



City of McMinnville
Planning Department
231 NE Fifth Street
McMinnville, OR 97128
(503) 434-7311

www.mcminnvilleoregon.gov

**Planning Commission
ZOOM Online Meeting:
February 18, 2021**

*Please Note that this meeting will be conducted via
ZOOM meeting software due to the COVID-19 event.*

6:30 PM Regular Meeting

ZOOM Meeting: You may join online via the following link:

<https://mcminnvilleoregon.zoom.us/j/91883120529?pwd=bjN2TTV2MU9qcnFQdXNGQ3FsYklnZz09>

Zoom ID: 918 8312 0529

Zoom Password: 166737

Or you can call in and listen via zoom: 1 699 900 9128

ID: 918 8312 0529

Public Participation:

Citizen Comments: If you wish to address the Planning Commission on any item not on the agenda, you may respond as the Planning Commission Chair calls for "Citizen Comments."

Public Hearing: To participate in the public hearings, please choose one of the following.

- 1) *Email in advance of the meeting – Email at any time up to 12 p.m. the day of the meeting to Sarah.Sullivan@mcminnvilleoregon.gov, that email will be provided to the planning commissioners, lead planning staff and entered into the record at the meeting.*
- 2) *By ZOOM at the meeting - Join the zoom meeting and send a chat directly to Planning Director, Heather Richards, to request to speak indicating which public hearing, and/or use the raise hand feature in zoom to request to speak once called upon by the Planning Commission chairperson. Once your turn is up, we will announce your name and unmute your mic.*
- 3) *By telephone at the meeting – If appearing via telephone only please sign up prior to the meeting by emailing the Planning Director, Heather.Richards@mcminnvilleoregon.gov as the chat function is not available when calling in zoom.*

----- MEETING AGENDA ON NEXT PAGE -----

The meeting site is accessible to handicapped individuals. Assistance with communications (visual, hearing) must be requested 24 hours in advance by contacting the City Manager (503) 434-7405 – 1-800-735-1232 for voice, or TDY 1-800-735-2900.

*Please note that these documents are also on the City's website, www.mcminnvilleoregon.gov. You may also request a copy from the Planning Department.

Commission Members	Agenda Items
Roger Hall, Chair	6:30 PM - REGULAR MEETING
Lori Schanche, Vice-Chair	1. Call to Order
Robert Banagay	2. Approval of Minutes
Ethan Downs	<ul style="list-style-type: none"> • September 17, 2020 (<i>Exhibit 1</i>)
Gary Langenwalter	3. Citizen Comments
Sylla McClellan	4. Public Hearing:
Brian Randall	A. <u>Quasi-Judicial Hearing: Zoning Text Amendment (ZC 1-20) and Three Mile Lane Development Review (TML 2-20)</u> (<i>Exhibit 2</i>)
Beth Rankin	Request: Approval to amend the Three Mile Lane Planned Development Overlay Ordinance (Ordinance No. 4131 as amended by Ordinance No. 4572) to allow additional freestanding signs for businesses that employ drive-up service within Zone 2 of the Overlay District.
Dan Tucholsky	Approval of a concurrent Three Mile Lane Development Review to allow for the construction of a second drive-through service lane and associated signage at a McDonald's restaurant.
Sidonie Winfield	Location: Zone 2 of the Three Mile Lane Plan Development Overlay District and 225 NE Norton Lane, more specifically described as Tax Lot 1602, Section 22CD, T.4 S., R 4 W., W.M.
	Application: Alexander Taam, Freiheit Architecture, on behalf of McDonald's Corporation, property owner
	5. Commissioner/Committee Member Comments
	6. Staff Comments
	7. Adjournment

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MINUTES – EXHIBIT 1

September 17, 2020
Planning Commission
Regular Meeting

6:30 pm
Zoom Online Meeting
McMinnville, Oregon

Members Present: Chair Roger Hall, Commissioners: Robert Banagay, Erin Butler, Susan Dirks, Gary Langenwaller, Roger Lizut, Beth Rankin, Lori Schanche and Ethan Downs – Youth Liaison

Members Absent:

Staff Present: Heather Richards – Planning Director, and Chuck Darnell – Senior Planner

1. Call to Order

Chair Hall called the meeting to order at 6:30 p.m.

Planning Director Richards introduced Ethan Downs the new Youth Liaison to the Planning Commission.

2. Approval of Minutes

- **May 21, 2020**

Commissioner Schanche noted two street names were spelled incorrectly on the first page of the minutes.

Commissioner Langenwaller moved to approve the May 21, 2020 meeting minutes as amended. The motion was seconded by Commissioner Dirks and passed 8-0.

3. Citizen Comments

None

4. Work Session:

- **HB 2001 – Missing Middle Housing Code Update**

Planning Director Richards gave a presentation on HB 2001. The bill required cities of certain sizes to allow “middle housing” in areas and properties that allowed for the development of detached single family dwellings. It included duplexes, triplexes, quadplexes, cottage clusters, and townhouses. Cities with a population over 25,000 shall allow the development of all middle housing types in areas zoned for residential use and a duplex on each lot or parcel zoned for

residential use. In McMinnville, development of detached single family dwellings was allowed in R-1, R-2, R-3, R-4, and O-R. Cities were required to adopt land use regulations and Comprehensive Plan amendments to address HB 2001 by June 30, 2022. There was a process to allow cities to identify infrastructure issues and request an extension to address issues. The extension requests were due by June 30, 2021. DLCD set up a rule making committee for HB 2001/2003 that had three separate subcommittees: Missing Middle Housing Code, Infrastructure Constraints, and Housing Production Strategy. The Rule Making Committee developed model codes for missing middle housing and proposed draft rules that cities needed to follow to implement HB 2001. There was a public hearing scheduled with LCDC for the Missing Middle OARs that were proposed for large cities on September 25. Staff had concerns about the parking standards. The proposed rule stated no more than one off-street parking space could be required per dwelling unit. The premise was that providing off-street parking was expensive and lead to less affordable housing, that to encourage reduction in greenhouse gas emissions they needed to discourage vehicle ownership, and that developers would do the right thing and respond to the marketplace. Additional needed parking would be provided with on-street parking availability. They assumed that missing middle housing equated to similar size as single dwelling units. McMinnville's current parking standards for multiple-family dwellings were one and one-half spaces per dwelling with less than three bedrooms, two spaces per dwelling unit with three or more bedrooms, and one space per dwelling unit which was expressly reserved for senior or handicapped persons. The parking standards for single family and two-family dwellings were two spaces per dwelling with four or fewer bedrooms, and one additional space for every two additional bedrooms. The concern was one size did not fit all and it should be a local decision. This was due to the following reasons: not all communities had reliable alternative modes of transportation to support employment, amenities, and school activities, developers did not always do the right thing and built to the minimum standards of the community, there was no empirical truth to the assumption that missing middle housing equated to the same size as a single dwelling unit, savings did not translate to the end user in a private market of more demand than supply, the City would need to amend the Comprehensive Plan to rely on on-street parking for residential parking needs, and fear of creating systemic inequality for lower-income households.

Planning Director Richards said another topic to be aware of was the language about master planned communities. They would need to get a legal opinion about whether or not it applied to planned developments in McMinnville. The concern was that developments like Baker Creek North that had recently been approved could be 280 lots of 4-plexes, which would quadruple the density in the neighborhood. The limitation was cities could not reduce density to lower than 15 units per acre. The Comprehensive Plan designations and density allocations needed to be changed to accommodate this. She expected it to be legally challenged. There was an opportunity to evaluate infrastructure for support of increased density in single family neighborhoods. There was a prescriptive process for review and approval of an extension if significant deficiency could be shown. McMinnville received \$100,000 to evaluate transportation, wastewater, water, and stormwater infrastructure systems. That work would begin soon in coordination with utility providers. If an extension was needed, the request must be submitted by June 30, 2021. Regarding ADUs, they could not regulate home ownership for the primary dwelling and could not require off-street parking. The City's code would need to be amended to accommodate the parking provisions and that work was planned to be part of the larger site and design review package.

- **Urban Growth Boundary Update**

Planning Director Richards said the City would be responding to the LCDC remand to the City of McMinnville for the MGMUP 2003-2023 which was first submitted in 2003 and modified in 2005. The LCDC remand was based on the Court of Appeals remand to LCDC. The Court's decision was that the City erred in its application of ORS 197.298 and that a correct application of the law could compel a different result. That meant the City needed to determine the land needed, refine the study area, identify buildable land in the study area, apply ORS 197.298 land selection for locational analysis, and evaluate land per Goal 14 location factors. For determining the need, they would use the HNA and BLI in the public record for the remand which was for the 20 year planning horizon of 2003-2023. The population forecast in 2023 was 44,055 with 2.54 persons per household and number of new housing units was 6,014. The buildable land needed at that time was 1,125.8 acres, 1,019.8 acres of residential and 106 acres of commercial. The summary of need was: 1,189 housing units, 54 acres for public schools, 254 acres for public parks, 47.60 acres for religious, 27.50 acres for other, 106 acres for commercial, and there was a surplus of 46 acres of industrial. The overall goal for density was 5.7 units per acre. The priority land selection scheme was to first look at land established in Urban Reserves, then exception land and farm or forest land (other than high value farm land) surrounded by exception land, then marginal lands designated pursuant to ORS 197.247, and last was farm and forest land. Surrounding McMinnville was high value farm land. Higher priority would be given to land of lower capability as measured by the capability classification system. Land of lower priority under subsection 1 may be included in an Urban Growth Boundary if land of higher priority was found to be inadequate to accommodate the amount of land estimated in subsection 1 for one or more of the following reasons: specific types of identified land needs could not be reasonably accommodated on higher priority lands, future urban services could not reasonably be provided to the higher priority lands due to topographical or other physical constraints, or maximum efficiency of land uses within a proposed Urban Growth Boundary required inclusion of lower priority lands in order to include or to provide services to higher priority lands.

Planning Director Richards discussed the process the City used for identifying the buildable land in the study area. She showed maps of the Court's direction regarding the study areas, physical barriers, resulting study areas after applying the barrier filters, further revisions, and priority selections. In 2008 a conservation easement was placed on 170 acres of land in the Grandhaven area. Of these, 81 acres were in the existing UGB. That land was designated for residential development on the plan map. The easement in effect removed the land from the City's buildable inventory and removed an additional 15 acres from the buildable land inventory due to lack of access for a total of 96 acres. Steep slopes greater than 25% were also removed as they were topographical barriers. The alternatives analysis had to be approaches for the UGB expansion within the 2003 planning and legal framework. They had to meet the principles and objectives of the original MGMUP, achieve the stated need for McMinnville's future, apply it to the priority structure in ORS 197.298, and apply the Goal 14 (Urbanization) Factors. The principles in the original MGMUP included: contain urban expansion within natural and physical boundaries, encourage development consistent with "smart growth" principles, allow increased densities to help meet housing needs, minimize public facilities costs, avoid identified hazards and natural resources, respect historic development patterns and land uses, and comply with state land use laws. They had to establish a methodology for applying the Goal 14 factors. The factors related to UGB amendments were: Factors 1 & 2 which related to land need for housing, employment, etc. These were acknowledged for McMinnville and were not applicable to where the boundary was drawn. Factors 3-7 were called the "location" factors. They were applicable for deciding where to expand the UGB. Factor 3 was about the orderly and economic provision for public facilities and services. The screening criteria were the costs to extend water, sewer, and roads outside the UGB and the costs to expand capacity in the UGB. Factor 4 was about maximum efficiency of land uses within and on the fringe of the existing urban area. The screening criteria

were urban integration, development costs, suitability for bike/ped, commercial, multi-story development, and parks/schools. Factor 5 was about environmental, energy, economic, and social consequences. The screening criteria were critical wildlife habitat, natural hazards, affordable housing suitability, and neighborhood commercial suitability. Factor 6 was about the retention of agricultural land as defined, with Class I being the highest priority for retention and Class VI the lowest priority. The screening criterion was the non-irrigated soil classification map. Factor 7 was about compatibility of the proposed urban uses with nearby agricultural activities. The screening criteria were high value farmland and perimeter proximity to the high value farmland.

Planning Director Richards explained the priority selection and application of the Goal 14 factors for the exception areas and lower value resource lands, higher value resource lands with low Goal 14 factor scores, and alternatives. She reviewed how the alternatives achieved the stated need and City Council discussed which scenario to use. The Council concurred to use Alternative 3B with some tweaks and wanted to hold a public open house in October. In November there would be a joint City Council/County Commission public hearing, and in December there would be separate City Council and County Commission deliberation meetings. The plan was to send the submittal to DLCD by December 31, 2020.

There was discussion regarding how responding to the court remand held the City to the planning horizon to 2023 and if they were successful, the next step was to move forward with an Urban Reserve Area process. There was further discussion regarding the missing middle housing and how cities should not anticipate more than 3% increase in density, anticipated legal challenges to the remand work, and timeline of the remand review.

5. Work Session: Goal 7 Natural Hazards Study Discussion

This agenda item was not discussed.

6. Commissioner Comments

None

7. Staff Comments

Planning Director Richards said Commissioner Perron had resigned from the Commission. The vacancy was currently being advertised.

8. Adjournment

Chair Hall adjourned the meeting at 8:02 p.m.

Heather Richards
Secretary



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EXHIBIT 2 - STAFF REPORT

DATE: February 18, 2021
TO: Planning Commission
FROM: Jamie Fleckenstein, Associate Planner
SUBJECT: PUBLIC HEARING: ZC 2-20 (Zoning Text Amendment) and TML 2-20 (Three Mile Lane Development Review for a McDonald's restaurant at 225 NE Norton Lane)

STRATEGIC PRIORITY & GOAL:



GROWTH & DEVELOPMENT CHARACTER

Guide growth & development strategically, responsively & responsibly to enhance our unique character.

OBJECTIVE/S: Strategically plan for short and long-term growth and development that will create enduring value for the community

Report in Brief:

This proceeding is a combined quasi-judicial public hearing of the Planning Commission to consider two separate and distinct applications: a Zoning Text Amendment (ZC 2-20) amending the Three Mile Lane Planned Development Overlay Ordinance (Ordinance No. 4131 as amended by Ordinance No. 4572) and a Three Mile Lane Development Review (TML 2-20) for a McDonald's Restaurant in the Three Mile Lane Planned Development Overlay District.

The applications have been submitted concurrently, and are therefore subject to the hearing procedure that affords the most opportunity for public hearing and notice. Although the applications support the applicant's goal of redeveloping an existing restaurant site, they each need to be treated as individual decisions and are governed by different regulations and criteria. The order of consideration and approval should be:

1. Zoning Text Amendment (ZC 2-20)
2. Three Mile Lane Development Review (TML 2-20)

Planning Commission is making a recommendation to the City Council for both applications. The approval of Three Mile Lane Development Review (TML 2-20) is contingent upon the approval of Zoning Text Amendment (ZC 2-20).

Attachments:

Attachment A: ZC 2-20 Decision Document

Attachment B: ZC 2-20 Application

Attachment C: TML 2-20 Decision Document

Attachment D: TML 2-20 Application

Attachment E: Three Mile Lane Planned Development Overlay Ordinance (Ord. Nos. 4131 and 4572)

ZC 2-20 considers the amendment of the Three Mile Lane Planned Development Overlay Ordinance (Ordinance No. 4131 as amended by Ordinance No. 4572) to allow additional freestanding signs for businesses that employ drive-up service within Zone 2 of the Overlay District. The following language is proposed to be added to Section 5B of the Three Mile Lane Planned Development Overlay which regulates signage on commercially and industrially designated properties within Zone 2:

“Additional freestanding signs are permitted with businesses that employ drive-up service. One such sign, not to exceed 36 square feet in area or six feet in height, is allowed per order station. In addition, one secondary sign, a maximum of 15 square feet in area and five feet in height, is allowed per order station. Any freestanding sign that has copy facing toward a public street shall be located a minimum of thirty (30) feet from that street’s property line. Wall mount signs shall be exempt from this.”

The proposed amendment is based on language found in McMinnville Municipal Code (MMC) Chapter 17.62 – Signs, which currently does not apply to land in the Three Mile Lane Planned Development Overlay.

TML 2-20 considers approval of Three Mile Lane Development Review to allow for the construction of a second drive-through service lane and additional associated signage at an existing McDonald’s restaurant.

Background:

Subject Property & Request

The proposed Zoning Text Amendment (ZC 2-20) would amend signage regulations for commercial and industrial properties within Zone 2 of the Three Mile Lane Planned Development Overlay District to allow additional freestanding signs for uses that employ drive-up service. **See Exhibits 1 & 2.** Commercially designated properties border the north side of Highway 18 within Zone 2. Properties in the northern portion of Zone 2 are residential. All properties south of Highway 18 within Zone 2 are also commercially designated. Commercial uses south of Highway 18 include the Comfort Inn & Suites hotel, The Diner restaurant, and the Altimus Plaza office center. Commercial uses north of Highway 18 include a McDonald’s restaurant, Red Lion Hotel, and a welding supply store. Currently, the McDonald’s restaurant is the only commercial use that employs drive-up service.

The McDonald’s restaurant, the subject property for the proposed Three Mile Lane Development Review (TML 2-20) is located at 225 NE Norton Lane. **See Exhibit 3.** The site is currently developed with a restaurant use that employs one (1) drive-up service lane. The application is for the approval of a re-development of the McDonald’s site to include the addition of a second drive-up service lane with associated additional freestanding signage that would be allowed by the adoption of Zoning Text Amendment ZC 2-20. **See Exhibit 4.** The subject property is bordered by commercially zoned land to the east and west, residentially zoned land to the north, and by the Highway 18 right-of-way immediately to the south.

Neighborhood Meeting

Neighborhood meetings are not required for Zoning Text Amendment or Three Mile Lane Development Review applications as specified in Chapter 17.72 of the Zoning Ordinance.

Discussion

Attachments:

Attachment A: ZC 2-20 Decision Document

Attachment B: ZC 2-20 Application

Attachment C: TML 2-20 Decision Document

Attachment D: TML 2-20 Application

Attachment E: Three Mile Lane Planned Development Overlay Ordinance (Ord. Nos. 4131 and 4572)

Summary of Criteria for ZC 2-20

Zoning Text Amendments are not typically initiated by property owners, and there aren't specific review criteria for Zoning Text Amendments found in the Zoning Ordinance. At the request of the applicant, a Zoning Text Amendment application was created, and review criteria consistent with MMC Section 17.72.020 – Application Submittal Requirements were provided. In accordance with Section 17.72.020(G), the Planning Director, required materials to demonstrate compliance with the following three criteria:

1. The proposed change is consistent with the relevant goals and policies of the McMinnville Comprehensive Plan and this code.
2. There is a public need for a change of the kind in question.
3. The need will be best served by changing the classification of the particular piece of property in question as compared with other available property.

Summary of Criteria for TML 2-20

The application is subject to the policies and procedures of the Three Mile Lane Planned Development Overlay district (adopted by Ordinance No. 4131 and amended by Ordinance No. 4572). Section 4 includes the following policies that regulate development within the Three Mile Lane Planned Development Overlay district:

- A. The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- B. A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- C. Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:
 1. The minimization of entrances onto Three Mile Lane;
 2. The development of on-site circulation systems, connecting to adjoining properties, including public frontage roads;
 3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
 4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.
- D. Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- E. Mixed housing-type residential developments shall be allowed and encouraged in those areas designated as residential.
- F. Temporary signage shall be allowed as per Section 17.62.060(B)(3) of the McMinnville Zoning Ordinance (No. 3380).

