a type of attached housing within single-story or multi-story buildings. Apartment dwelling units may share common walls, ceilings, or floors.

#### A. Characteristics.

1. Site Sizes: Single walk-ups, block apartments, and many courtyard apartments can fit on a 100 x 100 foot lot. Bigger developments with multiple walk-up buildings may be as large as 250,000 square feet, or 500 x 500 foot lots.

**Applicant Response:**

*The subject site is 214,759 sf and within the range identified for walk-up apartments and is therefore compliant.*

2. Height Range: Apartment heights vary depending on the type and the location.

**Applicant Response:**

*The proposed development is for three-story apartment buildings which is fully compatible with the existing and proposed adjacent development.*

3. Density Ranges: Apartment densities vary depending on building type and site design layout.

**Applicant Response:**

*The proposed development has a density of just under 28 units per acre which is typical of "Walk-up Apartments".*

#### B. Types of Apartments.

##### 2. Walk-Up Apartments.

- a. Description: Buildings are limited to three stories, and consist of about four to 12 units each, accessible from a single open-air stairwell. Dwelling units are typically constructed in Type V frame construction with fire sprinklers. Individual apartment buildings are arranged around common open space and shared parking areas.

**Applicant Response:**

*The proposed development is 12-unit to 24-unit three-story buildings with open-air stairways that provide access to the living units. The buildings are proposed to be Type V-B wood-framed construction with fire sprinklers. The buildings are located adjacent to several common open spaces.*

- b. Appropriate Context: Walk-up apartments are appropriate adjacent to or within a single dwelling neighborhood depending on site design, orientation to the street, location of parking, and the massing and scale of buildings.

**Applicant Response:**

The subject site and proposed development is located within a C-3 Commercial zoned area to the North, an R-4 Medium, High Density parcel adjacent on the West side and the M-L Limited Light Industrial zone to the East. The existing neighborhood has a variety of building scales from the multi-story Willamette Valley Medical Center to the East, single story Medical Offices and a three-story motel to the North.

- c. Also Named: Woody Walk-Ups, Single Stair Walk-Ups

**Applicant Response:**

The proposed development is also referred to as a “woody walkup” and provides open air stairways to the upper living units.

- d. Variations: May have an internal stair. Generally, in this case, the maximum number of units per floor are four. They can be designed with front and back windows for cross ventilation. Buildings can be separated to offer access to light and air on three sides.

**Applicant Response:**

The proposed building design provides open air stairways on each side of the building to serve the upper floor units. The stairways serve as access to two ground floor and four upper floor units per stairway.

- e. Lot Sizes: Vary widely, from 10,000 to 250,000 square feet

**Applicant Response:**

The subject site is 214,759 sf which is within the identified range.

- f. Density Range: 15 - 30 units per acre. (Note, maximum density will be governed by McMinnville's municipal code.)

**Applicant Response:**

The proposed density is just under 28 units per acre which is within the identified allowable density range.

- g. Building Height: Usually 3 stories; can be 2 stories. (Note, maximum height will be governed by McMinnville's Municipal Code.)

**Applicant Response:**

The proposed building height is three-stories.

- h. Construction Type and Building Code Issues: Typically Type V frame construction. Sprinklers for fire suppression are required.

**Applicant Response:**

The proposed Construction Type is Type V-b, non-rated with NFPA 13-R Fire sprinklers.

C. Development Standards. The applicable development standards are as follows:

**Applicant Response:**

|                  |                |                           |
|------------------|----------------|---------------------------|
| <u>Required:</u> |                | <u>Proposed:</u>          |
| Lot Width:       | 50 feet min.,  | Subject Site: 320.20 feet |
| Lot Depth:       | 100 feet min.  | Subject Site: 670.96 feet |
| Lot Size:        | 5,000 sf Min., | Subject Site: 214,759 sf. |
| Front Setback:   | 15 feet min.,  | Proposed: 15 feet         |
| Side Setback:    | 10 feet min,   | Proposed: 10 feet min.    |
| Rear Setback:    | 20 feet min.,  | Proposed: 79'-6"          |
| Building Height: |                | Proposed: 36'-4"          |

D. Design Standards. The Apartment Design Standards for multi-dwelling housing are standards that apply to apartment housing types. These standards are related to site design and building frontage, parking, compatibility with neighboring homes, open space, and private space.

1. Context and Site Design. Site design standards are intended to facilitate the development of attractive multi-dwelling housing. They encourage good site and building design, which contributes to livability, safety, and sustainability; helps create a stronger community, and fosters a quality environment for residents and neighbors.
  - a. Mirror the scale of blocks and the block-like structure of the surrounding neighborhood.

**Applicant Response:**

*The existing surrounding neighborhood has a variety of building sizes, scales and heights that range from mobile homes to the west, large multi-story hospital to the east, large single story medical office building to the north and a three-story hotel to the north. The proposed Site Plan provides a variety of three-story buildings ranging from 12 living units to 24 living units. There are portions of three buildings that have single story and two-story pop-outs providing additional scale and variety.*

- b. Connect the internal network of streets and paths to those of the surrounding area where possible.

**Applicant Response:**

*Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive aisles provide access to the parking areas and buildings. There is a network of pathways and sidewalks that connect the parking areas, opens spaces and building entrances.*

- c. Configure apartments, parking areas, and common open space in clusters that mirror the scale of blocks of the surrounding neighborhood or are no more than 10,000 square feet in area per cluster.

**Applicant Response:**

*The neighborhood immediately adjacent to the subject site consists of large blocks, large sites and large scale buildings. The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters. This proposed Site Plan is compatible with existing adjacent development and provide smaller "clusters" of development.*



*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.*

*Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.*

*Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.*

- d. Residential units must be oriented to a common open space, including a common green, a plaza, or a pocket park.

**Applicant Response:**

*The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The buildings on the proposed Site Plan are all oriented to common open spaces. The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provide the areas of each.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. Buildings A and B are directly adjacent to common open space. Building A is adjacent Open Space 01, Building B is adjacent Open Space 02.*

*Buildings C and D are grouped in a cluster and are oriented around Common Open Space 03.*

*Buildings E, F and G are also grouped in a cluster oriented around Common Open Space 04. There is also a Courtyard Open Space 05 between Building E and Building G.*

- e. Orient all buildings around a shared open space that meets the requirements of a Common Open Space.

**Applicant Response:**

*The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provides the areas of each space. There are a variety of uses proposed for the Common Open Spaces. These include both passive and active Open Space.*

*There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.*

- f. Align buildings to surrounding streets.

**Applicant Response:**

*There is only one public street, SE Norton Lane, that abuts the subject site. There are two buildings that are adjacent to the SE Norton Lane frontage, Building A and Building B. These two buildings are aligned, on the long building axis, along the street frontage.*

- g. Connect to surrounding neighborhoods, schools, parks, and other neighborhood destinations.

**Applicant Response:**

*The subject site has limited street frontage and the adjacent properties do not have existing site circulation systems to connect to. The existing sidewalk along SE Norton Lane will be extended with the public road extension. The proposed Site Plan provides several connections from the internal site circulation system to the street frontage and new public sidewalk. There are sidewalks along the norther property boundary and the southern property boundary that will provide opportunities for connections to future development of the adjacent properties.*

2. Large Site Design Requirements.

1. Break up parking into smaller areas and access from side streets when possible.

**Applicant Response:**

*The Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive break up the parking areas into smaller sections preventing long rows of parking. The narrow portion of the site is on the street frontage preventing additional site access points.*

2. Connect parking areas, building entries, and open spaces with paved walkways.

**Applicant Response:**

*The proposed Site Plan provides a network of paved walkways that connect the building entries, common open spaces and parking areas.*

3. Buffer parking areas with landscaping.

**Applicant Response:**

*The proposed Site Plan provides landscaped areas to serve as buffers to the exterior perimeter and to buffer the buildings from the parking. There are landscape islands throughout all parking areas that provide shade trees, ground cover and shrubbery to buffer the parking area.*

4. Minimize the width and number of driveways and curb cuts.

**Applicant Response:**

*The proposed Site Plan has a single 26-foot-wide driveway/curb cut onto SE Norton Lane.*

5. Provide different types of open space throughout site, both active and passive, including playgrounds, trails, volleyball courts, bocce ball courts, community gardens, etc.

**Applicant Response:**

*The proposed Site Plan provides a variety of common open spaces. They vary in size, shape and their proposed uses. There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some areas are more confined courtyard spaces and can provide a more intimate quiet setting.*

*There is a summary of the proposed Common Open Space areas on the Open Space Analysis Plan, drawing A1.0A.*

6. Align buildings to surrounding streets.

**Applicant Response:**

*The subject site has street frontage along SE Norton Lane. The proposed Site Plan orients two buildings along the street frontage. The long axis of these buildings provides the required alignment to the street frontage.*

7. Centrally locate common buildings and spaces

**Applicant Response:**

*The proposed Site Plan has buildings oriented around the common open spaces. There are three areas of the site where buildings orient to the common open spaces.*

8. Group apartments, parking, and open space into smaller clusters.

**Applicant Response:**

*The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.*

*Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.*

*Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.*

3. Pedestrian Access. On-site pedestrian circulation system shall include the following:

- a. Continuous connections between the primary buildings, streets abutting the site, ground-level entrances, common buildings, common open space, and vehicle and bicycle parking areas.

**Applicant Response:**

*The proposed Site Plan provides a network of sidewalks and walkways that connect parking areas, building entries and common open spaces.*

- b. At least one pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.

**Applicant Response:**

*The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk. The buildings that are located along the street frontage provide a sidewalk connection from the building entries to the new public sidewalk along SE Norton Lane.*

- c. Pedestrian walkways shall be separated from vehicle parking and maneuvering areas by physical barriers such as planter strips, raised curbs, or bollards.

**Applicant Response:**

*Pedestrian walkways that are located adjacent vehicle parking are separated by concrete wheel stops and the walkways are of concrete which is contrasting to the asphaltic concrete paving.*

- d. Walkways shall be constructed with the hard surface material, shall be permeable for stormwater, and shall be no less than 3 feet to 5 feet wide. If adjacent to a parking area where vehicles will overhang the walkway, a 7-foot-wide walkway shall be provided. The walkways shall be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials.

**Applicant Response:**

*Pedestrian walkways that are located adjacent vehicle parking are proposed to be concrete which is contrasting to the asphaltic concrete paving. The walkways are all 5'-0" wide. Concrete wheel stops are located 2 feet from the sidewalk in the parking spaces and will provide the 5'-0" minimum clear walkway width.*

- e. Spacing requirement: No further than 200 feet apart, on center. At least 1 pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.

**Applicant Response:**

*The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk.*

- f. May be co-located with a common green.

**Applicant Response:**

*The proposed network of walkways on the Site Plan provides circulation to all parking areas, building entries and the common green open spaces.*

4. Parking Lot Location and Design.

a. Characteristics.

1. A parking lot is a storage space for cars and should provide secure storage.
2. It is also a place where everyone is a pedestrian while getting to or from their car. Therefore, it should be designed primarily for the ease, safety and comfort of a person rolling or on foot.
3. Clearly defined pathways through parking lots and garages to building entrances, surrounding sidewalks, and transit stops enhance pedestrian safety. These pathways also provide an opportunity to improve the appearance of parking lots.
4. Design parking lots and garages so that vehicles are not the dominant feature.
5. To encourage bicycling as a mode choice, bike parking areas should include bike repair, maintenance, and cleaning stations.

b. Universal Design Standards for Nine Parking Spaces or More.

1. Parking lot pathways should be designed as part of the seamless accessibility network described in Apartment Standards, particularly the required Through Connection.

**Applicant Response to (a) and (b):**

*The proposed Site Plan provides a network of parking area sidewalks and pathways that provide convenient access for the residents to all areas of the site include the common open spaces, common facilities and the public street frontage.*

2. Driveways to shared parking areas are:

1. Limited to one driveway per street frontage.
2. Parallel parking is permitted on a driveway that crosses a front, side or rear yard abutting a street, but not within the required yard setback.

**Applicant Response:**

*The subject site and proposed Site Plan does not have any shared parking areas or site access.*

c. Parking Lots For Small Multi-Dwelling Sites - Containing More Than Nine Parking Spaces but Fewer Than 16 Parking Spaces. Off-street parking may be arranged in clusters, subject to the following standards:

1. Residential developments with fewer than 16 dwellings are permitted parking clusters of not more than five contiguous spaces.
2. Residential developments with 16 dwellings or more are permitted parking clusters of not more than eight contiguous spaces.



3. Parking clusters must be separated from other spaces by at least four feet of landscaping.
4. Clustered parking areas may be covered.

**Applicant Response:**

*The subject site and proposed Site Plan provides a parking area greater than 16 spaces therefore this is not applicable.*

d. Parking Lots For Medium-to-Large Multi-Dwelling Sites - Containing More Than 16 Parking Spaces.

1. Interior landscaping, minimum area
2. Interior landscaping shall be required for off-street parking areas 5,000 square feet or greater in size.

**Applicant Response:**

*Parking area landscaping is proposed to be provided for all parking areas and is shown on the proposed Landscape Plan.*

3. For parking lots less than 50,000 square feet, the minimum landscaped area is 5 percent.

**Applicant Response:**

*Parking area landscaping proposed is greater than the 5% minimum and is shown on the proposed Landscape Plan.*

4. For parking lots 50,000 square feet and greater, the minimum landscaped area is 8 percent.

**Applicant Response:**

*The proposed Site Plan does not include parking areas over 50,000 sf.*

5. Planted areas may take the form of landscape areas and planter bays.

**Applicant Response:**

*This is understood and the landscape islands have been included in the overall parking lot landscape area.*

6. Landscaped areas along a through connection count toward required interior landscaping.

**Applicant Response:**

*This is understood and the landscape areas along through connections have been included in the overall interior landscape area.*

7. Landscaped islands and peninsulas shall be evenly distributed throughout all parking areas and separated no more than 60 feet from another. Such islands shall be provided with raised curbs, be a minimum of five feet in width, and shall each contain at least one deciduous tree. To achieve the maximum canopy coverage, all trees shall be non-columnar and have

root systems that form deep before spreading to decrease the episodes of buckled pavement.

**Applicant Response:**

*Landscape islands have been incorporated into the proposed Site Plan and parking area design. Parking area landscape islands and planting beds will be contained with raised concrete curbs. The spacing of the landscape islands is proposed to exceed the 60 foot maximum spacing in several locations primarily due to the parking stall width and spacing standards. An Administrative Variance is being requested to increase the spacing to 63 feet maximum.*

8. Trees may line the required Through Connection, and/or be clustered within landscape islands or planter bays, and/or shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.

**Applicant Response:**

*Trees are proposed within all of the landscape islands and planter bays throughout the parking areas providing the design tree canopy effect and shading.*

9. When a parking area abuts property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line.

**Applicant Response:**

*Six-foot-high sight obscuring fences are proposed along the side and rear property lines. Buffer plantings and trees are also proposed along these property lines and are shown in the Landscape Plan.*

- e. Parking Lot Setbacks Adjacent to Buildings and Structures. Where an off-street parking or vehicular use area is located adjacent to a building or structure, the off-street parking or vehicular use area shall be set back from the exterior wall of the building or structure by a minimum five-foot-wide landscape strip, or by a minimum five-foot-wide paved pedestrian walkway.

**Applicant Response:**

*The proposed Site Plan maintains a minimum 10-foot separation of parking areas from the closest point of any building or structure.*

- f. Parking Lot Location. Off-street parking spaces and vehicle maneuvering areas shall not be located:
  1. Within of 20 feet from any street property line, except alley property lines;

**Applicant Response:**

*The parking areas on the proposed Site Plan are located at a distance greater than 20 feet from the street frontage property line.*

2. Between a street property line and the front façade of cottages located closest to the street property line. This standard does not apply to alleys.

**Applicant Response:**

*This is not applicable to this proposed Site Plan and development.*

3. Off-street parking spaces shall not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.

**Applicant Response:**

*The parking areas on the proposed Site Plan are located at a distance of 6-feet from the North and South (side) property lines and 10 feet from the West (rear) property line. For the areas less than 10 feet, an Administrative Variance is being requested to the reduced setback. This is compatible with the existing adjacent development that also has parking located 6 feet from the adjacent property lines.*

*An Administrative Variance under separate application is being requested for the non-compliance of this issue.*

4. Landscaping, fencing, or walls at least three feet tall shall separate clustered parking areas and parking structures from common courtyards and public streets.

**Applicant Response:**

*The is not applicable to the proposed Site Plan as there are no parking areas proposed adjacent to common courtyards and public streets.*

5. Garages and carports (whether shared or individual) must not abut common courtyards.

**Applicant Response:**

*This is not applicable as no carports or garages are proposed.*

6. Individual attached garages up to 200 square feet shall be exempt from the calculation of maximum building footprint for cottages.

**Applicant Response:**

*This is not applicable as no garages are proposed.*

7. Individual detached garages must not exceed 400 square feet in floor area.

**Applicant Response:**

*This is not applicable as no garages are proposed*

8. Garage doors for attached and detached individual garages must not exceed 20 feet in width.

**Applicant Response:**

*This is not applicable as no garages are proposed*

- g. Parking Lot Required Through Connections. Through Connections may be multi-modal or used exclusively for bicycle and pedestrian access and need to meet the standards in Table 1.

**Applicant Response:**

*The proposed Site Plan and parking area layout does not create “through connections”.*

5. Common Open Space.

a. Characteristics.

- 1. Provide opportunities for formal and informal recreational use by residents of all ages. This could be a shared recreational facility including sports fields, play structures, bike tracks, courts, swimming pool, or other options.

**Applicant Response:**

*The proposed common open areas provide formal and informal recreations opportunities for the residents. There are formal seating areas and courtyards with benches and raised planters with seating walls, large open lawn areas for informal sports activities and formal picnic areas with picnic tables and barbeques.*

- 2. Provide tall deciduous trees for summer shade and winter solar access. When possible, preserve and incorporate large existing trees at least 9 inches in diameter as a focal point of open spaces.

**Applicant Response:**

*The landscape design includes deciduous shade trees adjacent the formal sitting areas and picnic areas and they are adjacent to the larger open lawn areas. These will provide the desired summer shading and will permit winter sun exposure. The existing subject site does not contain any existing trees.*

- 3. Enhance the usability of the space through the inclusion of elements including seating, outdoor lighting, weather protection and/or shade structures, and art, among other features.

**Applicant Response:**

*The proposed common open areas provide seating areas and courtyards with benches and raised planters with seating walls. The formal picnic areas include picnic tables and barbeques.*

- 4. Incorporate landscaping that receives at least 50 percent of its irrigation from harvested rainwater.

**Applicant Response:**

*As this is an identified characteristic it is not a requirement. The proposed design does not have the ability to store and harvest rain water.*

5. Provide opportunities for food cultivation including a community garden and/or incorporating cultivated species into the landscaping.

**Applicant Response:**

*Community gardens are not proposed.*

6. A maximum of 50 percent of common open space may be provided in a rooftop deck that includes shared amenities, weather protection, and landscaping, and is accessible to all residents.

**Applicant Response:**

*Rooftop deck areas are not proposed.*

7. A shared outdoor courtyard or shared street/woonerf that is fronted by individual entrances, windows, and balconies There should be a combination of hardscape and landscaped space and/or planters.

**Applicant Response:**

*The proposed Common Open Space Courtyards between Buildings E and G and between Buildings F and G have building entries and ground floor patios adjacent to them. There is also decorative hardscape concrete paving with an enhanced score joint pattern. Benches and raised planters with seat height walls help to define the courtyard spaces. Enhanced landscape plantings also define the courtyard spaces.*

b. Required Elements, General.

1. A common open space shall be provided that is centrally located and designed with a clear function that enhances the livability of residents. These functions shall include passive and active uses. The open space shall be accessible to all residents and if possible be fronted by clearly defined unit entrances. The common open space shall serve as the focus of surrounding buildings. Entries and windows shall face the common open space to provide informal surveillance. Common open spaces shall be accessible to all residents.

**Applicant Response:**

*The proposed Common Open Spaces are centrally located in three primary building cluster groups. The on-site walkway systems connects all of the open spaces, building entries and parking areas throughout the site making accessible to all residents of the development.*

*There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.*

2. Common open space shall be a minimum of 15 percent of the site.



**Applicant Response:**

*The amount of total proposed Common Open Space provided is greater than the 15% of the site area required. The total site area is 214,759 sf, thus 32,214 sf of Common Open Space is required. 32,769 sf of Common Open Space is provided.*

*An Open Space Analysis Plan has been prepared that delineates each proposed Common Open Space and identifies and summarizes the areas of each. See drawing A1.0A included within this application.*

6. Private Open Space.

a. Characteristics.

1. Every dwelling needs private open space for relief from indoors and to provide access to fresh air, light, and nature. Private open space may take many forms based on the size of unit.
2. They should translate into a perception of an increase in living space and the ability to invite the outdoors in. Additionally, these open spaces can provide environmental benefits with plants that consume carbon dioxide and help reduce stormwater runoff. Spaces should be adequate to be usable, allowing space for a chair to sit in, a place to barbecue or hang clothes to dry, or for a pet to curl up.
3. Private open space should enhance the residential function of the building while also improving the appearance of the building. They should be integrated into the overall architectural form and add detail to the façade.
4. Placement can vary based on privacy concerns. It can be combined across multiple floors.

**Applicant Response to (a) 1-4:**

*The proposed living unit and building design provides private open space for each unit. The ground floor living units have on-grade patios that are 96 sf each. The ground floor patios are directly adjacent to the living rooms and provide a door onto the patio with adjacent living room windows. This blends the living room space and the patios into usable living areas. The ground floor patios are screened with landscaping to maintain privacy from the adjacent walkways and parking areas.*

*The upper-level living units each have 72 sf private decks that are directly adjacent to the living rooms with the living room windows and a door opening directly onto the deck area. This also extends the living space from the living room onto the private deck.*

*The placement of the decks and patios provides privacy to adjacent living units and provides greater articulation on the building facades improving the overall appearance and character of the buildings.*

b. Required Elements.

1. All units shall have a minimum of 36 square feet of private open space that allows for personalization and private use of the space and contributes to the livability and function of the dwelling. Any exterior private open spaces shall be supplemented with operable windows to allow for cross-ventilation, increase airflow and provide the ability to control access to the outdoors.

**Applicant Response:**

*The ground floor living unit patios are 96 sf each and the upper floor living unit decks are 72 sf each. Both exceed the minimum areas required.*

2. At least 50 percent of the upper units shall have a balcony that is accessible from the interior of the unit that is a minimum of 60 square feet with no dimension less than 6 feet. These balconies can be designed to be up to the full width of the apartment in order to provide adequate space for use and allow greater indoor/outdoor flow. Balconies can be cantilevered, semi-recessed, or fully recessed. They should be located based on privacy and environmental concerns. If balconies are transparent, adequate storage should be provided within the unit or the larger building so that balconies do not become informal storage spaces.

**Applicant Response:**

*All of the upper floor living units have decks that are 72 sf each with a minimum dimension of 6 feet. This exceeds the minimum number of units and the minimum deck area required. The decks are placed to maintain privacy between adjacent living units.*

3. Private outdoor space at the ground level must meet the requirements of Universal Standards: Front Yard regardless of whether the private outdoor space is in the front, side, or rear of a building.

**Applicant Response:**

*The ground floor living unit patios are all screened with landscaping and are setback from walkways, street frontage and adjacent parking areas as required.*

- c. Supplemental Elements. In addition to meeting the required elements above, projects must provide private open space in the form of one of the options listed below.

1. A "Juliet-style" balcony of 12-inch dimension that allows residents to bring a sense of the outdoors into the unit. Must have doors that can open inwards or full height sliding glass doors to allow the introduction of fresh air and sunlight. If this item is selected, units must also include operable windows to increase airflow/ability to control access to the outdoors.
2. An upper story rooftop deck or terrace that may include space for outdoor seating, dining, and planters for cultivation. This terrace may be stepped back on structures over two stories so as to reduce the visual impact of upper floors.
3. Alternative option that meets the concept and guiding principles.

**Applicant Response:**

All upper floor living units provide decks that are 72 sf each, thus meeting the supplementary requirements. All unit decks are directly adjacent to the living rooms and provide operable windows and a deck access door directly onto the deck.

7. Alleys.

**Applicant Response:**

The subject site does not have the opportunity for providing alleys and this is not applicable.

8. Landscaping

a. Characteristics.

1. Use landscape elements, particularly plant materials, in an organized and harmonious manner that will enhance, protect and promote the economic, ecological, and aesthetic environment of McMinnville.

**Applicant Response:**

The proposed landscape design provides a cohesive design that utilizes plant materials to enhance the overall site appearance, enhance the outdoor living environment and provide an overall aesthetically pleasing site character. The choice and selection of plant materials incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity.

The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Shade trees will soften the built environment, reduce heat island, and provide habitat for birds. Flowering Shrubs will soften the buildings and enhance the aesthetic value of the site and provide food for local pollinators.

Lawn areas will provide a pastoral aesthetic reminiscent of the Willamette Valley, promoting harmony with adjacent landscapes. These areas will provide opportunities outdoor activities that promote the mental and physical health of the residential community, and in turn, the greater community of McMinnville. Finally, the regular maintenance of the landscape will help provide jobs for the local economy.

2. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:
  - a. Reduce soil erosion and the volume and rate of discharge of stormwater runoff.

**Applicant Response:**

Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.

- b. Aid in energy conservation by shading structures from energy losses caused by weather and wind.

**Applicant Response:**

*Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.*

- c. Mitigate the loss of natural resources.

**Applicant Response:**

*Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.*

- d. Provide parking lot landscaping to reduce the harmful effects of heat, noise, and glare associated with motor vehicle use.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects. The combination of landscape plant materials and sight obscuring fences buffer noise from the properties.*

- e. Create safe, attractively landscaped areas adjacent to public streets.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.*

- f. Require the planting of street trees along the City's rights-of-way.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.*

- g. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character, and value of existing neighborhoods.

**Applicant Response:**

*The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adject to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and*

groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.

- h. Provide shade, and seasonal color.

**Applicant Response:**

*The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.*

- i. Reduce glare, noise, and heat.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.*

*Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.*

- j. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.

**Applicant Response:**

*Visual noise will be mitigated with the use of a site obscuring fence, trees, and evergreen shrubs.*

*Lighting impacts are mitigated by the selection and placement of light fixtures that provide full cut-off of light pollution to the abutting properties.*

- k. Unify development and enhance and define public and private places.

**Applicant Response:**

*The landscape design helps unify the development with the use of repetition of design elements in private and public areas. These elements include repetition of form, texture, and color of the plant materials.*

*Public spaces meant to be used by all residents are defined with lawn areas, walkways, benches, raise planters, and flowering trees.*

*Landscaping adjacent to private areas is defined by ornamental plantings of shrubs, grasses and groundcovers.*

- l. Preserve existing mature trees.



**Applicant Response:**

*There are no existing trees onsite.*

- m. Enhance the urban forest and tree canopy.

**Applicant Response:**

*The urban forest is enhanced with the planting of trees throughout the site, including shade trees and flowering trees. In total, 115 trees are proposed, including street trees.*

- n. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.

**Applicant Response:**

*The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.*

- o. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.

**Applicant Response:**

*A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.*

*The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically branched and high canopy structure for the interphase between public and private space.*

*Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs*

and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

- p. Support McMinnville as a community that cares about its appearance. It is further recognized that landscaping increases property values, attracts potential residents and businesses to McMinnville, and creates safer, more pleasant living and working environments for all residents and visitors to the city.

**Applicant Response:**

*The proposed landscape design will use a variety of trees, shrubs, grasses and groundcovers to provide an aesthetically pleasing appearance that will increase the value of the property, attract residents and create a safe and pleasing living environment.*

*The proposed trees throughout the site will provide structure, shade, pleasing fall color and a net increase to the urban forest canopy. The ornaments landscape elements will soften the built environment and offer a pleasing appearance for those living and visiting the community.*

b. Required Elements.

- 1. All areas of the site not occupied by the structures or paved areas shall be landscaped in an attractive and functional manner.

**Applicant Response:**

*The proposed landscape design provides landscape plant materials in all areas of the site not occupied by structures or parking and paved areas. The proposed landscape design provides an aesthetically pleasing design and use of a variety of plant materials.*

- 2. A minimum of 20 percent of the net site shall be landscaped. Paved pedestrian paths, when integrated within the landscaped area, may satisfy up to 5 percent of this requirement. Landscaped setback areas, landscaped common open spaces, eco-roofs, vegetated stormwater facilities, preserved natural areas, and planter areas can be credited toward the minimum landscape standard.

**Applicant Response:**

*The proposed Site Plan and landscape design provides 30 percent of the subject site area in landscaping. More specifically, the site area is 214,759 sf. 64,157 sf of landscape area is provided, which is 30 percent.*

9. Privacy and Screening.

- a. Characteristics. Low walls or fences are encouraged to provide separation between private open space and common open space, streets, or internal circulation paths. Fences should be designed to integrate with the architecture of the building and add visual interest through the use of materials, color, and detail.

**Applicant Response:**

*The proposed landscape design provides landscape screening and buffering to ground floor living unit patios (private open space). The plant materials used for screening these areas blend harmoniously with the adjacent landscape design providing an attractive and effective privacy screen.*

- b. Required Elements.