In addition, Section 5 includes standards for permanent signage within the Three Mile Lane Planned Development Overlay district. Section 5 is proposed to be amended by ZC 2-20 allow and regulate additional freestanding signs for commercial uses that employ drive-up service.

Summary of Issues for ZC 2-20

The owner of the McDonald's restaurant at 225 NE Norton Lane wishes to redevelop the property to add a second drive-thru lane in response to increased public demand, due in large part to changing social

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Attachment B: ZC 2-20 Application

Attachment C: TML 2-20 Decision Document

Attachment D: TML 2-20 Application

Attachment E: Three Mile Lane Planned Development Overlay Ordinance (Ord. Nos. 4131 and 4572)

habits and consumer patterns triggered by the ongoing COVID-19 pandemic. However, the Three Mile Lane Planned Development Overlay (adopted by Ordinance No. 4131 in 1981 and amended by Ordinance No. 4572 in 1994) only allows only one (1) freestanding sign per commercial property within the area designated as Zone 2, where the McDonald's is located. This prohibits the addition of a second drive-thru lane with freestanding signs such as a Menu Board and Order Board.

The chapter of the Zoning Ordinance that regulates signage throughout the City (MMC Chapter 17.62 – Signs) was amended in 2016 to include language permitting and regulating additional freestanding signs for businesses that employ drive-up service to address this issue elsewhere in the City. However, Chapter 17.62 does not apply to the Three Mile Lane Planned Development Overlay per MMC 17.62.020. Without an amendment to the language of the Three Mile Lane Planned Development Overlay Ordinance, the restaurant owner would be limited in the signs, and therefore, drive-thru order stations allowed on the site. Therefore, the applicant initiated the Zoning Text Amendment process to introduce the same language that was added to the signs Chapter in 2016 to the Three Mile Lane Planned Development Overlay that was last updated in 1994. The goal is to provide the flexibility for businesses to respond to shifting consumer demands.

The proposed amendment would allow up to two (2) additional signs per drive-up service lane/order station. Each sign would have height and area restrictions appropriate to signs used for this purpose. The proposed amendment would not alter other existing sign regulations within the Three Mile Lane Overlay, nor would it change the existing review procedure that requires all development and signage to be reviewed and approved by the Three Mile Lane Review Committee. The limited applicability of the proposal (impacting only commercial or industrial properties in Zone 2) would make just ten (10) properties potentially eligible for additional signage. Of those ten (10) properties, only one (1) currently has a business that employs drive-up service – the McDonald's restaurant at 225 NE Norton Lane.

Summary of Issues for TML 2-20

The applicant's proposal to add a second drive-thru service lane and associated signage to an existing restaurant is subject to is contingent upon the approval and adoption of the Zoning Text Amendment ZC 2-20. Without the approval of ZC 2-20, the necessary signs to support a drive-thru service lane would not be allowed.

The redevelopment of the site to accommodate a second drive-thru lane includes reorganization of on-site circulation and parking, relocation of the solid waste and recycling enclosure, and the provision of new landscaping around the site. Proposed site development changes meet applicable standards and guidelines for on-site parking and solid waste/recycling enclosure plans. The applicant submitted a Landscape Plan Review application (L 30-20) prior to these concurrent applications to the Landscape Review Committee for review. The Landscape Review Committee approved the Landscape Plan and praised the overall site design.

Proposed signage to support two (2) drive-thru service lanes is generally consistent with the standards and objectives of the Three Mile Lane Planned Development Overlay. Proposed Digital Pre-Browse Boards exceed the height proposed in Zoning Text Amendment ZC 2-20, so a condition of approval requiring compliance with height standards is recommended. Staff finds the proposed signage is of a quality and design that is suitable for the proposed site, and is compatible with the use. All proposed signage is located to have minimal impact on neighboring properties and the public right-of-way.

Public Comments

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Attachment B: ZC 2-20 Application

Attachment C: TML 2-20 Decision Document

Attachment D: TML 2-20 Application

Attachment E: Three Mile Lane Planned Development Overlay Ordinance (Ord. Nos. 4131 and 4572)

Notice of the proposed applications was mailed to property owners and published in the newspaper. As of the date of this Staff Report, the following public comments were received:

- ZC 2-20
 - No comments received
- TML 2-20
 - Email from Dorothy McQueen on February 1, 2021 expressing concerns about sight, sound, and security issues with increased parking lot traffic and questioned if McDonald's is improving the fence along the north property line.
 - Staff Response: The northern property line of the McDonald's property has an existing mature hedge that creates a solid screen in front of a solid wooden fence that appears to be in fair condition.

Agency Comments

Notice of the proposed applications was sent to affected agencies and departments. Agency comments were received from the McMinnville Building and Engineering Departments, McMinnville Water and Light, and Comcast. They responded with no comments.

Fiscal Impact:

Not Applicable

Planning Commission Options for ZC 2-20 (Zoning Text Amendment) and TML 2-20 (Three Mile Lane Development Review) – each to be voted on separately:

- 1) Close the public hearing and forward a recommendation for **APPROVAL** of the application to the McMinnville City Council, per the decision document provided which includes the findings of fact.
- 2) **CONTINUE** the public hearing to a specific date and time.
- 3) Close the public hearing, but **KEEP THE RECORD OPEN** for the receipt of additional written testimony until a specific date and time.
- 4) Close the public hearing and **DENY** the application, providing findings of fact for the denial, specifying which criteria are not satisfied, or specifying how the applicant has failed to meet the burden of proof to demonstrate all criteria are satisfied, in the motion to deny.

Staff Recommendation:

Staff has reviewed the proposals for consistency with the applicable standards and criteria. Absent any new evidence to the contrary presented during the hearing, staff finds that, subject to the recommended conditions specified in the attached Decision Documents, the applications submitted by the applicant and the records contain sufficient evidence to find the applicable criteria are satisfied.

Staff **RECOMMENDS APPROVAL** of both applications, subject to the conditions specified in the attached Decision Documents.

Suggested Motion for ZC 2-20:

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Attachment A: ZC 2-20 Decision Document

Attachment B: ZC 2-20 Application

Attachment C: TML 2-20 Decision Document

Attachment D: TML 2-20 Application

Attachment E: Three Mile Lane Planned Development Overlay Ordinance (Ord. Nos. 4131 and 4572)

BASED ON THE FINDINGS OF FACT, THE CONCLUSIONARY FINDINGS FOR APPROVAL, AND THE MATERIALS SUBMITTED BY THE APPLICANT, THE PLANNING COMMISSION RECOMMENDS THAT THE CITY COUNCIL APPROVE ZC 2-20.

Suggested Motion for TML 2-20:

BASED ON THE FINDINGS OF FACT, THE CONCLUSIONARY FINDINGS FOR APPROVAL, AND THE MATERIALS SUBMITTED BY THE APPLICANT, THE PLANNING COMMISSION RECOMMENDS THAT THE CITY COUNCIL APPROVE TML 2-20, SUBJECT TO THE CONDITIONS OF APPROVAL PROVIDED IN THE DECISION DOCUMENT.

JF

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Attachment C: TML 2-20 Decision Document

Attachment D: TML 2-20 Application

Attachment E: Three Mile Lane Planned Development Overlay Ordinance (Ord. Nos. 4131 and 4572)

Exhibit 1: Three Mile Lane Planned Development Overlay, Zone 2

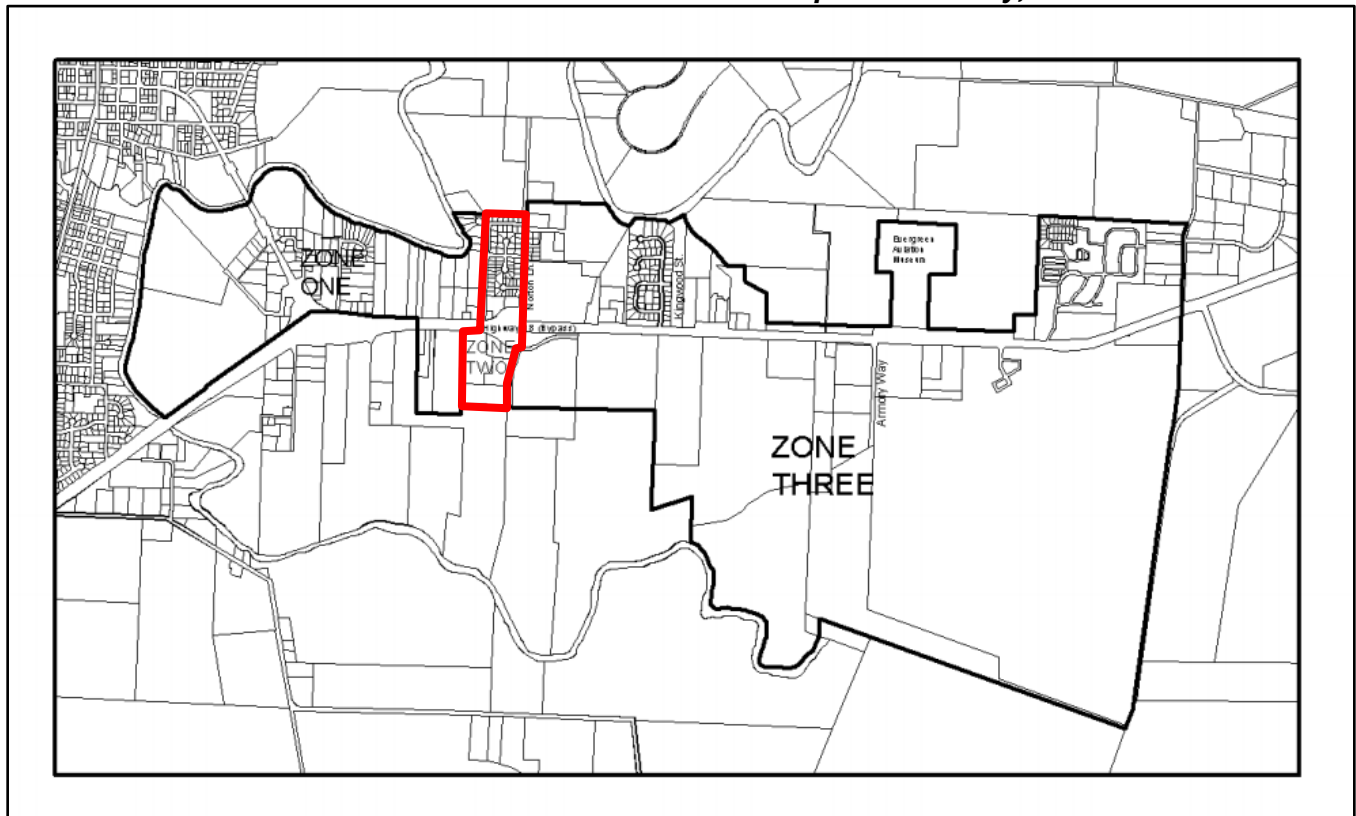


Exhibit 2: Three Mile Lane Planned Development Overlay, Zone 2 – Zoning

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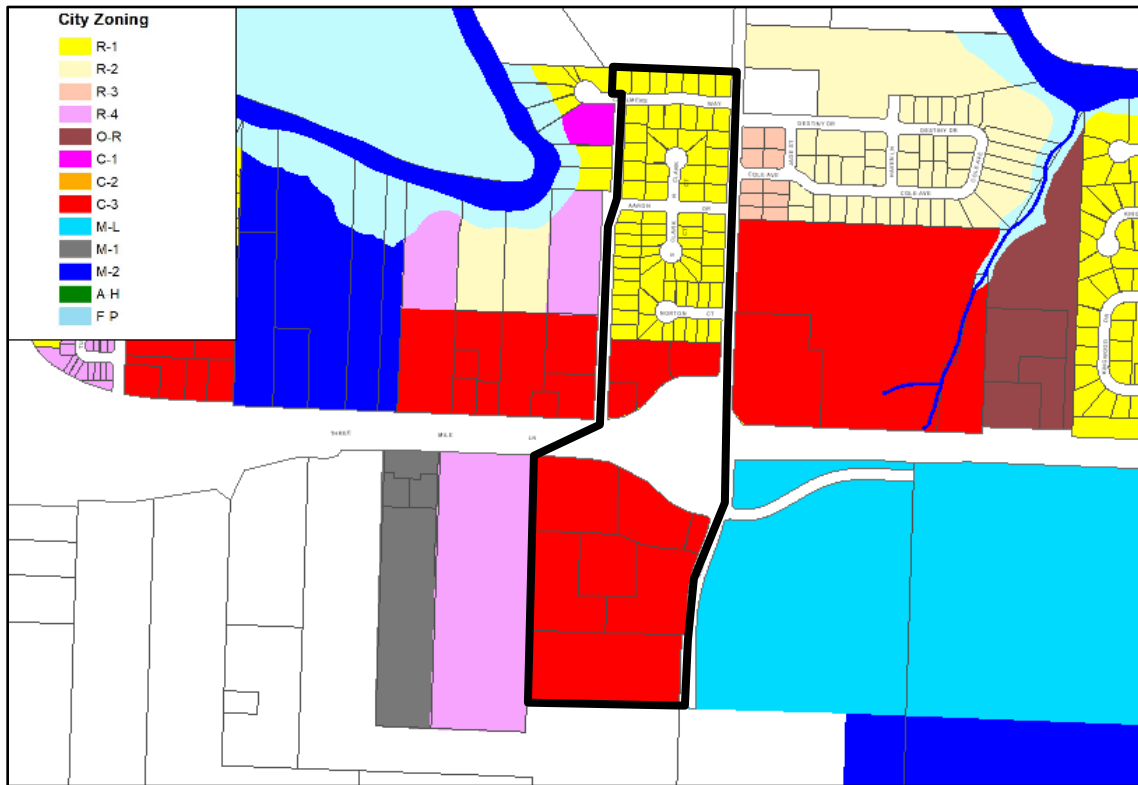
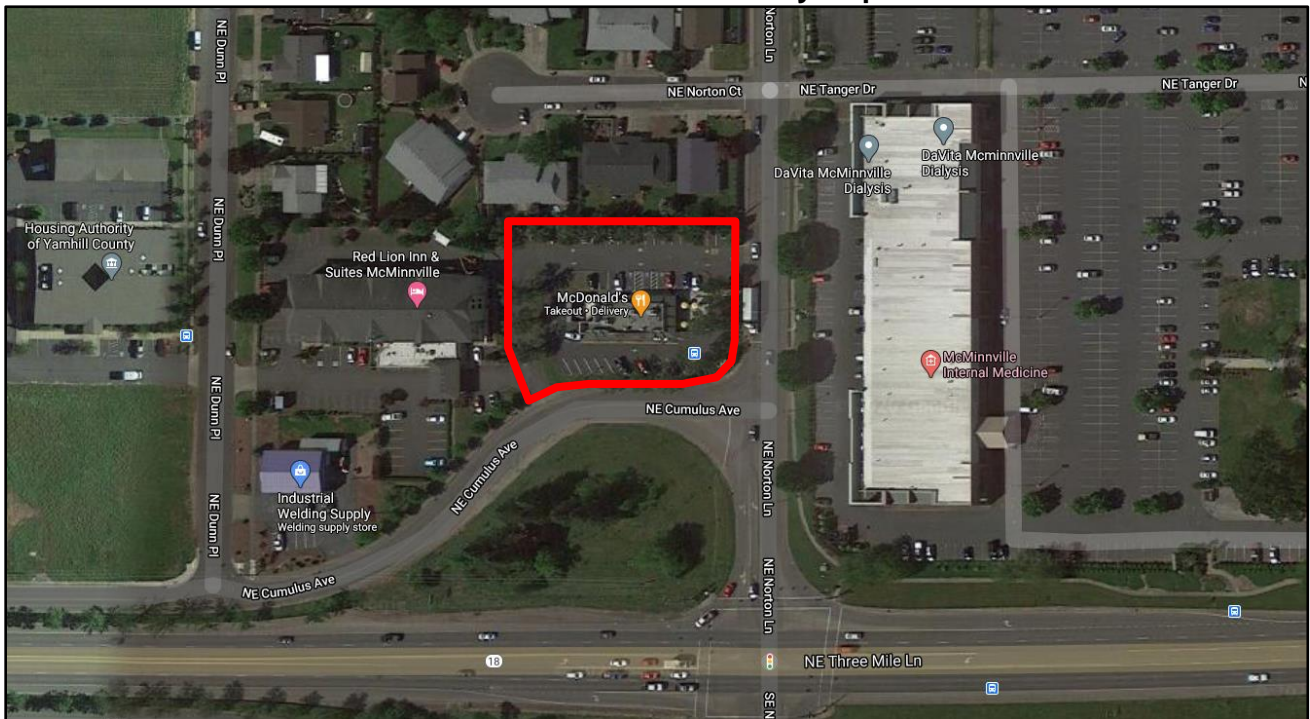


Exhibit 3: TML 2-20 Vicinity Map



Attachments:

Attachment A: ZC 2-20 Decision Document

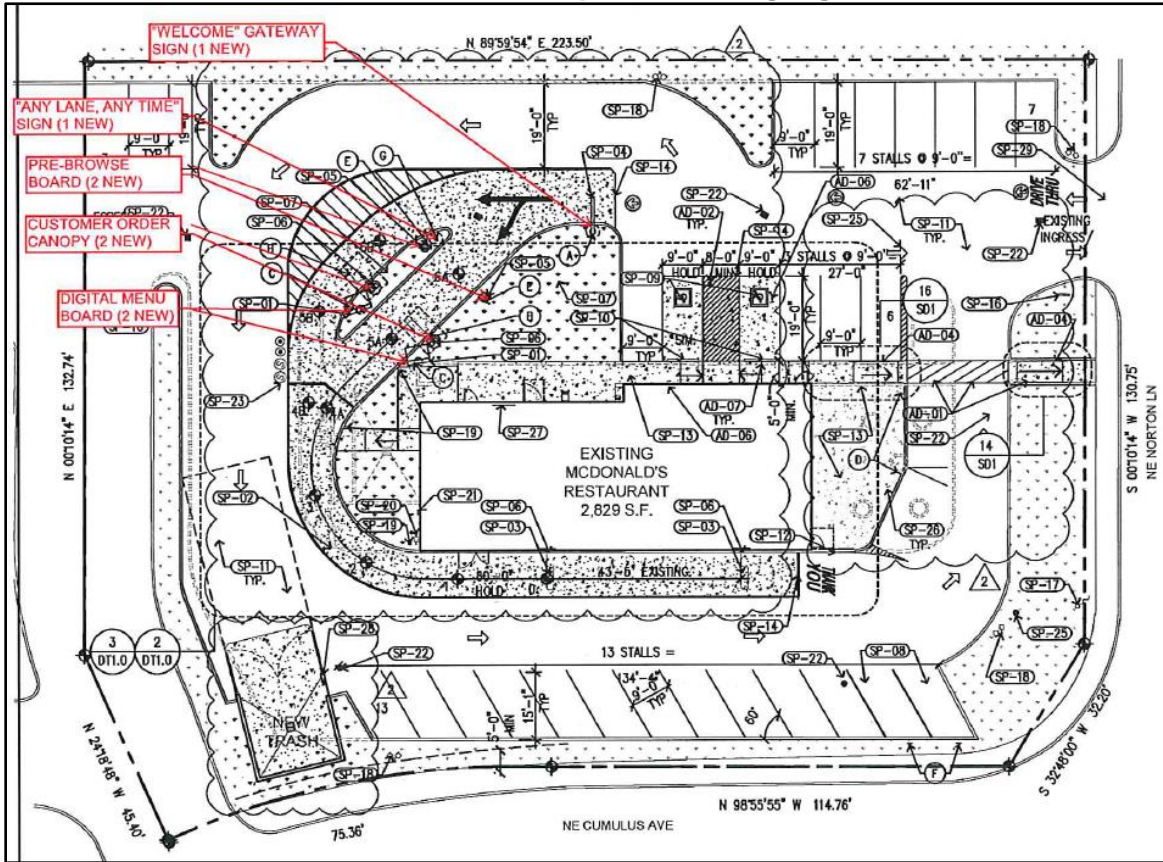
Attachment B: ZC 2-20 Application

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Exhibit 4: TML 2-20 Proposed Site/Signage Plan



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Attachment C: TML 2-20 Decision Document

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PLANNING DEPARTMENT**
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DECISION, CONDITIONS, FINDINGS OF FACT AND CONCLUSIONARY FINDINGS FOR THE APPROVAL OF A ZONING TEXT AMENDMENT TO THE THREE MILE LANE PLANNED DEVELOPMENT OVERLAY ORDINANCE (ORDINANCE NO. 4131 AS AMENDED BY ORDINANCE NO. 4572) TO ALLOW ADDITIONAL FREESTANDING SIGNS FOR BUSINESSES THAT EMPLOY DRIVE-UP SERVICE WITHIN ZONE 2 OF THE OVERLAY DISTRICT

- DOCKET:** ZC 2-20 (Zoning Text Amendment)
- REQUEST:** Application to amend the Three Mile Lane Planned Development Overlay Ordinance (Ordinance No. 4131 as amended by Ordinance No. 4572) to allow additional freestanding signs for businesses that employ drive-up service within Zone 2 of the overlay district.
- LOCATION:** Not applicable
- ZONING:** Not applicable
- APPLICANT:** Alexander Taam, Freiheit Architecture, on behalf of McDonald's Corporation
- STAFF:** Jamie Fleckenstein, Associate Planner
- DATE DEEMED COMPLETE:** January 22, 2021
- HEARINGS BODY & ACTION:** The McMinnville Planning Commission makes a recommendation for approval or denial to the City Council.
- HEARING DATE & LOCATION:** February 18, 2021, Civic Hall, 200 NE 2nd Street, McMinnville, Oregon, and Zoom Online Meeting ID 918 8312 0529.
- PROCEDURE:** An application for a Zoning Text Amendment is processed in accordance with the procedures in Section 17.72.120 of the McMinnville Municipal Code. The application is reviewed by the Planning Commission in accordance with the quasi-judicial public hearing procedures specified in Section 17.72.130 of the McMinnville Municipal Code.
- CRITERIA:** The specific criteria for Zoning Text Amendment requests on the Zoning Text Amendment application have been deemed necessary by the Planning Director in accordance with MMC Section 17.72.020(G), and require the applicant to demonstrate that:
1. The proposed change is consistent with the relevant goals and policies of the McMinnville Comprehensive Plan and this code.
 2. There is a public need for a change of the kind in question.

Attachments:
Attachment 1 – Application and Attachments

I. APPLICATION SUMMARY:

Request

The proposal is for an Zoning Text Amendment to amend the Three Mile Lane Planned Development Overlay Ordinance (Ordinance No. 4131 as amended by Ordinance No. 4572) to allow additional freestanding signs for businesses that employ drive-up service within Zone 2 of the overlay district.

The applicant is proposing to add the following language to Section 5B of the Three Mile Lane Planned Development Overlay which regulates signage on commercially and industrially designated properties within Zone 2:

“Additional freestanding signs are permitted with businesses that employ drive-up service. One such sign, not to exceed 36 square feet in area or six feet in height, is allowed per order station. In addition, one secondary sign, a maximum of 15 square feet in area and five feet in height, is allowed per order station. Any freestanding sign that has copy facing toward a public street shall be located a minimum of thirty (30) feet from that street’s property line. Wall mount signs shall be exempt from this.”

The proposed language is found in Section 17.62.070(F) – Permanent Sign Regulations of the McMinnville Zoning Ordinance. Section 17.62.070(F) was added to Section 17.62.070 in 2011 (Ordinance No. 4935) and amended to the current language seen above in 2016 (Ordinance No. 5013). However, Section 17.62.020-Scope states that “...this chapter does not apply to lands within the areas described in [...] the Three Mile Lane Ordinances 4572 and 4131.” Therefore, the applicant is requesting the zoning text amendment to the Three Mile Lane Planned Development Overlay Ordinance to include language allowing Drive-Up Service Signs as permitted in the City of McMinnville outside of the Three Mile Lane Overlay District.

An application for a Three Mile Lane Development Review has been submitted concurrently with the Zoning Text Amendment application. The Three Mile Lane development proposed by the applicant is the addition of a second drive-thru service lane with associated signage at an existing McDonald’s restaurant located in Zone 2 of the Three Mile Lane Planned Development Overlay District.

Summary of Criteria & Issues

The application (ZC 2-20) is subject to Zoning Text Amendment review criteria provided in the Zoning Text Amendment application and consistent with Section 17.74.020 of the McMinnville Municipal Code (MMC). Zoning Text Amendment requests are processed in accordance with Section 17.72.120. The goals and policies in Volume II of the Comprehensive Plan are also independent approval criteria for all land use decisions.

The specific criteria for Zoning Text Amendment requests on the Zoning Text Amendment application have been deemed necessary by the Planning Director in accordance with MMC Section 17.72.020(G), and require the applicant to demonstrate that:

4. The proposed change is consistent with the relevant goals and policies of the McMinnville Comprehensive Plan and this code.
5. There is a public need for a change of the kind in question.
6. The need will be best served by changing the classification of the particular piece of property in question as compared with other available property.

The applicant has provided findings to support the request for a Zoning Text Amendment. These will be discussed in detail in Section VII (Conclusionary Findings) below.

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II. CONDITIONS:

Not applicable.

III. ATTACHMENTS:

1. ZC 2-20 Application and Attachments (on file with the Planning Department)

IV. COMMENTS:**Agency Comments**

This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Engineering Department, Public Works Department, Building Department, Parks Department, Wastewater Services; and City Manager; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Oregon Department of State Lands; Oregon Department of Transportation; Frontier Communications; Comcast; Recology; and Northwest Natural Gas. In addition, notice was provided to the Oregon Department of Land Conservation and Development. The following comments were received:

- McMinnville Building Department
No building code issues.
- McMinnville Engineering Department
No comments.
- McMinnville Water & Light
MW&L has no comments on either of these submittals.
- Comcast
We have no conflicts with this project.

Public Comments

Notice of this request was mailed to property owners located within Zone 2 of the Three Mile Lane Planned Development Overlay District. Notice of the public hearing was also provided in the News Register on Tuesday, February 9, 2021. As of the date of the Planning Commission public hearing on February 18, 2021, no public testimony had been received by the Planning Department.

V. FINDINGS OF FACT - PROCEDURAL FINDINGS

1. The applicant, Alexander Taam, Freiheit Architecture, on behalf of McDonald's Corporation, submitted the Zoning Text Amendment application (ZC 2-20) on November 24, 2020. Application fees were paid on December 9, 2020.
2. The applicant was not required to hold a neighborhood meeting, per MMC 17.72.095(A)(1).
3. The application was deemed incomplete on January 5, 2021. The applicant submitted revised application materials on January 7, 2021.
4. Based on the revised application materials, the application was deemed complete on January 22, 2021. Based on that date, the 120 day land use decision time limit expires on May 22, 2021.

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5. Notice of the application was referred to the following public agencies for comment in accordance with Section 17.72.120 of the Zoning Ordinance: McMinnville Fire Department, Police Department, Engineering Department, Public Works Department, Building Department, Parks Department, Wastewater Services; and City Manager; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Oregon Department of State Lands; Oregon Department of Transportation; Frontier Communications; Comcast; Recology; and Northwest Natural Gas. In addition, notice was provided to the Oregon Department of Land Conservation and Development. In addition, notice was provided to the Oregon Department of Land Conservation and Development. Comments received from agencies are addressed in the Decision Document.
6. Notice of the application and the February 18, 2021 Planning Commission public hearing was mailed to property owners within Zone 2 of the Three Mile Lane Planned Development Overlay District in accordance with Section 17.72.120 of the Zoning Ordinance on January 29, 2021.
7. Notice of the application was provided to the Department of Land Conservation and Development on February 2, 2021.
8. Notice of the application and the February 18, 2021 Planning Commission public hearing was published in the News Register on Tuesday, February 9, 2021, in accordance with Section 17.72.120 of the Zoning Ordinance.
9. No public testimony was submitted to the Planning Department prior to the Planning Commission public hearing.
10. On February 18, 2021, the Planning Commission held a duly noticed public hearing to consider the request.

VI. FINDINGS OF FACT - GENERAL FINDINGS

Not applicable.

VII. CONCLUSIONARY FINDINGS:

The Conclusionary Findings are the findings regarding consistency with the applicable criteria for the application. The applicable criteria for a Zone Change are specified in 17.74.020 of the Zoning Ordinance, which include consistency with the Comprehensive Plan.

Comprehensive Plan Volume II:

The following Goals, Policies, and Proposals from Volume II of the Comprehensive Plan provide criteria applicable to this request:

The implementation of most goals, policies, and proposals as they apply to this application are accomplished through the provisions, procedures, and standards in the city codes and master plans, which are sufficient to adequately address applicable goals, policies, and proposals as they apply to this application.

The following additional findings are made relating to specific Goals and Policies:

CHAPTER IV: ECONOMY OF McMINNVILLE

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GOAL IV 1: TO ENCOURAGE THE CONTINUED GROWTH AND DIVERSIFICATION OF McMINNVILLE’S ECONOMY IN ORDER TO ENHANCE THE GENERAL WELL-BEING OF THE COMMUNITY AND PROVIDE EMPLOYMENT OPPORTUNITIES FOR ITS CITIZENS.

APPLICANT’S RESPONSE: The proposal is consistent with this goal because the proposed language would allow the growth of this restaurant through additional signage supporting a more efficient drive-thru configuration. With the addition of the updated drive-thru more customers will be able to access this restaurant which encourages the continued growth and diversification of McMinnville’s economy.

FINDING: SATISFIED. Staff concurs with the applicant’s finding.

GOAL IV 2: TO ENCOURAGE THE CONTINUED GROWTH OF McMINNVILLE AS THE COMMERCIAL CENTER OF YAMHILL COUNTY IN ORDER TO PROVIDE EMPLOYMENT OPPORTUNITIES, GOODS, AND SERVICES FOR THE CITY AND COUNTY RESIDENTS.

Policy 21.03 The City shall support existing businesses and industries and the establishment of locally owned, managed, or controlled small businesses. (Ord.4796, October 14, 2003)

APPLICANT’S RESPONSE: The proposed language we seek to adopt would allow continued growth of McMinnville as the commercial center for Yamhill County because it would encourage other businesses to utilize the allowable signage and increase public relations in the area. Other restaurants of the same nature would also have the opportunity to upgrade their drive-up services resulting in providing more services to the city and county residents.

FINDING: SATISFIED. Adoption of the proposed text amendment would support existing business within Zone 2 of the Three Mile Lane Planned Development Overlay District. The additional signage allowed would allow expansion of commercial and industrial uses that employ drive-thru services to respond to changing public demand.

GOAL IV 3: TO ENSURE COMMERCIAL DEVELOPMENT THAT MAXIMIZES EFFICIENCY OF LAND USE THROUGH UTILIZATION OF EXISTING COMMERCIALY DESIGNATED LANDS, THROUGH APPRIATELY LOCATING FUTURE COMMERCIAL LANDS, AND DISCOURAGING STRIP DEVELOPMENT.

Policy 34.00 The City of McMinnville shall develop and maintain guidelines concerning the size, placement, and type of signs in commercial areas.

APPLICANT’S RESPONSE: The proposal ensures the maximum efficiency of land use through the utilization of the existing McDonald’s and the existing commercially designated land it is on. Our proposal is based from using the existing restaurant by enhancing the current service with more instances of signage; maintaining size, placement and type of signage in commercial areas. Our proposal is simply to increase the instance quantity of signage drive-thru establishments.

FINDING: SATISFIED. Staff concurs with the applicant’s finding. Staff adds that the proposed text amendment would apply guidelines concerning the size, placement, and type of additional signs associated with drive-thru services on commercially designated lands within Zone 2 of the Three Mile Lane Planned Development Overlay District. Additionally, the proposed text amendment would not change the requirement for all signage in the Three Mile Lane area to undergo review and approval by the Three Mile Lane Design Committee.

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GOAL X 1: TO PROVIDE OPPORTUNITIES FOR CITIZEN INVOLVEMENT IN THE LAND USE DECISION MAKING PROCESS ESTABLISHED BY THE CITY OF McMinnville.

Policy 188.00 The City of McMinnville shall continue to provide opportunities for citizen involvement in all phases of the planning process. The opportunities will allow for review and comment by community residents and will be supplemented by the availability of information on planning requests and the provision of feedback mechanisms to evaluate decisions and keep citizens informed.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. McMinnville continues to provide opportunities for the public to review and obtain copies of the application materials and completed staff report prior to the holding of advertised public hearing(s). All members of the public have access to provide testimony and ask questions during the public review and hearing process.

Three Mile Lane Planned Development Overlay

The Three Mile Lane Planned Development Overlay was adopted by Ordinance No. 4131 and amended by Ordinance No. 4572. The following Sections of the Three Mile Lane Planned Development Overlay (Ord. No. 4131 as amended by Ord. No. 4572) are applicable to the request:

Section 1. Statement of Purpose. The City of McMinnville finds that the development of lands on the north and south sides of Three Mile Lane (Oregon Highway 18) is acceptable if provisions are made to integrate the dual functions of the highway as a bypass around the City and a land service arterial for adjacent properties. The City also finds that development along Three Mile Lane is desirable if it is of good quality and design, if the living and working environments developed are compatible with each other, and if the living environment includes open spaces, parks, and features buffering of residential uses from the highway. The City recognized, during the comprehensive planning process, that to insure that these concerns are met, proper designations on the McMinnville Comprehensive Plan and Zoning Maps must be made, and, further, that a planned development overlay must be placed over the area establishing specific conditions for development.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. Staff finds that the proposed text amendment to the Three Mile Lane Planned Development Overlay is consistent with the Statement of Purpose. The proposed language supports desirable development, encourages good quality and design of commercial lands, and further helps to ensure the compatibility of living and working environments.

Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit “A”:

- A. The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- B. A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- C. Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:
 - 1. The minimization of entrances onto Three Mile Lane;
 - 2. The development of on-site circulation systems, connecting to adjoining properties, including public frontage roads;
 - 3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
 - 4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or

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on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.

- D. Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- E. Mixed housing-type residential developments shall be allowed and encouraged in those areas designated as residential.
- F. Temporary signage shall be allowed as per Section 17.62.060(B)(3) of the McMinnville Zoning Ordinance (No. 3380). (Ord. 4988 §1, 2015)

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. As described in specific findings above, the proposed text amendment to the Three Mile Lane Planned Development Overlay is consistent with the applicable goals and policies of the McMinnville Comprehensive Plan, Volume II.

Section 5. Signs. The following regulations apply to those properties designated as either industrial or commercial on the McMinnville Comprehensive Plan Map and which are located within the area described in Exhibit “A.” All signage must be approved by the Three Mile Lane Design Review Committee and may not exceed the maximums established in this section. The Three Mile Lane Design Review Committee may take into account color selection; sign material; relationship to site and building design; size, form, mass, height, and scale of sign; the sign’s location and proximity to existing signs on adjacent parcels; and other sign design elements appropriate to ensuring that the proposed sign is compatible with the objectives of this ordinance. The Three Mile Lane Design Review Committee may require a proposed sign to be redesigned as necessary to make it consistent with the objectives of this ordinance including, but not limited to, restricting the size and height of the sign, its location, form, color, and material.

The Three Mile Lane Planned Development Overlay area is hereby divided into three “sign zones” which are depicted on Exhibit “B” which is attached hereto and is by this reference herein incorporated. Signage shall be regulated within the various sign zones as follows:

- B. Zone 2. The following regulations apply to commercially and industrially designated properties which fall within zone 2:
 - 1. That only one free-standing sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit “B”, subject to the following:
 - a. The maximum height of any free-standing sign shall be 30 feet, except as provided in subsection (B) (1) (c) below.
 - b. The maximum square footage per sign face is 64 square feet, and there shall be a maximum of two sign faces per free-standing sign, except as provided in subsection (B) (1) (c) below.
 - c. If two or more properties share a single free-standing sign, said sign shall be a maximum of 35 feet in height, and each property sharing the sign shall be entitled to up to 64 square feet of sign face per side on a maximum of two sides. The shared free-standing sign would exhaust the allowance for free-standing signs listed in subsection (B) (1) (a) above for all parcels represented.
 - d. A free-standing sign is allowed only on properties with frontage on the Highway 18 frontage road.
 - 2. In lieu of a free-standing sign as listed in subsection 1 above, one monument-type sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit “B”, subject to the following:
 - a. Maximum square footage per sign face shall be 50 square feet, and there shall be a maximum of two sign faces per monument sign.

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- b. Maximum sign height shall be six feet, and the top of the sign shall not be more than eight feet above the center line grade of the Three Mile Lane frontage road.
- c. Monument signs shall be non-illuminated, indirectly illuminated, neon, or may have individually back-lit letters, providing that no plastic sign cabinets are allowed.
- 3. All other signs, with the exception of incidental directional signage, must be flush against the buildings and not protrude more than 12 inches from the building face, subject to the following:
 - a. Total area of building signage may not exceed eight percent of the total area of the face of the building which is most closely parallel to Highway 18 or the frontage road and may not exceed four percent on the other sides.
 - b. The area of building signs shall be measured as the area around or enclosing each sign cabinet, or where sign cabinets are not used, the area shall be that within a single continuous perimeter composed of any straight line geometric figure which encloses the extreme limits of the advertising message.
- 4. If a parcel is divided after the date of the adoption of this ordinance, the sign allowances established in subsections 1 and 2 above will apply only to the original parcel or what remains of it and newly created parcels are limited to the sign allowances established in subsection 3 above except as provided in subsection (B) (5) below.
- 5. The Three Mile Lane Design Review Committee may allow the sharing of a single free-standing sign by an original parcel or what remains of it and parcels divided after the date of the adoption of this ordinance. The total sign size shall not exceed that allowed for the original parcel.
- 6. Portable signs and signs with flashing or moving parts are prohibited.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. The proposed amendment to this section is not inconsistent with existing sign regulations for Zone 2 of the Three Mile Lane Planned Development Overlay District. The amendment would allow and regulate additional signs only for uses that employ drive-thru services, and does not revise other sign standards as adopted by Ordinance No. 4572.

Section 6. Procedures for Review:

A. No development or signage may take place within the Three Mile Lane Planned Development Overlay without such development or signage gaining the approval of the Three Mile Lane Design Review Committee.

B. Zone changes submitted for approval shall be processed under the requirements and standards of Chapter 17.51 of McMinnville Ordinance 3380 (Zoning Ordinance)

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. The proposed amendment is not inconsistent with existing procedures for review of the Three Mile Lane Planned Development Overlay District. Additional signage for uses employing drive-thru services would be subject to review and approval by the Three Mile Lane Design Review Committee.