1. All fences on the interior of the development shall be no more than 3 feet high. Fences along the rear or side property lines of the development may be up to 6 feet high. Chain-link fences are prohibited.

**Applicant Response:**

*There are no internal site fences proposed. There is a 6-foot sight obscuring fence proposed on the site side and rear property lines. The fence will not be chain-link fencing.*

2. Mechanical and communication equipment and outdoor garbage and recycling areas shall be screened so they are not visible from streets and common open spaces.

**Applicant Response:**

*The proposed trash and recycling enclosures provide a solid evergreen screen along the sides of the facilities, as illustrated on the Landscape Plan.*

3. Utilities such as transformers, heating and cooling, electric meters, and other utility equipment shall not be located within 5 feet of a front entrance and shall be screened with sight-obscuring materials.

**Applicant Response:**

*The proposed locations of any on-site transformers or other utility equipment has not yet been determined. The transformers and equipment will be screened as required.*

4. The placement of balconies above the first story shall not create a direct line of sight into the living spaces or backyards of adjacent residential properties.

**Applicant Response:**

*The proposed Site Plan does not locate buildings that would orient living unit balconies that would create a line of sight into adjacent properties.*

10. Front Yard.

- a. Characteristics.

1. For all housing types the front setback—even when it is small or zero, should be designed to provide a transition from the public realm of the street to the private realm of the dwelling.

2. The front setback provides a vital transition between the public area of the street and the private spaces within the dwelling. The smaller the front setback is, the more important the concept of layering public to private spaces becomes. When multi-dwelling units are on the ground floor of the building and face the perimeter of the site and surrounding streets, they must meet the standards of this section.

b. Required Elements.

1. Dwelling units located on the ground floor of the building and facing the perimeter of the site and surrounding streets must meet the requirements of either:
  - a. Front Yard Type 1: Neighborhood

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provides a landscape design that meets or exceeds the requirements for a Type 1 Front Yard (Neighborhood Type). The "Gateway" area is landscaped at the minimum depth with grasses, the "Front Yard" is fully landscaped and provides a hedge screen at 3 feet to provide privacy to the unit patios.*

11. Compatibility.

- a. Characteristics. New multi-dwelling housing should be compatible with its surrounding context while introducing new shape, size, and detail variation, enabling different housing styles and types to sit side-by-side harmoniously.

**Applicant Response:**

*The existing developed neighborhood contains a wide variety of uses, building styles and scales. The existing uses include a medical office building, a three-story hotel, the large multi-story hospital (Willamette Valley Medical Center) and single-story mobile homes in a mobile home park. The proposed three-story apartment buildings are well within the range and scale of existing adjacent developments. The proposed buildings range in size from twelve units to twenty-four units also providing a variety in the scale of buildings within the proposed development.*

b. Required Elements.

1. On a site with multiple buildings of varying scales (or that vary from the surrounding context), provide a gradual transition between scales. For example, locate dwellings that are similar in scale and density along the street frontage and transition to lower scale and density buildings toward the rear of the site. Use rear driveways and landscaping as a buffer backing up to adjacent properties of a different scale.

**Applicant Response:**

*The proposed Site Plan locates similar scale buildings adjacent to the street frontage and along the northerly property line that are compatible and in context with the existing adjacent development. The buildings in the rear of the site are set back nearly 80 feet to provide separation and a buffer from the single-story mobile home park adjacent to the rear of the site.*

2. Arrange building volumes and setbacks in a way that reflects neighborhood patterns along street frontages and contributes to the desired character.

**Applicant Response:**

*The existing neighborhood character is quite diverse and does not have any established patterns to follow. The proposed Site Plan provides a street frontage compatible with similar developments within the community and provides a pedestrian friendly street scape.*

3. Arrange courtyard apartments so that end units reflect a neighborhood context of detached units along the street frontage.

**Applicant Response:**

*This element is not applicable to the garden style apartments.*

4. Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.

**Applicant Response:**

*The existing neighborhood has a variety of height and scale buildings. The existing large medical office building adjacent to the site to the north is single story. However, the proposed Stratus Village to the West of the medical Office Building and directly adjacent the subject site to the North is proposed to be a three-story building.*

5. Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.

**Applicant Response:**

*The proposed site and building design does not propose stepped back upper floors. The building design, the proposed roof design and building articulation provide the desired human scale elements without stepping upper floors.*

- c. Supplemental Elements. In addition to meeting the required elements above, projects must respond to the compatibility requirement in the form of three of the options listed below.

1. Use roof forms and bays to break up the overall mass of larger residential structures.

**Applicant Response:**

*The proposed building design includes a variety of roof forms that include hipped sloped roofs, intersecting gable roof forms and large breaks in the overall roof at building indentations. The proposed roof design breaks up the overall building mass into smaller sections thus reducing the overall appearance of the building mass and scale.*

2. Walls incorporate vertical wall offsets, projections, or recesses to reduce building façades into smaller volumes and define visually distinct living unit modules.

**Applicant Response:**

*The proposed building design includes offsets in the façade wall planes. The deck forms project from the primary building wall plane and the intersecting gable roof form serves to break-up the overall building mass. The larger buildings have central indentations that serve to further break-up the building mass.*

3. Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.

**Applicant Response:**

*This element is not utilized in the building design.*

4. Mark a distinct physical transition between the base and upper floors of a building through a change in brick pattern, change in materials and/or wall surface pattern, articulation of a floor line, or change in window types.

**Applicant Response:**

*The proposed building design includes a variety of building materials and textures. The building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building.*

5. Use horizontal elements the entire width of the front façade to mark a break between floors or along the roofline including band course, band molding, bellyband, or belt course.

**Applicant Response:**

*The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building.*

6. Use a variation in roof forms on all four elevations of a structure to visually break up monotony including pitched or sloping roof elements, variations in pitch and height of roof planes, dormers, eaves, gable, or dormer end brackets, corbels, or decorative wood timbers.

**Applicant Response:**

*This element is not utilized in the building design.*

7. Limit continuous ridgelines to less than 40 feet in length and continuous eaves to 25 feet in length.

**Applicant Response:**



*This element is not utilized in the building design.*

8. Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.

**Applicant Response:**

*This element is not utilized in the building design.*

12. Wall and Roof Design.

A. Characteristics.

1. For buildings that front the street, avoid long, monotonous, uninterrupted walls. Modulate buildings walls and roofs to prevent large, uninterrupted walls and building mass.

**Applicant Response:**

*The proposed building design provides significant articulation and variety in walls planes. The longest wall plane is less than 20 feet before intersecting or recessed wall planes. Projecting or recessed building elements provide a great amount of articulation and variety of wall planes and roof forms.*

2. Differentiate between the base of the building and the top of the building to enhance the pedestrian realm. Make base treatment cohesive across façades and integrate with the architectural character of the building.

**Applicant Response:**

*The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building. This design concept is applied to all buildings on site.*

3. Multi-dwelling development must address the following design objectives:
  - A. Articulation – All street-facing buildings shall incorporate design elements that break up façades into smaller planes.
  - B. Eyes on the street – A certain percentage of the area of each street-facing façade must be windows or entrance doors.
  - C. Main entrance – On street-facing façades, at least one main entrance must meet standards for location, orientation, and visibility.
  - D. Detailed Design – All street-facing buildings shall include several features.

**Applicant Response:**

*The proposed site and building design address the four design objectives. The building design provides building and roof forms to break up the facades into smaller planes; the street facing units provide windows and glazed doors that face the street frontage; the open entry stairway to the units faces the street frontage; and the street facing building elevations provide a variety of design features.*

B. Required Elements.

1. Articulation.

- A. For multi-dwelling buildings with 30 to 60 feet of street frontage, a minimum of one of the following elements shall be provided along the street-facing façades.
- B. For buildings with over 60 feet of street frontage, at least one element below shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.
  1. A porch at least 5 feet deep.
  2. A balcony that is at least 2 feet deep and is accessible from an interior room.
  3. A bay window that extends at least 2 feet.
  4. A section of the façade that is recessed by at least 2 feet deep and 6 feet long.
  5. A gabled dormer.
- C. Buildings under 30 feet in length are exempt from these requirements.

**Applicant Response:**

*The proposed street frontage buildings have over 60 feet of street frontage. The proposed building design does not have a wall plane over 20 feet between articulation or design features to break up the facades. These include the recessed front porch/patio and upper floor decks that are 6 feet in depth. These projecting elements also have gabled and hipped roof dormers.*

2. Eyes on The Street.

- A. At least 15 percent of the area of each street-facing façade must be windows or entrance doors. Windows used to meet this standard must be transparent and allow views from the building to the street. Glass blocks and privacy windows in bathrooms do not meet this standard.
- B. Window area is considered the entire area within the outer window frame, including any interior window grid.
- C. Doors used to meet this standard must face the street or be at an angle of no greater than 45 degrees from the street.

**Applicant Response:**

*The proposed building design provides 22% window area on the street facing facades. This includes the living room, bedroom and bath windows and the glazed door in the living room leading to the decks and ground floor patios.*

3. Main Entrances. Main entrances must meet both of the following standards.

- A. Be no further than 8 feet behind the longest street-facing wall of the building.
- B. Face the street, be at an angle of up to 45 degrees from the street, or open onto a porch. If the entrance opens up onto a porch, the porch must meet all of these additional standards.
  1. Be at least 25 square feet in area with a minimum 4 feet depth.
  2. Have at least one porch entry facing the street.
  3. Have a roof that is no more than 12 feet above the floor of the porch.

4. Have a roof that covers at least 30 percent of the porch area.

**Applicant Response:**

*The proposed building provides open air enclosed stairways to serve the living units on that side of the building including the ground floor units. The stairway provides a roof covering.*

4. Detailed Design.

- A. For multi-dwelling buildings with up to 30 feet or more of street frontage, a minimum of two of the elements shall be provided along the street-facing façade or façades.
- B. For buildings with over 30 feet of street frontage, at least one element shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.
  1. Covered porch at least 5 feet deep, as measured horizontally from the face of the main building façade to the edge of the deck, and at least 5 feet wide.

**Applicant Response:**

*The proposed design for Building 'A' has 70 feet of street frontage. The façade is broken up into three primary articulated sections. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. The longest wall plane between these sections is 20 feet. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.*

*The proposed design for Building 'B' has 140 feet of street frontage. The façade is broken up into six articulated sections and the longest wall plane between these sections is 20 feet. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.*

2. Recessed entry area at least 2 feet deep, as measured horizontally from the face of the main building façade, and at least 5 feet wide.

**Applicant Response:**

*The proposed building recessed entries and open-air stairways to the upper floor living units. These entries are 8 feet wide and 18 feet deep.*

3. Offset on the building face of at least 16 inches from one exterior wall surface to the other.

**Applicant Response:**

*The proposed street facing building designs provide multiple changes in wall planes. The depth of the wall plane variations are 6 feet in several locations and 10 feet in additional locations. These variations occur multiple times on all street frontage building façade elevations.*

4. Dormer that is at least 4 feet wide and integrated into the roof form.

**Applicant Response:**

*This design element is not utilized in the building design.*

5. Roof eaves with a minimum projection of 12 inches from the intersection of the roof and the exterior walls.

**Applicant Response:**

*The proposed building roof design provides 24-inch, 18-inch and 12-inch roof overhangs.*

6. Roofline offsets of at least 2 feet from the top surface of one roof to the top surface of the other.

**Applicant Response:**

*The design element is not utilized in the building design.*

7. Horizontal lap siding between 3 to 7 inches wide (the visible portion once installed). The siding material may be wood, fiber-cement, or vinyl.

**Applicant Response:**

*The proposed building elevation design includes fiber cement horizontal lap siding with a 6-inch exposure.*

8. Brick, cedar shingles, stucco, or other similar decorative materials covering at least 40 percent of the street-facing façade.

**Applicant Response:**

*This design element is not utilized in the building design.*

9. Gable roof, hip roof, or gambrel roof design.

**Applicant Response:**

*The proposed building roof design includes hip roofs and gable roof forms.*

10. Window trim around all windows at least 3 inches wide and 5/8 inches deep.

**Applicant Response:**

*This design element is not utilized in the building design.*

11. Window recesses, in all windows, of at least 3 inches as measured horizontally from the face of the building façade.

**Applicant Response:**

*This design element is not utilized in the building design.*

12. Balcony that is at least 3 feet deep, 5 feet wide, and accessible from an interior room.

**Applicant Response:**

*The proposed building and living unit design includes recessed upper floor decks that are 6 feet deep and 12 feet wide.*

13. Bay window at least 2 feet deep and 5 feet long.

**Applicant Response:**

*This design element is not utilized in the building design.*

14. One roof pitch of at least 500 square feet in area that is sloped to face the southern sky and has its eave line-oriented within 30 degrees of the true north/ south axis.

**Applicant Response:**

*This design element is not utilized in the building design.*

## **CHAPTER 17.21 R-4 MEDIUM, HIGH DENSITY RESIDENTIAL ZONE**

Although the subject site is located within the C-3 Commercial Zone, Section 17.33.010 (6) provides that Apartments are subject to the provisions of the R-4 zone. Therefore the following narrative addresses the compliance with the applicable provisions of the R-4 zone.

### **17.21.010 Permitted uses.**

In an R-4 zone, the following uses and their accessory uses are permitted:

- A. Tiny Houses, Single detached dwelling;
- B. Middle Housing
  1. Plexes: Duplex Dwelling; Triplex Dwelling; Quadplex Dwelling (minimum lot size of seven thousand square feet)
  2. Cottage Clusters
  3. Townhouses
- C. Apartments; Multiple dwelling subject to the following:
  1. Developments with five or more units.
  2. The property on which the use will be located has direct access from a major collector or minor arterial street, or a local collector street within 600' of a collector or arterial street; or
  3. The property is located within one-half mile of a planned or existing transit route; or
  4. The property is within one-quarter mile from a planned or existing neighborhood or commercial shopping area.

**Applicant Response:**

*The proposed development is for 138 apartment units located in seven three story buildings. The subject site does provide direct access to a local collector street that is within 600 feet of a designated collector.*

**17.21.030 Lot size.**

In an R-4 zone, the lot size shall not be less than five thousand square feet, except that the lot area for attached single dwelling lots shall average one thousand-five hundred square feet in area. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The subject site is 214,759 sf (4.93 Acres) and therefore meets the requirement.*

**17.21.040 Yard requirements.**

In an R-4 zone, each lot shall have yards of the following size unless otherwise provided for in Section 17.54.050:

- A. A front yard shall not be less than fifteen feet, except as provided in Section 17.11.030(C), Table 1(d), Cottage Clusters.
- B. A side yard shall not be less than six feet. An exterior side yard shall not be less than fifteen feet, except as provided in Section 17.11.030(C), Table 1(f), Cottage Clusters.
- C. A rear yard shall not be less than twenty feet, except as provided in Section 17.11.030(C), Table 1(c), Cottage Clusters.
- D. Whether attached to a residence or as a separate building, a covered storage facility for a vehicle on which the main opening is toward a street shall be located not less than twenty feet to the property line bordering the street;
- E. All yards shall be increased, over the requirements of this section, one foot for each two feet of building height over thirty-five feet. (Ord. 4912 §3, 2009; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The yards on the proposed Site Plan are as follows:*

- Front Yard: 15 feet provided, 15 feet required.*
- Side Yard: 10 feet provided, 6 feet required.*
- Rear Yard: 79'-6" provided, 20 feet required.*

*The proposed yards meet the requirements.*

**17.21.050 Building height.**

In an R-4 zone, a building shall not exceed sixty feet in height. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The proposed building height is 36'-4". The current underlying Three Mile Lane Planned Development Overlay Ordinance 4709 has a height limitation of 35 feet. The concurrent Planning Commission Variance Application, submitted concurrent within this application, is requesting a height variance to 37 feet. Additional compliance narrative addressing the proposed building height is in the applicant response to the Planning Commission Variance Application narrative.*

**17.21.060 Density requirements.**

In an R-4 zone, the maximum density for single attached dwelling may not exceed four dwelling units per 5,000 square feet. Density maximum may not apply to any other permitted housing types, including accessory dwelling units. (Ord. 4796 §1(b), 2003; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**



The allowable density for the 214,759 sf subject site is 210 living units. The Site Plan for the proposed development includes 138 apartment units. The proposed density is less than the allowable and is therefore compliant.

## 17.33 C-3 GENERAL COMMERCIAL ZONE

### 17.33.010 Permitted uses.

In a C-3 zone, the following uses and their accessory uses are permitted:

6. Apartments, Multiple dwellings subject to the provisions of the R-4 zone, except that within the City Center Housing Overlay Zone designated in Chapter 17.66, density limitations of the R-4 zone shall not apply, and any special development standards of the Overlay Zone shall supersede those of the R-4 zone.

#### **Applicant Response:**

The proposed development is for Apartments and is therefore subject to the R-4 zone development standards.

## CHAPTER 17.57 LANDSCAPING

### 17.57.010 Purpose and Intent:

The purpose and intent of this Chapter is to encourage and, where appropriate, require the use of landscape elements, particularly plant materials, in proposed developments in an organized and harmonious manner that will enhance, protect and promote the economic, ecological and aesthetic environment of McMinnville. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:

- A. Provide guidelines and standards that will:
  1. Reduce soil erosion and the volume and rate of discharge of storm water runoff.

#### **Applicant Response:**

Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.

2. Aid in energy conservation by shading structures from energy losses caused by weather and wind.

#### **Applicant Response:**

Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.

3. Mitigate the loss of natural resources.

#### **Applicant Response:**

Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.

4. Provide parking lot landscaping to reduce the harmful effects of heat, noise and glare associated with motor vehicle use.

**Applicant Response:**

The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.

5. Create safe, attractively landscaped areas adjacent to public streets.

**Applicant Response:**

Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.

6. Require the planting of street trees along the City's rights-of-way.

**Applicant Response:**

Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.

7. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character and value of existing neighborhoods.

**Applicant Response:**

The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adjacent to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.

8. Provide shade, and seasonal color.

**Applicant Response:**

The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.

9. Reduce glare, noise and heat.

**Applicant Response:**

The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.

Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.

- B. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.

**Applicant Response:**

The proposed Landscape Plan and design provides trees, buffer and screening plantings along the site perimeter. This will buffer the effects of on-site parking area and building lighting from the adjacent abutting properties. The overall landscape plan and development also aids in buffering sound both from the subject site and from the adjacent abutting properties.

- C. Unify development and enhance and define public and private places.

**Applicant Response:**

The proposed Landscape Plan and design will provide street trees along the SE Norton Lane street frontage. The landscape design also provides screening of private spaces for the living units and the proposed open common areas on site.

- D. Preserve existing mature trees.

**Applicant Response:**

The existing undeveloped site was historically agricultural uses and there are no existing trees.

- E. Enhance the urban forest and tree canopy.

**Applicant Response:**

The proposed Landscape Plan and design provides a variety of trees that will enhance and contribute to the overall urban tree canopy.

- F. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.

**Applicant Response:**

The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.

- G. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.

**Applicant Response:**

A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.

The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically branched and high canopy structure for the interphase between public and private space.

Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

H. Support McMinnville as a community that cares about its appearance.

**Applicant Response:**

The proposed Landscape Plan design provides an esthetically pleasing and attractive landscape design. This overall site and landscape design enhances the character of the existing neighborhood and provides a livable community environment.

**17.57.070 Area Determination—Planning factors.**

- A. Landscaping shall be accomplished within the following ranges:
  - 3. Multiple-dwelling, twenty-five percent of the gross area. This may be reduced to not less than fifteen percent upon approval of the review committee. (The gross area to be landscaped may only be reduced by the review committee if there is a showing by the applicant that the intent and purpose of this chapter and subsection B of this section are met.)

**Applicant Response:**

The proposed Site Plan and Landscape Plan provide 30 percent of the site area in landscaping.

- 4. A parking lot or parking structure built in any zone providing parking spaces as required by the zoning ordinance shall be landscaped in accordance with the commercial requirements set forth above in subsection 2 of this section.

**Applicant Response:**

The proposed Site Plan and Landscape Plan provide parking lot landscaping as identified in 17.11.090 Residential Design Standards for Apartments.

B. The following factors shall be considered by the applicant when planning the landscaping in order to accomplish the purpose set out in Section 17.57.010. The Landscape Review Committee shall have the authority to deny an application for failure to comply with any or all of these conditions:

1. Compatibility with the proposed project and the surrounding and abutting properties and the uses occurring thereon.

**Applicant Response:**

The adjacent uses: residential, commercial and agriculture would not be adversely affected by the proposed multi-family development. Uses to the north, south and west of the site will be sufficiently screened with a site obscuring fence and evergreen hedge. Additionally, the residential use to the west will be buffered with more than 15' between the property line and any structure.

2. Screening the proposed use by sight-obscuring, evergreen plantings, shade trees, fences, or combinations of plantings and screens.

**Applicant Response:**

Screening is proposed on north, south and west perimeters of the site. Screening will be achieved with a site obscuring fence and evergreen hedge.

3. The retention of existing trees and natural areas that may be incorporated in the development of the project. The existing grade should be preserved to the maximum practical degree. Existing trees shall be provided with a watering area equal to at least one-half the crown area.

**Applicant Response:**

The site was previously open for agricultural use and there are no existing trees on site.

4. The development and use of islands and plantings therein to break up parking areas.

**Applicant Response:**

A total of 37 parking lot islands are used to break up parking areas. Each island has a shade tree and shrubs / grasses to help break up the parking areas.

5. The use of suitable street trees in the development of new subdivisions, shopping centers and like developments. Certain trees shall be prohibited in parking areas: poplar, willow, fruit, nut, birch, conifer, and ailanthus.

**Applicant Response:**

None of the proposed trees are prohibited. Many of the trees are listed on the City of McMinnville street tree list, including the Green Vase Zelkova prosed along SE Norton Lane.

6. Suitable watering facilities or irrigation systems must be included in or near all planted areas;

**Applicant Response:**

The proposed development will have an automatic irrigation system with a water efficient controller and rain sensor. The irrigation system will be commercial grade with separate zones for lawn and shrub areas.

**CHAPTER 17.60 OFF-STREET PARKING AND LOADING**

**17.60.050 Spaces—Location.**

- A. Except as provided below, required off-street parking spaces for dwellings shall be located on the same lot with the dwelling.

**Applicant Response:**

All required parking is provided on the same site as all proposed dwelling units.

**17.60.060 Spaces. Number required.**

Except for the southerly 100 feet of Block 10 and the northerly 100 feet of Block 11, Rowland's Addition and the area bounded by Second Street, Adams Street, Fourth Street, and Galloway Street, at the time of erection of a new structure or at the time of enlargement or change of use of an existing structure, off-street parking spaces shall be provided as follows unless greater requirements are otherwise established. Where square feet are specified, the area measured shall be the gross floor area primary to the functioning of the particular use of the property but shall exclude space devoted to off-street parking or unloading.

**Applicant Response:**

The Off-Street parking required and provided is as follows:

Parking Required:

|                             |  |
|-----------------------------|--|
| One- and Two-Bedroom Units: | 1.5 spaces/unit required, (120 units x 1.5 =180) |
| Three-Bedroom Units:        | 2 spaces per/unit required (18 units x 2 =18)    |
| Total Parking Required:     | 216 spaces                                       |

Provided Parking:

|                        |            |
|------------------------|------------|
| Standard Spaces:       | 188 spaces |
| Compact Spaces:        | 22 spaces  |
| ADA Accessible spaces: | 8 spaces   |
| Total Spaces Provided  | 218 spaces |

**17.60.070 Off-street loading requirements.**

- A. Buildings or structures to be built or substantially altered which receive and distribute materials and merchandise by trucks shall provide and maintain off-street loading berths in sufficient number and size to adequately handle the needs of the particular use.

**Applicant Response:**

The proposed multifamily apartment development does not require off-street loading spaces. None are proposed to be provided.



**17.60.080 Design requirements.**

- A. All parking lots and driving aisles shall be asphaltic cement concrete or Portland cement concrete with driving aisles, maneuvering aisle and parking spaces clearly marked, except that in an industrial zone, parking spaces which are in addition to those required by this chapter, may be surfaced with a minimum of treated gravel and maintained dust free.

**Applicant Response:**

*All parking areas and drive aisles on the proposed Site Plan are designed to be constructed of asphaltic concrete.*

- B. In a residential zone, a required front yard or a required side yard adjacent to the street shall not be used for any purpose except for off-street parking of motor vehicles, unless otherwise allowed by this ordinance, and such parking space shall not be less than twenty feet in depth from the property line.

**Applicant Response:**

*Parking areas or drive aisles are not proposed to be located within the front or side yard setbacks.*

- C. Safe access shall be provided as follows:
  - 1. Access aisles shall be of sufficient width for all vehicular turning and maneuvering.

**Applicant Response:**

*The drive aisles on the proposed Site Plan are 26 feet wide. The turn radii on the drive aisles are 18-foot minimum inside radius. These proposed dimensions provide safe and adequate width and turn radii for vehicles and for fire trucks and emergency vehicles.*

- 2. Groups of more than four parking spaces shall be so located and served by a driveway that their use will require no backing movements or other maneuvering within a street right-of-way other than an alley.

**Applicant Response:**

*The proposed site plan provides a single driveway access to SE Norton Lane. No backing movements are proposed onto SE Norton Lane.*

- 3. Driveways to off-street parking areas shall be designed and constructed to facilitate the flow of traffic and to provide for maximum safety of pedestrians and vehicular traffic on the site.

**Applicant Response:**

*The proposed driveway access is 26 feet wide which provides ample width for two-way traffic into and from the subject site. The Public Street frontage improvements include setback sidewalks that cross the driveway and will be constructed in accordance with the City of McMinnville standards.*

- 4. Clear vision areas shall be provided at driveway exits for all uses except single-detached dwellings and plexes, shall have minimum dimensions of ten feet measured along the street right-of-way and the edge of the driveway. In commercial and industrial zones, buildings and signs may be constructed with

cantilevers which extend out over the clear vision area at a height greater than eight feet when measured from the top of the curb, or where no curb exists, from the established centerline grade. Except for existing permanent buildings and structures (other than signs), nonconformities shall be made to comply with the provisions of this section within seven years from the date of its adoption.

**Applicant Response:**

*The proposed driveway access from SE Norton Lane provides the required clear vision area. A clear vision triangle is shown on the proposed Site Plan, drawings A1.0, A1.1, demonstrating compliance.*

5. Driveway cuts shall be a minimum of twenty feet from a street intersection.

**Applicant Response:**

*The proposed driveway location is centered on the intersection of SE Norton Lane and SE Stratus Avenue. This location is in compliance with City of McMinnville standards and policies.*

D. Parking areas shall be made compatible with surrounding uses as follows:

1. Parking spaces along the outer boundaries of a parking area shall be contained by a curb or a bumper rail so placed to prevent a motor vehicle from extending over an adjacent property, sidewalk, or street.

**Applicant Response:**

*The parking areas on the proposed Site Plan provides concrete curbs or wheel stops preventing encroachment into adjacent properties. Additionally, there is a 6-foot sight obscuring fence along the property boundaries.*

2. When a parking area in a commercial or industrial zone abuts a property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line. The responsibility for placement of the fence or wall lies with the commercial or industrial property.

**Applicant Response:**

*The proposed Site Plan provides a 6-foot sight obscuring fence along the adjacent property boundaries.*

3. Artificial lighting which may be provided shall be so deflected as not to shine or create glare in any residential zone or on any adjacent dwelling.

**Applicant Response:**

*The proposed Site Lighting Plan, drawings ME 1.0 demonstrates the photometric footprint of the proposed site light fixtures. The proposed fixtures provide full cut-off of light on the adjacent properties preventing light and glare shining on the adjacent sites.*

E. Space size minimum shall be as follows:

1. Handicap parking spaces shall be a minimum of twelve feet wide and 19 feet in length.

**Applicant Response:**

Handicap Accessible parking spaces have been designed in compliance with Oregon Structural Specialty Code and the ANSI A117.1 standards and requirements. Van Accessible stalls are 9'x19' with an 8' wide access aisle and standard Handicap Accessible spaces are 9'x19' with an 6' wide access aisle. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.

- 2. Compact and subcompact parking spaces shall be a minimum of eight feet by sixteen feet.

**Applicant Response:**

The Compact Spaces on the proposed Site Plan are 8' wide x 16' long and are in compliance. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.

- 3. Standard parking spaces shall be a minimum of eight feet six inches by nineteen feet.

**Applicant Response:**

The Standard Spaces on the proposed Site Plan are 9' wide x 19' long and are in compliance. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.

F. The type of space shall be set as follows:

- 1. Handicap spaces shall be required and designated as per current federal, state, and local regulations.

**Applicant Response:**

The Handicap Accessible parking spaces have been designed to be in compliance with the Oregon Structural Specialty Code and the ANSI 117.1 standards and requirements.

- 2. Standard spaces shall comprise not less than sixty-five percent of all newly constructed lot spaces.

**Applicant Response:**

The total parking spaces provided is 218 spaces, of which 188 spaces are standard spaces. This represents 86 percent of the spaces provided.