McMinnville Zoning Ordinance

The McMinnville Zoning Ordinance is Title 17 of the McMinnville Municipal Code. The following Sections of the McMinnville Zoning Ordinance (Ord. No. 3380) are applicable to the request:

Chapter 17.72. Applications and Review Process

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17.72.020 Application Submittal Requirements. Applications shall be filed on forms provided by the Planning Department and shall be accompanied by the following;

- A. A scalable site plan of the property for which action is requested. The site plan shall show existing and proposed features, such as access, lot and street lines with dimensions in feet, distances from property lines, existing and proposed buildings and significant features (slope, vegetation, adjacent development, drainage etc.)
- B. An explanation of intent, nature and proposed use of the development, and any pertinent background information.
- C. Property description and assessor map parcel numbers(s).
- D. A legal description of the property when necessary.
- E. Signed statement indicating that the property affected by the application is in the exclusive ownership or control of the applicant, or that the applicant has the consent of all partners in ownership of the affected property.
- F. Materials required by other sections of the McMinnville Zoning Ordinance specific to the land use application.
- G. Other materials deemed necessary by the Planning Director to illustrate compliance with applicable review criteria, or to explain the details of the requested land use action.

APPLICANT’S RESPONSE: [See responses below].

FINDING: SATISFIED. In response to the applicant’s request for a Zoning Text Amendment, the Planning Director required demonstration of compliance with the following three (3) criteria, as provided on the Zoning Text Amendment application form. See findings for individual criteria below:

1. The proposed change is consistent with the relevant goals and policies of the McMinnville Comprehensive Plan and this code.

APPLICANT’S RESPONSE: [See responses provided for specific goals, policies, and code].

FINDING: SATISFIED. As demonstrated in findings in this document, the proposed text amendment is consistent with Comprehensive Plan goals and policies, and this code.

2. There is a public need for a change of the kind in question.

APPLICANT’S RESPONSE: Given the current status of the nation more people are turning toward services that minimize human contact. Allowing additional signage to this drive-thru would fulfill the public’s need for a safe delivery method of food. Ultimately the allowance for more signage at business with drive-up services would allow for a more positive customer experience.

FINDING: SATISFIED. Staff concurs with the applicant’s finding. Since the start of the COVID-19 pandemic, consumer research has shown that the demand for drive-thru service and accommodation of digital ordering and pickup has increased as consumers seek a contactless experience. The text amendment request is in response to changing demographics in the restaurant industry stemming most recently from public health and safety concerns, and the need to have the flexibility to adjust business practices for those eligible uses to reflect the changing public need.

3. The need will be best served by changing the classification of the particular piece of property in question as compared to other available property.

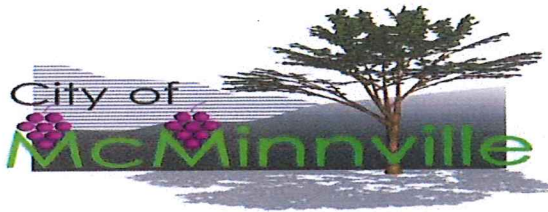
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APPLICANT’S RESPONSE: Comparing to other available properties is not applicable as this project is not evaluating alternative properties; our goal is to improve the existing drive-thru restaurant by increasing ease of the customer experience by providing a more efficient method of ordering and receiving food, the proposed additional signage will support that effort.

FINDING: SATISFIED. The proposed text amendment allowing additional signage for uses employing drive-thru services would only impact those commercial and industrial properties within Zone 2 of the Three Mile Lane Planned Development Overlay District. The same allowances are provided for other commercially designated properties throughout the City by MMC Section 17.62.070(F). Relocating existing commercial or industrial uses that employ drive-thru services within Zone 2 of the Three Mile Lane Planned Development Overlay District to an area outside of the overlay district so that the demand for additional drive-thru services can be met is not practical.

JF



Planning Department

Comprehensive Plan/Zoning Text Amendment

231 NE Fifth Street, McMinnville, Oregon 97128
Phone: 503.434.7311 Fax: 503.474.4955
www.mcminnvilleoregon.gov

Applicant:

Name: ALEXANDER TAAM
Company: FREIHEIT ARCHITECTURE
Mailing Address: 929 108TH AVE NE
City, State, Zip: BELLEVUE, WA 98004
Phone: 425-559-2773 Fax: _____
E-mail: ATAAM@FREIHEITARCH.COM

Authorized Representative:

Name: _____
Company: _____
Mailing Address: _____
City, State, Zip: _____
Phone: _____ Fax: _____
E-mail: _____

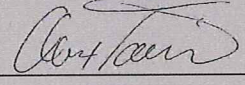
Property Owner:

Name: ALBERT PALACIOS
Company: MCDONALDS COPORATION
Mailing Address: 2999 OAK ROAD STE 900
City, State, Zip: WALNUT CREEK, CA 98004
Phone: 312-485-7551 Fax: _____
E-mail: albert.palacios@us.mcd.com

Property Owner's Signature:

Printed Name: Albert Palacios Date: 11/24/2020

Applicant's Signature: (if different from Property Owner)


Printed Name: ALEXANDER TAAM Date: 11/24/2020

Site Location and Description:

Project Address if Available: 225 NE NORTON LN
Project Location: MCMINNVILLE, OR Total Site Area: _____
Tax Map Number: 422CD01602 Comprehensive Plan Designation: _____ Zoning: C-3

Request:

The applicant seeks to amend text on Ordinance 4572 that was adopted for the Overlay District to allow additional free-standing signage for business that employ drive-up services

Project Type:

Residential Commercial Industrial Other: _____

Incomplete applications will not be processed until all of the required materials are submitted.

Final action on development application or zone change is required within 120 days in accordance with provisions of ORS 227.175

Office Use Only:

File No. 2020-20
Date Received _____
Fee 10823.00
Receipt No. _____
Received by SP

01.07.2021

McDonald's Remodel
225 NE Norton Lane
McMinnville, OR 97128

Planning Department

McDonald's Zoning Text Amendment:

The applicant seeks to amend Ordinance 4572 adopted by the Overlay District for the McDonalds at 225 NE Norton Lane, McMinnville, OR 97128. Under section 5B of ordinance 4572 the allowable signage is limited to one free-standing sign per parcel. The applicant is proposing an upgrade to the existing McDonald's drive-thru configuration which would add a second drive-thru lane. With the second drive-thru lane we are proposing the addition of 4 freestanding signs (1 pre-browse board, and 1 menu board per drive-thru service lane) and 2 directional signs. The property is within the 3 Mile Lane Planned Development Overlay District (Ordinance 4131 as amended by Ordinance 4572). Ordinances 4572/4131 contain provisions and regulations for signage for commercial properties within Zone 2 of the Overlay District, which the existing McDonald's property is located in. The applicant is proposing the city to add the following language to ordinance 4572, "Additional freestanding signs are permitted with businesses that employ drive-up service. One such sign, not to exceed 36 square feet in area or six feet in height, is allowed per order station. In addition, one secondary sign, a maximum of 15 square feet in area and five feet in height, is allowed per order station. Any freestanding sign that has copy facing toward a public street shall be located a minimum of 30 feet from that street's property line. Wall mount signs shall be exempt from this." This wording is based off of section 17.62.070(F) of the MMC. Please see our responses below to the amendment criteria listed on the application.

1. The applicant's proposal for additional language allowing more free-standing signs is consistent with McMinnville's Comprehensive Plan Goals as follows:

- **Goal IV #1: To encourage the continued growth and diversification of McMinnville's economy in order to enhance the general well-being of the community and provide employment opportunities for its citizens.**

The proposal is consistent with this goal because the proposed language would allow growth of this restaurant through additional signage supporting a more efficient drive-thru configuration. With the addition of the updated drive-thru more customers will be able to access this restaurant which encourages the continued growth of and diversification of McMinnville's economy.

- **Goal IV #2: To encourage the continued growth of McMinnville as the commercial center of Yamhill county in order to provide employment opportunities, goods, and services, for the city and the county residents.**
 - **Policy 21.03 The city shall support existing businesses and industries and the establishment of locally owned, managed, or controlled small businesses**

The proposed language we seek to adopt would allow continued growth of McMinnville as the commercial center for Yamhill County because it would encourage other businesses to utilize the allowable signage and increase public relations in the area. Other restaurants of the same nature would also have the opportunity to upgrade their drive-up services resulting in providing more services to the city and county residents.

- **Goal IV #3: To ensure commercial development that maximizes efficiency of land use through utilization of existing commercially designated lands, through appropriately locating future commercial lands, and discouraging strip development**
 - **Policy 34.00 The City of McMinnville shall develop and maintain guidelines concerning the size, placement, and type of signs in commercial areas.**

The proposal ensures that the maximum efficiency of land use through the utilization of the existing McDonalds and the existing commercially designated land it is on. Our proposal is based from using the existing restaurant by enhancing the current service with more instances of signage; maintaining size, placement and type of signage in commercial areas. Our proposal is simply to increase the instance quantity of signage drive-thru establishments.

The applicant's proposal for additional ordinance language allowing for more instances of free-standing signs is consistent with McMinnville's Ordinance 4572/4131:

- **Ordinance 4131 Section 1. Statement of Purpose. The City of McMinnville finds that the development of lands on the North and South sides of Three Mile Lane (Oregon Highway 18) is acceptable if provisions are made to integrate the dual functions of the highway as a bypass around the city and a land service arterial for adjacent properties. The City also finds that development along Three Mile Lane is desirable if it is of good quality and design, if the living environment included open spaces, parks, and features buffering of residential uses from the highway. The City recognized, during the comprehensive planning process, that to insure that these concerns are met, proper designations on the McMinnville Comprehensive Plan and Zoning Maps must be made, and, further, that a planner development overlay must be placed over the area establishing specific conditions for development.**

The proposal will allow this McDonalds and any future commercial businesses to better integrate the highway as a bypass around the city. Since this area has a lot of traffic, allowing additional free-standing signs can benefit the surrounding drive-up businesses for a more efficient drive-thru service and reduce traffic congestion. These services will also benefit adjacent properties by preventing traffic congestion and granting better recognition to the land service arterial for all adjacent properties. For this proposal additional landscaping and good quality design have been implemented to meet the development standards for the Three Mile Lane.

- **Ordinance 4131 Section 5. Signs (as amended by Ord. 4572). The following regulations apply to those properties designated as either industrial or commercial on the McMinnville Comprehensive Plan Map and which are located within the area described in Exhibit "A". All signage must be approved by the Three Mile Lane Design Review Committee and may not exceed the maximums established by this section. The Three Mile Lane Design Review Committee may take into account color selection; sign material; relationship to the site and building design; size, form, mass, height, and scale of sign; the sign's location and proximity to existing signs on adjacent parcels; and other sign design elements appropriate to ensuring that the proposed sign is compatible with the objectives of this ordinance. The Three Mile Lane Design Review Committee may require a proposed sign to be redesigned as necessary to make it consistent with the objectives of this ordinance including, but not limited to, restricting the size and height of the sign, its location, form, color, and material.**

The proposal is asking to revise the language of ordinance 4572 for the allowance for more instances of free-standing signage in the area. The additional signage we are proposing to install on our site has been designed to match the sleek and modern design of the existing McDonalds restaurant. The signs will be seamlessly integrated onto the property with additional landscaping to meet local standards.

2. Given the current status of the nation more people are turning toward services that minimize human contact. Allowing additional signage to this drive-thru would fulfill the public's need for a safe delivery method of food. Ultimately the allowance for more signage at business with drive-up services would also allow for a more positive customer experience.
3. Comparing to other available properties is not applicable as this project is not evaluating alternative properties; our goal is to improve the existing drive-thru restaurant by increasing ease of the customer experience by providing a more efficient method of ordering and receiving food, the proposed additional signage will support that effort.



Alexander Taam

Architectural Designer

FREIHEIT Architecture | 425-827-2100



**CITY OF MCMINNVILLE
PLANNING DEPARTMENT**
231 NE FIFTH STREET
MCMINNVILLE, OR 97128

503-434-7311
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DECISION, CONDITIONS, FINDINGS OF FACT AND CONCLUSIONARY FINDINGS FOR THE APPROVAL OF THREE MILE LANE DEVELOPMENT REVIEW FOR THE ADDITION OF A SECOND DRIVE-THRU SERVICE LANE AND ASSOCIATED SIGNAGE AT 225 NE NORTON LANE.

- DOCKET:** TML 2-20 (Three Mile Lane Development Review)
- REQUEST:** Application for Three Mile Lane Development Review to allow for the construction of a second drive-through service lane and associated signage at a McDonald's restaurant within the Three Mile Lane Planned Development Overlay District.
- LOCATION:** 225 NE Norton Lane (Tax Lot 1602, Section 22CD, T.4 S., R. 4 W., W.M.)
- ZONING:** C-3 PD (General Commercial Planned Development)
- APPLICANT:** Alexander Taam, Freiheit Architecture, on behalf of McDonald's Corporation, property owner
- STAFF:** Jamie Fleckenstein, Associate Planner
- DATE DEEMED COMPLETE:** January 22, 2021
- HEARINGS BODY & ACTION:** The McMinnville Planning Commission makes a recommendation for approval or denial to the City Council.
- HEARING DATE & LOCATION:** February 18, 2021, Civic Hall, 200 NE 2nd Street, McMinnville, Oregon, and Zoom Online Meeting ID 918 8312 0529.
- PROCEDURE:** The application for Three Mile Lane Development Review was submitted concurrently with an application for a Zoning Text Amendment. As described in Section 17.72.070 of the McMinnville Municipal Code, concurrent applications shall be processed simultaneously and subject to the hearing procedure that affords the most opportunity for public hearing and notice. Therefore, the application for Three Mile Lane Development Review shall be processed in accordance with the procedures in Section 17.72.120 of the McMinnville Municipal Code. The application is reviewed by the Planning Commission in accordance with the quasi-judicial public hearing procedures specified in Section 17.72.130 of the McMinnville Municipal Code.
- CRITERIA:** The applicable criteria are specified in Section 4 (Policies) and Section 5 (Signs) of the Three Mile Lane Planned Development Overlay (Ordinance No. 4131 as amended by Ordinance No. 4572). In addition, the goals, policies, and proposals in Volume II of the Comprehensive Plan are to be applied to all land use decisions

I. APPLICATION SUMMARY:

Subject Property & Request

The proposal is an application for Three Mile Lane Development Review to allow for the construction of a second drive-thru service lane and associated signage at a McDonald’s restaurant within the Three Mile Lane Planned Development Overlay district. The subject site is located at 225 NE Norton Lane and is more specifically described as Tax Lot 1602, Section 22CD, T.4 S., R. 4 W., W.M. The property is located within Zone 2 of the Three Mile Lane Planned Development Overlay district.

The subject property is located on the west side of NE Norton Lane, north of NE Cumulus Avenue. The subject property is zoned C-3 (General Commercial).

See Vicinity Map (Figure 1), Zoning Map (Figure 2), Applicant’s Proposed Site Plan (Figure 3), and Applicant’s Proposed Signage Plans (Figure 4) below.

Figure 1: Vicinity Map (Subject Site Approximate)

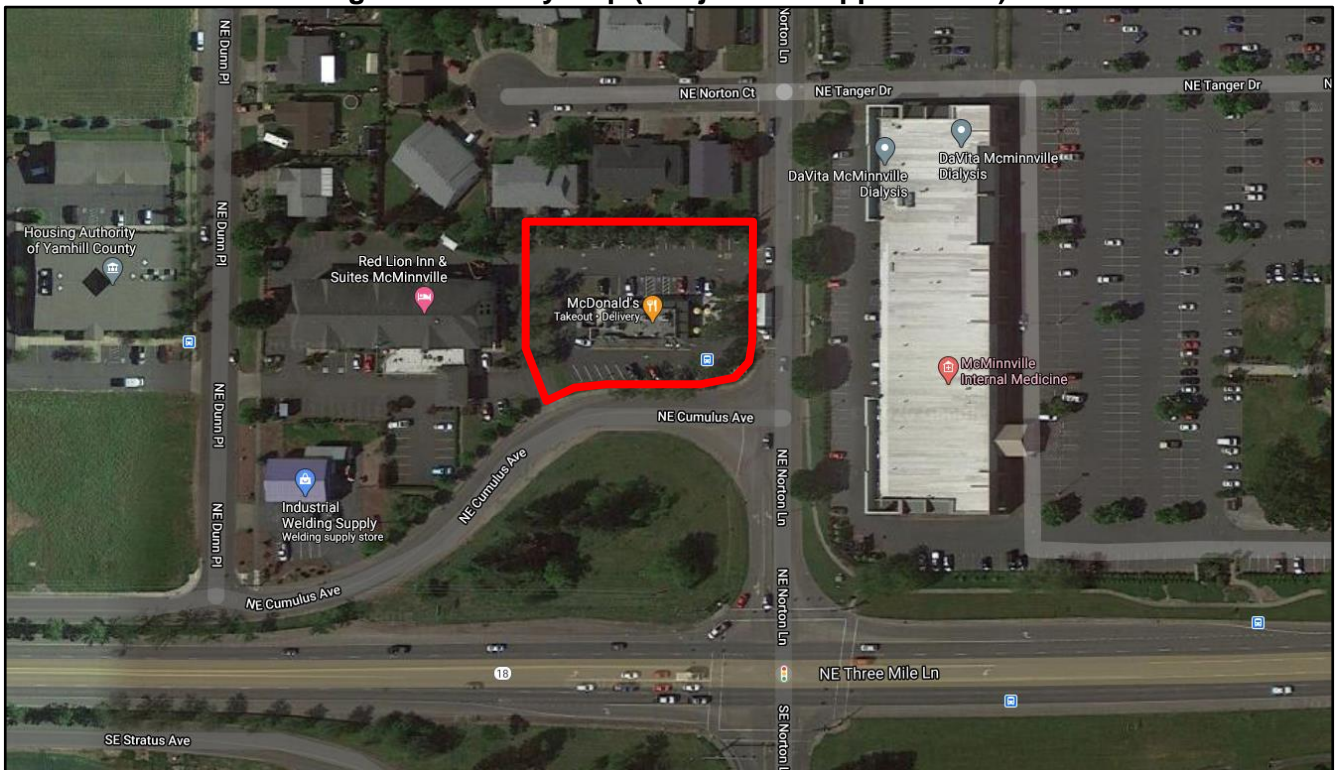


Figure 2: Zoning Map

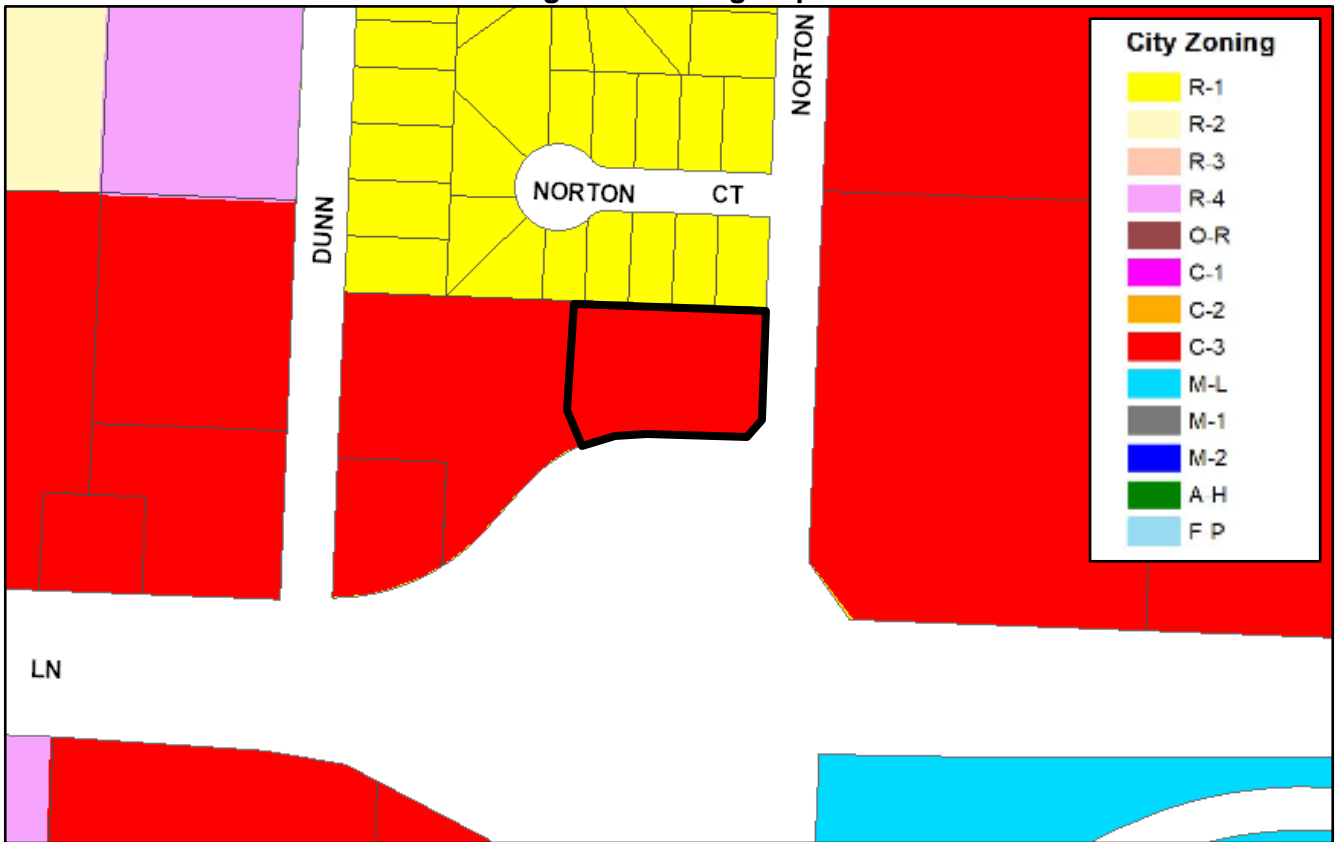


Figure 3: Applicant's Proposed Site Plan

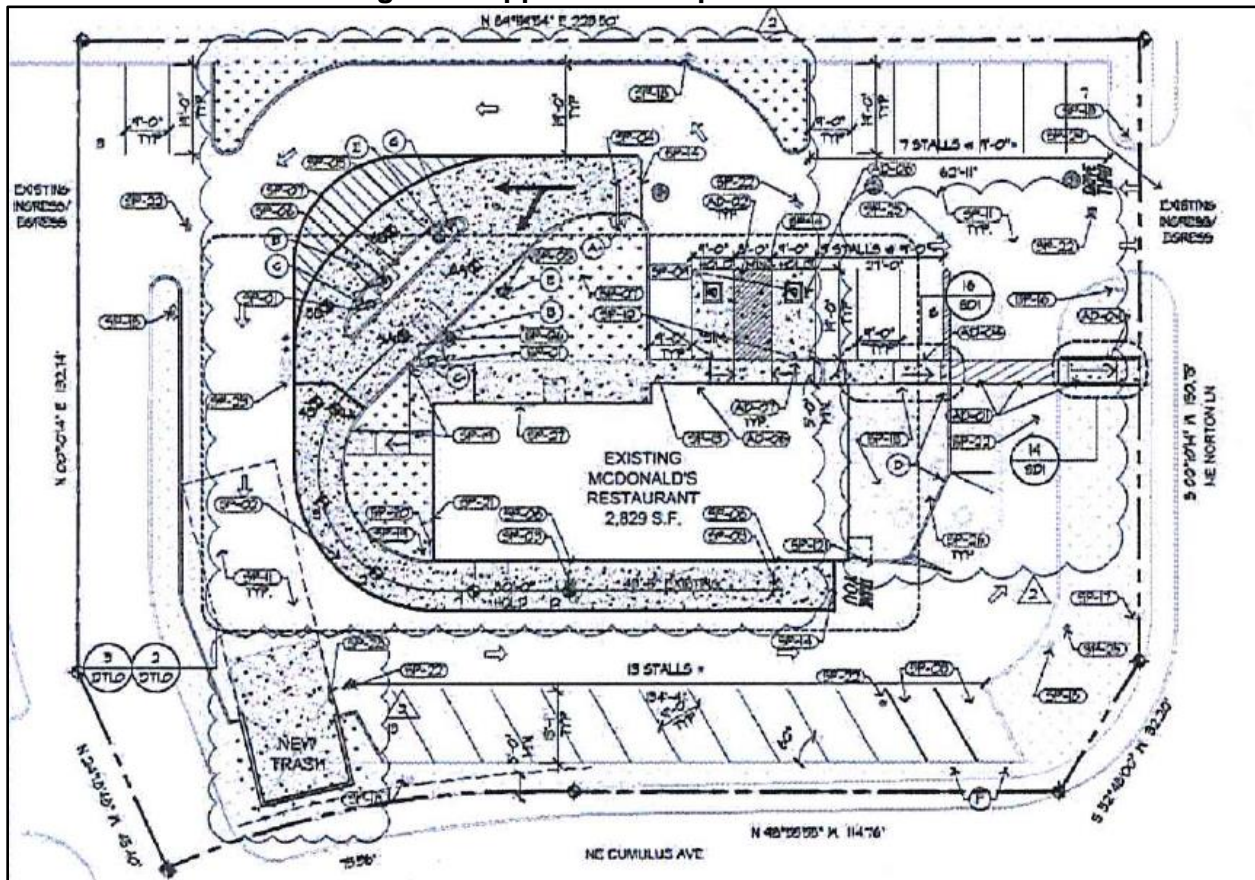
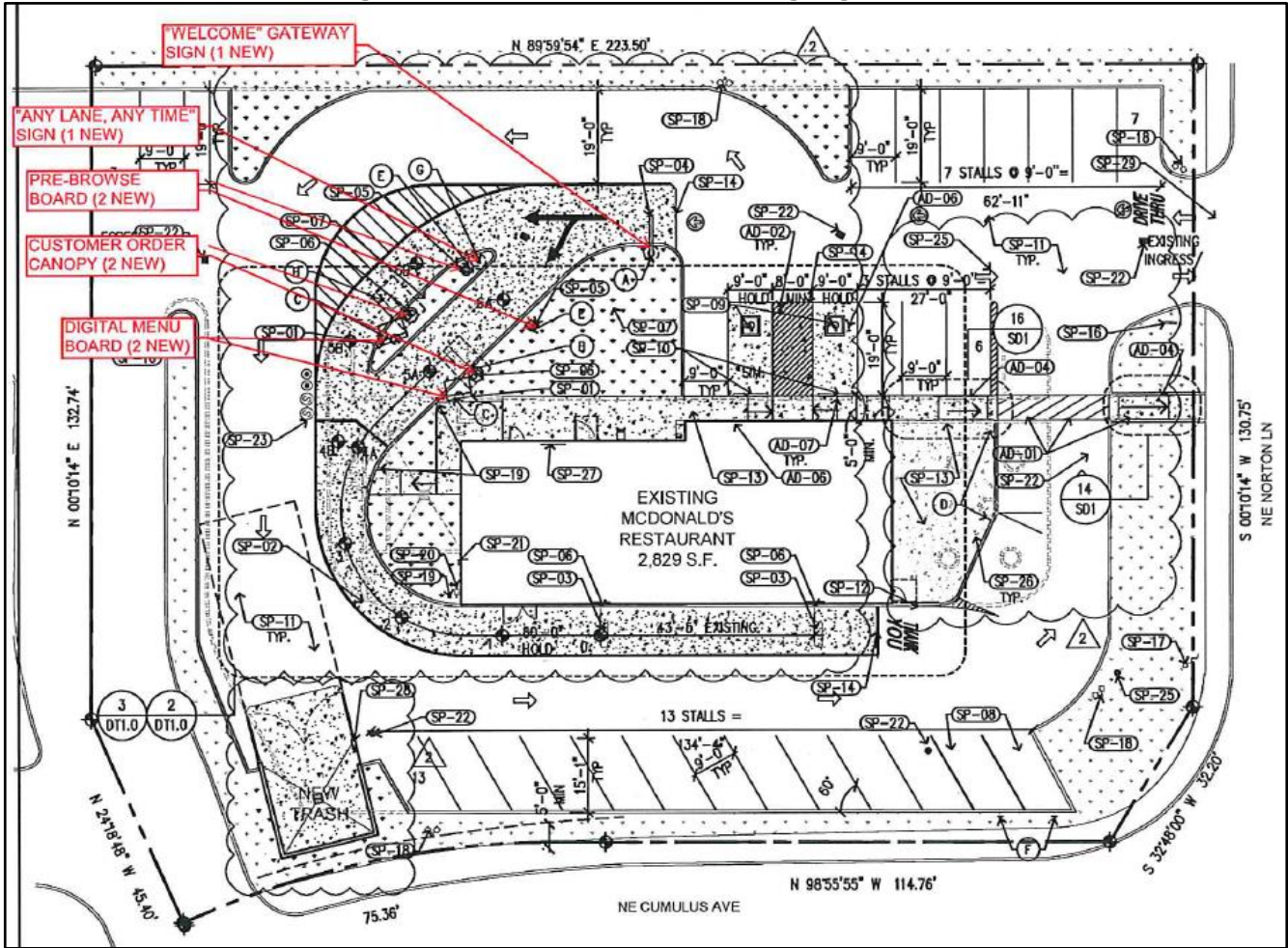
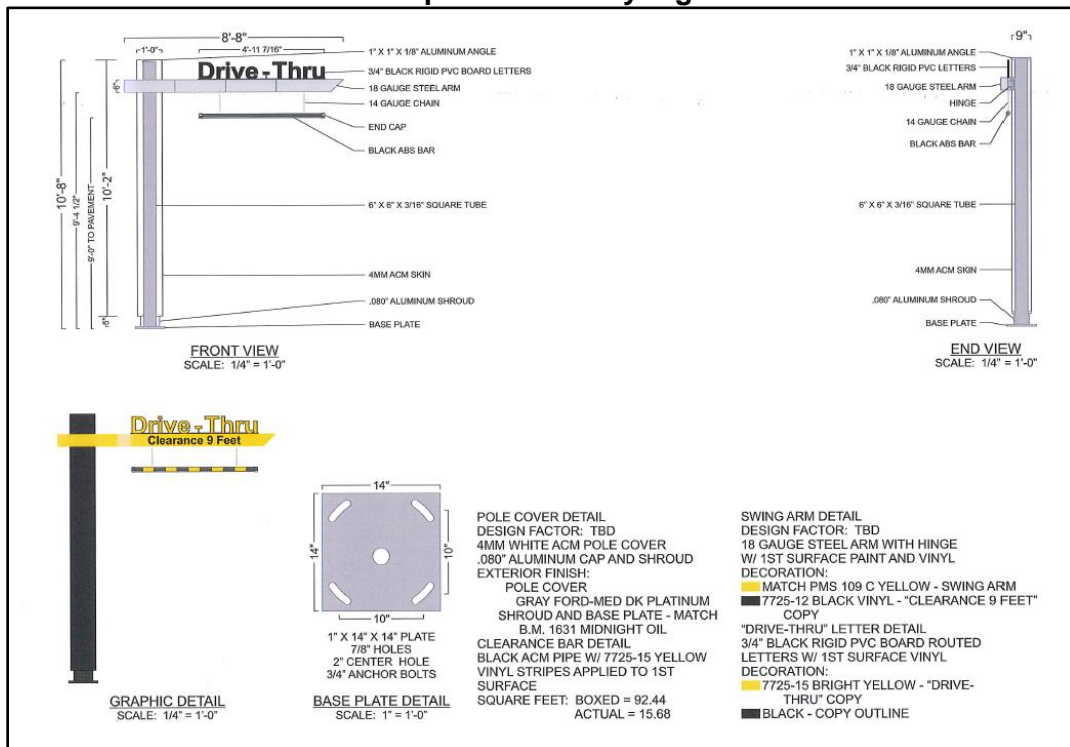


Figure 4: Applicant's Proposed Signage Plans

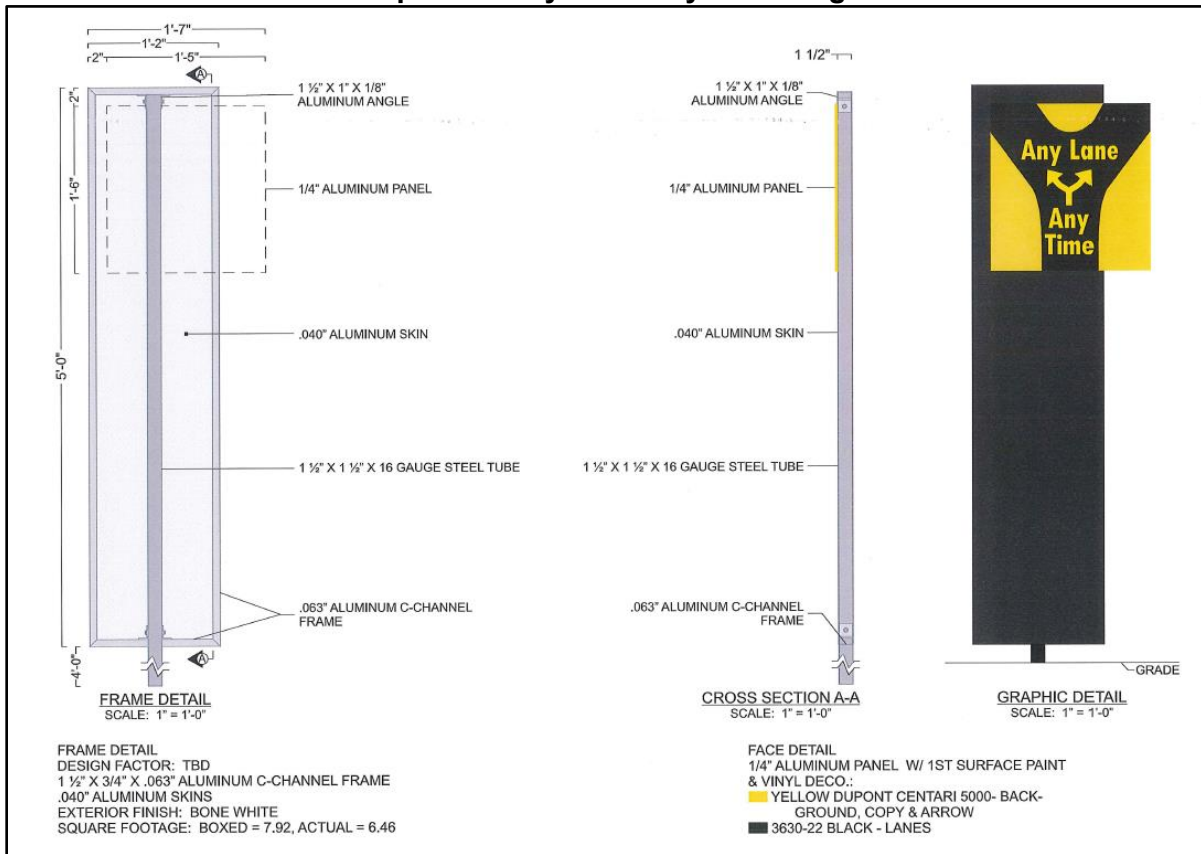


Proposed Gateway Sign

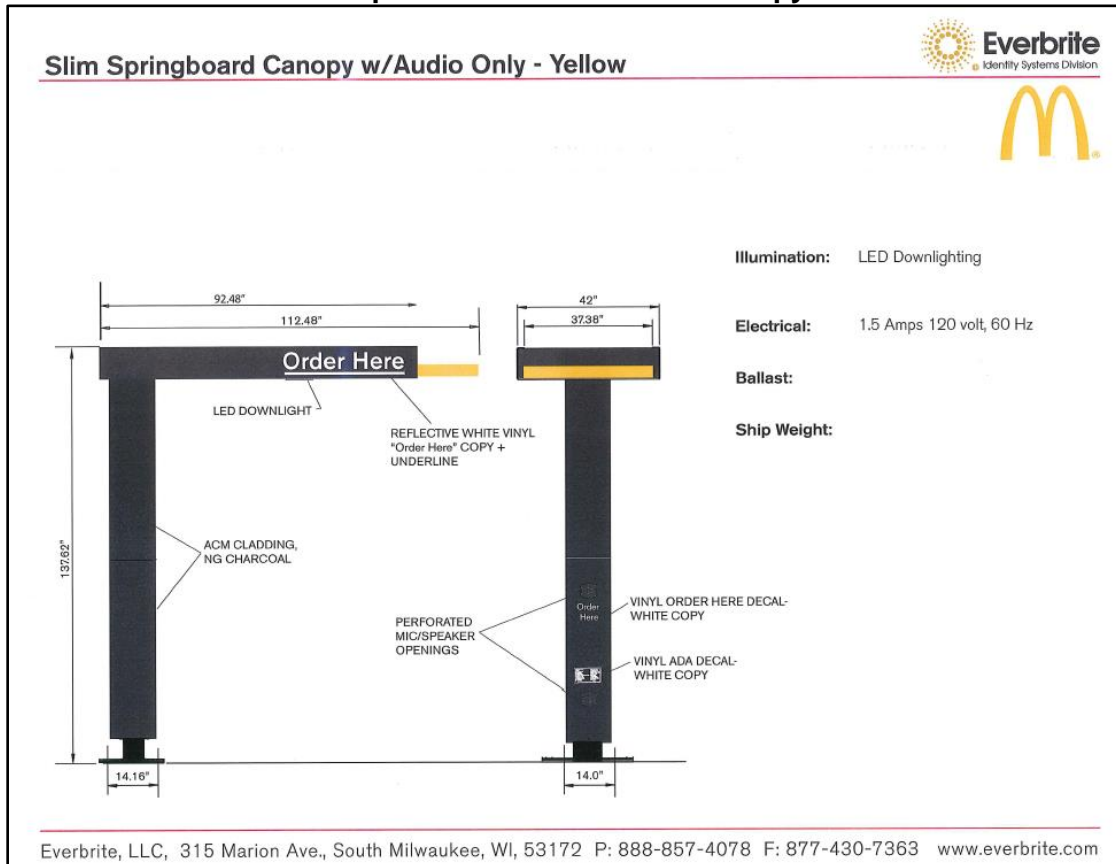


Attachments:
Attachment 1 – Application and Attachments

Proposed “Any Lane Any Time” Sign



Proposed Customer Order Canopy



Proposed Digital Pre-Browse Board

Frame	Hot dip galvanized + anti-graffiti powder coated steel	<h4 style="margin: 0;">ODMB 02 SINGLE</h4> <p>Displays Samsung OH55F</p> <p>Hardware Stratacache Spectra NG</p> <p>Heating/Cooling Watlow 100W Heater Sunon 120mm AC Fan</p> <p>Power Supply Units 60W DC Media Player Power Supply</p> <p>Power Cables 1 x IEC Power Cable</p> <p>Electrical Components Isolated Ground 2 x IG Receptacles 20A Circuit Breaker</p> <p>Communication Cables 2 x HDMI 1 x RS232</p> <p>Certification UL Certified</p>
Brackets	Hot dip galvanized	
Panels	Aluminium + anti-graffiti powdercoat	
Access fasteners	Security Torx	
Media player access	Dual camlock	
Eyebolt	Stainless crane on	
Baseplate	McDonalds spec triple mounting pattern option	

SCALE: 1/2" = 1'-0"

Proposed Digital Menu Board

MB

Frame	Hot dip galvanized + anti-graffiti powder coated steel
Brackets	Hot dip galvanized
Panels	Aluminium + anti-graffiti powdercoat
Access fasteners	Security Torx
Media player access	Dual camlock
Eyebolt	Stainless crane on
Baseplate	McDonalds spec triple mounting pattern option

ODMB 02 DOUBLE	
Displays	2 x Samsung OH55F
Hardware	2 x Stratacache Spectra NG
Heating/Cooling	Watlow 100W Heater Sunon 120mm AC Fan
Power Supply Units	2 x 60W DC Media Player Power Supply
Power Cables	2 x IEC Power Cables
Electrical Components	Isolated Ground 2 x IG Receptacles 20A Circuit Breaker
Communication Cables	4 x HDMI 2 x RS232
Certification	UL Certified

SCALE: 1/2" = 1'-0"

Summary of Criteria & Issues

The application is subject to the policies and procedures of the Three Mile Lane Planned Development Overlay district, which was originally adopted by Ordinance 4131 and later amended by Ordinance 4572. Section 4 of Ordinance 4572 includes the following policies that regulate development within the Three Mile Lane Planned Development Overlay district:

- A. The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- B. A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- C. Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:
 1. The minimization of entrances onto Three Mile Lane;
 2. The development of on-site circulation systems, connecting to adjoining properties, including public frontage roads;
 3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
 4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.
- D. Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- E. Mixed housing-type residential developments shall be allowed and encouraged in those areas designated as residential.
- F. Temporary signage shall be allowed as per Section 17.62.060(B)(3) of the McMinnville Zoning Ordinance (No. 3380).