**17.60.140 Bicycle Parking.**

A. Bicycle parking facilities shall be required as follows:

- 1. In any commercial (C-1, C-2, and C-3) or office/residential (O-R) zone, bicycle parking facility requirements shall be based on the amount of automobile parking required. The minimum number of bicycle parking spaces provided shall be ten percent of the automobile parking spaces required.
- 2. The uses exempted from bicycle parking requirements include: residential uses, drive-in theaters, mortuaries, motels, hotels, and automobile service stations.

**Applicant Response:**

*As this is a multi-family residential use Bicycle Parking spaces are not required. However, the applicant is proposing to provide bicycle parking (bike racks) at each building. The proposed Site Plan includes 40 Bicycle Parking Spaces.*

**CHAPTER 17.61 SOLID WASTE AND RECYCLING ENCLOSURE PLAN**

**17.61.030 Guidelines and Standards.**

- A. The location of an enclosure must allow for collection agency drive-in access. A fifty-foot (50) access approach is recommended. In addition to the approach, either an exit that allows the truck to move forward or a turn area with a minimum radius of 46.5 feet is preferred. Both approach and location shall be unobstructed and free of over head wires and low hanging trees. An eighteen-foot (18) minimum height clearance above the enclosure approach is required and a thirty-two-foot (32) vertical clearance is required above the container itself. The enclosure shall be of sufficient size to store trash and recycling receptacles, the size of which shall be determined by the collection agency and will be based on proposed use. A minimum distance of two-feet (2) is required between the container and existing or proposed structures. The enclosure shall be a minimum of six-feet (6) tall or six inches (6) higher than the top of the tallest container.

**Applicant Response:**

*The proposed Site Plan provides the required front access and turn radii. There are no overhead wires located within the subject site.*

*Due to limited access and site constraints, a Fire Truck Turnaround is provided at the rear of the site. This turnaround may be used by the trash collection vehicles and provides adequate maneuvering for vehicle turnaround. The proposed Site Plan has been submitted to Recology Western Oregon (collection service) for review and approval.*

*All proposed Trash Enclosure structures are located in excess of the 20-foot minimum.*

*The proposed Trash Enclosure has been designed in accordance with Recology Western Oregon Solid Waste Plan Guide and Standards.*

- B. Solid waste enclosures shall not be located within twenty-feet (20) of a required front or exterior yard and should be placed at the rear of a building whenever possible. Should an enclosure be placed within a required landscaped front or exterior yard, additional landscaping must be provided elsewhere on the property to compensate for the encroachment into the required landscaped yard. Any modifications to the required landscaping must meet the approval of the Landscape Review Committee.

**Applicant Response:**

*The proposed Site Plan does not locate trash enclosures within the front yard setbacks and the subject site does not contain exterior yards. The proposed Site Plan does locate Trash Enclosures 5 feet from the adjacent side yards. A 6' high sight obscuring fence is proposed to be located along the property line.*

- C. Any trash or recycling enclosure which is visible from the street must provide landscaping around three (3) sides of the structure. Climbing vines and screening shrubs or hedges are

appropriate and landscaping must be a minimum of three-feet (3) in height at the time of planting.

**Applicant Response:**

*The are no Trash Enclosures that will be visible from the street therefore this is not applicable.*

- D. Where a commercial or industrial zone abuts a residential zone, enclosures must be placed a minimum of thirty-feet (30) from any residential structure or as otherwise approved by the Planning Director.

**Applicant Response:**

*The adjacent properties to the North of the subject site are zoned C-3 Commercial. The property adjacent the subject site on the South is outside the City of McMinnville and the Urban Growth Boundary. The property to the West of the subject site is zoned residential and there are no trash enclosures proposed to be located within so feet of this adjacent residential property. The proposed trash enclosure at the rear of the site adjacent the North property line is 28'-4" East of this Westerly property line. Additional landscaping and screening is proposed adjacent this enclosure. There is also a six-foot sight obscuring fence proposed along westerly property line.*

*An Administrative Variance is being requested for this reduced setback.*

- E. Generally, the design of the structure should match the exterior surface of the building and can be constructed of masonry, wood or concrete blocks in combination with plant material capable of forming a complete evergreen hedge. The floor of the enclosure shall be a concrete holding pad which must extend eight-feet (8) beyond the gates.

**Applicant Response:**

*The proposed Trash Enclosures are to be constructed of split-faced concrete masonry units with a solid cap. Landscape screening is proposed along the sides of the trash enclosures to provide the required evergreen hedge.*

*The concrete slab under the enclosure and in front of the enclosure will be reinforced and extend a minimum of 10 feet in front of the enclosure.*

- F. Gates that screen the containers are required and must remain closed at all times except at times of service.

**Applicant Response:**

*Painted steel gates are proposed on all trash enclosures. This is illustrated in the Trash Enclosure Plan 5/A1.4, and the Trash Enclosure Elevations, 6/A1.4.*

- G. Parking is prohibited in front of the enclosure and all parked vehicles must be located at a safe distance. A "No Parking" sign must be visibly placed on the gates of the enclosure.

**Applicant Response:**

*The proposed Site Plan shows "NO PARKING" pavement markings directly in front of the trash enclosures.*

- H. Solid waste and recycling enclosures must be placed in a location that is compatible with the City of McMinnville's Fire Code.

**Applicant Response:**

*All proposed trash enclosure locations provide substantial separation from buildings as required by the Fire Code.*

## CHAPTER 17.74 REVIEW CRITERIA

### 17.74.070 Planned Development Amendment - Review Criteria.

An amendment to an existing planned development may be either major or minor. Minor changes to an adopted site plan may be approved by the Planning Director. Major changes to an adopted site plan shall be processed in accordance with Section 17.72.120, and include the following:

- An increase in the amount of land within the subject site;
- An increase in density including the number of housing units;
- A reduction in the amount of open space; or
- Changes to the vehicular system which results in a significant change to the location of streets, shared driveways, parking areas and access.

An amendment to an existing planned development may be authorized, provided that the proposal satisfies all relevant requirements of this ordinance, and also provided that the applicant demonstrates the following:

- A. There are special physical conditions or objectives of a development which the proposal will satisfy to warrant a departure from the standard regulation requirements;

**Applicant Response:**

*The current Planned Development Overlay and previously approved Planned Development Amendment, Ordinance 5072, did not include a Master Plan, or specific Site Plan of the Subject Site. Therefore, the proposed Site Plan and Development is subject to the review and the approval criteria for a Planned Development Amendment.*

- B. Resulting development will not be inconsistent with the Comprehensive Plan objectives of the area;

**Applicant Response:**

*The proposed Planned Development Amendment and Development application has been designed to be consistent with the Comprehensive Plan objectives, Goals and Policies. Narrative addressing compliance of the Goals and Policies is included as a part of this overall application.*

- C. The development shall be designed so as to provide for adequate access to and efficient provision of services to adjoining parcels;

**Applicant Response:**

*The subject site is adjacent to existing development and the City of McMinnville city limits and the Urban Growth Boundary. The existing Altimus Medical Plaza Building complex is adjacent to the North, the Willamette Valley medical Center Hospital is*



adjacent Across SE Norton Lane to the East, the Evergreen Mobile Home Park is adjacent to the West and the city limits and UGB is adjacent to the South.

The proposed Site Plan and site design provides for the extension and Public improvements to SE Norton Lane for anticipated future development to the South.

- D. The plan can be completed within a reasonable period of time;

**Applicant Response:**

The proposed Development will be completed in a timely manner. The owners anticipate the start of construction as soon as permits can be obtained following the land-use approval process. Construction is anticipated to begin spring of 2024.

- E. The streets are adequate to support the anticipated traffic, and the development will not overload the streets outside the planned area;

**Applicant Response:**

The subject site is located is located on SE Norton Lane at the intersection of SE Stratus Avenue. SE Norton Lane, which is classified as a minor collector, provides direct access to Highway 18, which is classified as a major arterial. This network of roadways provides adequate access for vehicular traffic serving the proposed development.

A Transportation Impact Study has been prepared and is included with the overall land-use application and Planned Development Amendment application.

- F. Proposed utility and drainage facilities are adequate for the population densities and type of development proposed;

**Applicant Response:**

The proposed Site Plan and proposed site improvements will provide adequate utility and drainage services for the scope of the project and proposed number of living units.

There is an existing 10" public water main located with the SE Norton Lane right-of-way. A proposed 4" domestic water service and 6" fire suppression service will be installed on-site connecting to the existing public water main.

There is an existing 8" public sanitary service line located within the SE Norton Lane right-of-way. A new private sanitary service line will be installed throughout the site to serve the proposed buildings and living units. Due to existing grade elevations, a sanitary lift station will be provided on the subject site for gravity drainage to the public line.

There is an existing 15-foot private drainage easement running north-south along the westerly boundary of the subject site. A new on-site storm drainage system is proposed beneath the drive aisles, including required underground stormwater storage-detention before discharging into a riprap stormwater discharge facility. The stormwater discharge will be at pre-development flow rates in accordance with the State of Oregon Drainage laws.

- G. The noise, air, and water pollutants caused by the development do not have an adverse effect upon surrounding areas, public utilities, or the city as a whole.

**Applicant Response:**

*The proposed development of multi-family apartments units is consistent with the anticipated uses of the subject site as identified in the Comprehensive Plan and the Three Mile Lane Area Plan. As a proposed multifamily development, the use will not generate or produce excessive noise or air pollution nor will it generate water pollution having adverse effects upon the surrounding community of the city.*



**PROJECT DATA**

**SITE INFORMATION:**  
 ZONING: C3 - GENERAL COMMERCIAL, THREE-MILE LANE PLANNED DEVELOPMENT OVERLAY  
 MAP NO: T4SR4W-27, TAX LOT 701  
 SITE AREA: 214,759 SF, 4.93 ACRES

**BUILDING INFORMATION**  
 CONSTRUCTION TYPE: TYPE V-B  
 BUILDING HEIGHT: THREE-STORIES  
 OCCUPANCY GROUPS: R-2 RESIDENTIAL, APARTMENTS

**LIVING UNITS:**  
 ONE-BEDROOM, 1 BATH: 42 UNITS  
 TWO-BEDROOM, 1 BATH: 36 UNITS  
 TWO-BEDROOM, 2 BATH: 42 UNITS  
 THREE-BEDROOM, 2 BATH: 18 UNITS  
 TOTAL UNITS: 138 UNITS

**PARKING:**  
 REQUIRED:  
 1BR AND 2BR UNITS, 1.5 SPACES/UNIT= 180 SPACES  
 3BR UNITS, 2 SPACES/UNIT= 36 SPACES  
 TOTAL REQUIRED: 216 SPACES

**PROVIDED:**  
 STANDARD SPACES: 188 SPACES  
 COMPACT SPACES: 22 SPACES  
 ACCESSIBLE SPACES: 8 SPACES  
 TOTAL PARKING PROVIDED: 218 SPACES

**PROJECT TEAM**

**OWNER/DEVELOPER:**  
 KWDS, LLC  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**GENERAL CONTRACTOR:**  
 KOHL INC.  
 PO BOX 145  
 WILSONVILLE, OREGON 97070  
 (503) 781-5685  
 CONTACT: CHAD JURANEK

**ARCHITECT/PLANNER:**  
 REITER DESIGN ARCHITECT, INC.  
 7965 SW CIRRIUS DRIVE  
 BEAVERTON, OREGON 97008  
 (503) 574-3036  
 PROJECT ARCHITECT: SCOTT REITER

**CIVIL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 250 NW FRANKLIN AVE., SUITE 404  
 BEND, OREGON 97703  
 (541) 318-1161  
 PROJECT ENG.: JENNIFER VAN CAMP, PE

**STRUCTURAL ENGINEER:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT ENGINEER: JEFF SCHWINDT, PE

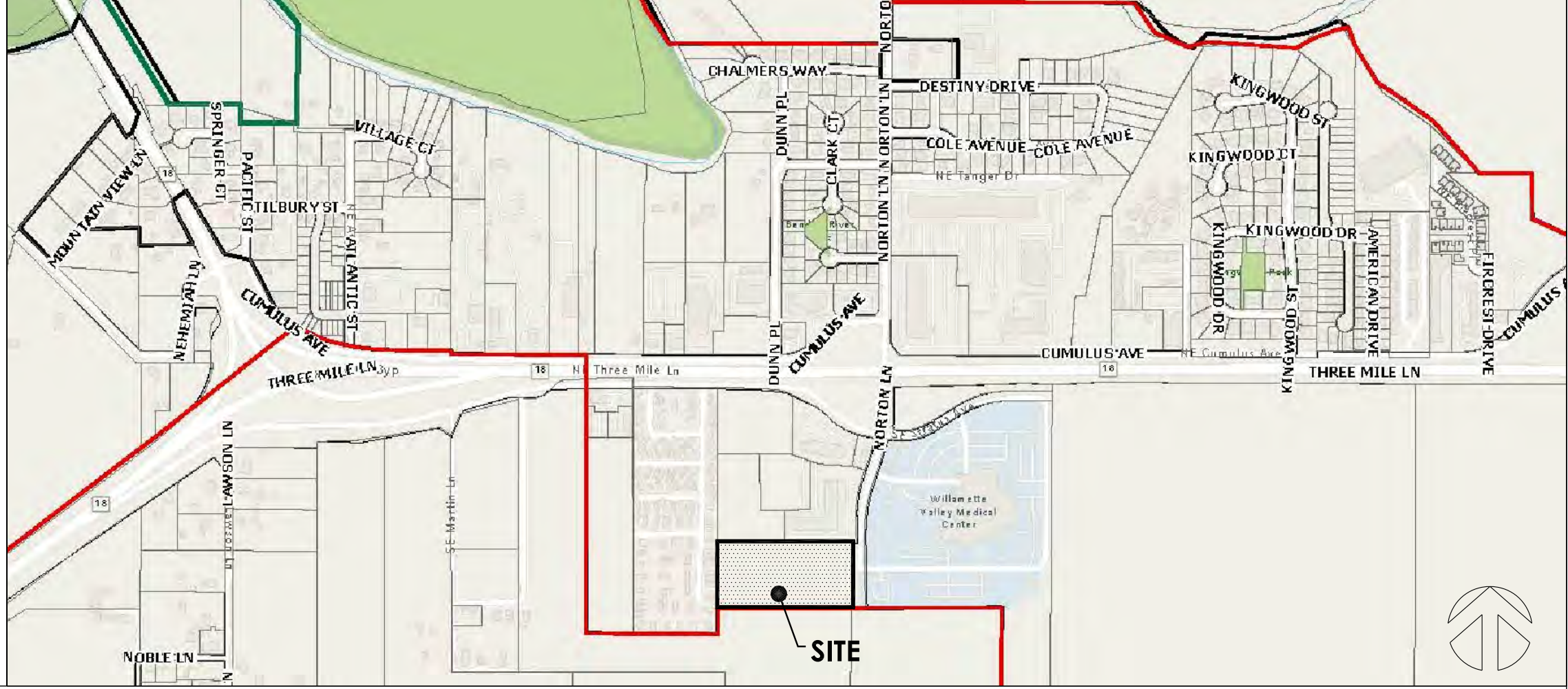
**TRANSPORTATION ENGINEER:**  
 LANCASTER-MOBY  
 321 SW 4TH, AVE, SUITE 400  
 PORTLAND, OREGON 97204  
 (503) 248-0313  
 CONTACT: DANIEL STUMPF

**LANDSCAPE ARCHITECT:**  
 HARPER HOUF PETERSON RIGHELLI, INC.  
 205 SE SPOKANE ST., SUITE 200  
 PORTLAND, OREGON 97202  
 (503) 221-1131  
 PROJECT LANDSCAPE ARCH.: JEFFERY CREEL

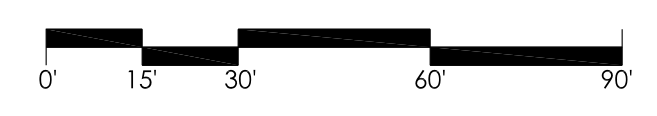
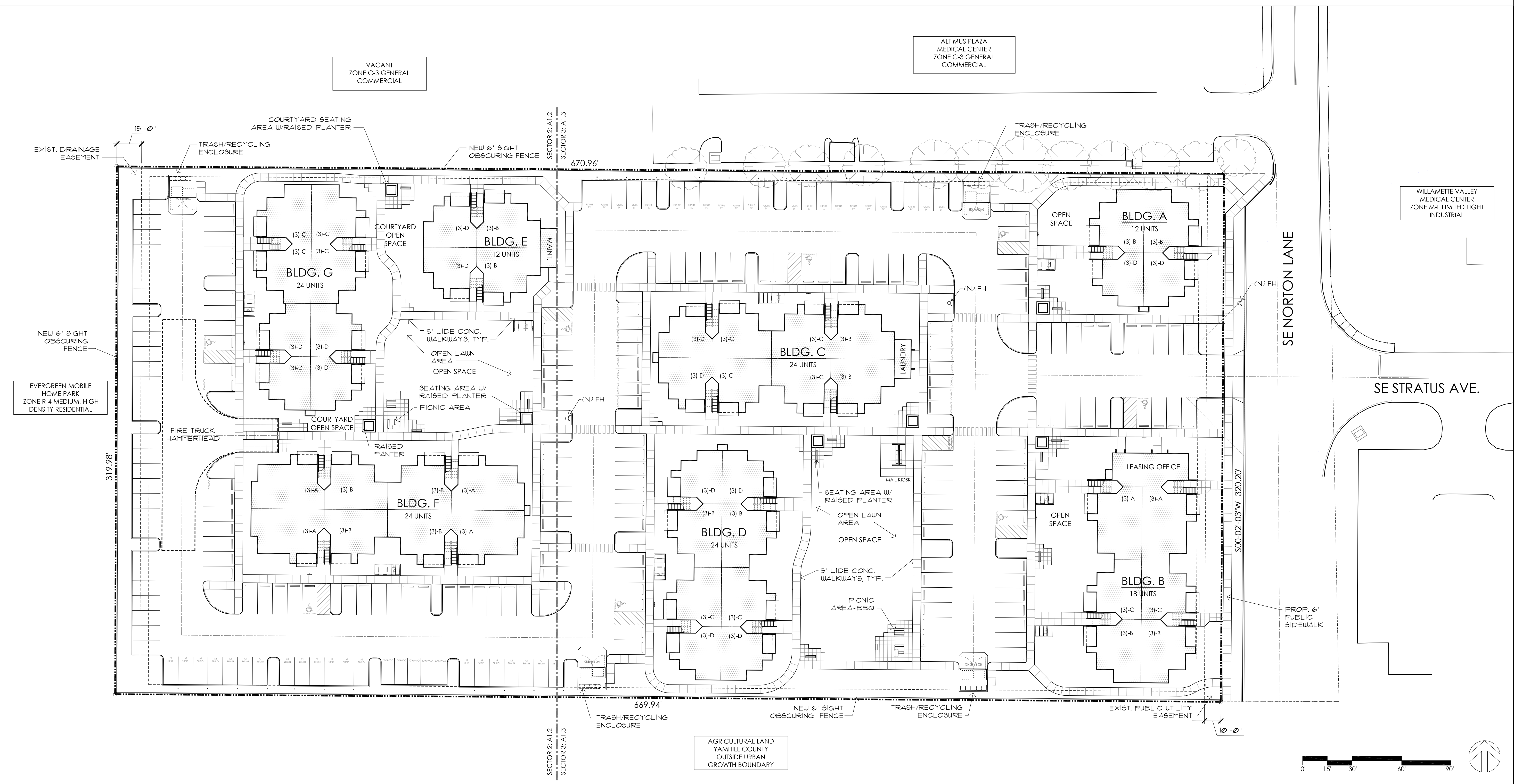
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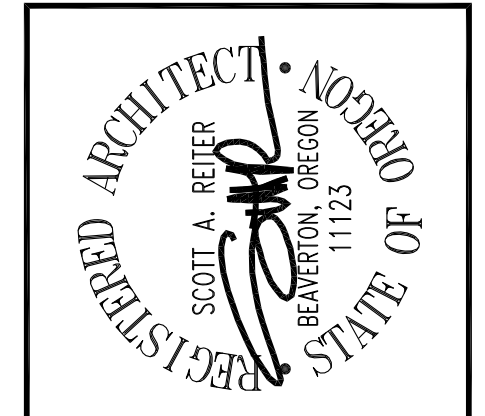
**VICINITY MAP**



**ATTACHMENT 7**



**CONCEPTUAL SITE PLAN** 1  
 Scale: 1" = 30'-0" A1.0



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3036

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

CONCEPTUAL SITE PLAN

| DATE               | REVISIONS |
|--------------------|-----------|
| SEPTEMBER 20, 2023 | AS NOTED  |
| AS NOTED           |           |

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 job no.: 2301

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LAND-USE REVIEW

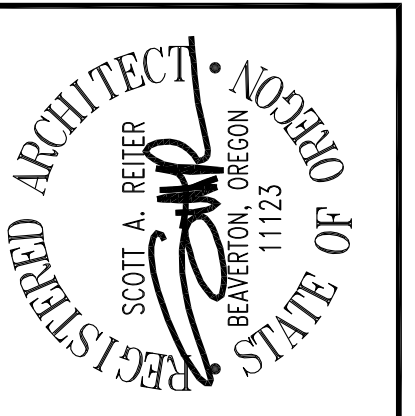
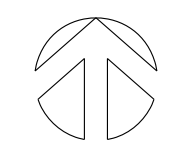
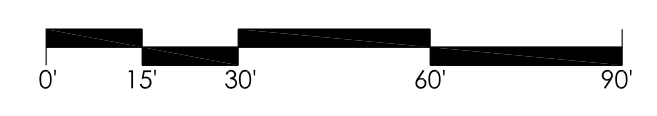
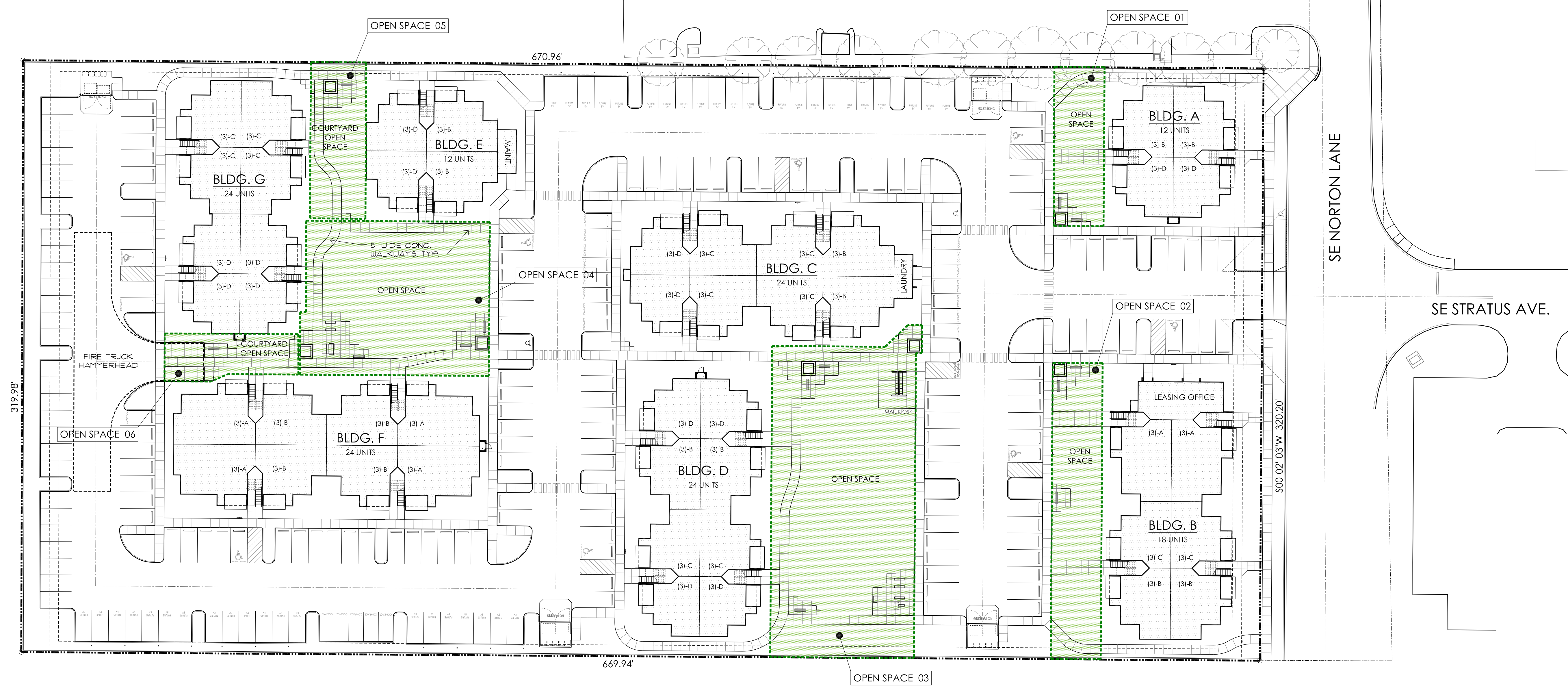


OPEN SPACE ANALYSIS

REQUIRED OPEN SPACE:  
 SITE AREA: 214,759 SF  
 REQUIRED OPEN SPACE 15%: 32,214 SF  
 PROVIDED OPEN SPACE: 32,562 SF

COMMON OPEN SPACE  
 OPEN SPACE 01: 2,329 SF  
 OPEN SPACE 02: 4,318 SF  
 OPEN SPACE 03: 13,233 SF  
 OPEN SPACE 04: 8,371 SF  
 OPEN SPACE 05: 2,576 SF  
 OPEN SPACE 06: 1,735 SF  
 TOTAL OPEN SPACE: 32,562 SF

PRIVATE OPEN SPACE  
 GROUND FLOOR UNIT PATIOS: 96 SF EACH  
 UPPER FLOOR UNIT DECKS: 72 SF EACH



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056

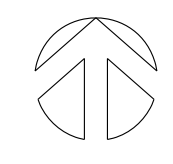
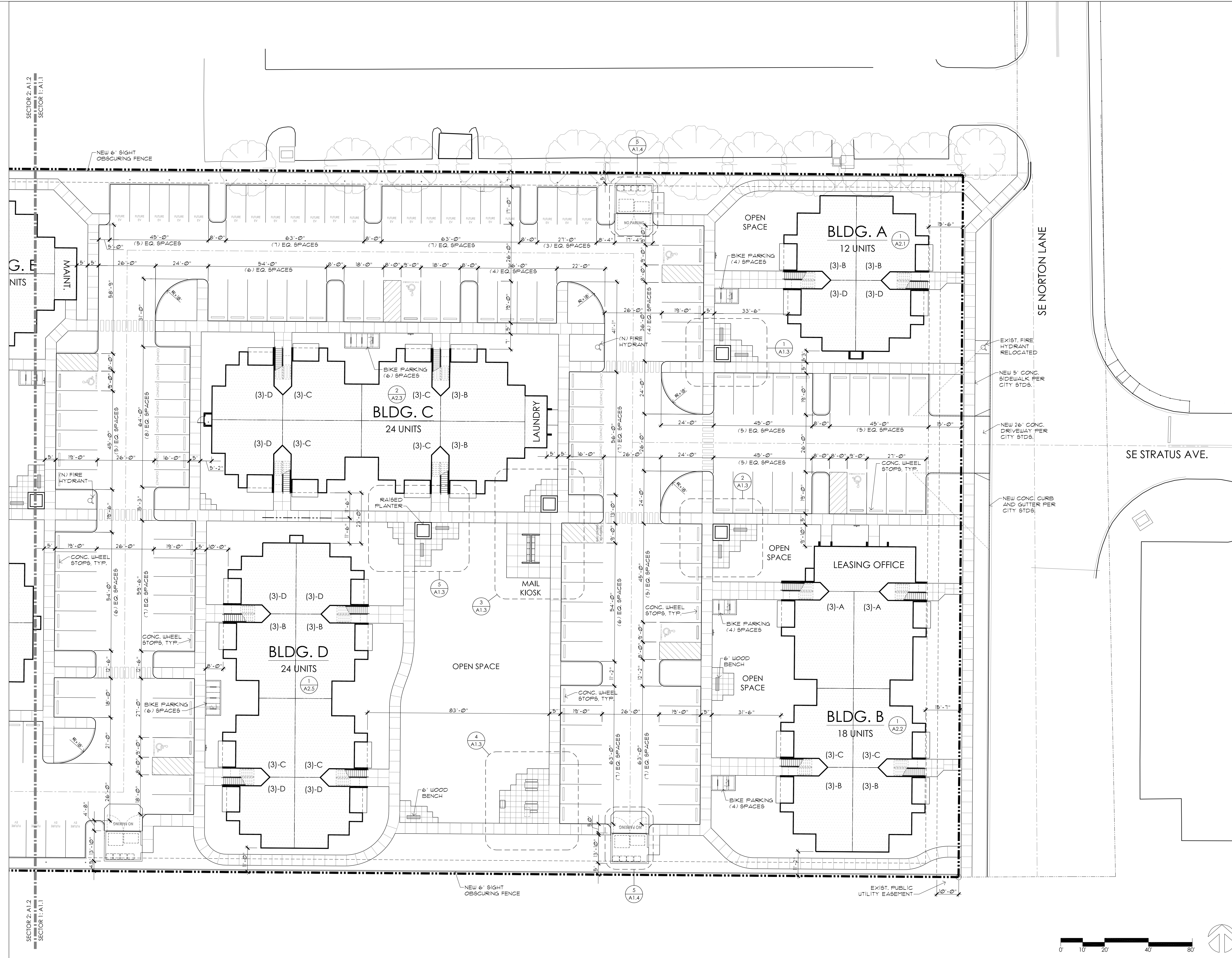
**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

LAND-USE REVIEW

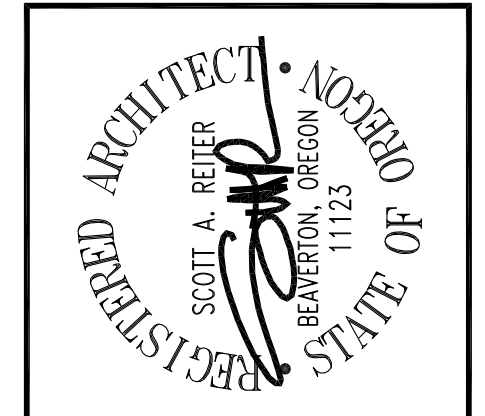
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| date: SEPTEMBER 15, 2023                    | AS NOTED   |
| scale:                                      |            |
| drawn:                                      |            |
| job no.: 2301                               |            |

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ENLARGED SITE PLAN - SECTOR 1 1  
 Scale: 1" = 20'-0" A1.1



**RDA**  
 REITER DESIGN ARCHITECT  
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 BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
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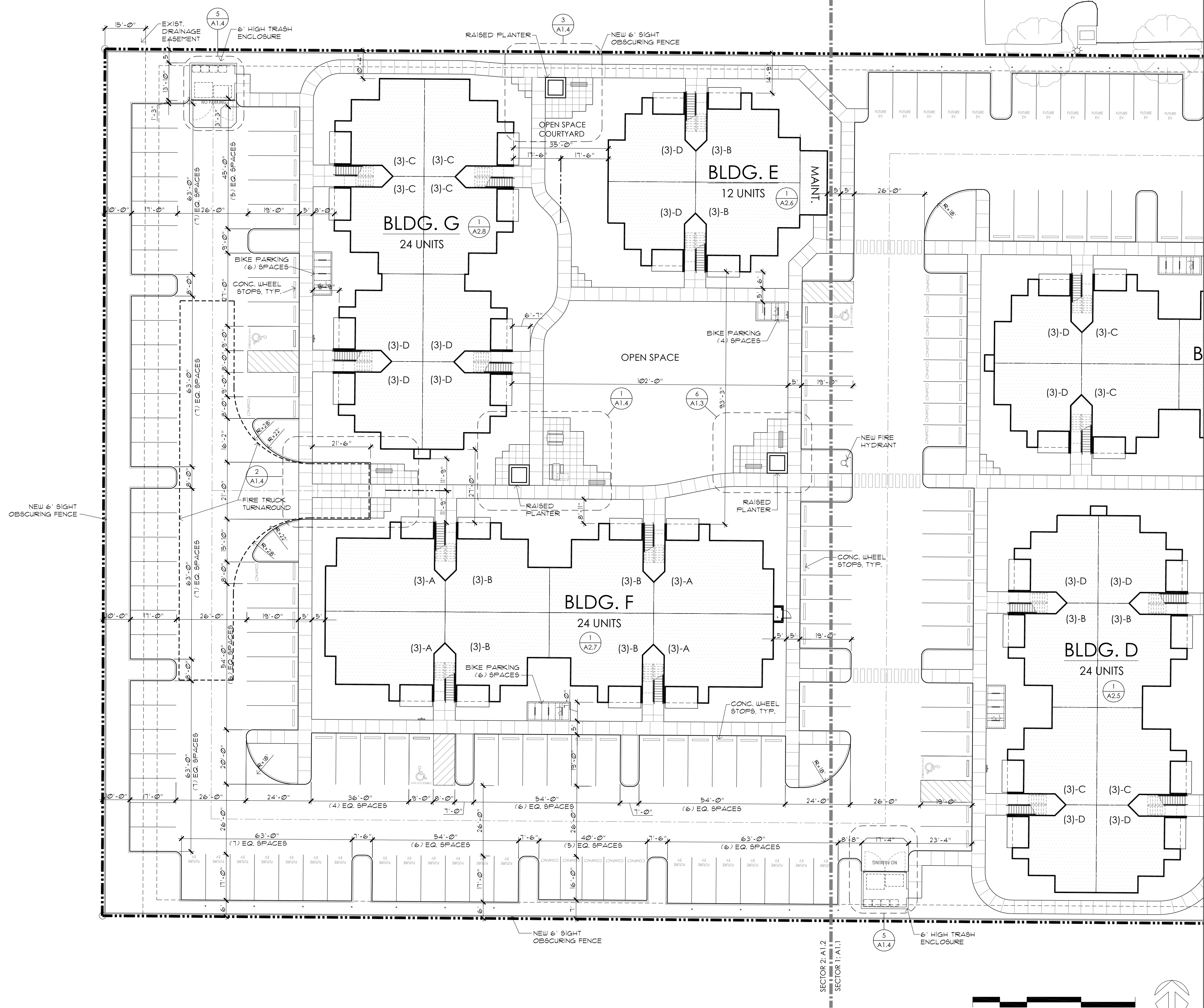
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| scale:   | AS NOTED           | drawn:     |  |
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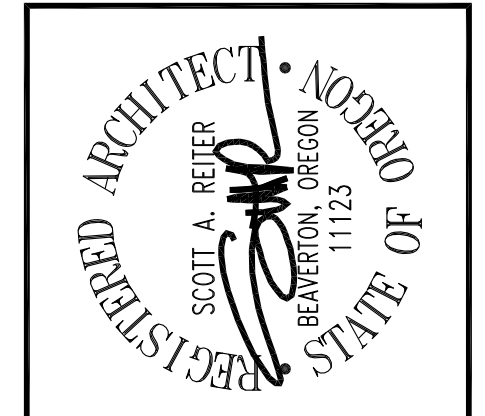
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LAND-USE REVIEW





ENLARGED SITE PLAN - SECTOR 2 1  
 Scale: 1" = 20'-0" A1.2



**RDA**  
 REITER DESIGN ARCHITECT  
 INCORPORATED  
 6107 SW MURRAY BLVD. #480  
 BEAVERTON, OREGON 97008 (503) 574-3056

**NORTON LANDING APARTMENTS**  
 SE NORTON LANE AND SE STRATUS AVE. MCMINNVILLE, OREGON  
 OWNER:  
**KWDS, LLC**  
 PO BOX 145 WILSONVILLE, OREGON 97070 (503) 781-5685

ENLARGED SITE PLAN - SECTOR 2

| DATE               | REVISIONS |
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| SEPTEMBER 20, 2023 | AS NOTED  |
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|                    |           |
|                    |           |

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 SCALE: AS NOTED  
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 JOB NO.: 2301

LAND-USE REVIEW

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# NORTON LANDING APARTMENTS

SE Norton Lane at SE Stratus Avenue

## Three Mile Lane Design Review Application Narrative

August 20, 2023

### **Project Description:**

The proposed project consists of the development of 138 apartment dwelling units located in seven three-story buildings on a 4.93 acre vacant site. The proposed development includes related site improvements and 3/4 public street improvements of SE Norton Lane along the subject site frontage.

The subject site has limited access to SE Norton Lane and has no other public frontage or access. The subject site has access to public utilities. There is an existing shallow 8" public sanitary sewer located in SE Norton Lane. There is an existing public water line located in SE Norton Lane. There is an existing 15-foot private storm drainage easement along the westerly property line. There is also a 10-foot public utility easement along the SE Norton Lane frontage.

The subject site lies within the Three Mile Lane Planned Development Overlay and the Three Mile Lane Area Plan. The subject site is subject to Planned Development Amendment Approval, Three Mile Lane Design Review, and the conditions of Approval of the prior Ordinances affecting the subject Site, including Ordinance 4709 and Ordinance 5072.

### **ASSESSOR MAP/TAX LOT**

Section 27, T.4S, R4W, WM, Tax Lot 701

### **ZONING:**

C-3 PD: General Commercial, Planned Development (Three Mile Lane PD Overlay)  
Ordinance 4709  
Ordinance 5072 (PDA 6-18)

Adjacent Property Zoning:

**NORTH:** C-3 PD: General Commercial  
**EAST:** M-L: Limited Light Industrial  
**SOUTH:** Outside Urban Growth Boundary, Yamhill County  
**WEST:** R-4 Medium, High Density Residential

### **DEVELOPMENT APPLICATION APPROVALS REQUESTED:**

The following land-use applications are being concurrently submitted for approval:

Planned Development Amendment to Ordinance 4709 and Ordinance 5072  
Three Mile Lane Design Review  
Landscape Plan Review  
Administrative Variances, Planning Commission Authority Variance

### **EXISTING CONDITIONS**

The Subject Site is currently undeveloped and used for farming. The site is generally flat, with a minor slope to the southwest. There are no significant or distinguishing natural features associated with this property.

Access to the property is from Highway 18 via the intersection with Norton Lane. There is a signalized intersection of Norton Lane (SE & NE) with Highway 18. Immediately south of the highway, there is a three-way stop intersection of SE Norton Lane and SE Stratus Avenue. Stratus Avenue loops around the Medical Center and intersects back with SE Norton Lane across from the subject site.

### **ACCESS and UTILITIES**

The property fronts on west side of SE Norton Lane, just south of the Altimus Plaza medical offices. The existing right-of-way for SE Norton Lane is 60 feet down to and through the Stratus Avenue Loop intersection, south of the Medical Center. The existing improvements extend through this intersection. The street is currently improved with curb and gutter and 28-foot paved section.

SE Norton Lane is improved to City standard south to the intersection with the Stratus Avenue loop. The street improvements extend approximately 180 feet along the frontage of the Subject Site. There is an additional 280 feet of site frontage that is currently not improved.

There are existing urban services and utilities within SE Norton Lane. These facilities are all available and were adequately sized to serve development of the subject site, consistent with the allowed commercial uses identified within Ordinance 4709.

### **SURROUNDING USES**

**North** - The property immediately north of the subject property is the Altimus Plaza Medical Offices. Further to the north at the Stratus Avenue intersection are the Comfort Inn & Suites and the Diner restaurant. The land west of the motel is vacant land, however there is a current application for development of multifamily residential.

**East** - The property immediately east of the subject property is developed with medical offices, specifically Willamette Valley Medical Center, including a Heliport. To the south of the medical center is vacant farm land, outside of the city limits and UGB. The Evergreen Aviation Museum, north of Hwy 18, and Municipal Airport, south of Hwy 18, are located just over a mile to the east.

**South** - The property immediately south of the subject property is vacant farm land, outside of the city limits and UGB.

**West** - The property immediately west of the subject property is the Evergreen Estates Mobile Home Park, which is zoned R-4.

### **NEIGHBORHOOD MEETING**

A Neighborhood Meeting was held on June 28, 2023. The Neighborhood Meeting Notice was mailed, posted and held in compliance with the requirements of McMinnville Zoning Ordinance, Chapter 17.72.095 Neighborhood Meetings.

The Neighborhood Meeting materials, notes and documentation of compliance with McMinnville Zoning Ordinance, Chapter 17.72.095 Neighborhood Meetings, are included within the Planned Development Amendment application.

### **RELATED ORDINANCES, PLANS AND POLICES:**

In addition to compliance of the applicable requirements of the City of McMinnville Zoning Ordinance, there are other related ordinances, plans and policies that the proposed development must demonstrate compliance of their applicable conditions of approval and related policies.

The following narrative provides the applicants response to the following:

- Ordinance 4709, Conditions of Approval
- Ordinance 5072, Conditions of Approval
- McMinnville Comprehensive Plan, Volume II, applicable Goals and Policies
- Three Mile Lane Area Plan, Ordinance 5126, Policies

### **Prior Land-Use Decisions Affecting Subject Site:**

#### **ORDINANCE 4131: THREE MILE LANE**

By Ordinance 4131, the City of McMinnville established Planned Development requirements for the Three Mile Lane area (Oregon Highway 18 Corridor). The area covered by this requirement extends north and south of Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18.

This ordinance established the Three-Mile Lane Planned Development overlay and identified specific conditions and policies for development. The ordinance also identified procedures for review and approval of proposed developments within the subject area.

#### **ORDINANCE 4709: (CPA 6-99/ZC 11-00)**

By Ordinance 4709, the City of McMinnville amended the Comprehensive Plan Map and rezoned the subject site to C-3 PD (General Commercial Planned Development), subject to 20 Conditions of Approval.

#### **ORDINANCE 5072: (PDA 6-18)**

By Ordinance 5072, the City of McMinnville amended and revised Condition 15 of Ordinance 4709 to allow multiple family dwellings. The original Condition 15 of Ordinance 4709 specifically limited the allowable uses which did not allow multiple family dwellings.

The revised Condition 15 applies specifically to Tax Lot 701. All other requirements and conditions of approval from Planned Development Ordinance 4709 remain in effect.

## **Conditions of Approval – Prior Ordinances:**

### **ORDINANCE 4709: CONDITIONS OF APPROVAL:**

1. That the zone change request shall not take effect until and unless CPA 6-99 is approved by City Council.

#### **Applicant Response:**

*This condition of approval is no longer applicable and Ordinance 4709 was approved by City Council.*

2. That the development and use of the site is subject to the provisions of the Three Mile Lane Planned Development Ordinance, as amended. Further, that a detailed Master Plan for the subject site shall be submitted to the McMinnville Planning Commission for review and approval, pursuant to the procedures of McMinnville Zoning Ordinance 17.51, Planned Development Overlay, prior to any development occurring on the site. The plan shall include, at a minimum, proposed land uses and their locations, building locations, proposed circulation patterns, proposed open spaces, grading and drainage information, location and size of public utilities and services, off-street parking areas, direct pedestrian access, and other information deemed necessary to convey the details of the proposed development plans to the Planning Commission.

#### **Applicant Response:**

*This current application is for a Planned Development Amendment to include the proposed Master Plan for the site. The application includes the detailed drawings and design as outlined in the above condition of approval. This application also includes the application for the Three Mile Lane Design Review.*

3. That detailed plans for the proposed commercial development showing site layout, proposed circulation pattern, signage, building elevations, landscaping, parking, and lighting must be submitted to and approved by the Three Mile Lane Design Review Committee prior to issuance of any building permits for said development. Approval or denial of such plans shall be based upon findings that, to the extent possible, the building and site design employs principles that will ensure compatibility with adjacent development and provide an architectural style appropriate to a “gateway” or entrance to the City of McMinnville. In addition, approval or denial of the exterior building design shall be based on a finding that, to the extent possible, the building design incorporates design and architectural features that would serve to break up the building’s horizontal plane and provide visual interest, This may include, but is not limited to, the use of vertical columns, gables, variety of compatible and complementary building materials, providing openings in the building façade, and landscaping at the building perimeter. Submitted plans should include detail as regard to building colors and materials (provide texture and relief), building height, planting design, window treatment, vertical and horizontal articulation, massing, voids to solids relationships, and other elements appropriate to ensure that the building and site design complies with the objectives and requirements of this planned development approval.

The provisions of Chapter 17.51 of the McMinnville Zoning Ordinance may be used to place conditions of any development and to determine whether or not specific uses are permissible. The applicant may appeal the decisions of the Three Mile Lane Design Review Committee to the Planning Commission if notice of the appeal is filed in the Planning Commission office within 15 days of the Committee's decision.

**Applicant Response:**

*This application is for multifamily housing and is not a commercial proposal. Therefore this condition of approval is not applicable.*

4. That final development plans include landscape plans submitted to and approved by the McMinnville Landscape Review Committee and Three Mile Lane Design Review Committee. A minimum of 14 percent of the site must be landscaped. An arborvitae hedge or similar type of planted visual screen shall be required along the property lines adjacent to the residentially zoned land to the west. Landscaping emphasis shall exist along the site's eastern edge, adjacent to Norton lane, including required street trees, with particular emphasis at the site's eastern driveway intersections at Norton Lane. In addition, landscape islands are required to be located throughout the proposed off-street parking areas.

Street trees within a curbside planting strip along the Norton Lane frontage are required to have a two-inch minimum caliper, exhibit size and growing characteristics appropriate for the particular planting strip and be spaced as appropriate for the selected species and as may be required for the locations of above ground utility vaults, transformers, light poles, and hydrants. All street trees shall be of good quality and shall conform to American Standards for Nursery Stock (ANSI Z60.1). The Planning Director reserves the right to reject any plant material which does not meet this standard.

**Applicant Response:**

*This proposed development and application includes the Landscape Plan Review application. The landscape design for the site includes the required screening and buffering to the adjacent properties along the North, West and South property lines. There are also enhanced plantings along the SE Norton Lane frontage.*

*The proposed development application also includes the SE Norton Lane street frontage improvements. These include the required street trees. The proposed street trees meet the specified requirements.*

5. That final development plans for the subject site include a detailed storm drainage plan which incorporates the requirements of City's Storm Drainage Master Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements needed to comply with the approved plan must be reflected on the final plat. If the final storm drainage plan incorporates the use of collection systems and easements, such must be private, rather than public, and private maintenance agreements must be approved by the City for them.

**Applicant Response:**

*This application includes a Preliminary Grading and Drainage Plan, drawing C2.0. It is understood that complete drainage design must be provided in accordance with the City of McMinnville Storm Drainage Master Plan. Detailed drainage design and stormwater analysis will be provided for permit submittal to the City Engineering Department.*

6. The final development plans for the subject site include a detailed sanitary sewerage collection plan which incorporates the requirements of City's Collection Systems Facilities Plan. This plan must be submitted to, and approved by, the City Engineering Department prior to issuance of any development permits. Any utility easements

needed to comply with the structures located within the subject site are required to connect to the sanitary sewer systems as soon as service is available.

**Applicant Response:**

*This application includes a Preliminary Utilities Plan, drawing C3.0. It is understood that complete sanitary sewerage design must be provided in accordance with the City of McMinnville Collection Systems Plan. Detailed sanitary sewerage design and analysis will be provided for permit submittal to the City Engineering Department.*

7. That the developer secure from the Department of Environmental Quality (DEQ) applicable storm runoff and site development permits prior to construction of the required site improvements. Evidence of such permit shall be submitted to the City Engineer.

**Applicant Response:**

*It is understood that the owner/developer must secure the applicable Department of Environmental Quality (DEQ) stormwater runoff permits. Evidence of the permits will be submitted to the City Engineer as required.*

8. That the developer enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and grading permit for a lot fill agreement from the City Building Division. All fill placed in the areas where construction is expected shall be engineered and shall meet with approval of the City Building Division and the City Engineering Department.

**Applicant Response:**

*It is understood that the owner/developer must enter into a construction permit agreement with the City Engineering Department for all public improvements and gain a fill and grading permit for a lot fill agreement from the City Building Division. It is also understood that all fill placed in the areas where construction is expected shall be engineered and shall meet with approval of the City Building Division and the City Engineering Department.*

9. That the developer extend water and power services to the subject site in accordance with McMinnville Water and Light requirements, including any necessary contracts and/or easements. Said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.

**Applicant Response:**

*It is understood that the owner/developer extend water and power services to the subject site, if required by McMinnville Water and Light. The required improvements, if any, will be designed and installed to the requirements of McMinnville Water and Light. Although, there is an existing public water main located within the NE Norton Lane right-of-way and power currently serves the adjacent property to the north.*

*It is further understood that said water service is to include required fire hydrants, which are necessary to be in working order prior to the issuance of building construction permits.*



10. That utilities shall be extended to the property boundaries by the applicant, as may be required by the City Engineer or McMinnville Water and Light.

**Applicant Response:**

*It is understood that McMinnville Water and Light may require utility extensions to the subject site, if not currently provided. The required improvements, if any, will be designed and installed to the requirements of McMinnville Water and Light. Although, there is an existing public water main located within the NE Norton Lane right-of-way in front of the subject site and power currently serves the adjacent property to the north.*

11. That no building shall exceed the height of 35 feet.

**Applicant Response:**

*The Applicant is proposing a Variance through the Planning Commission Authority to request approval to allow a roof height of 37 feet to the highest ridge line. The detailed response to the requested Variance is included within the Variance Application Narrative.*

12. That if outside lighting is provided, it must be directed down and away from residential areas and public streets.

**Applicant Response:**

*The proposed site lighting design is included in this application, drawing ME1.0. The proposed lighting design provides full cut-off light fixture that prevent light from entering the adjacent properties and the lighting design does not shine light onto the SE Norton Lane right-of-way.*

13. That signs located on the site shall be subject to the requirements of McMinnville Ordinance 4572 (B).

**Applicant Response:**

*It is understood that any proposed signage must be designed to be in compliance with the City of McMinnville Ordinance 4572 (B). A small site identification sign is proposed and the location is noted on the Site Plan, drawing A1.0 and A1.1.*

14. That all business, storage, or displays shall be conducted wholly within an enclosed building; except for off-street parking and loading.

**Applicant Response:**

*The proposed development is for multifamily housing and this condition of approval is not applicable.*

15. That the subject site is limited to professional office use or medical office use, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use or medical office use may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site."

**Applicant Response:**

*This condition of approval has been amended to include multifamily housing. The revised condition was included in Ordinance 5072 (PDA 6-18). A respond to the amended condition of approval follows in the response to address Ordinance 5072 below.*

16. That if restrictive covenants are proposed for the development, they must meet with the approval of the Planning Director.

**Applicant Response:**

*There are no restrictive covenants proposed with this development application. This condition of approval is not applicable.*

17. That the approved Master Plan as approved by the Planning Commission shall be placed on file with the Planning Department and become part of the zone and binding to the owner and developer. The developer will be responsible for requesting permission of the Planning Commission for any major changes of the details of the final development plans. Minor changes to the details of the adopted plan may be approved by the City Planning Director. It shall be the Planning Director's decision as to what constitutes a minor or major change. An appeal from a ruling by him may be made only to the Commission. Review of the Planning Director's decision by the Planning Commission may be initiated at the request of any one of the commissioners.

**Applicant Response:**

*The requirements of this condition of approval are understood by the developer/owner.*

18. That improvements to the Norton Lane frontage, to the southern most entry drive, or as may otherwise be required by the City of McMinnville, shall be done at the developer's expense and be finalized prior to release of any occupancy permits. Plans for improvement to Norton Lane shall be submitted to the City Engineer for review and approval prior to its construction.

**Applicant Response:**

*It is understood that SE Norton Lane frontage improvements are required for the proposed development and that those improvements will be at the owner/developers expense. It is further understood that the design of the proposed SE Norton Lane improvements shall be submitted to the City engineer for review and approval. It is also understood that the frontage improvements must be completed before any occupancy permits will be issued.*

19. The developer/owner of the subject site must grant to the Willamette Valley Medical Center and the City of McMinnville the right to cause all airspace above the surface of the subject site such as noise, vibrations, fumes, dust, fuel particles and all other effects as may be caused by the operations of aircraft landing at or taking off or operating at or on the Medical Center property and the McMinnville Municipal Airport. The owner must fully waive any right or cause action which he may now or in the future arise against the Willamette Valley Medical Center and the City of McMinnville due to such circumstances.

**Applicant Response:**

*The owner/developer understands the requirements of this condition of approval.*

20. That prior to the development of the site, the applicant shall sign a waiver of remonstrance against the future development of Norton Lane. The waiver shall be prepared by the City.

**Applicant Response:**

*The owner/developer understands the requirements of this condition of approval.*

**ORDINANCE 5072: CONDITIONS OF APPROVAL:**

1. That Condition 15 of Ordinance 4709, as amended by Ordinance 4863, be amended as follows:

That the subject site is limited to professional office use or medical office use, or multiple family dwellings, senior condominiums, senior apartments, or assisted living facilities, or other compatible, small scale commercial uses such as delicatessen, florist, or day care facility. Uses other than professional office use, medical office use, multiple family dwellings, or senior condominiums, senior apartments, or assisted living facilities, may not exceed fifteen (15) percent of the total floor area proposed to be constructed within the subject site. Drive-up restaurants; automobile, boat, trailer, or truck rental sales or service; building materials supply stores; recreational vehicle parks; storage garage or mini-warehouse buildings; and, automobile service stations are prohibited from locating on the site.

If the site is developed as multiple family dwellings, an area equivalent to 10 percent of the gross area of the site shall be reserved for usable open space for residents of the multiple-family development site. The usable open space area shall be a contiguous area with each dimension being at least 25 feet in length, shall be located outside of the front yard setback area, and may be counted towards the minimum 25 percent of the site area that must be landscaped."

**Applicant Response:**

*The proposed development is for 138 units of multiple family dwellings (apartments) and is therefore a complying use.*

*The subject site is 214, 759 sf. The proposed Site Plan provides 32,769 sf of common open space, which is 15% of the gross site area and is therefore compliant.*

2. All other requirements and conditions of approval from Planned Development Ordinance 4709 shall remain in effect.

**Applicant Response:**

*The response to the remaining Conditions of Approval of Ordinance 4709 has been addressed above in the response to Ordinance 4709.*

## **McMinnville Comprehensive Plan: VOLUME II GOALS AND POLICIES:**

Volume II, Goals and Policies, contains the goal, policy, and proposal statements which shall be applied to all land use decisions. Goal, policy, and proposal statements each have different purposes: goal statements are the most general principles; policy statements are directed to specific areas to further define the goal statements; and proposals are possible courses of action open to the City which shall be examined to further implement the goal and policy requirements. Each of these statement types is further defined below:

**GOALS:** are the broadly-based statements intended to set forth the general principles on which all future land use decisions will be made. Goals carry the full force of the authority of the City of McMinnville and are therefore mandated.

**POLICIES:** are the more precise and limited statements intended to further define the goals. These statements also carry the full force of the authority of the City of McMinnville and are therefore mandated.

**PROPOSALS:** are the possible courses of action available to the City to implement the goals and policies. These proposals are not mandated; however, examination of the proposals shall be undertaken in relation to all applicable land use requests.

The implementation of these goals, policy, and proposal statements shall occur in one of two ways. First, the specific goal, policy, or proposal shall be applied to a land use decision as a criterion for approval, denial, or modification of the proposed request. In this case the goal, the policy, or the proposal is directly applied. The second method for implementing these statements is through the application of provisions and regulations in ordinances and measures created to carry out the goals and policies. This method involves the indirect application of the statements. These ordinances and measures are included in Volume III of the McMinnville Comprehensive Plan.

## **CHAPTER V: HOUSING AND RESIDENTIAL DEVELOPMENT**

### **GOAL V 1: TO PROMOTE DEVELOPMENT OF AFFORDABLE, QUALITY HOUSING FOR ALL CITY RESIDENTS.**

#### General Housing Policies:

- 58.00 City land development ordinances shall provide opportunities for development of a variety of housing types and densities.

***Applicant Response:***

*The proposed development provides multifamily housing that has been designed to be in compliance with the applicable sections of the City of McMinnville Zoning Ordinance and applicable ordinances within the planned development area of the subject site.*

- 59.00 Opportunities for multiple dwelling and mobile home developments shall be provided in McMinnville to encourage lower-cost renter and owner-occupied housing. Such housing shall be located and developed according to the residential policies in this plan and the land development regulations of the City.

**Applicant Response:**

*The proposed multifamily development has been designed to be in compliance with the applicable sections of the City of McMinnville Zoning Ordinance, including the Residential Design Standards for apartments, 17.11.090 and the applicable ordinances within the planned development area of the subject site.*

Low-Cost Housing Policies:

- 64.00 The City of McMinnville shall work in cooperation with other governmental agencies, including the Mid-Willamette Valley Council of Governments and the Yamhill County Housing Authority, and private groups to determine housing needs, provide better housing opportunities and improve housing conditions for low- and moderate-income families.
- 65.00 The City of McMinnville shall coordinate with the Mid-Willamette Valley Council of Governments to develop a "fair share" plan to allocate low-cost housing throughout the tri-county region.
- 66.00 The City of McMinnville shall continue to allow development of its fair share of the region's low-cost housing. The share accepted will be based on quantifiable studies which take into account the amount of the low-cost housing already in the community and the overall housing opportunities in the city and region.
- 67.00 Subsidized low-cost housing shall be dispersed throughout the McMinnville urban area. Dispersal plans shall be coordinated with appropriate agencies.

**Applicant Response:**

*The proposed development is for market-rate multifamily housing and is not proposed to be a low-income or subsidized development.*

**GOAL V 2: TO PROMOTE A RESIDENTIAL DEVELOPMENT PATTERN THAT IS LAND INTENSIVE AND ENERGY-EFFICIENT, THAT PROVIDES FOR AN URBAN LEVEL OF PUBLIC AND PRIVATE SERVICES, AND THAT ALLOWS UNIQUE AND INNOVATIVE DEVELOPMENT TECHNIQUES TO BE EMPLOYED IN RESIDENTIAL DESIGNS.**

Policies:

- 68.00 The City of McMinnville shall encourage a compact form of urban development by directing residential growth close to the city center, to designated neighborhood activity centers, and to those areas where urban services are already available before committing alternate areas to residential use. (Ord. 5098, December 8, 2020)

**Applicant Response:**

*The subject site is designated C-3 Commercial and has been identified for Medium-High Density Residential under the Three Mile Lane Planned Development Amendment to Ordinance 4709.*

*The subject site has existing urban services available to the site.*

**Planned Development Policies:**

- 72.00 Planned developments shall be encouraged as a favored form of residential development as long as social, economic, and environmental savings will accrue to the residents of the development and the city.