In addition, Section 5 of Ordinance 4572 includes standards for permanent signage within the Three Mile Lane Planned Development Overlay district. These standards will be described in more detail in Section VII (Conclusionary Findings) below.

The applicant has provided findings to support the request for Three Mile Lane Development Review. These will be discussed in detail in Section VII (Conclusionary Findings) below.

II. CONDITIONS:

1. That the decision for approval of Three Mile Lane Development Review (TML 2-20) is not rendered, and does not take effect, until and unless the Zoning Text Amendment request (ZC 2-20) is approved by the City Council.
2. That the two (2) Digital Pre-Browse Boards shall not exceed five (5) feet in height.

III. ATTACHMENTS:

1. TML 2-20 Application and Attachments (on file with the Planning Department)

IV. COMMENTS:**Agency Comments**

This matter was referred to the following public agencies for comment: McMinnville Fire Department, Police Department, Engineering Department, Public Works Department, Building Department, Parks Department, Wastewater Services; and City Manager; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Oregon Department of State Lands; Oregon Department of Transportation; Frontier Communications; Comcast; Recology; and Northwest Natural Gas. In addition, notice was provided to the Oregon Department of Land Conservation and Development. The following comments were received:

- McMinnville Building Department
No building code issues.
- McMinnville Engineering Department
No comments.
- McMinnville Water & Light
MW&L has no comments on either of these submittals.
- Comcast
We have no conflicts with this project.

Public Comments

Notice of this request was mailed to property owners located within 100 feet of the subject site, as required by Section 17.72.110 (Applications and Permits – Director’s Review with Notification). The following comments have been received:

- Email from Dorothy McQueen on February 1, 2021.
 - Concerns about sight, sound, and security issues with increased parking lot traffic and questioned if McDonald’s is improving the fence along the north property line.

V. FINDINGS OF FACT - PROCEDURAL FINDINGS

1. The applicant, Alexander Taam of Freiheit Architecture, on behalf of McDonald’s Corporation, property owner, submitted the Three Mile Lane Development Review application (TML 2-20) on November 24, 2020. Application fees were paid on December 9, 2020.
2. The application was deemed incomplete on January 5, 2021. The applicant submitted revised application materials on January 7, 2021.
3. Based on the revised application materials, the application was deemed complete on January 22, 2021. Based on that date, the 120 day land use decision time limit expires on May 22, 2021.
4. Notice of the application was referred to the following public agencies for comment in accordance with Section 17.72.120 of the Zoning Ordinance: McMinnville Fire Department, Police Department, Engineering Department, Public Works Department, Building Department,

Attachments :

Attachment 1 – Application and Attachments

Parks Department, Wastewater Services; and City Manager; McMinnville Water and Light; McMinnville School District No. 40; Yamhill County Planning Department; Oregon Department of State Lands; Oregon Department of Transportation; Frontier Communications; Comcast; Recology; and Northwest Natural Gas. In addition, notice was provided to the Oregon Department of Land Conservation and Development. In addition, notice was provided to the Oregon Department of Land Conservation and Development. Comments received from agencies are addressed in the Decision Document.

5. Notice of the application and the February 18, 2021 Planning Commission public hearing was mailed to property owners within 100 feet of the subject property in accordance with Section 17.72.110 of the Zoning Ordinance on January 29, 2021.
6. Notice of the application and the February 18, 2021 Planning Commission public hearing was published in the News Register on Tuesday, February 9, 2021, in accordance with Section 17.72.120 of the Zoning Ordinance.
7. Public testimony received by the Planning Department prior to the Planning Commission public hearing are addressed in the Decision Document.
8. On February 18, 2021, the Planning Commission held a duly noticed public hearing to consider the request.

VI. FINDINGS OF FACT - GENERAL FINDINGS

1. **Location:** 225 NE Norton Lane (Tax Lot 1602, Section 22CD, T.4 S., R. 4 W., W.M.)
2. **Size:** 0.81 acres
3. **Comprehensive Plan Map Designation:** Commercial
4. **Zoning:** C-3PD (General Commercial Planned Development)
5. **Overlay Zones/Special Districts:** Three Mile Lane Planned Development Overlay District (Ordinance No. 4131 as amended by Ord. No. 4572)
6. **Current Use:** Restaurant with Drive-Thru Service
7. **Inventoried Significant Resources:**
 - a. **Historic Resources:** None
 - b. **Other:** None
8. **Other Features:**
 - a. **Slopes:** The site is relatively flat.
9. **Utilities:**
 - a. **Water:** The property is served by a water main in NE Norton Lane. The treatment plant has sufficient treatment capacity.
 - b. **Sewer:** The property is served by a sewer main in NE Norton Lane. The municipal water reclamation facility has sufficient capacity to accommodate expected waste flows resulting from the use.
 - c. **Stormwater:** The property is served by a stormwater sewer main in NE Norton Lane.
 - d. **Other Services:** Other services are available to the property. No overhead utilities are present adjacent to the property.

10. **Transportation:** NE Norton Lane is classified as a Minor Collector in the Transportation System Plan (TSP). NE Cumulus Avenue is classified as a Minor Collector.

VII. CONCLUSIONARY FINDINGS:

The Conclusionary Findings are the findings regarding consistency with the applicable criteria for the application. The applicable criteria for Three Mile Lane Development Review are specified in the Three Mile Lane Planned Development Ordinance (Ordinance No. 4131 as amended by Ordinance No. 4572).

In addition, the goals, policies, and proposals in Volume II of the Comprehensive Plan are to be applied to all land use decisions as criteria for approval, denial, or modification of the proposed request. Goals and policies are mandated; all land use decisions must conform to the applicable goals and policies of Volume II. "Proposals" specified in Volume II are not mandated, but are to be undertaken in relation to all applicable land use requests.

Comprehensive Plan Volume II:

The following Goals, Policies, and Proposals from Volume II of the Comprehensive Plan provide criteria applicable to this request:

The implementation of most goals, policies, and proposals as they apply to this application are accomplished through the provisions, procedures, and standards in the city codes and master plans, which are sufficient to adequately address applicable goals, policies, and proposals as they apply to this application.

The following additional findings are made relating to specific Goals and Policies:

GOAL X 1: TO PROVIDE OPPORTUNITIES FOR CITIZEN INVOLVEMENT IN THE LAND USE DECISION MAKING PROCESS ESTABLISHED BY THE CITY OF McMINNVILLE.

Policy 188.00 The City of McMinnville shall continue to provide opportunities for citizen involvement in all phases of the planning process. The opportunities will allow for review and comment by community residents and will be supplemented by the availability of information on planning requests and the provision of feedback mechanisms to evaluate decisions and keep citizens informed.

APPLICANT'S RESPONSE: None.

FINDING: SATISFIED. The process for a Three Mile Lane Development Review request provides an opportunity for citizen involvement throughout the process through the public notice and the application review process. Throughout the process, there are opportunities for the public to review and obtain copies of the application materials prior to the Planning Director's decision on the application. All members of the public have access to provide testimony and ask questions during the public review process.

McMinnville Zoning Ordinance

The following Sections of the McMinnville Zoning Ordinance (Ord. No. 3380) provide criteria applicable to the request:

CHAPTER 17.03. GENERAL PROVISIONS

17.03.020 Purpose. The purpose of this ordinance is to encourage appropriate and orderly physical development in the City through standards designed to protect residential, commercial, industrial, and civic areas from the intrusions of incompatible uses; to provide opportunities for establishments to concentrate for efficient operation in mutually beneficial relationship to each other and to shared

services; to provide adequate open space, desired levels of population densities, workable relationships between land uses and the transportation system, and adequate community facilities; to provide assurance of opportunities for effective utilization of the land resource; and to promote in other ways public health, safety, convenience, and general welfare.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. The purpose of the Zoning Ordinance is met by the proposal as described in the Conclusionary Findings contained in this Decision Document.

CHAPTER 17.60. OFF STREET PARKING AND LOADING

17.60.060 Spaces—Number required. Except for the southerly 100 feet of Block 10 and the northerly 100 feet of Block 11, Rowland's Addition and the area bounded by Second Street, Adams Street, Fourth Street, and Galloway Street, at the time of erection of a new structure or at the time of enlargement or change of use of an existing structure, off-street parking spaces shall be provided as follows unless greater requirements are otherwise established. Where square feet are specified, the area measured shall be the gross floor area primary to the functioning of the particular use of the property but shall exclude space devoted to off-street parking or unloading.

- | | |
|---|---|
| <p>C. Commercial land use category</p> <p>8. Drive-in restaurants or similar drive-in uses for the sale of beverages, food, or refreshments for consumption on the premises</p> | <p>One space per four seats or one space per 200 square feet of floor area, whichever is greater.</p> |
|---|---|

APPLICANT’S RESPONSE: Per section 17.60 of the MMC it requires us to have one space per 200 square feet of floor area. Since the buildings square footage is 2,829 sqft we would need a minimum of 14 stalls. We are currently proposing 26 with the new site configuration.

FINDING: SATISFIED. Staff concurs with the applicant’s finding.

17.60.140 Bicycle parking.

- A. Bicycle parking facilities shall be required as follows:
1. In any commercial (C-1, C-2, and C-3) or office/residential (O-R) zone, bicycle parking facility requirements shall be based on the amount of automobile parking required. The minimum number of bicycle parking spaces provided shall be ten percent of the automobile parking spaces required.
 2. The uses exempted from bicycle parking requirements include: residential uses, drive-in theaters, mortuaries, motels, hotels, and automobile service stations.
 3. In all zones, for each fifteen automobile parking spaces required, a required automobile parking space may be eliminated if five bicycle parking spaces are provided.
- B. Bicycle parking facilities shall be provided pursuant to the following design standards. (as modified by the Bicycle Parking Administrative Rule of September 14, 1984):
1. At a minimum, a bicycle parking facility shall consist of a stationary object to which the user can lock the frame and both wheels with a user-provided six-foot cable or chain and lock.
 2. Fixed objects which are intended to serve as bicycle parking facilities but not obviously designed for such purposes shall be clearly labeled as available for bicycle parking.
 3. Bicycle parking facilities shall provide a least an eighteen inch clearance between adjacent bicycles.
 4. Aisles between bicycle parking facilities shall be at least five-feet in width.
 5. Paving is not required for bicycle parking areas, but the outside ground surface shall be finished or planted in such a way that the surface will remain free from mud or dust. Bicycle parking may be provided within a required landscape area.

6. Bicycle parking should be situated at least as conveniently as the most convenient car parking area. Bicycle and automobile parking areas shall be separated by a physical barrier or sufficient distance to protect parked bicycles from damage by automobiles. (Ord. 4261 §1, 1983; Ord. 4128 (part), 1981; Ord. 3380 (part), 1968).

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. Two (2) bicycle parking spaces are required (Ten (10) percent of 14 automobile parking spaces). An existing bicycle rack that provides at least two (2) bicycle parking spaces is to remain near the restaurant entrance following the site reconfiguration.

CHAPTER 17.61. SOLID WASTE AND RECYCLING ENCLOSURE PLAN

17.61.010 Purpose and Intent. The purpose and intent of this chapter is to provide efficient, safe and convenient locations for solid waste and recycling containers, to provide adequate on-site maneuvering for collection vehicles and to ensure that solid waste enclosures for all commercial, industrial and multi-family development are in compliance with the solid waste and recycling pick-up standards of the service provider while contributing to the overall appearance and attractiveness of the City of McMinnville.

APPLICANT’S RESPONSE: Since we had to reconfigure the drive-thru the trash corral has been relocated away from the building to make sure the site circulation still works. Due to the proposed location, per section 17.61 of the MMC, we will be providing landscaping 3’ tall around the three sides of the structure as well as the required space needed for a garbage truck to approach the trash enclosure.

FINDING: SATISFIED. Staff concurs with the applicant’s finding. The addition of the second drive-thru service lane necessitated the relocation of the trash enclosure. By complying with the guidelines and standards of this chapter, the solid waste and recycling enclosure is in an efficient, safe, and convenient location with adequate on-site maneuvering for service, and the aesthetics of the enclosure have been addressed.

17.61.030 Guidelines and Standards.

- A. The location of an enclosure must allow for collection agency drive-in access. A fifty-foot (50) access approach is recommended. In addition to the approach, either an exit that allows the truck to move forward or a turn area with a minimum radius of 46.5 feet is preferred. Both approach and location shall be unobstructed and free of over head wires and low hanging trees. An eighteen-foot (18) minimum height clearance above the enclosure approach is required and a thirty-two-foot (32) vertical clearance is required above the container itself. The enclosure shall be of sufficient size to store trash and recycling receptacles, the size of which shall be determined by the collection agency and will be based on proposed use. A minimum distance of two-feet (2) is required between the container and existing or proposed structures. The enclosure shall be a minimum of six-feet (6) tall or six inches (6) higher than the top of the tallest container.
- B. Solid waste enclosures shall not be located within twenty-feet (20) of a required front or exterior yard and should be placed at the rear of a building whenever possible. Should an enclosure be placed within a required landscaped front or exterior yard, additional landscaping must be provided elsewhere on the property to compensate for the encroachment into the required landscaped yard. Any modifications to required landscaping must meet the approval of the Landscape Review Committee.
- C. Any trash or recycling enclosure which is visible from the street must provide landscaping around three (3) sides of the structure. Climbing vines and screening shrubs or hedges are appropriate and landscaping must be a minimum of three-feet (3) in height at the time of planting.

- D. Where a commercial or industrial zone abuts a residential zone, enclosures must be placed a minimum of thirty-feet (30) from any residential structure or as otherwise approved by the Planning Director.
- E. Generally, the design of the structure should match the exterior surface of the building and can be constructed of masonry, wood or concrete blocks in combination with plant material capable of forming a complete evergreen hedge. The floor of the enclosure shall be a concrete holding pad which must extend eight-feet (8) beyond the gates.
- F. Gates that screen the containers are required and must remain closed at all times except at times of service.
- G. Parking is prohibited in front of the enclosure and all parked vehicles must be located at a safe distance. A “No Parking” sign must be visibly placed on the gates of the enclosure.
- H. Solid waste and recycling enclosures must be placed in a location that is compatible with the City of McMinnville’s Fire Code.

APPLICANT’S RESPONSE: None

FINDING: SATISFIED. The proposed location of the enclosure allows for collection agency access. Sufficient space for the approach and overhead clearance is provided. The subject site is zoned C-3 (General Commercial) and there are no required yards in the C-3 zone, except side and rear yards that are adjacent to a residential zone. The subject site is adjacent to residentially zoned land, however the trash enclosure is proposed along a property line that is not adjacent to that residential area. Therefore there are no applicable setbacks for the proposed location of the trash enclosure. Furthermore, the location of the enclosure is over 30 feet from any residential structure. No parking is proposed in front of the enclosure.

The enclosure is visible from the Cumulus Avenue right-of-way, therefore landscaping around three (3) sides of the enclosure is required. The approved landscape plan (L 30-20) shows that the evergreen shrub *Ligustrum japonicum* ‘Texanum’ (Waxleaf Privet) is will be planted as a hedge along three (3) sides of the enclosure that are visible from the street.

The enclosure plan indicates the structure will be made from concrete masonry units painted to match the base color of the building. As mentioned above, an evergreen hedge of Waxleaf Privet would be on three (3) sides of the enclosure.

The McMinnville Fire Department has had the opportunity to review the application and provide comments; no comments were received.

17.61.040 Procedure. The applicant is responsible for contacting the collection agency for information regarding the size of containers required relative to proposed use *prior* to submittal of building plans. Two (2) copies of a Solid Waste and Recycling Enclosure plan shall be included in the submitted site (plot) plan or as a separate plan to allow for Planning Department review. At a minimum, the Enclosure Plan will illustrate the location, size and height of the proposed trash enclosure in addition to listing construction materials and any required landscaping. The structure must conform to the approved site plan at the time of final inspection.

APPLICANT’S RESPONSE: None

FINDING: SATISFIED. The applicant has submitted a Solid Waste and Recycling Enclosure Plan with the application materials. The location, size, height, construction materials, and finishes are illustrated. Required landscaping around the enclosure is indicated on the landscape plan included in the application materials.

Three Mile Lane Planned Development Overlay

The Three Mile Lane Planned Development Overlay was adopted by Ordinance No. 4131 and amended by Ordinance No. 4572. The following Sections of the Three Mile Lane Planned Development Overlay (Ord. No. 4131 as amended by Ord. No. 4572 and by proposed Zoning Text Amendment ZC 2-20) are applicable to the request:

Section 1. Statement of Purpose. The City of McMinnville finds that the development of lands on the north and south sides of Three Mile Lane (Oregon Highway 18) is acceptable if provisions are made to integrate the dual functions of the highway as a bypass around the City and a land service arterial for adjacent properties. The City also finds that development along Three Mile Lane is desirable if it is of good quality and design, if the living and working environments developed are compatible with each other, and if the living environment includes open spaces, parks, and features buffering of residential uses from the highway. The City recognized, during the comprehensive planning process, that to insure that these concerns are met, proper designations on the McMinnville Comprehensive Plan and Zoning Maps must be made, and, further, that a planned development overlay must be placed over the area establishing specific conditions for development.

APPLICANT’S RESPONSE: The owner has elected to revise the drive-thru to add a second lane. To support this addition, we need to reconfigure the drive-thru lane layout. We are proposing new signage for the addition of the second drive-thru lane, which includes relocating the gateway, adding a small direction sign, a second pre-browse board, a second order-ready canopy, and a second menu board.