**Applicant Response:**

*The subject site is located within the Three Miler Lane Planned Development Overlay. The proposed development has been designed to be in compliance with the applicable ordinances within the Three Mile Lane PD overly including Ordinance 4709 and Ordinance 5072.*

- 74.00 Distinctive natural, topographic, and aesthetic features within planned developments shall be retained in all development designs.

**Applicant Response:**

*The subject site does not contain any distinctive natural or topographic features.*

- 75.00 Common open space in residential planned developments shall be designed to directly benefit the future residents of the developments. When the open space is not dedicated to or accepted by the City, a mechanism such as a homeowners association, assessment district, or escrow fund will be required to maintain the common area.

**Applicant Response:**

*The proposed Site Plan provides Common Open Spaces in compliance with 17.11.090. The proposed Common Open Spaces occur within the private market-rate apartment development and will be maintained by the development ownership.*

- 76.00 Parks, recreation facilities, and community centers within planned developments shall be located in areas readily accessible to all occupants.

**Applicant Response:**

*The proposed Site Plan provides Common Open for recreational use by the apartment residents. There are no other recreation facilities or community centers proposed within this development.*

- 77.00 The internal traffic system in planned developments shall be designed to promote safe and efficient traffic flow and give full consideration to providing pedestrian and bicycle pathways.

**Applicant Response:**

*The proposed development and Site Plan is limited to a single 4.93 acre site and does not include any internal public streets. The proposed Site Plan does include internal drive aisles and parking areas that will provide safe and efficient access to parking and the buildings throughout the site. There is a network of internal sidewalks and pathways that connect the parking, buildings and common open spaces to the public street frontage.*

- 78.00 Traffic systems within planned developments shall be designed to be compatible with the circulation patterns of adjoining properties.



**Applicant Response:**

*The subject site has limited public street frontage and is limited to a single driveway onto SE Norton Lane.*

**Multiple Dwelling Development Policies:**

- 86.00 Dispersal of new multi-dwelling housing development will be encouraged throughout the City in areas designated for residential and mixed-use development to encourage a variety of housing types throughout the community and to avoid an undue concentration of multi dwelling development in specific areas of the community leading to a segregation of multi dwelling development in McMinnville from residential neighborhoods. Dispersal policies will be consistent with the Great Neighborhood Principles.

In areas where there are the amenities, services, infrastructure and public facilities to support a higher density of multi-dwelling development, and the area is commensurate with a higher concentration of multi-dwelling development without creating an unintended segregation of multi-dwelling development, such as McMinnville's downtown, the area surrounding Linfield University and Neighborhood Activity Centers, a higher concentration of multi-dwelling development will be encouraged. (Ord. 5098, December 8, 2020)

**Applicant Response:**

*As provided for by the adoption of Ordinance 4709, amended by Ordinance 5072, the subject site is located in an area of McMinnville that has the existing infrastructure, public facilities to support the density of the proposed development. The proposed development will provide needed housing for the support staff of the adjacent medical facilities and the Willamette Valley medical Center Hospital, as well as other nearby employment centers.*

- 87.00 Residential developments at densities beyond that normally allowed in the multiple-dwelling zone shall be allowed in the core area subject to review by the City. These developments will be encouraged for (but not limited to) the provision of housing for the elderly.

**Applicant Response:**

*The proposed development is within the allowable density for the subject site. The subject site is not located within the core area and this does not apply.*

- 88.00 Deleted as per Ord. 4796, October 14, 2003.

- 89.00 Zoning standards shall require that all multiple-dwelling housing developments provide landscaped grounds. (Ord. 4796, October 14, 2003)

**Applicant Response:**

*The proposed development includes detailed Landscape Plans that have been designed for review in conjunction with all aspects of the McMinnville Zoning Codes. Multiple locations with Zoning Code require landscaping, including the 17.11 the Residential Design Standards. The proposed landscape design provides landscaping throughout all areas of the site not covered by buildings, parking or drive aisles.*

- 90.00 Greater residential densities shall be encouraged to locate along major and minor arterials, within one-quarter mile from neighborhood and general commercial shopping centers or within neighborhood activity centers, and within a one-half mile wide corridor centered on existing or planned public transit routes. (Ord. 5098, December 8, 2020; Ord. 4840, January 11, 2006; Ord. 4796, October 14, 2003)

**Applicant Response:**

*The subject site is located on SE Norton Lane at the intersection of SE Stratus Avenue. SE Norton Lane, which is classified as a minor collector, provides direct access to Highway 18, which is classified as a major arterial. This network of roadways provides adequate access for vehicular traffic serving the proposed development.*

*Yamhill County Transit, Route 2, serves SE Norton Lane with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave.*

- 91.00 Multiple dwelling housing developments, including condominiums, but excluding campus living quarters, shall be required to access off of arterials or collectors or streets determined by the City to have sufficient traffic carrying capacities to accommodate the proposed development. (Ord. 4573, November 8, 1994)

**Applicant Response:**

*The subject site and proposed development has access from SE Norton Lane which is a minor collector. Norton Lane connects directly to Highway 18 that is a major arterial. The existing network of public roadways has sufficient carry capacity for the traffic generated by the proposed development.*

*A Transportation Impact Study has been prepared and is included in this application.*

- 92.00 High-density housing developments shall be encouraged to locate along existing or potential public transit routes.

**Applicant Response:**

*The subject site is located along an existing public transit route, Yamhill County Transit, Route 2, with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave.*

- 92.01 High-density housing shall not be located in undesirable places such as near railroad lines, heavy industrial uses, or other potential nuisance areas unless design factors are included to buffer the development from the incompatible use.

**Applicant Response:**

*The subject site is not located near railroad lines or heavy industrial uses.*

- 92.02 High-density housing developments shall, as far as possible, locate within reasonable walking distance to shopping, schools, and parks, or have access, if possible, to public transportation. (Ord. 4796, October 14, 2003)

**Applicant Response:**

*The subject site is located along an existing public transit route, Yamhill County Transit, Route 2, with a transit stop located at the intersection of SE Norton Lane and SE Stratus Ave. The subject site is not located near schools, shopping or parks.*

- 92.03 Housing developments for the elderly shall, as far as possible, locate near community centers, parks, and shopping areas, or where transportation services can be provided to enable access to these activity areas. (Ord. 4796, October 14, 2003)

**Applicant Response:**

*The proposed project is not an Elderly Housing development.*

**Urban Policies:**

- 99.00 An adequate level of urban services shall be provided prior to or concurrent with all proposed residential development, as specified in the acknowledged Public Facilities Plan. Services shall include, but not be limited to:
1. Sanitary sewer collection and disposal lines. Adequate municipal waste treatment plant capacities must be available.
  2. Storm sewer and drainage facilities (as required).
  3. Streets within the development and providing access to the development, improved to city standards (as required).
  4. Municipal water distribution facilities and adequate water supplies (as determined by City Water and Light). (as amended by Ord. 4796, October 14, 2003)

**Applicant Response:**

*The subject site has existing water and sanitary sewer lines located within the SE Norton Lane right-of-way. The existing sanitary sewer is an 8" main, which has adequate capacity for the proposed development.*

*There is an existing 10" water main located in the SE Norton Lane right-of-way. This line has adequate capacity for domestic and fire line services for the proposed development and additional new fire hydrants.*

*The subject site is accessed by SE Norton Lane, which currently terminates just south of the intersections with SE Stratus Avenue. The proposed development will provide public street frontage improvements along the SE Norton Lane right-of-way to the southern boundary of the site and the current McMinnville City limits.*

*There are no public streets proposed or required within the proposed development. The Site Plan provides a network of drive aisles and parking areas serving the buildings and living units. A Fire Truck turnaround has been provided at the rear of the site.*

**Urbanization, Great Neighborhood Principles:**

- 187.10 The City of McMinnville shall establish Great Neighborhood Principles to guide the land use patterns, design, and development of the places that McMinnville citizens live, work, and play. The Great Neighborhood Principles will ensure that all developed places include characteristics and elements that create a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood with enduring value, whether that place is a completely new development or a redevelopment or infill project within an existing built area.
- 187.20 The Great Neighborhood Principles shall encompass a wide range of characteristics and elements, but those characteristics and elements will not function independently. The Great Neighborhood Principles shall be applied together as an integrated and assembled approach to neighborhood design and development to create a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood, and to create a neighborhood that supports today's technology and infrastructure, and can accommodate future technology and infrastructure.
- 187.30 The Great Neighborhood Principles shall be applied in all areas of the city to ensure equitable access to a livable, egalitarian, healthy, social, inclusive, safe, and vibrant neighborhood for all McMinnville citizens.
- 187.40 The Great Neighborhood Principles shall guide long range planning efforts including, but not limited to, master plans, small area plans, and annexation requests. The Great Neighborhood Principles shall also guide applicable current land use and development applications.
- 187.50 The McMinnville Great Neighborhood Principles are provided below. Each Great Neighborhood Principle is identified by number below (numbers 1 – 13), and is followed by more specific direction on how to achieve each individual principle.

1. Natural Feature Preservation. Great Neighborhoods are sensitive to the natural conditions and features of the land.
  - a. Neighborhoods shall be designed to preserve significant natural features including, but not limited to, watercourses, sensitive lands, steep slopes, wetlands, wooded areas, and landmark trees.

***Applicant Response:***

*The existing subject site is vacant agricultural land and the no existing natural features or trees present.*

2. Scenic Views. Great Neighborhoods preserve scenic views in areas that everyone can access.
  - a. Public and private open spaces and streets shall be located and oriented to capture and preserve scenic views, including, but not limited to, views of significant natural features, landscapes, vistas, skylines, and other important features.

**Applicant Response:**

*The existing subject site does not have any scenic views or existing natural features. The proposed site design does create large open space areas within the subject site for residents to recreate.*

3. Parks and Open Spaces. Great Neighborhoods have open and recreational spaces to walk, play, gather, and commune as a neighborhood.
  - a. Parks, trails, and open spaces shall be provided at a size and scale that is variable based on the size of the proposed development and the number of dwelling units.
  - b. Central parks and plazas shall be used to create public gathering spaces where appropriate.
  - c. Neighborhood and community parks shall be developed in appropriate locations consistent with the policies in the Parks Master Plan.

**Applicant Response:**

*The proposed site plan creates a variety of open space types for the residents. There are large open spaces, space courtyard style gathering and sitting areas and picnic and barbeque areas. There is a network of onsite pathways that connect the building entries, parking areas and the public street frontage to the open spaces throughout the site.*

4. Pedestrian Friendly. Great Neighborhoods are pedestrian friendly for people of all ages and abilities.
  - a. Neighborhoods shall include a pedestrian network that provides for a safe and enjoyable pedestrian experience, and that encourages walking for a variety of reasons including, but not limited to, health, transportation, recreation, and social interaction.
  - b. Pedestrian connections shall be provided to commercial areas, schools, community facilities, parks, trails, and open spaces, and shall also be provided between streets that are disconnected (such as cul-de-sacs or blocks with lengths greater than 400 feet).

**Applicant Response:**

*The subject site provides a network of walkways and pathways throughout the site connecting the living units to the open spaces, parking areas and the public street frontage. The subject site is also served by Yamhill County Transit with a transit stop at the corner of SE Norton Land and SE Stratus Avenue.*

*The network of pedestrian walkways and pathways connect to all existing adjacent public sidewalks and street frontage. There are not existing adjacent trails.*

5. Bike Friendly. Great Neighborhoods are bike friendly for people of all ages and abilities.
  - a. Neighborhoods shall include a bike network that provides for a safe and enjoyable biking experience, and that encourages an increased

- use of bikes by people of all abilities for a variety of reasons, including, but not limited to, health, transportation, and recreation.
- b. Bike connections shall be provided to commercial areas, schools, community facilities, parks, trails, and open spaces.

**Applicant Response:**

*The proposed street frontage improvements along the SE Norton Lane Frontage includes a bike lane.*

6. Connected Streets. Great Neighborhoods have interconnected streets that provide safe travel route options, increased connectivity between places and destinations, and easy pedestrian and bike use.
  - a. Streets shall be designed to function and connect with the surrounding built environment and the existing and future street network, and shall incorporate human scale elements including, but not limited to, Complete Streets features as defined in the Comprehensive Plan, grid street networks, neighborhood traffic management techniques, traffic calming, and safety enhancements.
  - b. Streets shall be designed to encourage more bicycle, pedestrian and transit mobility with a goal of less reliance on vehicular mobility.

**Applicant Response:**

*The subject site has limited street frontage. There are no additional current or future rights of way adjacent the site.*

7. Accessibility. Great Neighborhoods are designed to be accessible and allow for ease of use for people of all ages and abilities.
  - a. To the best extent possible all features within a neighborhood shall be designed to be accessible and feature elements and principles of Universal Design.
  - b. Design practices should strive for best practices and not minimum practices.

**Applicant Response:**

*The proposed on-site network of walkways and pathways will be designed and built to be fully accessible to all residents of the proposed development and the surrounding community.*

8. Human Scale Design. Great Neighborhoods have buildings and spaces that are designed to be comfortable at a human scale and that foster human interaction within the built environment.
  - a. The size, form, and proportionality of development is designed to function and be balanced with the existing built environment.
  - b. Buildings include design elements that promote inclusion and interaction with the right-of-way and public spaces, including, but not limited to, building orientation towards the street or a public space and placement of vehicle-oriented uses in less prominent locations.
  - c. Public spaces include design elements that promote comfortability and ease of use at a human scale, including, but not limited to, street



trees, landscaping, lighted public areas, and principles of Crime Prevention through Environmental Design (CPTED).

**Applicant Response:**

*The proposed site and building design provides a variety of scales for the buildings and usable open spaces. The building forms are heavily articulated to reduce mass and scale. The street frontage building are oriented along the street frontage and provide direct access to the building entries. The on-site parking areas are screened from the public street frontage and the buildings are oriented around the common open spaces within the site.*

9. Mix of Activities. Great Neighborhoods provide easy and convenient access to many of the destinations, activities, and local services that residents use on a daily basis.
  - a. Neighborhood destinations including, but not limited to, neighborhood-serving commercial uses, schools, parks, and other community services, shall be provided in locations that are easily accessible to surrounding residential uses.
  - b. Neighborhood-serving commercial uses are integrated into the built environment at a scale that is appropriate with the surrounding area.
  - c. Neighborhoods are designed such that owning a vehicle can be optional.

**Applicant Response:**

*The existing subject site is vacant is located along the existing of the current urban growth boundary and the McMinnville city limits. The subject site is near the Highway 18 and Norton Lane intersections, and adjacent to the Yamhill County Transit bus line ant stop. A large employment center, Willamette Valley Medical Center across SE Norton Lane from the subject site.*

10. Urban-Rural Interface. Great Neighborhoods complement adjacent rural areas and transition between urban and rural uses.
  - a. Buffers or transitions in the scale of uses, buildings, or lots shall be provided on urban lands adjacent to rural lands to ensure compatibility.

**Applicant Response:**

*The subject site is located adjacent the current McMinnville City Limits and the Urban Growth Boundary. The scale and mass of the proposed development serves as a transition from the higher density larger scale buildings adjacent to the site, to the agricultural lands outside the Urban Growth Boundary that is adjacent to the site.*

*The proposed site plan does not place buildings adjacent the southerly property line to the south. This "steps down" the massing of the site to the UGB. The proposed site plan places the parking area adjacent to the UGB to soften the transition. Parking is proposed within 6 feet of this property line. A six-foot sight obscuring fence and landscape screening and buffering with continuous shrubs and trees*

are proposed along this boundary to mitigate the proximity of the parking to the UGB.

11. Housing for Diverse Incomes and Generations. Great Neighborhoods provide housing opportunities for people and families with a wide range of incomes, and for people and families in all stages of life.
  - a. A range of housing forms and types shall be provided and integrated into neighborhoods to provide for housing choice at different income levels and for different generations.

**Applicant Response:**

*The proposed development is for market rate housing and will be located adjacent to the proposed low-income housing project, Status Village. It is anticipated that the residents of both projects will represent a wide diversity of residents from young people just leaving home to live on their own, young families with small children, middle class working families and senior citizens.*

12. Housing Variety. Great Neighborhoods have a variety of building forms and architectural variety to avoid monoculture design.
  - a. Neighborhoods shall have several different housing types.
  - b. Similar housing types, when immediately adjacent to one another, shall provide variety in building form and design.

**Applicant Response:**

*The subject site is located adjacent to a variety of uses and subsequent building scales and building forms. The proposed development is for needed multi-family housing apartment units. The proposed development is for market rate housing and will be located adjacent to the proposed low-income housing project, Status Village. This accomplishes the Great Neighborhood Principle of providing a variety of housing types within the subject neighborhood.*

13. Unique and Integrated Design Elements. Great Neighborhoods have unique features, designs, and focal points to create neighborhood character and identity. Neighborhoods shall be encouraged to have:
  - a. Environmentally friendly construction techniques, green infrastructure systems, and energy efficiency incorporated into the built environment.
  - b. Opportunities for public art provided in private and public spaces.
  - c. Neighborhood elements and features including, but not limited to, signs, benches, park shelters, street lights, bike racks, banners, landscaping, paved surfaces, and fences, with a consistent and integrated design that are unique to and define the neighborhood.

### **Three Mile Lane Area Plan:**

Ordinance 5126 amended the McMinnville Comprehensive Plan adopting the Three Mile Lane Area Plan. The Subject Site is located within the area identified by the three Mile Lane Area Plan and therefore development must comply with the Polices outlined in the Plan.

The Three Mile Lane area is a unique district in the southeast portion of the City of McMinnville. The area contains approximately 1,340 acres of land with a variety of existing land uses and several large vacant parcels. The Three Mile Lane Area Plan is intended to create an implementable vision for the area's future land uses and multi-modal transportation system.

As an Area Plan, the Three Mile Lane Area Plan shall serve as a guiding document for land uses and public facilities in the delineated area of this plan. Specific standards for development will be identified in McMinnville's Master Plans and Municipal Code.

### **POLICIES**

The following policies shall guide development and future planning decisions in the Three Mile Lane area. These policies implement the Three Mile Lane Area Plan goals and describe how Great Neighborhood Principles are expected to be expressed in the future growth and development of the Three Mile Lane Area.

1. Require future development to be consistent with the design elements of the Three Mile Lane Area Plan.

#### ***Applicant Response:***

*The subject site lies within the area identified as Medium-High Density Residential in the Area Plan. The applicable Urban Design Elements identified in the Area Plan for the subject site are addressed as follows:*

#### **KEY URBAN DESIGN ELEMENTS**

- Transitions between uses: Health care facilities and surrounding residential areas. Healthcare facilities are often active around the clock with bright lighting and they generate significant vehicle traffic. They also require a lot of delivery traffic and, in the case of a major medical center, helicopter use. Buffering between uses should be considered, particularly senior housing or market-rate apartments with trees, landscaping and other treatments. Assisted living or nursing care facilities, however, would benefit from close proximity to the hospital.

#### ***Applicant Response:***

*The subject site abuts a large medical office building on the North side of the site. The Willamette Valley Medical Center Hospital is located on the East side of SE Norton Lane. The hospital is set back from SE Norton Lane a significant distance providing a significant buffer from the subject site.*

*The medical office building to the North has a row of mature trees along the property line abutting the subject site. The proposed Landscape Plan provides additional landscape buffering including trees, shrubs and a sight obscuring fence adjacent the property line.*

- Transitions between uses: Health care facilities and other commercial uses. The scale and orientation of existing uses, as related to future uses should be considered. For example, while Senior Housing might benefit from a location within walking distance of a retail

center, there should be careful site planning to ensure the housing isn't directly adjacent to loading or parking facilities. It may be most feasible to place health-care related housing with an orientation south towards views and the river.

**Applicant Response:**

*This is not applicable to the subject site.*

- Walkability between uses. Convenient, safe connections between a variety of uses in this area will be important to current and future users.

**Applicant Response:**

*The subject site has street frontage along SE Norton Lane. The proposed development includes Public Street Frontage improvements that will extend along the entire SE Norton Lane Frontage. This sidewalk provides a direct link and connection to the existing sidewalks along SE Norton Lane and Status Avenue. This existing sidewalk network connects the Medical Office Building (Altimus Plaza), the Willamette Valley Medical Center Hospital and up to the intersection of Cumulus Avenue and Highway 18.*

*The proposed and existing public sidewalk network also provides direct access to public transit and the Yamhill County Transit stop at the intersection of SE Norton Lane and SE Stratus Avenue.*

- Visual quality of buildings facing OR 18. New development should avoid placing loading docks or creating blank walls visible from passing vehicles.

**Applicant Response:**

*This is not applicable to the subject site.*

2. Public improvements and private development shall strive to protect tree groves and mature individual trees.

**Applicant Response:**

*There are no existing mature trees that will be impacted by the proposed SE Norton Lane street frontage improvements.*

3. Riparian corridors and adjacent native landscape shall be protected.

**Applicant Response:**

*The subject site is not adjacent any riparian corridors and does not contain any native landscape materials.*

*The proposed Landscape Plan does propose the use and incorporation of native plant materials and hybrids in the landscape design.*

4. The built environment will be designed to provide and protect views to rolling hills and volcanoes and to enhance visual and physical access to the North Yamhill River. New streets and open spaces will be oriented to capture views.

**Applicant Response:**

*The subject site does not have exposure to any existing views of the surrounding area. The upper floor living units may have the opportunity to capture views of the surroundings.*

5. Enhancing connections to existing trails and open space, such as connections into Joe Dancer Park and McBee Park, and creating a public greenway along South Yamhill River with trails and connections to the Three Mile Lane Area is a priority.

**Applicant Response:**

*While the subject site does not have existing adjacent trails or open space, the proposed Site Plan and internal network of sidewalks, provides the availability of connection to future development and trails to the south of the site.*

6. New gathering spaces will be designed to incorporate natural areas and views.

**Applicant Response:**

*The proposed Site Plan provides a variety of common open spaces and gathering spaces.*

7. Require native landscape plantings with seasonal variation and tree plantings that include shade streets with mature tree canopy.

**Applicant Response:**

*The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.*

8. A network of sidewalks and trails will connect people to key locations within the Three Mile Lane Area.

**Applicant Response:**

*The proposed Site Plan provides a network of internal sidewalks connecting the buildings, parking and common open spaces to the SE Norton Lane frontage. The proposed development includes Public Street Frontage improvements that will extend along the entire SE Norton Lane Frontage. This sidewalk provides a direct link and connection to the existing sidewalks along SE Norton Lane and Status Avenue. This existing sidewalk network connects the Medical Office Building (Altimus Plaza), the Willamette Valley medical Center Hospital and up to the intersection of Cumulus Avenue and Highway 18.*

9. The Three Mile Lane Area will have safe bicycle routes for residents and touring cyclists.

**Applicant Response:**

*The proposed development includes the extension of SE Norton Lane. The proposed public street frontage improvements includes a road section with a bike lane. This roadway improvements and bike lane will lead to the proposed future network of bikeways and trails identified in the Three Mile Lane Area Plan.*

10. Proposed new streets will connect to the existing local street grid, consistent with the conceptual designs in the Three Mile Lane Area Plan and in compliance with Transportation System Plan standards.

**Applicant Response:**

*The proposed development includes the extension of SE Norton Lane. The proposed public street frontage improvements includes a road section with a bike lane. These improvements follow the proposed alignment of future roadways, bikeways and trails identified in the Three Mile Lane Area Plan.*

11. New commercial developments should be designed to be at a walkable, human scale and for ease of use by all ages and abilities.

**Applicant Response:**

*The proposed development does not contain commercial uses and this is not applicable.*

12. New commercial, office, mixed-use, and multi-family developments should be designed to reflect the micro-climate and enhance outdoor life through the incorporation of features such as porches, balconies, courtyards, plazas, etc.

**Applicant Response:**

*The proposed multifamily development includes covered decks and patios for all of the living units. These covered areas will allow residents the opportunity to have access to private, weather protected outdoor living areas.*

13. New commercial, office, mixed-use, and industrial campus developments should promote inclusion and interaction within the right-of-way.

**Applicant Response:**

*The proposed development does not contain commercial, office, mixed-use or industrial uses and this is not applicable.*

14. Encourage mixed-use development where feasible.

**Applicant Response:**

*The proposed development does not contain commercial or mixed-use and this is not applicable.*

15. Proposed site landscape for new development should strive to reflect patterns of wine industry—eg, rows of vines, southern orientation, shelter belts of trees – and consider functional site planning of vineyard and farm complexes as conceptual models.

**Applicant Response:**

*The proposed development and Landscape Plan maximizes the usable site area for landscaping and common outdoor open space. Given the target density necessary for market rate apartments, the site development does not afford the opportunity to develop mini-vineyards or farm related uses.*

16. New development should consider adjacency to agricultural fields and respect this heritage through careful transitions.

**Applicant Response:**

*The subject site abuts the McMinnville City Limit and urban growth boundary to the South. The adjacent land remains under agricultural uses. The proposed Site Plan*



*provides a significant separation of the proposed buildings to the adjacent Southerly property line.*

17. Architectural building design that includes simple roof forms (industrial and agricultural) is encouraged in the Three Mile Lane Area.

**Applicant Response:**

*The proposed building and elevation design utilizes simple hip roof and gable roof forms. The building articulation provides relief in the wall and roof. The proposed building materials include lap siding and panel siding with battens are traditional to residential developments and are compatible with other residential developments in the neighborhood.*

18. Encourage a diversity of future housing forms, types, and design that respect the current character of the area.

**Applicant Response:**

*The proposed development is for multiple family housing in a varying scale of buildings. The variety of building scale is compatible with existing adjacent development.*

19. Ensure that new commercial and industrial campus development creates a welcoming and visible interface with Three Mile Lane.

**Applicant Response:**

*The proposed development does not contain commercial or industrial use and this is not applicable.*

20. Encourage site design and architecture that visibly convey the historic or current industry on the site (e.g., aviation, winemaking).

**Applicant Response:**

*The subject site is currently, and historically vacant. The subject site have been historically been farmed agricultural land.*

21. New commercial, mixed-use, office, and industrial campus development should consider using local materials for cladding and building structure (timber, corrugated steel cladding, red brick), and incorporating vibrant color.

**Applicant Response:**

*The proposed development does not contain commercial, office, mixed-use or industrial uses and this is not applicable.*

22. Public safety services shall be considered as part of master planning, including access, response times and opportunity for substations if needed.

**Applicant Response:**

*The subject site has limited frontage and access to public streets. The proposed Site Plan provides direct access to SE Norton Lane for fire and emergency service vehicles. The onsite circulation provides adequate maneuvering and turning radii for fire and EMS vehicles and provides a Fire Truck turnaround at the rear of the site.*

23. Ensure that no incompatible heavy industrial uses are allowed along Highway 18 in the Three Mile Lane Area or as part of the Innovation Campus.

**Applicant Response:**

*The proposed development does not contain industrial uses and this is not applicable.*

24. Significant natural features shall be inventoried and protected as much as possible within new development plans.

**Applicant Response:**

*The subject site is existing farmed agricultural land and does not contain significant natural features.*

## **McMinnville Zoning Code, Title 17: Compliance to Applicable Chapters**

### **CHAPTER 17.11 RESIDENTIAL DESIGN AND DEVELOPMENT STANDARDS**

#### **17.11.090 Apartments.**

Apartments are a type of attached housing within single-story or multi-story buildings. Apartment dwelling units may share common walls, ceilings, or floors.

##### A. Characteristics.

1. Site Sizes: Single walk-ups, block apartments, and many courtyard apartments can fit on a 100 x 100 foot lot. Bigger developments with multiple walk-up buildings may be as large as 250,000 square feet, or 500 x 500 foot lots.

**Applicant Response:**

*The subject site is 214,759 sf and within the range identified for walk-up apartments and is therefore compliant.*

2. Height Range: Apartment heights vary depending on the type and the location.

**Applicant Response:**

*The proposed development is for three-story apartment buildings which is fully compatible with the existing and proposed adjacent development.*

3. Density Ranges: Apartment densities vary depending on building type and site design layout.

**Applicant Response:**

*The proposed development has a density of just under 28 units per acre which is typical of "Walk-up Apartments".*

##### B. Types of Apartments.

###### 2. Walk-Up Apartments.

- a. Description: Buildings are limited to three stories, and consist of about four to 12 units each, accessible from a single open-air stairwell. Dwelling units are typically

constructed in Type V frame construction with fire sprinklers. Individual apartment buildings are arranged around common open space and shared parking areas.

**Applicant Response:**

*The proposed development is 12-unit to 24-unit three-story buildings with open-air stairways that provide access to the living units. The buildings are proposed to be Type V-B wood-framed construction with fire sprinklers. The buildings are located adjacent to several common open spaces.*

- b. Appropriate Context: Walk-up apartments are appropriate adjacent to or within a single dwelling neighborhood depending on site design, orientation to the street, location of parking, and the massing and scale of buildings.

**Applicant Response:**

*The subject site and proposed development is located within a C-3 Commercial zoned area to the North, an R-4 Medium, High Density parcel adjacent on the West side and the M-L Limited Light Industrial zone to the East. The existing neighborhood has a variety of building scales from the multi-story Willamette Valley Medical Center to the East, single story Medical Offices and a three-story motel to the North.*

- c. Also Named: Woody Walk-Ups, Single Stair Walk-Ups

**Applicant Response:**

*The proposed development is also referred to as a "woody walkup" and provides open air stairways to the upper living units.*

- d. Variations: May have an internal stair. Generally, in this case, the maximum number of units per floor are four. They can be designed with front and back windows for cross ventilation. Buildings can be separated to offer access to light and air on three sides.

**Applicant Response:**

*The proposed building design provides open air stairways on each side of the building to serve the upper floor units. The stairways serve as access to two ground floor and four upper floor units per stairway.*

- e. Lot Sizes: Vary widely, from 10,000 to 250,000 square feet

**Applicant Response:**

*The subject site is 214,759 sf which is within the identified range.*

- f. Density Range: 15 - 30 units per acre. (Note, maximum density will be governed by McMinnville's municipal code.)

**Applicant Response:**

*The proposed density is just under 28 units per acre which is within the identified allowable density range.*

- g. Building Height: Usually 3 stories; can be 2 stories. (Note, maximum height will be governed by McMinnville's Municipal Code.)

**Applicant Response:**

*The proposed building height is three-stories.*

- h. Construction Type and Building Code Issues: Typically Type V frame construction. Sprinklers for fire suppression are required.

**Applicant Response:**

*The proposed Construction Type is Type V-b, non-rated with NFPA 13-R Fire sprinklers.*

C. Development Standards. The applicable development standards are as follows:

**Applicant Response:**

| <u>Required:</u>             | <u>Proposed:</u>          |
|------------------------------|---------------------------|
| Lot Width: 50 feet min.,     | Subject Site: 320.20 feet |
| Lot Depth: 100 feet min.     | Subject Site: 670.96 feet |
| Lot Size: 5,000 sf Min.,     | Subject Site: 214,759 sf. |
| Front Setback: 15 feet min., | Proposed: 15 feet         |
| Side Setback: 10 feet min,   | Proposed: 10 feet min.    |
| Rear Setback: 20 feet min.,  | Proposed: 79'-6"          |
| Building Height:             | Proposed: 36'-4"          |

D. Design Standards. The Apartment Design Standards for multi-dwelling housing are standards that apply to apartment housing types. These standards are related to site design and building frontage, parking, compatibility with neighboring homes, open space, and private space.

- 1. Context and Site Design. Site design standards are intended to facilitate the development of attractive multi-dwelling housing. They encourage good site and building design, which contributes to livability, safety, and sustainability; helps create a stronger community, and fosters a quality environment for residents and neighbors.
  - a. Mirror the scale of blocks and the block-like structure of the surrounding neighborhood.

**Applicant Response:**

*The existing surrounding neighborhood has a variety of building sizes, scales and heights that range from mobile homes to the west, large multi-story hospital to the east, large single story medical office building to the north and a three-story hotel to the north. The proposed Site Plan provides a variety of three-story buildings ranging from 12 living units to 24 living units. There are portions of three buildings that have single story and two story pop-outs providing additional scale and variety.*

- b. Connect the internal network of streets and paths to those of the surrounding area where possible.

**Applicant Response:**

*Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive aisles provide access to the parking areas and buildings. There is a network of pathways and sidewalks that connect the parking areas, opens spaces and building entrances.*

- c. Configure apartments, parking areas, and common open space in clusters that mirror the scale of blocks of the surrounding neighborhood or are no more than 10,000 square feet in area per cluster.

**Applicant Response:**

*The neighborhood immediately adjacent to the subject site consists of large blocks, large sites and large scale buildings. The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters. This proposed Site Plan is compatible with existing adjacent development and provide smaller "clusters" of development.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.*

*Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.*

*Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.*

- d. Residential units must be oriented to a common open space, including a common green, a plaza, or a pocket park.

**Applicant Response:**

*The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The buildings on the proposed Site Plan are all oriented to common open spaces. The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provide the areas of each.*

*Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. Buildings A and B are directly adjacent to common open space. Building A is adjacent Open Space 01, Building B is adjacent Open Space 02.*

*Buildings C and D are grouped in a cluster and are oriented around Common Open Space 03.*

*Buildings E, F and G are also grouped in a cluster oriented around Common Open Space 04. There is also a Courtyard Open Space 05 between Building E and Building G.*

- e. Orient all buildings around a shared open space that meets the requirements of a Common Open Space.

**Applicant Response:**

*The Open Space Analysis Plan, drawing A1.0-A identifies each open space and provides the areas of each space. There are a variety of uses proposed for the Common Open Spaces. These include both passive and active Open Space.*

There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.

- f. Align buildings to surrounding streets.

**Applicant Response:**

*There is only one public street, SE Norton Lane, that abuts the subject site. There are two buildings that are adjacent to the SE Norton Lane frontage, Building A and Building B. These two buildings are aligned, on the long building axis, along the street frontage.*

- g. Connect to surrounding neighborhoods, schools, parks, and other neighborhood destinations.

**Applicant Response:**

*The subject site has limited street frontage and the adjacent properties do not have existing site circulation systems to connect to. The existing sidewalk along SE Norton Lane will be extended with the public road extension. The proposed Site Plan provides several connections from the internal site circulation system to the street frontage and new public sidewalk. There are sidewalks along the norther property boundary and the southern property boundary that will provide opportunities for connections to future development of the adjacent properties.*

2. Large Site Design Requirements.

1. Break up parking into smaller areas and access from side streets when possible.

**Applicant Response:**

*The Subject site has limited street frontage and abuts a single public street, SE Norton Lane. Internal site circulation and drive break up the parking areas into smaller sections preventing long rows of parking. The narrow portion of the site is on the street frontage preventing additional site access points.*

2. Connect parking areas, building entries, and open spaces with paved walkways.

**Applicant Response:**

*The proposed Site Plan provides a network of paved walkways that connect the building entries, common open spaces and parking areas.*

3. Buffer parking areas with landscaping.

**Applicant Response:**

*The proposed Site Plan provides landscaped areas to serve as buffers to the exterior perimeter and to buffer the buildings from the parking. There are*



landscape islands throughout all parking areas that provide shade trees, ground cover and shrubbery to buffer the parking area.

4. Minimize the width and number of driveways and curb cuts.

**Applicant Response:**

The proposed Site Plan has a single 26-foot-wide driveway/curb cut onto SE Norton Lane.

5. Provide different types of open space throughout site, both active and passive, including playgrounds, trails, volleyball courts, bocce ball courts, community gardens, etc.

**Applicant Response:**

The proposed Site Plan provides a variety of common open spaces. They vary in size, shape and their proposed uses. There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some areas are more confined courtyard spaces and can provide a more intimate quiet setting.

There is a summary of the proposed Common Open Space areas on the Open Space Analysis Plan, drawing A1.0A.

6. Align buildings to surrounding streets.

**Applicant Response:**

The subject site has street frontage along SE Norton Lane. The proposed Site Plan orients two buildings along the street frontage. The long axis of these buildings provides the required alignment to the street frontage.

7. Centrally locate common buildings and spaces

**Applicant Response:**

The proposed Site Plan has buildings oriented around the common open spaces. There are three areas of the site where buildings orient to the common open spaces.

8. Group apartments, parking, and open space into smaller clusters.

**Applicant Response:**

The long narrow site has very limited opportunities and constraints for arranging the buildings, parking and open space. The proposed Site Plan groups the buildings and open space into three separate clusters.

Buildings A and B are required to be adjacent the street frontage and are bisected by the only available site access point. A common open space provides separation of the building to the parking area.

*Buildings C and D are grouped in a cluster and are oriented around a large common open space and the open space is adjacent a parking area.*

*Buildings E, F and G are also grouped in a cluster oriented around a large common open space and the open space is adjacent to a parking area.*

**3. Pedestrian Access.** On-site pedestrian circulation system shall include the following:

- a. Continuous connections between the primary buildings, streets abutting the site, ground-level entrances, common buildings, common open space, and vehicle and bicycle parking areas.

**Applicant Response:**

*The proposed Site Plan provides a network of sidewalks and walkways that connect parking areas, building entries and common open spaces.*

- b. At least one pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.

**Applicant Response:**

*The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk. The buildings that are located along the street frontage provide a sidewalk connection from the building entries to the new public sidewalk along SE Norton Lane.*

- c. Pedestrian walkways shall be separated from vehicle parking and maneuvering areas by physical barriers such as planter strips, raised curbs, or bollards.

**Applicant Response:**

*Pedestrian walkways that are located adjacent vehicle parking are separated by concrete wheel stops and the walkways are of concrete which is contrasting to the asphaltic concrete paving.*

- d. Walkways shall be constructed with the hard surface material, shall be permeable for stormwater, and shall be no less than 3 feet to 5 feet wide. If adjacent to a parking area where vehicles will overhang the walkway, a 7-foot-wide walkway shall be provided. The walkways shall be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials.

**Applicant Response:**

*Pedestrian walkways that are located adjacent vehicle parking are proposed to be concrete which is contrasting to the asphaltic concrete paving. The walkways are all 5'-0" wide. Concrete wheel stops are located 2 feet from the sidewalk in the parking spaces and will provide the 5'-0" minimum clear walkway width.*

- e. Spacing requirement: No further than 200 feet apart, on center. At least 1 pedestrian connection to an abutting street frontage for every 200 linear feet of street frontage.

**Applicant Response:**

The subject site has 320.20 feet of frontage along SE Norton Lane. Two pedestrian connections are required. The proposed Site Plan provides four connections from the network of on-site pedestrian circulation system sidewalks and walkways to the SE Norton Lane public sidewalk.

- f. May be co-located with a common green.

**Applicant Response:**

The proposed network of walkways on the Site Plan provides circulation to all parking areas, building entries and the common green open spaces.

4. Parking Lot Location and Design.

a. Characteristics.

1. A parking lot is a storage space for cars and should provide secure storage.
2. It is also a place where everyone is a pedestrian while getting to or from their car. Therefore, it should be designed primarily for the ease, safety and comfort of a person rolling or on foot.
3. Clearly defined pathways through parking lots and garages to building entrances, surrounding sidewalks, and transit stops enhance pedestrian safety. These pathways also provide an opportunity to improve the appearance of parking lots.
4. Design parking lots and garages so that vehicles are not the dominant feature.
5. To encourage bicycling as a mode choice, bike parking areas should include bike repair, maintenance, and cleaning stations.

b. Universal Design Standards for Nine Parking Spaces or More.

1. Parking lot pathways should be designed as part of the seamless accessibility network described in Apartment Standards, particularly the required Through Connection.

**Applicant Response to (a) and (b):**

The proposed Site Plan provides a network of parking area sidewalks and pathways that provide convenient access for the residents to all areas of the site include the common open spaces, common facilities and the public street frontage.

2. Driveways to shared parking areas are:

1. Limited to one driveway per street frontage.
2. Parallel parking is permitted on a driveway that crosses a front, side or rear yard abutting a street, but not within the required yard setback.

**Applicant Response:**

The subject site and proposed Site Plan does not have any shared parking areas or site access.

- c. Parking Lots For Small Multi-Dwelling Sites - Containing More Than Nine Parking Spaces but Fewer Than 16 Parking Spaces. Off-street parking may be arranged in clusters, subject to the following standards:

1. Residential developments with fewer than 16 dwellings are permitted parking clusters of not more than five contiguous spaces.
2. Residential developments with 16 dwellings or more are permitted parking clusters of not more than eight contiguous spaces.
3. Parking clusters must be separated from other spaces by at least four feet of landscaping.
4. Clustered parking areas may be covered.

**Applicant Response:**

*The subject site and proposed Site Plan provides a parking area greater than 16 spaces therefore this is not applicable.*

d. Parking Lots For Medium-to-Large Multi-Dwelling Sites - Containing More Than 16 Parking Spaces.

1. Interior landscaping, minimum area
2. Interior landscaping shall be required for off-street parking areas 5,000 square feet or greater in size.

**Applicant Response:**

*Parking area landscaping is proposed to be provided for all parking areas and is shown on the proposed Landscape Plan.*

3. For parking lots less than 50,000 square feet, the minimum landscaped area is 5 percent.

**Applicant Response:**

*Parking area landscaping proposed is greater than the 5% minimum and is shown on the proposed Landscape Plan.*

4. For parking lots 50,000 square feet and greater, the minimum landscaped area is 8 percent.

**Applicant Response:**

*The proposed Site Plan does not include parking areas over 50,000 sf.*

5. Planted areas may take the form of landscape areas and planter bays.

**Applicant Response:**

*This is understood and the landscape islands have been included in the overall parking lot landscape area.*

6. Landscaped areas along a through connection count toward required interior landscaping.

**Applicant Response:**

*This is understood and the landscape areas along through connections have been included in the overall interior landscape area.*

7. Landscaped islands and peninsulas shall be evenly distributed throughout all parking areas and separated no more than 60 feet from another. Such

islands shall be provided with raised curbs, be a minimum of five feet in width, and shall each contain at least one deciduous tree. To achieve the maximum canopy coverage, all trees shall be non-columnar and have root systems that form deep before spreading to decrease the episodes of buckled pavement.

**Applicant Response:**

*Landscape islands have been incorporated into the proposed Site Plan and parking area design. Parking area landscape islands and planting beds will be contained with raised concrete curbs. The spacing of the landscape islands is proposed to exceed the 60 foot maximum spacing in several locations primarily due to the parking stall width and spacing standards. An Administrative Variance is being requested to increase the spacing to 63 feet maximum.*

8. Trees may line the required Through Connection, and/or be clustered within landscape islands or planter bays, and/or shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.

**Applicant Response:**

*Trees are proposed within all of the landscape islands and planter bays throughout the parking areas providing the design tree canopy effect and shading.*

9. When a parking area abuts property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line.

**Applicant Response:**

*Six-foot-high Sight obscuring fences are proposed along the side and rear property lines. Buffer plantings and trees are also proposed along these property lines.*

- e. Parking Lot Setbacks Adjacent to Buildings and Structures. Where an off-street parking or vehicular use area is located adjacent to a building or structure, the off-street parking or vehicular use area shall be set back from the exterior wall of the building or structure by a minimum five-foot-wide landscape strip, or by a minimum five-foot-wide paved pedestrian walkway.

**Applicant Response:**

*The proposed Site Plan maintains a minimum 10-foot separation of parking areas from the closest point of any building or structure.*

- f. Parking Lot Location. Off-street parking spaces and vehicle maneuvering areas shall not be located:
  1. Within of 20 feet from any street property line, except alley property lines;

**Applicant Response:**

*The parking areas on the proposed Site Plan are located at a distance greater than 20 feet from the street frontage property line.*

2. Between a street property line and the front façade of cottages located closest to the street property line. This standard does not apply to alleys.

**Applicant Response:**

*This is not applicable to this proposed Site Plan and development.*

3. Off-street parking spaces shall not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.

**Applicant Response:**

*The parking areas on the proposed Site Plan are located at a distance of 6-feet from the North and South (side) property lines and 10 feet from the West (rear) property line. For the areas less than 10 feet, an Administrative Variance is being requested to the reduced setback. This is compatible with the existing adjacent development that also has parking located 6 feet from the adjacent property lines.*

*An Administrative Variance under separate application is being requested for the non-compliance of this issue.*

4. Landscaping, fencing, or walls at least three feet tall shall separate clustered parking areas and parking structures from common courtyards and public streets.

**Applicant Response:**

*The is not applicable to the proposed Site Plan as there are no parking areas proposed adjacent to common courtyards and public streets.*

5. Garages and carports (whether shared or individual) must not abut common courtyards.

**Applicant Response:**

*This is not applicable as no carports or garages are proposed.*

6. Individual attached garages up to 200 square feet shall be exempt from the calculation of maximum building footprint for cottages.

**Applicant Response:**

*This is not applicable as no garages are proposed.*

7. Individual detached garages must not exceed 400 square feet in floor area.

**Applicant Response:**

*This is not applicable as no garages are proposed*

8. Garage doors for attached and detached individual garages must not exceed 20 feet in width.



**Applicant Response:**

*This is not applicable as no garages are proposed*

- g. Parking Lot Required Through Connections. Through Connections may be multi-modal or used exclusively for bicycle and pedestrian access and need to meet the standards in Table 1.

**Applicant Response:**

*The proposed Site Plan and parking area layout does not create "through connections".*

5. Common Open Space.

a. Characteristics.

- 1. Provide opportunities for formal and informal recreational use by residents of all ages. This could be a shared recreational facility including sports fields, play structures, bike tracks, courts, swimming pool, or other options.

**Applicant Response:**

*The proposed common open areas provide formal and informal recreations opportunities for the residents. There are formal seating areas and courtyards with benches and raised planters with seating walls, large open lawn areas for informal sports activities and formal picnic areas with picnic tables and barbeques.*

- 2. Provide tall deciduous trees for summer shade and winter solar access. When possible, preserve and incorporate large existing trees at least 9 inches in diameter as a focal point of open spaces.

**Applicant Response:**

*The landscape design includes deciduous shade trees adjacent the formal sitting areas and picnic areas and they are adjacent to the larger open lawn areas. These will provide the desired summer shading and will permit winter sun exposure. The existing subject site does not contain any existing trees.*

- 3. Enhance the usability of the space through the inclusion of elements including seating, outdoor lighting, weather protection and/or shade structures, and art, among other features.

**Applicant Response:**

*The proposed common open areas provide seating areas and courtyards with benches and raised planters with seating walls. The formal picnic areas include picnic tables and barbeques.*

- 4. Incorporate landscaping that receives at least 50 percent of its irrigation from harvested rainwater.

**Applicant Response:**

*As this is an identified characteristic it is not a requirement. The proposed design does not have the ability to store and harvest rain water.*

5. Provide opportunities for food cultivation including a community garden and/or incorporating cultivated species into the landscaping.

**Applicant Response:**

*Community gardens are not proposed.*

6. A maximum of 50 percent of common open space may be provided in a rooftop deck that includes shared amenities, weather protection, and landscaping, and is accessible to all residents.

**Applicant Response:**

*Rooftop deck areas are not proposed.*

7. A shared outdoor courtyard or shared street/woonerf that is fronted by individual entrances, windows, and balconies There should be a combination of hardscape and landscaped space and/or planters.

**Applicant Response:**

*The proposed Common Open Space Courtyards between Buildings E and G and between Buildings F and G have building entries and ground floor patios adjacent to them. There is also decorative hardscape concrete paving with an enhanced score joint pattern. Benches and raised planters with seat height walls help to define the courtyard spaces. Enhanced landscape plantings also define the courtyard spaces.*

b. Required Elements, General.

1. A common open space shall be provided that is centrally located and designed with a clear function that enhances the livability of residents. These functions shall include passive and active uses. The open space shall be accessible to all residents and if possible be fronted by clearly defined unit entrances. The common open space shall serve as the focus of surrounding buildings. Entries and windows shall face the common open space to provide informal surveillance. Common open spaces shall be accessible to all residents.

**Applicant Response:**

*The proposed Common Open Spaces are centrally located in three primary building cluster groups. The on-site walkway systems connects all of the open spaces, building entries and parking areas throughout the site making accessible to all residents of the development.*

*There are large open lawn areas that would encourage recreational uses such as frisbee, play areas for ball sports, dog walking or just lounging on a blanket. There are also two different picnic areas with picnic tables and a barbeque. There are several seating areas with a raised planter with a seat height wall and decorative tress and landscaping, benches and shade trees. Some of these seating areas border the larger lawn areas while some are in more confined courtyard spaces and can provide a more intimate quiet setting.*

2. Common open space shall be a minimum of 15 percent of the site.

**Applicant Response:**

*The amount of total proposed Common Open Space provided is greater than the 15% of the site area required. The total site area is 214,759 sf, thus 32,214 sf of Common Open Space is required. 32,769 sf of Common Open Space is provided.*

*An Open Space Analysis Plan has been prepared that delineates each proposed Common Open Space and identifies and summarizes the areas of each. See drawing A1.0A included within this application.*

6. Private Open Space.

a. Characteristics.

1. Every dwelling needs private open space for relief from indoors and to provide access to fresh air, light, and nature. Private open space may take many forms based on the size of unit.
2. They should translate into a perception of an increase in living space and the ability to invite the outdoors in. Additionally, these open spaces can provide environmental benefits with plants that consume carbon dioxide and help reduce stormwater runoff. Spaces should be adequate to be usable, allowing space for a chair to sit in, a place to barbecue or hang clothes to dry, or for a pet to curl up.
3. Private open space should enhance the residential function of the building while also improving the appearance of the building. They should be integrated into the overall architectural form and add detail to the façade.
4. Placement can vary based on privacy concerns. It can be combined across multiple floors.

**Applicant Response to (a) 1-4:**

*The proposed living unit and building design provides private open space for each unit. The ground floor living units have on-grade patios that are 96 sf each. The ground floor patios are directly adjacent the living rooms and provide a door onto the patio with adjacent living room windows. This blends the living room space and the patios into usable living areas. The ground floor patios are screened with landscaping to maintain privacy from the adjacent walkways and parking areas.*

*The upper-level living units each have 72 sf private decks that are directly adjacent the living rooms with the living room windows and a door opening directly onto the deck area. This also extends the living space from the living room onto the private deck.*

*The placement of the decks and patios provides privacy to adjacent living units and provides greater articulation on the building facades improving the overall appearance and character of the buildings.*

b. Required Elements.

1. All units shall have a minimum of 36 square feet of private open space that allows for personalization and private use of the space and contributes to

the livability and function of the dwelling. Any exterior private open spaces shall be supplemented with operable windows to allow for cross-ventilation, increase airflow and provide the ability to control access to the outdoors.

**Applicant Response:**

*The ground floor living unit patios are 96 sf each and the upper floor living unit decks are 72 sf each. Both exceed the minimum areas required.*

2. At least 50 percent of upper units shall have a balcony that is accessible from the interior of the unit that is a minimum of 60 square feet with no dimension less than 6 feet. These balconies can be designed to be up to the full width of the apartment in order to provide adequate space for use and allow greater indoor/outdoor flow. Balconies can be cantilevered, semi-recessed, or fully recessed. They should be located based on privacy and environmental concerns. If balconies are transparent, adequate storage should be provided within the unit or the larger building so that balconies do not become informal storage spaces.

**Applicant Response:**

*All of the upper floor living units have decks that are 72 sf each with a minimum dimension of 6 feet. This exceeds the minimum number of units and the minimum deck area required. The decks are placed to maintain privacy between adjacent living units.*

3. Private outdoor space at the ground level must meet the requirements of Universal Standards: Front Yard regardless of whether the private outdoor space is in the front, side, or rear of a building.

**Applicant Response:**

*The ground floor living unit patios are all screened with landscaping and are setback from walkways, street frontage and adjacent parking areas as required.*

- c. Supplemental Elements. In addition to meeting the required elements above, projects must provide private open space in the form of one of the options listed below.

1. A "Juliet-style" balcony of 12-inch dimension that allows residents to bring a sense of the outdoors into the unit. Must have doors that can open inwards or full height sliding glass doors to allow the introduction of fresh air and sunlight. If this item is selected, units must also include operable windows to increase airflow/ability to control access to the outdoors.
2. An upper story rooftop deck or terrace that may include space for outdoor seating, dining, and planters for cultivation. This terrace may be stepped back on structures over two stories so as to reduce the visual impact of upper floors.
3. Alternative option that meets the concept and guiding principles.

**Applicant Response:**

*All upper floor living units provide decks that are 72 sf each, thus meeting the supplementary requirements. All unit decks are directly adjacent to*

the living rooms and provide operable windows and a deck access door directly onto the deck.

7. Alleys.

**Applicant Response:**

The subject site does not have the opportunity for providing alleys and this is not applicable.

8. Landscaping

a. Characteristics.

1. Use landscape elements, particularly plant materials, in an organized and harmonious manner that will enhance, protect and promote the economic, ecological, and aesthetic environment of McMinnville.

**Applicant Response:**

The proposed landscape design provides a cohesive design that utilizes plant materials to enhance the overall site appearance, enhance the outdoor living environment and provide an overall aesthetically pleasing site character. The choice and selection of plant materials incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity.

The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Shade trees will soften the built environment, reduce heat island, and provide habitat for birds. Flowering Shrubs will soften the buildings and enhance the aesthetic value of the site and provide food for local pollinators.

Lawn areas will provide a pastoral aesthetic reminiscent of the Willamette Valley, promoting harmony with adjacent landscapes. These areas will provide opportunities outdoor activities that promote the mental and physical health of the residential community, and in turn, the greater community of McMinnville. Finally, the regular maintenance of the landscape will help provide jobs for the local economy.

2. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:

- a. Reduce soil erosion and the volume and rate of discharge of stormwater runoff.

**Applicant Response:**

Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.

- b. Aid in energy conservation by shading structures from energy losses caused by weather and wind.

**Applicant Response:**

*Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.*

- c. Mitigate the loss of natural resources.

**Applicant Response:**

*Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.*

- d. Provide parking lot landscaping to reduce the harmful effects of heat, noise, and glare associated with motor vehicle use.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects. The combination of landscape plant materials and sight obscuring fences buffer noise from the properties.*

- e. Create safe, attractively landscaped areas adjacent to public streets.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.*

- f. Require the planting of street trees along the City's rights-of-way.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.*

- g. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character, and value of existing neighborhoods.

**Applicant Response:**

*The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adject to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.*



- h. Provide shade, and seasonal color.

**Applicant Response:**

*The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.*

- i. Reduce glare, noise, and heat.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.*

*Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.*

- j. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.

**Applicant Response:**

*Visual noise will be mitigated with the use of a site obscuring fence, trees, and evergreen shrubs.*

*Lighting impacts are mitigated by the selection and placement of light fixtures that provide full cut-off of light pollution to the abutting properties.*

- k. Unify development and enhance and define public and private places.

**Applicant Response:**

*The landscape design helps unify the development with the use of repetition of design elements in private and public areas. These elements include repetition of form, texture, and color of the plant materials.*

*Public spaces meant to be used by all residents are defined with lawn areas, walkways, benches, raised planters, and flowering trees.*

*Landscaping adjacent to private areas are defined by ornamental plantings of shrubs, grasses and groundcovers.*

- l. Preserve existing mature trees.

**Applicant Response:**

*There are no existing trees onsite.*

- m. Enhance the urban forest and tree canopy.

**Applicant Response:**

*The urban forest is enhanced with the planting of trees throughout the site; including shade trees and flowering trees. In total, 115 trees are proposed, including street trees.*

- n. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.

**Applicant Response:**

*The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.*

- o. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.

**Applicant Response:**

*A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.*

*The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically branched and high canopy structure for the interphase between public and private space.*

*Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.*

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

- p. Support McMinnville as a community that cares about its appearance. It is further recognized that landscaping increases property values, attracts potential residents and businesses to McMinnville, and creates safer, more pleasant living and working environments for all residents and visitors to the city.

**Applicant Response:**

*The proposed landscape design will use a variety of trees, shrubs, grasses and groundcovers to provide an aesthetically pleasing appearance that will increase the value of the property, attract residents and create a safe and pleasing living environment.*

*The proposed trees throughout the site will provide structure, shade, pleasing fall color and a net increase to the urban forest canopy. The ornamental landscape elements will soften the built environment and offer a pleasing appearance for those living and visiting the community.*

b. Required Elements.

- 1. All areas of the site not occupied by the structures or paved areas shall be landscaped in an attractive and functional manner.

**Applicant Response:**

*The proposed landscape design provides landscape plant materials in all areas of the site not occupied by structures or parking and paved areas. The proposed landscape design provides an aesthetically pleasing design and use of a variety of plant materials.*

- 2. A minimum of 20 percent of the net site shall be landscaped. Paved pedestrian paths, when integrated within the landscaped area, may satisfy up to 5 percent of this requirement. Landscaped setback areas, landscaped common open spaces, eco-roofs, vegetated stormwater facilities, preserved natural areas, and planter areas can be credited toward the minimum landscape standard.

**Applicant Response:**

*The proposed Site Plan and landscape design provides 30 percent of the subject site area in landscaping. More specifically, the site area is 214,759 sf. 64,157 sf of landscape area is provided, which is 30 percent.*

9. Privacy and Screening.

- a. Characteristics. Low walls or fences are encouraged to provide separation between private open space and common open space, streets, or internal circulation paths. Fences should be designed to integrate with the architecture of the building and add visual interest through the use of materials, color, and detail.

**Applicant Response:**

*The proposed landscape design provides landscape screening and buffering to ground floor living unit patios (private open space). The plant materials used for screening these areas blends harmoniously with the adjacent landscape design providing an attractive and effective privacy screen.*

b. Required Elements.

1. All fences on the interior of the development shall be no more than 3 feet high. Fences along the rear or side property lines of the development may be up to 6 feet high. Chain-link fences are prohibited.