Since we had to reconfigure the drive-thru the trash corral has been relocated away from the building to make sure the site circulation still works. Due to the proposed location, per section 17.61 of the MMC, we will be providing landscaping 3’ tall around three sides of the structure as well as the required space needed for a garbage truck to approach the trash enclosure.

Per section 17.60 of the MMC it requires us to have one space per 200 square feet of floor area. Since the buildings square footage is 2,829 sqft we would need a minimum of 14 stalls. We are currently proposing 26 with the new site configuration. [...]

FINDING: SATISFIED. Staff concurs with the applicant’s findings, and adds that the second drive-thru service lane and signage proposed for the commercial site is found to be acceptable for the Three Mile Lane Planned Development Overlay district.

The proposed site design efficiently incorporates the two drive-up service lanes into the existing driveway and parking aisles. To accommodate this, surplus parking was removed, additional landscape area was included, and the trash and recycling enclosure was relocated. Parking requirements and trash enclosure standards continue to be met as described in findings for those sections of the McMinnville Zoning Ordinance described above.

A landscape plan (docket L 30-20) for the proposed site design was reviewed against the standards and regulations of the Landscaping Chapter of the McMinnville Zoning Ordinance and approved by the Landscape Review Committee.

Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit “A”:

- A. The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.

APPLICANT’S RESPONSE: None.

Attachments :

Attachment 1 – Application and Attachments

FINDING: SATISFIED. The proposal is consistent with the applicable Comprehensive Plan goals and policies, as well as Zoning Ordinance requirements, as described in the findings for those policies above.

- B. A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. The southernmost portion of the subject property is located more than 120 feet from the center line of Highway 18, and proposed structures and signage are located further north from the southernmost property line.

- C. Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:

1. The minimization of entrances onto Three Mile Lane;
2. The development of on-site circulation systems, connecting to adjoining properties, including public frontage roads;
3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. Access requirements for the Three Mile Lane overlay district are met, or are not applicable to this application. No new entrances onto Three Mile Lane are proposed. The shared access between the subject property and the adjoining property to the west from the Cumulus Avenue is not proposed to be altered, nor is the access to the subject site from Norton Lane. The on-site circulation system is being revised to accommodate a second drive-thru service lane for the restaurant, but does not alter the connections to adjoining properties or public frontage roads as mentioned above.

- D. Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. A landscape plan (L 30-20) for the proposed development has been reviewed and approved by the Landscape Review Committee.

- E. Mixed housing-type residential developments shall be allowed and encouraged in those areas designated as residential.

APPLICANT’S RESPONSE: None.

FINDING: NOT APPLICABLE. No residential development is proposed as part of the application. The proposed development and signage is for a commercially designated property.

- F. Temporary signage shall be allowed as per Section 17.62.060(B)(3) of the McMinnville Zoning Ordinance (No. 3380).

APPLICANT’S RESPONSE: None.

FINDING: NOT APPLICABLE. No temporary signage is being proposed as part of the application.

Section 5. Signs. The following regulations apply to those properties designated as either industrial or commercial on the McMinnville Comprehensive Plan Map and which are located within the area described in Exhibit “A.” All signage must be approved by the Three Mile Lane Design Review Committee and may not exceed the maximums established in this section. The Three Mile Lane Design Review Committee may take into account color selection; sign material; relationship to site and building design; size, form, mass, height, and scale of sign; the sign’s location and proximity to existing signs on adjacent parcels; and other sign design elements appropriate to ensuring that the proposed sign is compatible with the objectives of this ordinance. The Three Mile Lane Design Review Committee may require a proposed sign to be redesigned as necessary to make it consistent with the objectives of this ordinance including, but not limited to, restricting the size and height of the sign, its location, form, color, and material.

- B. Zone 2. The following regulations apply to commercially or industrially designated properties which fall within zone 2:
1. That only one free-standing sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit “B”, subject to the following:
 - a) The maximum height of any free-standing sign shall be 30 feet, except as provided in subsection (B)(1)(c) below.
 - b) The maximum square footage per sign face is 64 square feet and there shall be a maximum of two sign faces per free-standing sign, except as provided in subsection (B)(1)(c) below.
 - c) If two or more properties share a single free-standing sign, said sign shall be a maximum of 35 feet in height, and each property sharing the sign shall be entitled to up to 64 square feet of sign face per side on a maximum of two sides. The shared free-standing sign would exhaust the allowance for free-standing signs listed in subsection (B)(1)(a) above for all parcels represented.
 - d) A free-standing sign is allowed only on properties with frontage on the Highway 18 frontage road.
 2. In lieu of a free-standing sign as listed in subsection 1 above, one monument-type sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit “B”, subject to the following:
 - a) Maximum square footage per sign face shall be 50 square feet, and there shall be a maximum of two sign faces per monument sign.
 - b) Maximum sign height shall be six feet, and the top of the sign shall not be more than eight feet above the center line grade of the Three Mile Lane frontage road.
 - c) Monument signs shall be non-illuminated, indirectly illuminated, neon, or may have individually back-lit letters, providing that no plastic sign cabinets are allowed.
 3. All other signs, with the exception of incidental directional signage, must be flush against the buildings and not protrude more than 12 inches from the building face, subject to the following:
 - a) Total area of building signage may not exceed eight percent of the total area of the face of the building which is most closely parallel to Highway 18 or the frontage road and may not exceed four percent on the other sides.
 - b) The area of building signs shall be measured as the area around or enclosing each sign cabinet, or where sign cabinets are not used, the area shall be that

within a single continuous perimeter composed of any straight line geometric figure which encloses the extreme limits of the advertising message.

4. If a parcel is divided after the date of the adoption of this ordinance, the sign allowances established in subsections 1 and 2 above will apply only to the original parcel or what remains of it and newly created parcels are limited to the sign allowances established in subsection 3 above except as provided in subsection (B)(5) below.
5. The Three Mile Lane Design Review Committee may allow the sharing of a single free-standing sign by an original parcel or what remains of it and parcels divided after the date of the adoption of this ordinance. The total sign size shall not exceed that allowed for the original parcel.
6. Portable signs and signs with flashing or moving parts are prohibited.
7. Additional freestanding signs are permitted with businesses that employ drive-up service. One such sign, not to exceed 36 square feet in area or six feet in height, is allowed per order station. In addition, one secondary sign, a maximum of 15 square feet in area and five feet in height, is allowed per order station. Any freestanding sign that has copy facing toward a public street shall be located a minimum of thirty (30) feet from that street's property line. Wall mount signs shall be exempt from this.

APPLICANT'S RESPONSE: None.

FINDING: SATISFIED WITH CONDITIONS OF APPROVAL #1 AND #2. The proposed re-development of the site includes eight (8) signs associated with the two (2) drive-thru service lanes: one (1) "Welcome" Gateway Sign, one (1) "Any Lane, Any Time" Directional Sign, two (2) Pre-Browse Boards (one per lane), two (2) Customer Order Canopies (one per lane), and two (2) Digital Menu Boards (one per lane). Staff notes that the property shares a free-standing sign with the adjacent property, which exhausts the allowance for free-standing signs listed in subsection (B)(1)(a) above for the subject site. Therefore, proposed signage must comply with subsection (B)(7) or be deemed incidental directional signage per subsection (B)(3). Subsection (B)(7) is proposed by the applicant in a concurrent application to be included in the Three Mile Lane Planned Development Ordinance. Therefore, a condition of approval is included to require the adoption of Zoning Text Amendment (ZC 2-20) prior to the approval of the Three Mile Lane Development Review for the subject property.

The "Welcome" Gateway Sign, "Any Lane, Any Time" Directional Sign, and each "Order Here" Customer Order Canopies are considered incidental directional signs and are exempt per subsection (B)(3).

The proposed Digital Menu Boards are less than six (6) feet in height with surface areas of approximately 20 square feet. These dimensions are less than the maximum height and area dimensions for the first additional freestanding sign allowed per order station. One (1) sign meeting these standards is permitted for each drive-up order station.

The proposed Digital Pre-Browse Boards are approximately six (6) feet in height with surface areas of approximately 10 square feet. While the area of the Pre-Browse Boards is below the maximum allowed, the proposed height of approximately six (6) feet exceeds the maximum height of five (5) feet for secondary signs allowed per order station. Therefore, a condition of approval is included to require that the Digital Pre-Browse Boards be a maximum of five (5) feet in height. One (1) sign meeting these standards is permitted for each drive-up order station.

Generally, the proposed signage is of a quality and design that is suitable for the proposed site, and is compatible with the use, and with the objectives of the Three Mile Lane Planned Development Overlay Ordinance. All proposed signage is located to have minimal impact on neighboring properties and the public right-of-way.

CONDITIONS FOR FINDING: That the decision for approval of Three Mile Lane Development Review (TML 2-20) is not rendered, and does not take effect, until and unless the Zoning Text Amendment request (ZC 2-20) is approved by the City Council.

That the two (2) Digital Pre-Browse Boards shall not exceed five (5) feet in height.

Section 6. Procedures for Review:

- A. No development or signage may take place within the Three Mile Lane Planned Development Overlay without such development or signage gaining the approval of the Three Mile Lane Design Review Committee.

APPLICANT’S RESPONSE: None.

FINDING: SATISFIED. The applicant submitted an application for Three Mile Lane Development Review, which was reviewed and processed prior to any new site development or installation of signage on the subject site.

JF

569-20-000652-PL19



Planning Department
 231 NE Fifth Street ◦ McMinnville, OR 97128
 (503) 434-7311 Office ◦ (503) 474-4955 Fax
www.mcminnvilleoregon.gov

Office Use Only:	
File No.	TML 2-20
Date Received	
Fee	1385. ⁰⁰
Receipt No.	
Received by	SPS

Three Mile Lane Development Review

Applicant Information

Applicant is: Property Owner Contract Buyer Option Holder Agent Other _____

Applicant Name ALEXANDER TAAM Phone 425-559-2773

Contact Name _____ Phone _____
(If different than above)

Address 929 108TH AVE NE

City, State, Zip BELLEVUE, WA 98004

Contact Email ATAAM@FREIHEITARCH.COM

Property Owner Information

Property Owner Name MCDONALDS COPORATION Phone _____
(If different than above)

Contact Name ALBERT PALACIOS Phone 312-485-7551

Address 2999 OAK ROAD STE 900

City, State, Zip WALNUT CREEK, CA 98004

Contact Email albert.palacios@us.mcd.com

Site Location and Description

(If metes and bounds description, indicate on separate sheet)

Property Address 225 NE NORTON LN

Assessor Map No. R4422CD01602 - Total Site Area _____

Subdivision _____ Block _____ Lot _____

Comprehensive Plan Designation _____ Zoning Designation C-3

August 10, 2020

17-500 McMinnville
Planning Department

225 NE E Norton Ln
McMinnville OR
503-434-7311

Project Narrative

McDonald's Restaurant Revision for the 3 Mile Lane Design Review:

The owner has elected to revise the drive-thru to add a second lane. To support this addition, we need to reconfigure the drive-thru lane layout. We are proposing new signage for the addition of the second drive-thru lane, which includes relocating the gateway, adding a small directional sign, a second pre-browse board, a second order-ready canopy, and a second menu board.

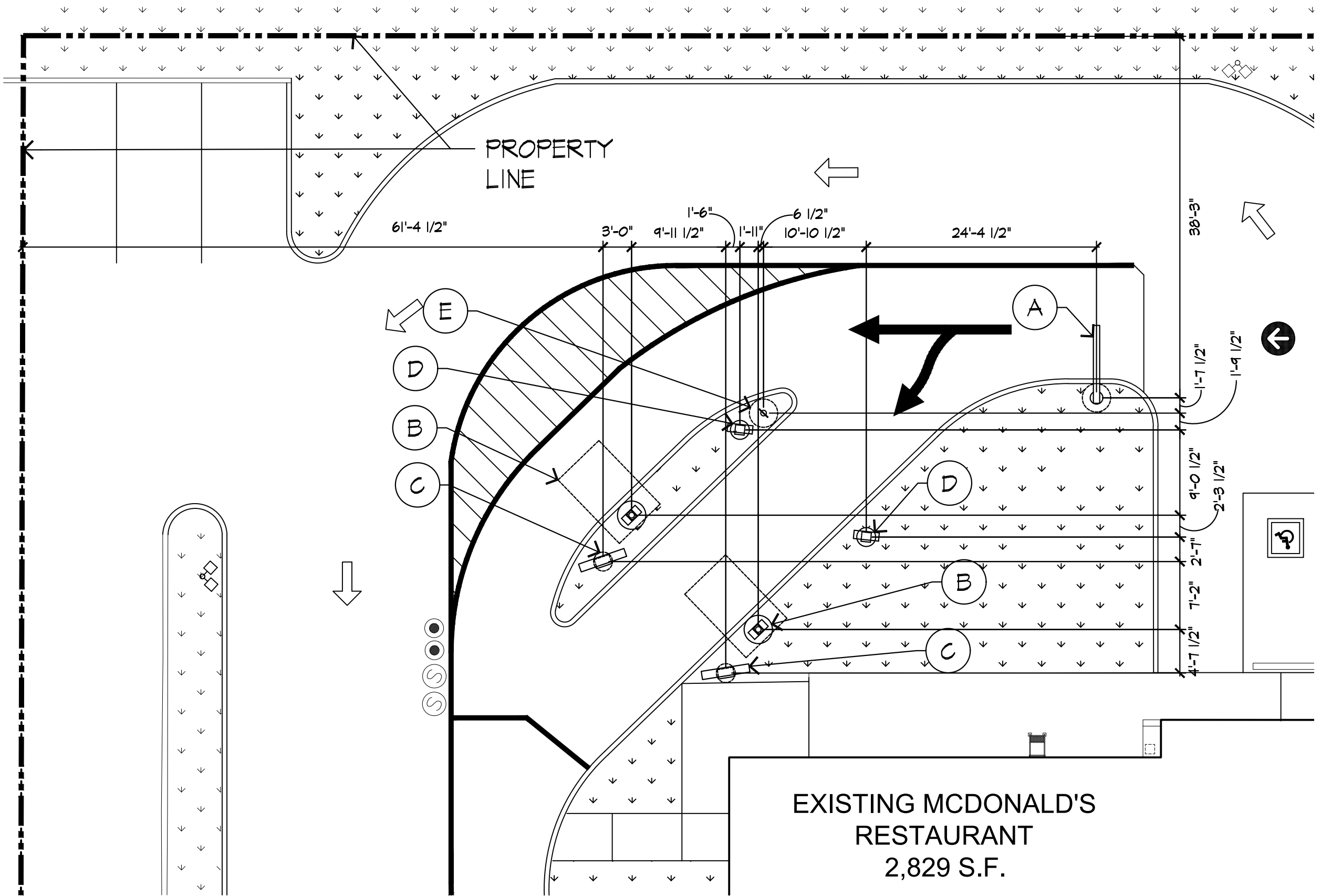
Since we had to reconfigure the drive-thru the trash corral has been relocated away from the building to make sure the site circulation still works. Due to the proposed location, per section 17.61 of the MMC, we will be providing landscaping 3' tall around three sides of the structure as well as the required space needed for a garbage truck to approach the trash enclosure.

Per section 17.60 of the MMC it requires us to have one space per 200 square feet of floor area. Since the buildings square footage is 2,829 sqft we would need a minimum of 14 stalls. We are currently proposing 26 with the new site configuration.

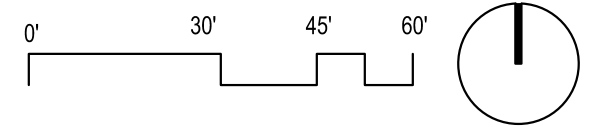
Revisions are clouded noted by delta 2 dated 6.1.2020

Enclosed with this submittal please find the revised plans and additional reports and documents regarding these revisions.

- SIGN KEY:**
- (A) "WELCOME" GATEWAY SIGN (1 NEW)
 - (B) CUSTOMER ORDER CANOPY (2 NEW)
 - (C) DIGITAL MENU BOARD (2 NEW)
 - (D) PRE-BROWSE BOARD (2 NEW)
 - (E) "ANY LANE, ANY TIME" SIGN (1 NEW)

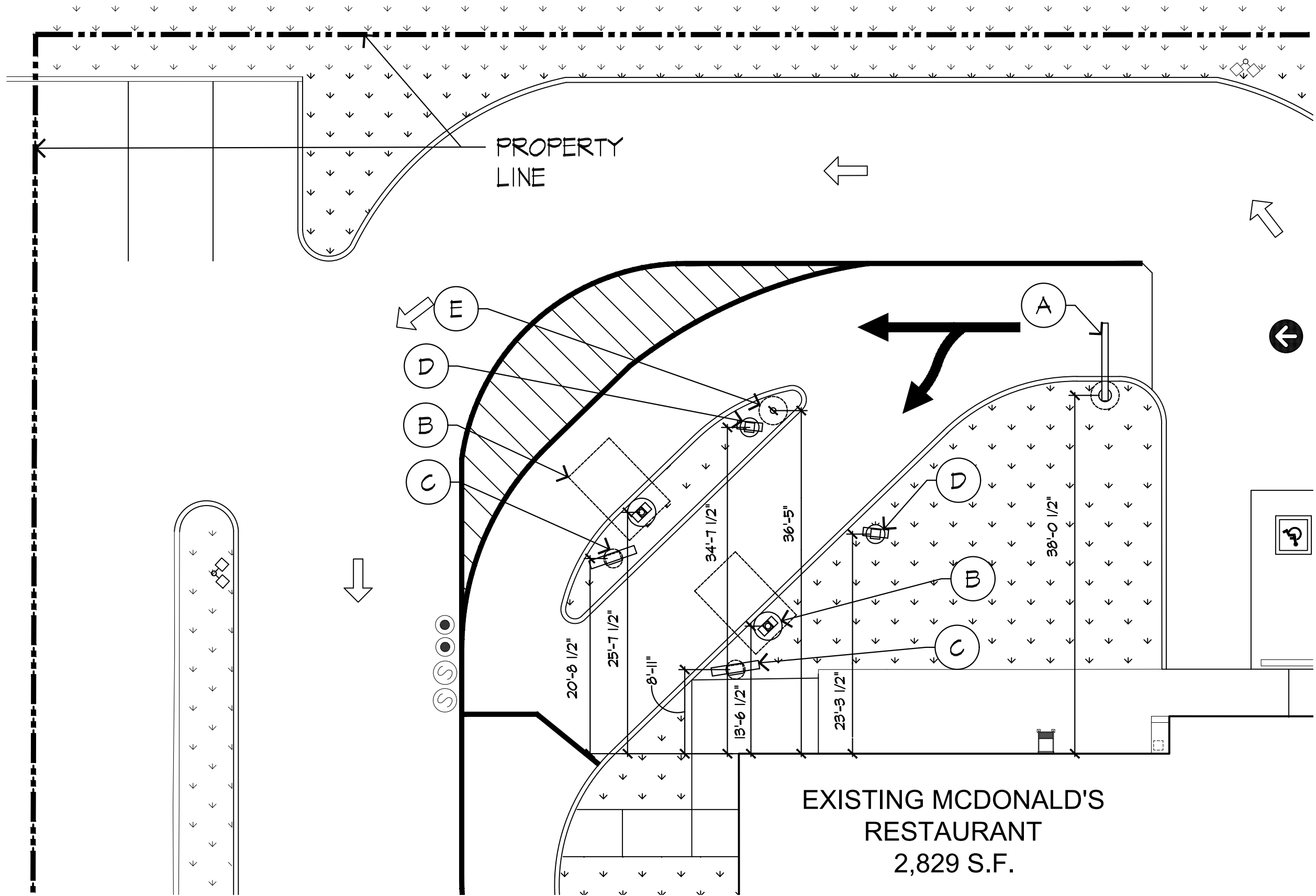


SIGN LOCATION SKETCH (PROP. LINE)
 SCALE: 1" = 30'