**Applicant Response:**

*There are no internal site fences proposed. There is a 6-foot sight obscuring fence proposed on the site side and rear property lines. The fence will not be chain-link fencing.*

2. Mechanical and communication equipment and outdoor garbage and recycling areas shall be screened so they are not visible from streets and common open spaces.

**Applicant Response:**

*The proposed trash and recycling enclosures provide a solid evergreen screen along the sides of the facilities, as illustrated on the Landscape Plan.*

3. Utilities such as transformers, heating and cooling, electric meters, and other utility equipment shall not be located within 5 feet of a front entrance and shall be screened with sight-obscuring materials.

**Applicant Response:**

*The proposed locations of any on-site transformers or other utility equipment has not yet been determined. The transformers and equipment will be screened as required.*

4. The placement of balconies above the first story shall not create a direct line of sight into the living spaces or backyards of adjacent residential properties.

**Applicant Response:**

*The proposed Site Plan does not locate buildings that would orient living unit balconies that would create a line of sight into adjacent properties.*

10. Front Yard.

a. Characteristics.

1. For all housing types the front setback—even when it is small or zero, should be designed to provide a transition from the public realm of the street to the private realm of the dwelling.
2. The front setback provides a vital transition between the public area of the street and the private spaces within the dwelling. The smaller the front setback is, the more important the concept of layering public to private spaces becomes. When multi-dwelling units are on the ground floor of the building and face the perimeter of the site and surrounding streets, they must meet the standards of this section.

b. Required Elements.

1. Dwelling units located on the ground floor of the building and facing the perimeter of the site and surrounding streets must meet the requirements of either:

- a. Front Yard Type 1: Neighborhood

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provides a landscape design that meets or exceeds the requirements for a Type 1 Front Yard (Neighborhood Type). The "Gateway" area is landscaped at the minimum depth with grasses, the "Front Yard" is fully landscaped and provides a hedge screen at 3 feet to provide privacy to the unit patios.*

11. Compatibility.

- a. Characteristics. New multi-dwelling housing should be compatible with its surrounding context while introducing new shape, size, and detail variation, enabling different housing styles and types to sit side-by-side harmoniously.

**Applicant Response:**

*The existing developed neighborhood contains a wide variety of uses, building styles and scales. The existing uses include a medical office building, a three-story hotel, the large multi-story hospital (Willamette Valley Medical Center) and single-story mobile homes in a mobile home park. The proposed three-story apartment buildings are well within the range and scale of existing adjacent developments. The proposed buildings range in size from twelve units to twenty-four units also providing a variety in the scale of buildings within the proposed development.*

b. Required Elements.

1. On a site with multiple buildings of varying scales (or that vary from the surrounding context), provide a gradual transition between scales. For example, locate dwellings that are similar in scale and density along the street frontage and transition to lower scale and density buildings toward the rear of the site. Use rear driveways and landscaping as a buffer backing up to adjacent properties of a different scale.

**Applicant Response:**

*The proposed Site Plan locates similar scale buildings adjacent the street frontage and along the northerly property line that are compatible and in context with the existing adjacent development. The buildings in the rear of the site are set back nearly 80 feet to provide separation and a buffer from the single-story mobile home park adjacent the rear of the site.*

2. Arrange building volumes and setbacks in a way that reflects neighborhood patterns along street frontages and contributes to the desired character.

**Applicant Response:**

*The existing neighborhood character is quite diverse and does not have any established patterns to follow. The proposed Site Plan provides a street frontage compatible with similar developments within the community and provides a pedestrian friendly street scape.*

3. Arrange courtyard apartments so that end units reflect a neighborhood context of detached units along the street frontage.

**Applicant Response:**

*This element is not applicable to the garden style apartments.*

4. Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.

**Applicant Response:**

*The existing neighborhood has a variety of height and scale buildings. The existing large medical office building adjacent the site to the north is single story. However, the proposed Stratus Village to the West of the medical Office Building and directly adjacent the subject site to the North is proposed to be a three-story building.*

5. Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.

**Applicant Response:**

*The proposed site and building design does not propose stepped back upper floors. The building design, the proposed roof design and building articulation provides the desired human scale elements without stepping upper floors.*

- c. Supplemental Elements. In addition to meeting the required elements above, projects must respond to the compatibility requirement in the form of three of the options listed below.

1. Use roof forms and bays to break up the overall mass of larger residential structures.

**Applicant Response:**

*The proposed building design includes a variety of roof forms that include hipped sloped roofs, intersecting gable roof forms and large breaks in the overall roof at building indentations. The proposed roof design breaks up the overall building mass into smaller sections thus reducing the overall appearance of the building mass and scale.*

2. Walls incorporate vertical wall offsets, projections, or recesses to reduce building façades into smaller volumes and define visually distinct living unit modules.

**Applicant Response:**

*The proposed building design includes offsets in the façade wall planes. The deck forms project from the primary building wall plane and the intersecting gable roof form serves to break-up the overall building mass. The larger buildings have central indentations that serve to further break-up the building mass.*



3. Step back upper floors so that the first two stories frame the street and relate to the human scale and reduce the visual impact of the third and higher floor.

**Applicant Response:**

*This element is not utilized in the building design.*

4. Mark a distinct physical transition between the base and upper floors of a building through a change in brick pattern, change in materials and/or wall surface pattern, articulation of a floor line, or change in window types.

**Applicant Response:**

*The proposed building design includes a variety of building materials and textures. The building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building.*

5. Use horizontal elements the entire width of the front façade to mark a break between floors or along the roofline including band course, band molding, bellyband, or belt course.

**Applicant Response:**

*The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building.*

6. Use a variation in roof forms on all four elevations of a structure to visually break up monotony including pitched or sloping roof elements, variations in pitch and height of roof planes, dormers, eaves, gable, or dormer end brackets, corbels, or decorative wood timbers.

**Applicant Response:**

*This element is not utilized in the building design.*

7. Limit continuous ridgelines to less than 40 feet in length and continuous eaves to 25 feet in length.

**Applicant Response:**

*This element is not utilized in the building design.*

8. Step down taller buildings next to smaller buildings to enable buildings of larger scale but similar proportions to blend in with surroundings.

**Applicant Response:**

*This element is not utilized in the building design.*

## 12. Wall and Roof Design.

### A. Characteristics.

1. For buildings that front the street, avoid long, monotonous, uninterrupted walls. Modulate buildings walls and roofs to prevent large, uninterrupted walls and building mass.

**Applicant Response:**

*The proposed building design provides significant articulation and variety in walls planes. The longest wall plane is less than 20 feet before intersecting or recessed wall planes. Projecting or recessed building elements provide a great amount of articulation and variety of wall planes and roof forms.*

2. Differentiate between the base of the building and the top of the building to enhance the pedestrian realm. Make base treatment cohesive across façades and integrate with the architectural character of the building.

**Applicant Response:**

*The proposed building design has a distinct change in material and texture from the first floor to the two upper floors. This creates a distinct base to the building. There is a contrasting trim band that delineates the transition between the base material and the material on the upper two floors of the building. This design concept is applied to all buildings on site.*

3. Multi-dwelling development must address the following design objectives:
  - A. Articulation – All street-facing buildings shall incorporate design elements that break up façades into smaller planes.
  - B. Eyes on the street – A certain percentage of the area of each street-facing façade must be windows or entrance doors.
  - C. Main entrance – On street-facing façades, at least one main entrance must meet standards for location, orientation, and visibility.
  - D. Detailed Design – All street-facing buildings shall include several features.

**Applicant Response:**

*The proposed site and building design address the four design objectives. The building design provides building and roof forms to break up the facades into smaller planes; the street facing units provide windows and glazed doors that face the street frontage; the open entry stairway to the units faces the street frontage; and the street facing building elevations provide a variety of design features.*

**B. Required Elements.**

**1. Articulation.**

- A. For multi-dwelling buildings with 30 to 60 feet of street frontage, a minimum of one of the following elements shall be provided along the street-facing façades.
- B. For buildings with over 60 feet of street frontage, at least one element below shall be provided for every 30 feet of street frontage. Elements shall be distributed along the length of the façade so that there are no more than 30 feet between two elements.
  1. A porch at least 5 feet deep.
  2. A balcony that is at least 2 feet deep and is accessible from an interior room.
  3. A bay window that extends at least 2 feet.

4. A section of the façade that is recessed by at least 2 feet deep and 6 feet long.
  5. A gabled dormer.
- C. Buildings under 30 feet in length are exempt from these requirements.

**Applicant Response:**

*The proposed street frontage buildings have over 60 feet of street frontage. The proposed building design does not have a wall plane over 20 feet between articulation or design features to break up the facades. These include the recessed front porch/patio and upper floor decks that are 6 feet in depth. These projecting elements also have gabled and hipped roof dormers.*

2. Eyes on The Street.

- A. At least 15 percent of the area of each street-facing façade must be windows or entrance doors. Windows used to meet this standard must be transparent and allow views from the building to the street. Glass blocks and privacy windows in bathrooms do not meet this standard.
- B. Window area is considered the entire area within the outer window frame, including any interior window grid.
- C. Doors used to meet this standard must face the street or be at an angle of no greater than 45 degrees from the street.

**Applicant Response:**

*The proposed building design provides 22% window area on the street facing facades. This includes the living room, bedroom and bath windows and the glazed door in the living room leading to the decks and ground floor patios.*

3. Main Entrances. Main entrances must meet both of the following standards.

- A. Be no further than 8 feet behind the longest street-facing wall of the building.
- B. Face the street, be at an angle of up to 45 degrees from the street, or open onto a porch. If the entrance opens up onto a porch, the porch must meet all of these additional standards.
  1. Be at least 25 square feet in area with a minimum 4 feet depth.
  2. Have at least one porch entry facing the street.
  3. Have a roof that is no more than 12 feet above the floor of the porch.
  4. Have a roof that covers at least 30 percent of the porch area.

**Applicant Response:**

*The proposed building provides open air enclosed stairways to serve the living units on that side of the building including the ground floor units. The stairway provides a roof covering.*

4. Detailed Design.

- A. For multi-dwelling buildings with up to 30 feet or more of street frontage, a minimum of two of the elements shall be provided along the street-facing façade or façades.
- B. For buildings with over 30 feet of street frontage, at least one element shall be provided for every 30 feet of street frontage. Elements shall be

distributed along the length of the façade so that there are no more than 30 feet between two elements.

1. Covered porch at least 5 feet deep, as measured horizontally from the face of the main building façade to the edge of the deck, and at least 5 feet wide.

**Applicant Response:**

*The proposed design for Building 'A' has 70 feet of street frontage. The façade is broken up into three primary articulated sections. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. The longest wall plane between these sections is 20 feet. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.*

*The proposed design for Building 'B' has 140 feet of street frontage. The façade is broken up into six articulated sections and the longest wall plane between these sections is 20 feet. This includes unit deck/patio projections that are 6 feet deep and 12 feet wide. Between these deck/patio projects is the recessed entry and open-air stairway to the upper living units.*

2. Recessed entry area at least 2 feet deep, as measured horizontally from the face of the main building façade, and at least 5 feet wide.

**Applicant Response:**

*The proposed building recessed entries and open-air stairways to the upper floor living units. These entries are 8 feet wide and 18 feet deep.*

3. Offset on the building face of at least 16 inches from one exterior wall surface to the other.

**Applicant Response:**

*The proposed street facing building designs provide multiple changes in wall planes. The depth of the wall plane variations are 6 feet in several locations and 10 feet in additional locations. These variations occur multiple times on all street frontage building façade elevations.*

4. Dormer that is at least 4 feet wide and integrated into the roof form.

**Applicant Response:**

*This design element is not utilized in the building design.*

5. Roof eaves with a minimum projection of 12 inches from the intersection of the roof and the exterior walls.

**Applicant Response:**

*The proposed building roof design provides 24 inch, 18 inch and 12 inch roof overhangs.*

6. Roofline offsets of at least 2 feet from the top surface of one roof to the top surface of the other.

**Applicant Response:**

*The design element is not utilized in the building design.*

7. Horizontal lap siding between 3 to 7 inches wide (the visible portion once installed). The siding material may be wood, fiber-cement, or vinyl.

**Applicant Response:**

*The proposed building elevation design includes fiber cement horizontal lap siding with a 6-inch exposure.*

8. Brick, cedar shingles, stucco, or other similar decorative materials covering at least 40 percent of the street-facing façade.

**Applicant Response:**

*This design element is not utilized in the building design.*

9. Gable roof, hip roof, or gambrel roof design.

**Applicant Response:**

*The proposed building roof design includes hip roofs and gable roof forms.*

10. Window trim around all windows at least 3 inches wide and 5/8 inches deep.

**Applicant Response:**

*This design element is not utilized in the building design.*

11. Window recesses, in all windows, of at least 3 inches as measured horizontally from the face of the building façade.

**Applicant Response:**

*This design element is not utilized in the building design.*

12. Balcony that is at least 3 feet deep, 5 feet wide, and accessible from an interior room.

**Applicant Response:**

*The proposed building and living unit design includes recessed upper floor decks that are 6 feet deep and 12 feet wide.*

13. Bay window at least 2 feet deep and 5 feet long.

**Applicant Response:**

*This design element is not utilized in the building design.*

14. One roof pitch of at least 500 square feet in area that is sloped to face the southern sky and has its eave line-oriented within 30 degrees of the true north/ south axis.

**Applicant Response:**

*This design element is not utilized in the building design.*

**CHAPTER 17.21 R-4 MEDIUM, HIGH DENSITY RESIDENTIAL ZONE**

Although the subject site is located within the C-3 Commercial Zone, Section 17.33.010 (6) provides that Apartments are subject to the provisions of the R-4 zone. Therefore the following narrative addresses the compliance with the applicable provisions of the R-4 zone.

**17.21.010 Permitted uses.**

In an R-4 zone, the following uses and their accessory uses are permitted:

- A. Tiny Houses, Single detached dwelling;
- B. Middle Housing
  - 1. Plexes: Duplex Dwelling; Triplex Dwelling; Quadplex Dwelling (minimum lot size of seven thousand square feet)
  - 2. Cottage Clusters
  - 3. Townhouses
- C. Apartments; Multiple dwelling subject to the following:
  - 1. Developments with five or more units.
  - 2. The property on which the use will be located has direct access from a major collector or minor arterial street, or a local collector street within 600' of a collector or arterial street; or
  - 3. The property is located within one-half mile of a planned or existing transit route; or
  - 4. The property is within one-quarter mile from a planned or existing neighborhood or commercial shopping area.

**Applicant Response:**

*The proposed development is for 138 apartment units located in seven three story buildings. The subject site does provide direct access to a local collector street that is within 600 feet of a designated collector.*

**17.21.030 Lot size.**

In an R-4 zone, the lot size shall not be less than five thousand square feet, except that the lot area for attached single dwelling lots shall average one thousand-five hundred square feet in area. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The subject site is 214,759 sf (4.93 Acres) and therefore meets the requirement.*

**17.21.040 Yard requirements.**

In an R-4 zone, each lot shall have yards of the following size unless otherwise provided for in Section 17.54.050:

- A. A front yard shall not be less than fifteen feet, except as provided in Section 17.11.030(C), Table 1(d), Cottage Clusters.
- B. A side yard shall not be less than six feet. An exterior side yard shall not be less than fifteen feet, except as provided in Section 17.11.030(C), Table 1(f), Cottage Clusters.
- C. A rear yard shall not be less than twenty feet, except as provided in Section 17.11.030(C), Table 1(c), Cottage Clusters.



- D. Whether attached to a residence or as a separate building, a covered storage facility for a vehicle on which the main opening is toward a street shall be located not less than twenty feet to the property line bordering the street;
- E. All yards shall be increased, over the requirements of this section, one foot for each two feet of building height over thirty-five feet. (Ord. 4912 §3, 2009; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The yards on the proposed Site Plan are as follows:*

- Front Yard: 15 feet provided, 15 feet required.*
- Side Yard: 10 feet provided, 6 feet required.*
- Rear Yard: 79'-6" provided, 20 feet required.*

*The proposed yards meet the requirements.*

**17.21.050 Building height.**

In an R-4 zone, a building shall not exceed sixty feet in height. (Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The proposed building height is 36'-4". The current underlying Three Mile Lane Planned Development Overlay Ordinance 4709 has a height limitation of 35 feet. The concurrent Planning Commission Variance Application, submitted concurrent within this application, is requesting a height variance to 37 feet. Additional compliance narrative addressing the proposed building height is in the applicant response to the Planning Commission Variance Application narrative.*

**17.21.060 Density requirements.**

In an R-4 zone, the maximum density for single attached dwelling may not exceed four dwelling units per 5,000 square feet. Density maximum may not apply to any other permitted housing types, including accessory dwelling units. (Ord. 4796 §1(b), 2003; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

**Applicant Response:**

*The allowable density for the 214,759 sf subject site is 210 living units. The Site Plan for the proposed development includes 138 apartment units. The proposed density is less than the allowable and is therefore compliant.*

**17.33 C-3 GENERAL COMMERCIAL ZONE**

**17.33.010 Permitted uses.**

In a C-3 zone, the following uses and their accessory uses are permitted:

- 6. Apartments, Multiple dwellings subject to the provisions of the R-4 zone, except that within the City Center Housing Overlay Zone designated in Chapter 17.66, density limitations of the R-4 zone shall not apply, and any special development standards of the Overlay Zone shall supersede those of the R-4 zone.

**Applicant Response:**

*The proposed development is for Apartments and is therefore subject to the R-4 zone development standards.*

**CHAPTER 17.57 LANDSCAPING**

**17.57.010 Purpose and Intent:**

The purpose and intent of this Chapter is to encourage and, where appropriate, require the use of landscape elements, particularly plant materials, in proposed developments in an organized and harmonious manner that will enhance, protect and promote the economic, ecological and aesthetic environment of McMinnville. Landscaping is considered by McMinnville to be an integral part of a complete comprehensive development plan. The City recognizes the value of landscaping in achieving the following objectives:

A. Provide guidelines and standards that will:

1. Reduce soil erosion and the volume and rate of discharge of storm water runoff.

**Applicant Response:**

*Temporary erosion control measures will be implemented to prevent soil erosion during construction. Soils will be protected from erosion post construction with the use of plant materials and 3" deep bark mulch throughout the site.*

2. Aid in energy conservation by shading structures from energy losses caused by weather and wind.

**Applicant Response:**

*Deciduous trees are placed throughout the site to provide shade in the summer months and allow warming sun in the winter.*

3. Mitigate the loss of natural resources.

**Applicant Response:**

*Currently, the site is cleared for agricultural use. The design will provide permanent plant materials for the site and will be a net gain of natural resources in the form of carbon sequestering trees, shrubs and grasses.*

4. Provide parking lot landscaping to reduce the harmful effects of heat, noise and glare associated with motor vehicle use.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.*

5. Create safe, attractively landscaped areas adjacent to public streets.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color. Low flowering shrubs, ornamental grasses, and groundcovers adjacent to SE Norton Lane will provide an attractive landscape viewable for users of the public street.*

6. Require the planting of street trees along the City's rights-of-way.

**Applicant Response:**

*Landscaping adjacent to the public street (SE Norton Lane) will include Green Vase Zelkova trees that will provide shade in the summer and attractive fall color.*

7. Provide visual screens and buffers that mitigate the impact of conflicting land uses to preserve the appearance, character and value of existing neighborhoods.

**Applicant Response:**

*The landscape design proposes the use of perimeter landscaping. Perimeter landscaping in parking areas includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Pedestrian walkways adjacent to property lines are buffered with a site obscuring fence and plant materials. Overall, the use of trees, shrubs and groundcover are compatible with the character of the existing residential and commercial uses that have similar plantings.*

8. Provide shade, and seasonal color.

**Applicant Response:**

*The choice and selection of plant materials incorporates a variety of trees, shrubs, grasses, and groundcover. Trees will provide shade, flowers and fall color. Ornamental shrubs and groundcover will provide seasonal flowers and color.*

9. Reduce glare, noise and heat.

**Applicant Response:**

*The landscape design proposes the use of interior and perimeter landscaping. Perimeter landscaping includes a site obscuring fence and evergreen hedge to eliminate glare from vehicles. Large shade trees in the interior landscape islands will help reduce heat island effects.*

*Noise mitigation is most critical for the residential use west of the site. The design will include a 15' setback to help mitigate noise along the west property line.*

- B. Promote compatibility between land uses by reducing the visual noise and lighting impacts of specific developments on users of the site and abutting properties.

**Applicant Response:**

*The proposed Landscape Plan and design provides trees, buffer and screening plantings along the site perimeter. This will buffer the effects of on-site parking area and building lighting from the adjacent abutting properties. The overall landscape plan and development also aids in buffering sound both from the subject site and from the adjacent abutting properties.*

- C. Unify development and enhance and define public and private places.

**Applicant Response:**

The proposed Landscape Plan and design will provide street trees along the SE Norton Lane street frontage. The landscape design also provides screening of private spaces for the living units and the proposed open common areas on site.

- D. Preserve existing mature trees.

**Applicant Response:**

The existing undeveloped site was historically agricultural uses and there are no existing trees.

- E. Enhance the urban forest and tree canopy.

**Applicant Response:**

The proposed Landscape Plan and design provides a variety of trees that will enhance and contribute to the overall urban tree canopy.

- F. Encourage the use of plants native to the Willamette Valley to the maximum extent feasible, in order to reduce watering requirements and agricultural chemical applications, and to provide a sense of regional identity with plant communities unique to the area.

**Applicant Response:**

The landscape design incorporates native plant varieties and hybrids to help reduce water requirements, reduce chemical applications, and help provide a sense of regional identity. Blue Fescue is used as a drought resistant groundcover throughout the site. Thuja occidentalis "Golden Globe" is a hybrid of the native Thuja and will provide an evergreen presence in winter months, reminiscent of Pacific Northwest landscapes. Finally, White Wonder Dogwood trees, a hybrid of the native Cornus nuttallii, are placed throughout the site. The Dogwoods will provide the familiar white flower in spring that is common in Pacific Northwest landscapes.

- G. Establish and enhance a pleasant visual character and structure to the built environment that is sensitive to safety and aesthetic issues.

**Applicant Response:**

A pleasant visual character and structure is established using a variety of trees, shrubs, and groundcover throughout the site.

The main east-west interior walkway and open spaces are lined with Aristocrat Flowering Pears, giving them a unique visual character and structure. The north-south walkway between building D and open space is lined with European Hornbeams, giving a sense of separation between the common space and the apartments to the west. Crape Myrtles trees are placed in raised planters within amenity areas of the open spaces. The low canopy of the Crape Myrtle will enhance the planters by providing shade and a unique aesthetic. Finally, stately Green Vase Zelkova will line Norton Lane, providing a vertically branched and high canopy structure for the interphase between public and private space.

Shrubs and ornamental grasses throughout the site help provide a pleasant visual character, define spaces and facilitate safety. The perimeter of the site is lined with

evergreen shrubs that will provide a six-foot-tall screen between parking and adjacent uses. A mix of low shrubs and grasses skirt the buildings to provide separation between public and private areas. The low plantings help to soften the angular lines of buildings while keeping sight lines open for safety.

Grasses are used throughout the site to provide a sense of space inherent to the expansive grasslands of the Willamette Valley. Lawn areas in open spaces provide residents opportunities for active recreation. Residents can play a game of volleyball, pick up football or frisbee in these expansive areas.

H. Support McMinnville as a community that cares about its appearance.

**Applicant Response:**

*The proposed Landscape Plan design provides an esthetically pleasing and attractive landscape design. This overall site and landscape design enhances the character of the existing neighborhood and provides a livable community environment.*

**17.57.070 Area Determination—Planning factors.**

A. Landscaping shall be accomplished within the following ranges:

3. Multiple-dwelling, twenty-five percent of the gross area. This may be reduced to not less than fifteen percent upon approval of the review committee. (The gross area to be landscaped may only be reduced by the review committee if there is a showing by the applicant that the intent and purpose of this chapter and subsection B of this section are met.)

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provide 30 percent of the site area in landscaping.*

4. A parking lot or parking structure built in any zone providing parking spaces as required by the zoning ordinance shall be landscaped in accordance with the commercial requirements set forth above in subsection 2 of this section.

**Applicant Response:**

*The proposed Site Plan and Landscape Plan provide parking lot landscaping as identified in 17.11.090 Residential Design Standards for Apartments.*

B. The following factors shall be considered by the applicant when planning the landscaping in order to accomplish the purpose set out in Section 17.57.010. The Landscape Review Committee shall have the authority to deny an application for failure to comply with any or all of these conditions:

1. Compatibility with the proposed project and the surrounding and abutting properties and the uses occurring thereon.

**Applicant Response:**

*The adjacent uses: residential, commercial and agriculture would not be adversely affected by the proposed multi-family development. Uses to the north, south and west of the site will be sufficiently screened with a site obscuring fence and evergreen*

hedge. Additionally, the residential use to the west will be buffered with more than 15' between the property line and any structure.

2. Screening the proposed use by sight-obscuring, evergreen plantings, shade trees, fences, or combinations of plantings and screens.

**Applicant Response:**

Screening is proposed on north, south and west perimeters of the site. Screening will be achieved with a site obscuring fence and evergreen hedge.

3. The retention of existing trees and natural areas that may be incorporated in the development of the project. The existing grade should be preserved to the maximum practical degree. Existing trees shall be provided with a watering area equal to at least one-half the crown area.

**Applicant Response:**

The site was previously open for agricultural use and there are no existing trees on site.

4. The development and use of islands and plantings therein to break up parking areas.

**Applicant Response:**

A total of 37 parking lot islands are used to break up parking areas. Each island has a shade tree and shrubs / grasses to help break up the parking areas.

5. The use of suitable street trees in the development of new subdivisions, shopping centers and like developments. Certain trees shall be prohibited in parking areas: poplar, willow, fruit, nut, birch, conifer, and ailanthus.

**Applicant Response:**

None of the proposed trees are prohibited. Many of the trees are listed on the City of McMinnville street tree list, including the Green Vase Zelkova proseed along SE Norton Lane.

6. Suitable watering facilities or irrigation systems must be included in or near all planted areas;

**Applicant Response:**

The proposed development will have an automatic irrigation system with a water efficient controller and rain sensor. The irrigation system will be commercial grade with separate zones for lawn and shrub areas.

## CHAPTER 17.60 OFF-STREET PARKING AND LOADING

### 17.60.050 Spaces—Location.

- A. Except as provided below, required off-street parking spaces for dwellings shall be located on the same lot with the dwelling.

**Applicant Response:**

All required parking is provided on the same site as all proposed dwelling units.



**17.60.060 Spaces. Number required.**

Except for the southerly 100 feet of Block 10 and the northerly 100 feet of Block 11, Rowland's Addition and the area bounded by Second Street, Adams Street, Fourth Street, and Galloway Street, at the time of erection of a new structure or at the time of enlargement or change of use of an existing structure, off-street parking spaces shall be provided as follows unless greater requirements are otherwise established. Where square feet are specified, the area measured shall be the gross floor area primary to the functioning of the particular use of the property but shall exclude space devoted to off-street parking or unloading.

**Applicant Response:**

*The Off-Street parking required and provided is as follows:*

Parking Required:

*One- and Two-Bedroom Units: 1.5 spaces/unit required, (120 units x 1.5 =180)*

*Three-Bedroom Units: 2 spaces per/unit required (18 units x 2 =18)*

*Total Parking Required: 216 spaces*

Provided Parking:

*Standard Spaces: 188 spaces*

*Compact Spaces: 22 spaces*

*ADA Accessible spaces: 8 spaces*

*Total Spaces Provided 218 spaces*

**17.60.070 Off-street loading requirements.**

- A. Buildings or structures to be built or substantially altered which receive and distribute materials and merchandise by trucks shall provide and maintain off-street loading berths in sufficient number and size to adequately handle the needs of the particular use.

**Applicant Response:**

*The proposed multifamily apartment development does not require off-street loading spaces. None are proposed to be provided.*

**17.60.080 Design requirements.**

- A. All parking lots and driving aisles shall be asphaltic cement concrete or Portland cement concrete with driving aisles, maneuvering aisle and parking spaces clearly marked, except that in an industrial zone, parking spaces which are in addition to those required by this chapter, may be surfaced with a minimum of treated gravel and maintained dust free.

**Applicant Response:**

*All parking areas and drive aisles on the proposed Site Plan are designed to be constructed of asphaltic concrete.*

- B. In a residential zone, a required front yard or a required side yard adjacent to the street shall not be used for any purpose except for off-street parking of motor vehicles, unless otherwise allowed by this ordinance, and such parking space shall not be less than twenty feet in depth from the property line.

**Applicant Response:**

*Parking areas or drive aisles are not proposed to be located within the front or side yard setbacks.*

C. Safe access shall be provided as follows:

1. Access aisles shall be of sufficient width for all vehicular turning and maneuvering.

**Applicant Response:**

*The drive aisles on the proposed Site Plan are 26 feet wide. The turn radii on the drive aisles are 18-foot minimum inside radius. These proposed dimensions provide safe and adequate width and turn radii for vehicles and for fire trucks and emergency vehicles.*

2. Groups of more than four parking spaces shall be so located and served by a driveway that their use will require no backing movements or other maneuvering within a street right-of-way other than an alley.

**Applicant Response:**

*The proposed site plan provides a single driveway access to SE Norton Lane. No backing movements are proposed onto SE Norton Lane.*

3. Driveways to off-street parking areas shall be designed and constructed to facilitate the flow of traffic and to provide for maximum safety of pedestrians and vehicular traffic on the site.

**Applicant Response:**

*The proposed driveway access is 26 feet wide which provides ample width for two-way traffic into and from the subject site. The Public Street frontage improvements include setback sidewalks that cross the driveway and will be constructed in accordance with the City of McMinnville standards.*

4. Clear vision areas shall be provided at driveway exits for all uses except single-detached dwellings and plexes, shall have minimum dimensions of ten feet measured along the street right-of-way and the edge of the driveway. In commercial and industrial zones, buildings and signs may be constructed with cantilevers which extend out over the clear vision area at a height greater than eight feet when measured from the top of the curb, or where no curb exists, from the established centerline grade. Except for existing permanent buildings and structures (other than signs), nonconformities shall be made to comply with the provisions of this section within seven years from the date of its adoption.

**Applicant Response:**

*The proposed driveway access from SE Norton Lane provides the required clear vision area. A clear vision triangle is shown on the proposed Site Plan, drawings A1.0, A1.1, demonstrating compliance.*

5. Driveway cuts shall be a minimum of twenty feet from a street intersection.

**Applicant Response:**

*The proposed driveway location is centered on the intersection of SE Norton Lane and SE Stratus Avenue. This location is in compliance with City of McMinnville standards and policies.*

D. Parking areas shall be made compatible with surrounding uses as follows:

1. Parking spaces along the outer boundaries of a parking area shall be contained by a curb or a bumper rail so placed to prevent a motor vehicle from extending over an adjacent property, sidewalk, or street.

**Applicant Response:**

*The parking areas on the proposed Site Plan provides concrete curbs or wheel stops preventing encroachment into adjacent properties. Additionally, there is a 6-foot sight obscuring fence along the property boundaries.*

2. When a parking area in a commercial or industrial zone abuts a property in a residential zone, a site-obscuring fence or wall, either permanent or of living material, shall be placed along the affected property line. The responsibility for placement of the fence or wall lies with the commercial or industrial property.

**Applicant Response:**

*The proposed Site Plan provides a 6-foot sight obscuring fence along the adjacent property boundaries.*

3. Artificial lighting which may be provided shall be so deflected as not to shine or create glare in any residential zone or on any adjacent dwelling.

**Applicant Response:**

*The proposed Site Lighting Plan, drawings ME 1.0 demonstrates the photometric footprint of the proposed site light fixtures. The proposed fixtures provide full cut-off of light on the adjacent properties preventing light and glare shining on the adjacent sites.*

E. Space size minimum shall be as follows:

1. Handicap parking spaces shall be a minimum of twelve feet wide and 19 feet in length.

**Applicant Response:**

*Handicap Accessible parking spaces have been designed in compliance with Oregon Structural Specialty Code and the ANSI A117.1 standards and requirements. Van Accessible stalls are 9'x19' with an 8' wide access aisle and standard Handicap Accessible spaces are 9'x19' with an 6' wide access aisle. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.*

2. Compact and subcompact parking spaces shall be a minimum of eight feet by sixteen feet.

**Applicant Response:**

*The Compact Spaces on the proposed Site Plan are 8' wide x 16' long and are in compliance. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.*

3. Standard parking spaces shall be a minimum of eight feet six inches by nineteen feet.

**Applicant Response:**

*The Standard Spaces on the proposed Site Plan are 9' wide x 19' long and are in compliance. See the dimensioned Enlarged Site Plans A1.1 Sector 1 and A1.2 Sector 2.*

F. The type of space shall be set as follows:

1. Handicap spaces shall be required and designated as per current federal, state, and local regulations.

**Applicant Response:**

*The Handicap Accessible parking spaces have been designed to be in compliance with the Oregon Structural Specialty Code and the ANSI 117.1 standards and requirements.*

2. Standard spaces shall comprise not less than sixty-five percent of all newly constructed lot spaces.

**Applicant Response:**

*The total parking spaces provided is 218 spaces, of which 188 spaces are standard spaces. This represents 86 percent of the spaces provided.*

#### **17.60.140 Bicycle Parking.**

A. Bicycle parking facilities shall be required as follows:

1. In any commercial (C-1, C-2, and C-3) or office/residential (O-R) zone, bicycle parking facility requirements shall be based on the amount of automobile parking required. The minimum number of bicycle parking spaces provided shall be ten percent of the automobile parking spaces required.
2. The uses exempted from bicycle parking requirements include: residential uses, drive-in theaters, mortuaries, motels, hotels, and automobile service stations.

**Applicant Response:**

*As this is a multi-family residential use Bicycle Parking spaces are not required. However, the applicant is proposing to provide bicycle parking (bike racks) at each building. The proposed Site Plan includes 40 Bicycle Parking Spaces.*

### **CHAPTER 17.61 SOLID WASTE AND RECYCLING ENCLOSURE PLAN**

#### **17.61.030 Guidelines and Standards.**

- A. The location of an enclosure must allow for collection agency drive-in access. A fifty-foot (50) access approach is recommended. In addition to the approach, either an exit that allows the truck to move forward or a turn area with a minimum radius of 46.5 feet is preferred. Both approach and location shall be unobstructed and free of over head wires and low hanging trees. An eighteen-foot (18) minimum height clearance above the enclosure approach is required and a thirty-two-foot (32) vertical clearance is required

above the container itself. The enclosure shall be of sufficient size to store trash and recycling receptacles, the size of which shall be determined by the collection agency and will be based on proposed use. A minimum distance of two-feet (2) is required between the container and existing or proposed structures. The enclosure shall be a minimum of six-feet (6) tall or six inches (6) higher than the top of the tallest container.

**Applicant Response:**

*The proposed Site Plan provides the required front access and turn radii. There are no overhead wires located within the subject site.*

*Due to limited access and site constraints, a Fire Truck Turnaround is provided at the rear of the site. This turnaround may be used by the trash collection vehicles and provides adequate maneuvering for vehicle turnaround. The proposed Site Plan has been submitted to Recology Western Oregon (collection service) for review and approval.*

*All proposed Trash Enclosure structures are located in excess of the 20-foot minimum.*

*The proposed Trash Enclosure has been designed in accordance with Recology Western Oregon Solid Waste Plan Guide and Standards.*

- B. Solid waste enclosures shall not be located within twenty-feet (20) of a required front or exterior yard and should be placed at the rear of a building whenever possible. Should an enclosure be placed within a required landscaped front or exterior yard, additional landscaping must be provided elsewhere on the property to compensate for the encroachment into the required landscaped yard. Any modifications to required landscaping must meet the approval of the Landscape Review Committee.

**Applicant Response:**

*The proposed Site Plan locates the Trash Enclosures 5 feet from the adjacent side yards. A 6' high sight obscuring fence is proposed to be located along the property line. The existing medical office building on the adjacent site has its trash enclosure located approximately 5' from the property line.*

*Due to the limited site access and the Recology Western Oregon required for forward approach to all enclosures, locating the enclosures 20 feet from the exterior side yard would result in a significant loss of parking. As noted, the adjacent medical office building does not comply with this requirement. The provided 6' high sight obscuring fence will adequately screen the enclosure from adjacent properties. None of the proposed locations would place an enclosure near any buildings on adjacent properties.*

- C. Any trash or recycling enclosure which is visible from the street must provide landscaping around three (3) sides of the structure. Climbing vines and screening shrubs or hedges are appropriate and landscaping must be a minimum of three-feet (3) in height at the time of planting.

**Applicant Response:**

*There are no Trash Enclosures that will be visible from the street therefore this is not applicable.*

- D. Where a commercial or industrial zone abuts a residential zone, enclosures must be placed a minimum of thirty-feet (30) from any residential structure or as otherwise approved by the Planning Director.

**Applicant Response:**

*The adjacent properties to the North of the subject site are zoned C-3 Commercial. The property adjacent the subject site on the South is outside the City of McMinnville and the Urban Growth Boundary. The property to the West of the subject site is zoned residential and there are no trash enclosures proposed to be located within so feet of this adjacent residential property. The proposed trash enclosure at the rear of the site adjacent the North property line is 28'-4" East of this Westerly property line. Additional landscaping and screening is proposed adjacent this enclosure. There is also a six-foot sight obscuring fence proposed along westerly property line.*

*An Administrative Variance is being requested for this reduced setback.*

- E. Generally, the design of the structure should match the exterior surface of the building and can be constructed of masonry, wood or concrete blocks in combination with plant material capable of forming a complete evergreen hedge. The floor of the enclosure shall be a concrete holding pad which must extend eight-feet (8) beyond the gates.

**Applicant Response:**

*The proposed Trash Enclosures are to be constructed of split-faced concrete masonry units with a solid cap. Landscape screening is proposed along the sides of the trash enclosures to provide the required evergreen hedge.*

*The concrete slab under the enclosure and in front of the enclosure will be reinforced and extend a minimum of 10 feet in front of the enclosure.*

- F. Gates that screen the containers are required and must remain closed at all times except at times of service.

**Applicant Response:**

*Painted steel gates are proposed on all trash enclosures. This is illustrated in the Trash Enclosure Plan 5/A1.4, and the Trash Enclosure Elevations, 6/A1.4.*

- G. Parking is prohibited in front of the enclosure and all parked vehicles must be located at a safe distance. A "No Parking" sign must be visibly placed on the gates of the enclosure.

**Applicant Response:**

*The proposed Site Plan shows "NO PARKING" pavement markings directly in front of the trash enclosures.*

- H. Solid waste and recycling enclosures must be placed in a location that is compatible with the City of McMinnville's Fire Code.

**Applicant Response:**

*All proposed trash enclosure locations provide substantial separation from buildings as required by the Fire Code.*



# NORTON LANDING APARTMENTS

SE Norton Lane at SE Stratus Avenue

## Administrative Variance and Planning Commission Authority Variance Application Narrative August 20, 2023

### **Project Description:**

The proposed project consists of the development of 138 apartment dwelling units located in seven three-story buildings on a 4.93 acre vacant site. The proposed development includes related site improvements and 3/4 public street improvements of SE Norton Lane along the subject site frontage.

The subject site has limited access to SE Norton Lane and has no other public frontage or access. The subject site has access to public utilities. There is an existing shallow 8" public sanitary sewer located in SE Norton Lane. There is an existing public water line located in SE Norton Lane. There is an existing 15-foot private storm drainage easement along the westerly property line. There is also a 10-foot public utility easement along the SE Norton Lane frontage.

The subject site lies within the Three Mile Lane Planned Development Overlay and the Three Mile Lane Area Plan. The subject site is subject to Planned Development Amendment Approval, Three Mile Lane Deign Review, and the conditions of Approval of the prior Ordinances affecting the subject Site, Including Ordinance 4709 and Ordinance 5072.

### **ASSESSOR MAP/TAX LOT**

Section 27, T.4S, R4W, WM, Tax Lot 701

### **ZONING:**

C-3 PD: General Commercial, Planned Development (Three Mile Lane PD Overlay)  
Ordinance 4709  
Ordinance 5072 (PDA 6-18)

Adjacent Property Zoning:

**NORTH:** C-3 PD: General Commercial  
**EAST:** M-L: Limited Light Industrial  
**SOUTH:** Outside Urban Growth Boundary, Yamhill County  
**WEST:** R-4 Medium, High Density Residential

### **DEVELOPMENT APPLICATION APPROVALS REQUESTED:**

The following land-use applications are concurrently being submitted for approval on the subject site:

Planned Development Amendment to Ordinance 4709 and Ordinance 5072  
Three Mile Lane Design Review  
Landscape Plan Review  
Administrative Variances

### **EXISTING CONDITIONS**

The Subject Site is currently undeveloped and used for farming. The site is generally flat, with a minor slope to the southwest. There are no significant or distinguishing natural features associated with this property.

Access to the property is from Highway 18 via the intersection with Norton Lane. There is a signalized intersection of Norton Lane (SE & NE) with Highway 18. Immediately south of the highway, there is a three-way stop intersection of SE Norton Lane and SE Stratus Avenue. Stratus Avenue loops around the Medical Center and intersects back with SE Norton Lane across from the subject site.

### **ACCESS and UTILITIES**

The property fronts on west side of SE Norton Lane, just south of the Altimus Plaza medical offices. The existing right-of-way for SE Norton Lane is 60 feet down to and through the Stratus Avenue Loop intersection, south of the Medical Center. The existing improvements extend through this intersection. The street is currently improved with curb and gutter and 28-foot paved section.

SE Norton Lane is improved to City standard south to the intersection with the Stratus Avenue loop. The street improvements extend approximately 180 feet along the frontage of the Subject Site. There is an additional 280 feet of site frontage that is currently not improved.

There are existing urban services and utilities within SE Norton Lane. These facilities are all available and were adequately sized to serve development of the subject site, consistent with the allowed commercial uses identified within Ordinance 4709.

### **SURROUNDING USES**

**North** - The property immediately north of the subject property is the Altimus Plaza Medical Offices. Further to the north at the Stratus Avenue intersection are the Comfort Inn & Suites and the Diner restaurant. The land west of the motel is vacant land, however there is a current application for development of multifamily residential.

**East** - The property immediately east of the subject property is developed with medical offices, specifically Willamette Valley Medical Center, including a Heliport. To the south of the medical center is vacant farm land, outside of the city limits and UGB. The Evergreen Aviation Museum, north of Hwy 18, and Municipal Airport, south of Hwy 18, are located just over a mile to the east.

**South** - The property immediately south of the subject property is vacant farm land, outside of the city limits and UGB.

**West** - The property immediately west of the subject property is the Evergreen Estates Mobile Home Park, which is zoned R-4.

## **McMinnville Zoning Code, Title 17: Compliance to Applicable Chapters**

The following narrative addresses the Review Criteria for the requested Administrative Variances and Planning Commission Authority Variances. Additional detailed proposed development and project information is included in the other related applications for this proposed development.

### **CHAPTER 17.74 REVIEW CRITERIA**

#### **17.74.090 Administrative Variance-Review Criteria.**

The Planning Director may grant limited adjustments to the terms of this title as follows:

- A. Lot area: Maximum possible adjustment of one percent of the minimum lot area, but not more than 90 (ninety) square feet;
- B. Setbacks: Maximum adjustment of 10 (ten) percent of the required setback.
- C. These provisions shall be used sparingly and shall not be exceeded except by regular referral to the Planning Commission.
- D. Special conditions may be attached to adjustments if such conditions relate directly to the adjustments.

#### **Applicant Response:**

*The applicant is seeking approval of the following Administrative Variances:*

1. 17.11.090, D (4), d (7): *Landscape Islands shall be separated no more than 60 feet from another.*

#### Proposed:

*The Site Plan proposes a maximum distance of 63 feet between parking area landscape islands. This is an increase of 3 feet which is less than 10% of the required dimension. Standard parking stalls are 9 feet wide and the 60 foot limitation does not coincide with standard parking stall dimensions affecting an efficient parking layout and the best use of the site area to achieve the densities identified by the zoning codes. The requested increase of 5% has no adverse impacts upon the site or the amount of landscaping proposed on the Landscape Plan.*

2. 17.11.090, D (4) f (3): *Off-street parking spaces shall not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.*

#### Proposed:

*The parking areas on the proposed Site Plan are located at a distance of 6-feet from the North and South (side) property lines. This is less than the 10 feet required from the adjacent side property lines. This is compatible with existing adjacent development that also has parking located 6 feet from the adjacent property lines.*

#### **Applicant Response:**

*The requested adjustment to the landscape island spacing in parking areas is affected by the recently adopted Chapter 17.11 Residential Design Standards. Previously development in the vicinity of the subject site was*

not developed under these requirements. As noted above, the 60-foot spacing identified in the newly adopted standard does not take into account the standard or compact parking stall spacing. The net result is inefficient utilization of the available site area for parking thus affecting the overall net achievable density.

The requested adjustment for proposed parking areas to be located less than 10 feet from the adjacent property lines is compatible with the existing adjacent developments as they have parking located at 6 feet from the adjacent property lines. As the subject site has a designated C-3 General Commercial zoning designation, the parking setback is not required. As a multiple family apartment use, the Residential Design Standards of 17.11 are required. Previous adjacent built developments have not been required to meet this standard. The proposed variance is fully compatible with the adjacent developments.

3. 17.61.130 (D): Where a commercial or industrial zone abuts a residential zone, enclosures must be placed a minimum of thirty-feet (30) from any residential structure or as otherwise approved by the Planning Director

Proposed:

The property West of the subject site is zoned R-4 Medium-High Density Residential. The proposed trash enclosure at the rear of the site adjacent the North property line is 28'-4" East of this Westerly property line. The rear yard of the existing mobile home is more than 15 feet west of this property line therefore the proposed trash enclosure location is more than 30 feet from the adjacent residential structure.

Additional landscaping and screening is proposed adjacent this enclosure. There is also a six-foot sight obscuring fence with landscape buffering and screening proposed along westerly property line.

**17.74.100 Variance-Planning Commission Authority.**

The Planning Commission may authorize variances from the requirements of this title where it can be shown that, owing to special and unusual circumstances related to a specific piece of property, strict application of this title would cause an undue or unnecessary hardship, except that no variance shall be granted to allow the use of property for a purpose not authorized within the zone in which the proposed use would be located. In granting a variance, the Planning Commission may attach conditions which it finds necessary to protect the best interests of the surrounding property or neighborhood and otherwise achieve the purposes of this title.

**Applicant Response:**

The applicant is seeking approval of the following Variance through the Planning Commission Authority:

1. Planned Development Overlay Ordinance 4709, Condition 11:  
This Condition of Approval limits the Building height to 35' to the highest point of the roof. The applicant is seeking a Variance to increase the roof height to 37'-0" to the highest ridge line. The proposed building design with sloped and hip roof forms, maintains a low-profile roof line and the requested additional height to 37'-0" will not have any adverse impacts on the existing development in the surrounding area.

**17.74.110 Conditions for Granting Variance.**

A variance may be granted only in the event that the following circumstances substantially exist:

- A. Exceptional or extraordinary circumstances apply to the property which do not apply generally to other properties in the same zone or vicinity, and result from lot size or shape legally existing prior to the date of the ordinance codified in this title, topography, or other circumstance over which the applicant has no control;

**Applicant Response:**

*The allowable building height within the underlying C-3 General Commercial Zone is 80 feet. As this is a residential development, the requirements of the R-4 Medium-High Density Zone govern which allows a building height of 68 feet. The existing adjacent development includes numerous buildings that exceed the 35' height limitation. These include the Willamette Valley Medical Center Hospital and the Comfort Inn Motel which is also three-stories and exceeds 35' in building height.*

*Other properties within the City of McMinnville with the same zoning do not have this height restriction and would allow for a greater density of needed housing to be developed.*

- B. The variance is necessary for the preservation of a property right of the applicant substantially the same as owners of other property in the same zone or vicinity possess;

**Applicant Response:**

*The imposed height restriction limits the density and number of units that can be built on this site. The proposed development needs to achieve three story building heights to be financially feasible. Other adjacent developed properties have buildings exceeding the 35' height limitation and the proposed height preserves a comparable development opportunity already provided to the adjacent developments.*

- C. The variance would not be materially detrimental to the purposes of this title, or to property in the zone or vicinity in which the property is located, or otherwise conflict with the objectives of any city plan or policy;

**Applicant Response:**

*The proposed sloped roof forms have an eave height of 26'-9" with a ridge height just under 37 feet. With the sloped roofs and hip roof design, the impacts of the requested 37' height is not noticeable and results in less impact than a building with a vertical roof parapet wall at 35 feet. The proposed roof eave line provides a much lower perceived overall building height. Additionally, the adjacent properties in the vicinity of the subject site have roof heights far greater than 35 feet. The proposed height adjustment is fully compatible with the adjacent developed properties.*

- D. The variance requested is the minimum variance which would alleviate the hardship.

**Applicant Response:**

*The proposed three-story building design uses 8' high ceilings within the living units, which is considered minimum for market rate apartment units. Given the floor structure and the required heel trusses due to the new energy code requirements, the requested*

*height of 37 feet is the minimum height to accommodate the three-story units. As previously addressed, the project is not feasible if three story building cannot be utilized.*



# McMinnville Fire District

175 NE 1<sup>st</sup> Street

McMinnville, OR 97128

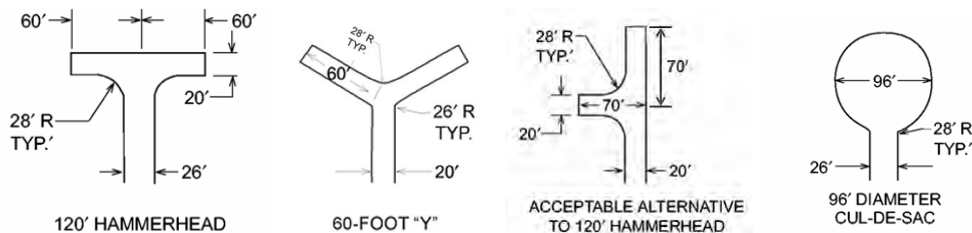
October 19, 2023

Re: Norton Landing Apartments, PDA 5-23, TML 4-23, VR 3-23 & L38-23

Tom and Amanda,

The Fire District has reviewed the site plan, below is a list of conditions of approval:

**DEAD END ROADS:** Dead end fire apparatus access roads in excess of 150 feet in length shall be provided with an approved fire apparatus turnaround. Diagrams of approved turnarounds shown below: (OFC 503.2.5, Appendix D)



**\*Please confirm east to west dimension of hammerhead, 70ft. minimum is required.**

**TURNING RADIUS:** The inside turning radius and outside turning radius shall be not less than 28 feet and 48 feet respectively, measured from the same center point. (OFC 503.2.4)

**MULTIPLE ACCESS ROADS:** Developments of one- and two-family dwellings where the number of dwelling units exceeds 30, multiple-family residential projects having more than 100 dwelling units and where vehicle congestion, adverse terrain conditions or other factors that could limit access, as determined by the fire code official, shall be provided with not less than two approved means of fire department access. Exceptions may be allowed for an approved automatic sprinkler system. The approval of fire sprinklers as an alternate, in lieu of adequate fire apparatus access, shall be accomplished in accordance with OFC 104.8. (OFC 503.1.2, D106 & D107)

**\*The installation of attic fire sprinklers will negate this requirement.**

**NO PARKING SIGNS:** Where fire apparatus roadways are not of sufficient width to accommodate parked vehicles and 20 feet of unobstructed driving surface, "No Parking" signs shall be installed on one or both sides of the roadway and in turnarounds as needed. Roads 26 feet wide or less shall be posted on both sides as a fire lane. Roads more than 26 feet wide to 32 feet wide shall be posted on one side as a fire lane.

Signs shall read "NO PARKING - FIRE LANE" and shall be installed with a clear space above grade level of 7 feet. Signs shall be 12 inches wide by 18 inches high and shall have red letters on a white reflective background. (OFC D103.6)

**ACCESS AND FIRE FIGHTING WATER SUPPLY DURING CONSTRUCTION:** Approved fire apparatus access roadways and firefighting water supplies shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. (OFC 501.4)

**PREMISE IDENTIFICATION:** New and existing buildings shall have approved address numbers, building numbers or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Address numbers shall be Arabic numerals or alphabet letters. Numbers shall be a minimum of 4 inches high with a minimum stroke width of 0.5 inch. (OFC 505.1)

**FIRE PROTECTION AND UTILITY EQUIPMENT IDENTIFICATION:** Rooms which contain controls for fire protection equipment and utilities shall be provided with identifiable markings on the building in an approved and visible manner for fire department access. (i.e., fire alarm control panel, fire sprinkler room, electrical room etc.) (OFC 509)

**FIRE HYDRANTS – COMMERCIAL BUILDINGS:** Where a portion of the building is more than 400 feet from a hydrant on a fire apparatus access road, as measured in an approved route around the exterior of the building, on-site fire hydrants and mains shall be provided. (OFC 507.1, 507.5.1)

Note: This distance may be increased to 600 feet for buildings equipped throughout with an approved automatic sprinkler system.

**FIRE FLOW:** The minimum fire flow and flow duration for commercial buildings shall be determined in accordance with Oregon Fire Code, Appendix B.

**STORZ ADAPTERS – FIRE HYDRANTS:** All fire hydrants with steamer ports shall be fitted with a 5-inch Storz hydrant adapter with a 4.5-inch female NH/NST thread connection. A 5-inch cap with cable shall be provided.

**STORZ ADAPTERS – FIRE DEPARTMENT CONNECTIONS:** All FDCs served by 4" or larger pipe shall be fitted with a 5-inch Storz hydrant adapter with a 4.5-inch female NH/NST thread connection. A 5-inch cap with cable shall be provided.

**KEY/KNOX BOXES:** Key boxes shall be installed on buildings and structures when:

- ✓ An elevator is installed.
- ✓ Equipped with an automatic fire extinguishing system.
- ✓ Equipped with a fire alarm system.
- ✓ Access is restricted due to security arrangements.

*Ty Darby  
Assistant Chief/Fire Marshal  
McMinnville Fire District*



DAVID EVANS  
AND ASSOCIATES INC.

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## MEMORANDUM

**DATE:** September 13, 2023  
**TO:** Tom Schauer – City of McMinnville  
**FROM:** Brad Choi, PE and Josh Anderson, PE, PTOE – David Evans and Associates, Inc. (DEA)  
**SUBJECT:** Norton Landing Transportation Impact Study – Review Comments

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DEA staff has reviewed the Norton Landing Transportation Impact Study prepared by Lancaster Mobley dated August 2023. **We agree with the conclusions in the study and have no need for any additional information.** The following is a summary of the TIA assumptions and findings.

### Summary of TIA:

- The proposed development is a 138-unit apartment complex located at the southern end and on the west side of SE Norton Lane (tax lot R4427 00701). Access to the proposed development is proposed at one driveway opposite SE Stratus Ave (south of Willamette Valley Medical Center).
- Three intersections were included in the study:
  - OR-18 at SE Norton Lane
  - SE Stratus Ave at SE Norton Lane (north intersection)
  - SE Stratus Ave at SE Norton Lane (project driveway/south intersection)
- The traffic study estimated the trip generation for the proposed development based on the equations for ITE land use code 220 *Multifamily Housing (Low-Rise) Not Close to Transit*. The estimated trip generation are 960 average weekday trips, 66 (16 entering, 50 exiting) trips in the AM peak hour, and 80 (50 entering, 30 existing) trips in the PM peak hour. There are no pass-by or other reductions to the trip generation.
- The study estimates the following trip distribution:
  - 35% to/from the northwest along NE Three Mile Lane north of OR-18
  - 30% to/from the southwest along OR-18, west of Three Mile Lane
  - 25% to/from the east along OR-18 east of Norton Lane
  - 10% to/from the north along Norton Lane north of OR-18
- No significant trends or crash patterns were identified at the study intersection. No safety-related mitigations are recommended.
- All three study intersections are expected to operate acceptably and well below the City and ODOT's adopted v/c standards in the existing (2023) and buildout (2025) years in the AM and PM peak hours.
- No sight distance related mitigation is recommended at the project access intersection.
- Traffic signal warrants and left-turn lane warrants are not projected to be met at the site access intersection.



# Oregon

Tina Kotek, Governor

Department of Transportation  
 Region 2 Tech Center  
 455 Airport Road SE, Building B  
 Salem, Oregon 97301-5397  
 Telephone (503) 986-2990  
 Fax (503) 986-2839

**DATE:** October 19, 2023

**TO:** Casey Knecht, PE  
 Development Review Coordinator  
 Arielle S. Ferber 2023.10.19  
 14:44:31 -07'00'

**FROM:** Arielle Ferber, PE  
 Region 2 Traffic Analysis Engineer

**SUBJECT:** Norton Landing Development (McMinnville, OR) – Outright Use  
 TIA Review Comments

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ODOT Region 2 Traffic has completed our review of the submitted traffic impact analysis (dated August 9, 2023) to address traffic impacts due to development west of the Norton Lane at Stratus Avenue (South) intersection in the city of McMinnville, with respect to consistency and compliance with ODOT's Analysis Procedures Manual, Version 2 (APM). The APM was most recently updated in September 2023. The current version is published online at: <http://www.oregon.gov/ODOT/TD/TP/Pages/APM.aspx>. As a result, we submit the following comments for the City's consideration:

Analysis items to note:

- This study does not contain a simulation-based queuing analysis. Such analysis would have been scoped if this study had been required under ODOT's authority.

Proposed mitigation comments:

1. ODOT maintains jurisdiction of the Salmon River Highway No. 39 (OR 18) and ODOT approval shall be required for all proposed mitigation measures to this facility.
2. No mitigation measures have been proposed. This conclusion appears reasonable for this proposed development.

Thank you for the opportunity to review this traffic impact analysis. As the analysis software files were not provided, Region 2 Traffic has only reviewed the submitted report.

This traffic impact study has been, for the most part, prepared in accordance with ODOT analysis procedures and methodologies. No further analysis work should be required.

If there are any questions regarding these comments, please contact me at (971) 208-1290 or [Arielle.Ferber@ODOT.state.or.us](mailto:Arielle.Ferber@ODOT.state.or.us)