SIGN KEY:

- (A) "WELCOME" GATEWAY SIGN (1 NEW)
- (B) CUSTOMER ORDER CANOPY (2 NEW)
- (C) DIGITAL MENU BOARD (2 NEW)
- (D) PRE-BROWSE BOARD (2 NEW)
- (E) "ANY LANE, ANY TIME" SIGN (1 NEW)



SIGN LOCATION SKETCH (BUILDING)

SCALE: 1" = 30'

PROJECT DATA

SITE AREA: 0.81 ACRE
 EXISTING PARKING: 49 STALLS
 PARKING REQUIRED: 1 PER 4 SEATS, OR 1 PER 100 SF OF DINING AREA, WHICHEVER IS GREATER= 16 STALLS (BASED ON 846 SF DINING AREA)
 PARKING PROVIDED: 49 STALLS
 ACCESSIBLE REQUIRED: 2 STALLS (BASED ON 49 PROVIDED)
 ACCESSIBLE PROVIDED: 2 STALLS
 ZONING: C3 COMMERCIAL
 BUILDING AREA: 2,829 SQ. FT.
 OCCUPANCY : A-2 (EXISTING)
 TYPE OF CONSTRUCTION: V-B (EXISTING)

ADA SITE PLAN KEYNOTES

- AD-01** REMOVE EXISTING & INSTALL NEW BROOM-FINISH, SLIP-RESISTANT CONCRETE AS SHOWN. PROVIDE 5% MAX SLOPE IN DIRECTION OF TRAVEL & 1.5% MAX CROSS SLOPE AT CROSSWALK; REGRADE PARKING LOT AND SEALCOAT AS NECESSARY. PROVIDE APPROPRIATE GRADED NON-ABRUPT TRANSITION TO ADJACENT PARKING LOT. ENSURE THERE IS NO CHANGE OF LEVEL GREATER THAN 1/4" OR 1/2" WITH BEVEL.
- AD-02** RELOCATE & RESTRIPE EXISTING BARRIER-FREE ACCESSIBLE STALLS (TYP. OF 2) AS SHOWN, WITH ACCESS AISLE BETWEEN STALLS. WIDTH OF STALLS AND ACCESS AISLE TO BE MEASURED FROM CENTER OF STRIPING. ENSURE ALL SLOPES AT NEW PARKING STALLS AND ACCESS AISLE DO NOT EXCEED 1.5% IN ALL DIRECTIONS. REGRADE PARKING LOT AND SEALCOAT AS NECESSARY. PROVIDE APPROPRIATE GRADED NON-ABRUPT TRANSITION TO ADJACENT PARKING STALLS. PAINT ACCESSIBLE SYMBOL AT STALLS PER 4/SD1. ENSURE EXISTING ACCESSIBLE PARKING STALL SIGNS ARE MOUNTED AT EACH STALL WITH BOTTOM OF THE LOWEST SIGN AT 60" MIN. ABOVE ADJACENT PARKING SURFACE (TYP. OF 2) PER DETAIL 6/SD1. INSTALL NEW "VAN ACCESSIBLE" SIGN TO BE LOCATED AT STALL WITH PASSENGER SIDE ADJACENT TO 96" MIN WIDE ACCESS AISLE.
- AD-03** REMEDIATION OCCURS IN **AD-02**
- AD-04** REMOVE EXISTING CURB RAMP & SIDEWALK & INSTALL NEW BROOM-FINISH, SLIP-RESISTANT CONCRETE PEDESTRIAN RAMP AS SHOWN. PROVIDE 8% MAX SLOPE IN DIRECTION OF TRAVEL & 1.5% MAX CROSS SLOPE. PROVIDE 5" DEEP X FULL RAMP WIDTH TOP & BOTTOM LANDINGS WITH 1.5% SLOPE IN ALL DIRECTIONS. PROVIDE EDGE PROTECTION & HANDRAILS AT EACH SIDE OF RAMP. HANDRAILS TO BE 34"-36" ABOVE RAMP SURFACE & EXTEND 12" MIN BEYOND TOP & BOTTOM OF RAMP; SEE DETAIL 15/SD1.
- AD-05** REMEDIATION OCCURS IN **AD-04**
- AD-06** REMOVE EXISTING CURB RAMP & CONCRETE SIDEWALKS & PROVIDE NEW SLIP RESISTANT, CONCRETE IN-LINE CURB RAMP AS SHOWN & PER DETAIL 9/SD1. SLOPE IN DIRECTION OF TRAVEL TO BE 8% MAX WITH 1.5% MAX CROSS SLOPE. PROVIDE 5'-0" MIN DEEP X FULL RAMP WIDTH TOP & BOTTOM LANDINGS WITH 1.5% MAX SLOPE IN ALL DIRECTIONS. ENSURE TOP AND BOTTOM TRANSITIONS ARE FLUSH WITH 1/4" MAX CHANGE IN LEVEL OR 1/2" WITH BEVEL. PROVIDE 5% MAX GUTTER PAN SLOPE.
- AD-07** REMOVE EXISTING SIDEWALK TO EXTENT SHOWN & INSTALL NEW WITH A SLIP RESISTANT BROOM FINISH. ENSURE 5'-0" MIN WIDTH AT SIDEWALK ON NON-DRIVE THRU SIDE OF BUILDING, AT EXTERIOR ENTRY DOOR. PROVIDE MAX 5% SLOPE IN DIRECTION OF TRAVEL & 1.5% MAX CROSS SLOPE. PROVIDE 5'X5' LANDING WITH MAX 1.5% SLOPE IN ALL DIRECTIONS AT EACH DOOR & CHANGE OF DIRECTION. ENSURE THERE IS NO CHANGE OF LEVEL OR THRESHOLD GREATER THAN 1/4" OR 1/2" WITH BEVEL. SEE DETAILS 8/SD1 & 11/SD1. REMOVE & REPLACE CONCRETE CURBS ALONG AREA OF NEW WORK. FLOAT ASPHALT AT AREAS OF PARKING LOT ADJACENT TO NEW CURBS AS NECESSARY TO MAINTAIN TYPICAL 6" MAX CURB. PROVIDE APPROPRIATE GRADED NON-ABRUPT TRANSITION TO EXISTING ASPHALT PARKING LOT. EXTENT SHOWN IS ASSUMED MIN WORK FOR ACCESSIBILITY COMPLIANCE. GC TO VERIFY WITH EXISTING CONDITIONS & CONFIRM EXTENT OF WORK WITH MCDONALD'S CONSTRUCTION MANAGER.
- AD-08** REMEDIATION OCCURS IN **AD-04**
- AD-09** SEE FLOOR PLAN.
- AD-10** REMEDIATION OCCURS IN **SP-06**
- AD-11** SEE FLOOR PLAN.
- AD-12** REMEDIATION OCCURS IN **AD-07**
- AD-13** THRU **AD-18** SEE FLOOR PLAN.
- AD-19** REMEDIATION OCCURS IN **SP-28**

SITE PLAN KEYNOTES

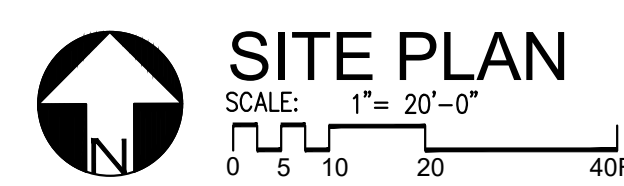
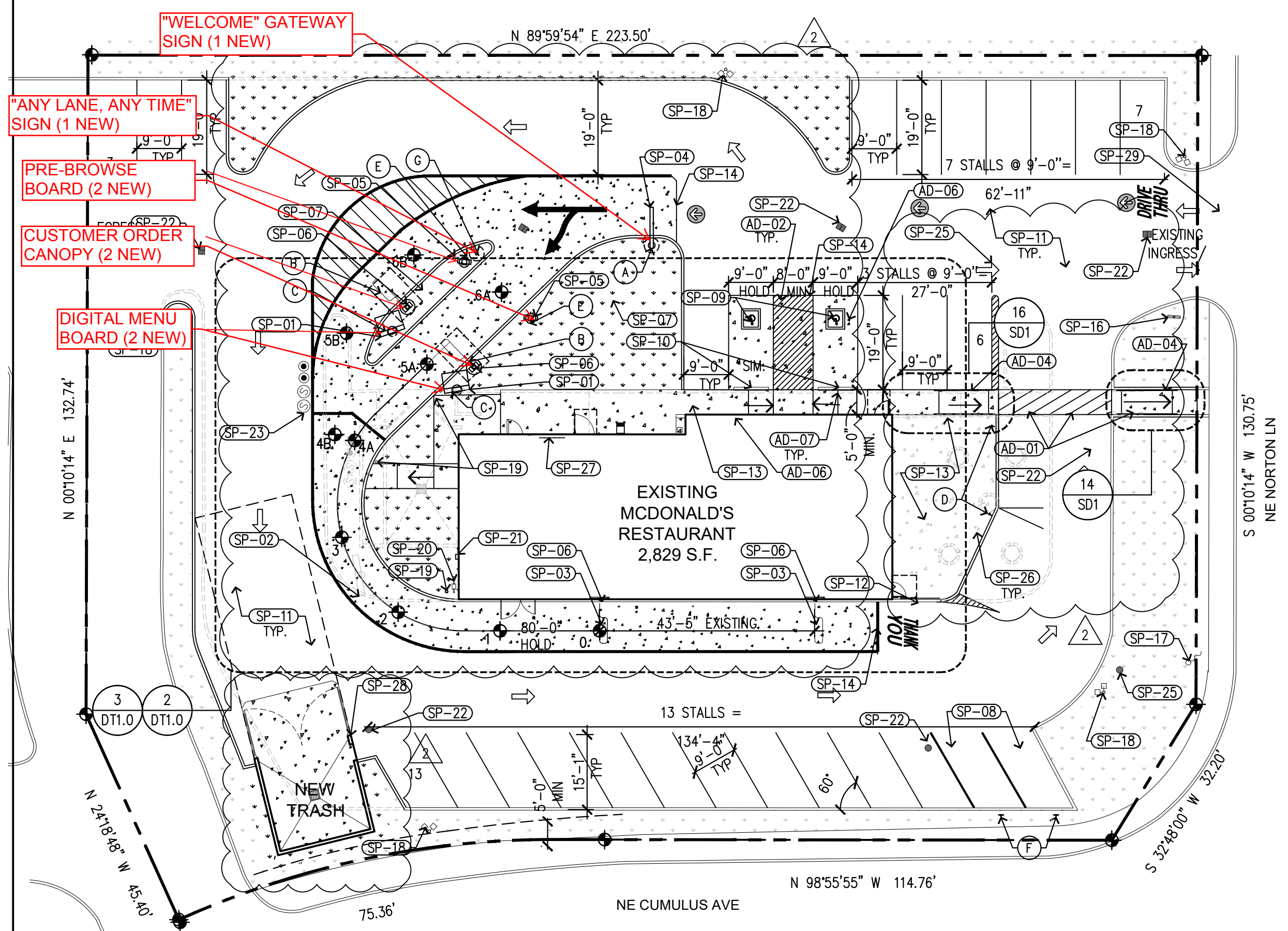
- SP-01** REMOVE EXISTING & INSTALL (1) NEW DIGITAL MENU BOARD AS SHOWN. SEE DETAIL 2/D1.0 FOR EXACT LOCATION AND 10/D1.1. LOCATE EXISTING UTILITIES PRIOR TO EXCAVATING FOOTING.
- SP-02** REMOVE EXISTING CUSTOMER ORDER DISPLAY & INSTALL (1) CUSTOMER ORDER CANOPY AS SHOWN PER DETAIL 6/D1.2. SEE DETAIL 2/D1.0 FOR EXACT LOCATION. LOCATE EXISTING UTILITIES PRIOR TO EXCAVATING FOOTING.
- SP-03** INSTALL NEW VEHICLE DETECTOR LOOP AT NEW ORDER POINT PER 2/D1.0. (OPTIONAL AT DRIVE THRU BOOTH) PATCH & REPAIR CONCRETE PAD AT DRIVE-THRU AS NECESSARY. SEE 18/D1.1 & 20/D1.1.
- SP-04** REMOVE EXISTING & INSTALL NEW GATEWAY SIGN PACKAGE AS SHOWN. COORDINATE LOCATION WITH MCDONALD'S CONSTRUCTION MANAGER TO ENSURE BEST VISIBILITY FROM SITE ENTRANCE. SEE DETAIL 8/D1.2 FOR MORE INFO. LOCATE UNDERGROUND UTILITIES PRIOR TO EXCAVATION.
- SP-05** REMOVE EXISTING & INSTALL NEW PRE-BROWSE MENU BOARD PER DETAIL 12/D1.1. VERIFY LOCATION WITH MCDONALD'S CONSTRUCTION MANAGER & DETAIL 2/D1.0.
- SP-06** INSTALL SPEECH/HEARING IMPAIRED SIGNAGE ON THE DRIVE-THRU C.O.D. & EACH DRIVE-THRU SERVICE WINDOW. SEE DETAILS 7/SD1 & 9/AA.4. PICTURE MENU, CLIPBOARD AND PENCILS ARE TO BE PROVIDED AT EACH DRIVE-THRU WINDOW.
- SP-07** PATCH AND REPAIR EXISTING LANDSCAPE AS NECESSARY TO ACCOMMODATE DEMOLITION AND NEW CONSTRUCTION. NEW LANDSCAPING TO MATCH EXISTING ADJACENT. ENSURE PLANTINGS DO NOT INHIBIT VIEW TO DRIVE THRU SIGNS.
- SP-08** DESIGNATED PULL FORWARD PARKING STALLS & SIGNAGE PER MCDONALD'S STANDARDS. EXISTING SIGNS TO REMAIN; PROTECT FROM DAMAGE. RESTRIPE STALLS TO BE (2) COATS TRAFFIC YELLOW.
- SP-09** REMOVE EXISTING ASPHALT PAVING FOR INSTALLATION OF NEW CONCRETE PAD AT ACCESSIBLE PARKING STALLS, ACCESS AISLE & CROSSWALK. COORDINATE WITH **AD-01** & **AD-02** FOR REQUIRED SLOPES. ENSURE FLUSH TRANSITION TO ADJACENT ASPHALT.
- SP-10** INSTALL CONCRETE WHEELSTOPS AT LOCATIONS SHOWN. SEE DETAIL 12/SD1.
- SP-11** PATCH AND REPAIR EXISTING ASPHALT PAVING FOR INSTALLATION OF NEW CONSTRUCTION SEAL COAT ALL ASPHALT WITH NEW ASPHALTIC LAYER. STRIPE PARKING STALLS AND SYMBOLS AS SHOWN. PAINTS TO BE TRAFFIC WHITE (2 COATS) AT PARKING AND TRAFFIC YELLOW (2 COATS) AT DRIVE-THRU. SEE DETAIL 1/SD1, 2/SD1, & 3/SD1.
- SP-12** REMOVE EXISTING GUARDRAIL FOR INSTALL OF NEW CONSTRUCTION. REINSTALL PER DETAIL 10/SD1. VERIFY REUSE WITH MCDONALD'S CONSTRUCTION MANAGER INSTALL PER MCDONALD'S SPECIFICATIONS. FINISH PER ELEVATIONS. ENSURE 18" CLEAR PROVIDED AT DOOR LATCH.
- SP-13** APPROXIMATE LINE OF NEW CONCRETE WORK TO EXISTING CONCRETE TO REMAIN.
- SP-14** APPROXIMATE LINE OF CONCRETE PAD TO ASPHALT TRANSITION.
- SP-15** NOT USED.
- SP-16** EXISTING DIRECTIONAL SIGN TO REMAIN. PROTECT FROM DAMAGE.
- SP-17** EXISTING FLAG POLE TO REMAIN. VERIFY IF LIGHT FOR FLAG POLE IS MOUNTED TO BUILDING. IF EXISTS, REMOVE & REINSTALL TO ACCOMMODATE NEW CONSTRUCTION.
- SP-18** EXISTING LOT LIGHTING TO REMAIN.
- SP-19** EXISTING BOLLARDS, PROTECT FROM DAMAGE. FINISH PER ELEVATIONS.
- SP-20** EXISTING GAS METER ASSEMBLY TO REMAIN; PROTECT FROM DAMAGE.
- SP-21** EXISTING ELECTRICAL METER ASSEMBLY TO REMAIN; PROTECT FROM DAMAGE.
- SP-22** EXISTING CATCH BASIN TO REMAIN.
- SP-23** RELOCATE EXISTING GREASE INTERCEPTOR TO LOCATION SHOWN; PROTECT FROM DAMAGE. REFER TO CIVIL FOR ADDITIONAL INFORMATION
- SP-24** NOT USED.
- SP-25** EXISTING MANHOLE TO REMAIN.
- SP-26** REMOVE EXISTING EXTERIOR SEATING & INSTALL NEW SEATING PACKAGE BY OWNER THAT PROVIDES 5% MIN ACCESSIBLE SEATS. LOCATE ACCESSIBLE SEATS ON 36" MIN WIDE ACCESSIBLE ROUTE & MARK WITH INTERNATIONAL SYMBOL OF ACCESSIBILITY. PROVIDE 30" WIDE X 19" DEEP X 27" AFF MIN KNEE SPACE & 30" WIDE X 48" DEEP MIN CLEAR FLOOR SPACE AT EACH ACCESSIBLE SEAT. ENSURE 34" AFF MAX TABLE HEIGHT. DISPERSE ACCESSIBLE SEATS AMONG VARIOUS TABLE TYPES & SIZES. PATCH & REPAIR EXISTING CONCRETE PATIO AS NECESSARY & ENSURE MAX 1.5% SLOPE IN ALL DIRECTIONS AT SEATING AREA. REPLACE PATIO AS NECESSARY TO ENSURE SLOPE REQUIREMENTS. PROTECT EXISTING PLANTERS FROM DAMAGE.
- SP-27** EXISTING BIKE RACK TO REMAIN; PROTECT FROM DAMAGE.
- SP-28** NEW TRASH CORRAL AT LOCATION SHOWN. SEE SHEETS T1.0 & T1.1 FOR MORE INFORMATION. PAINT TRASH ENCLOSURE GATES TO MATCH BASE BUILDING COLOR PER ELEVATION SHEETS A2.0 & A2.1
- SP-29** NEW FRONTAGE IMPROVEMENTS PER CIVIL DRAWINGS.

SIGN LEGEND

- (A) "WELCOME" GATEWAY SIGN (1 NEW)
- (B) CUSTOMER ORDER CANOPY (1 NEW)
- (C) DIGITAL MENU BOARD (1 NEW)
- (D) PULL FORWARD STALL SIGNS (2 EXISTING)
- (E) PRE-BROWSE BOARD (1 NEW)
- (F) MOBILE PICKUP STALL SIGNS (2 EXISTING)
- (G) "ANY LANE, ANY TIME" SIGN (1 NEW)

LEGEND

- EXISTING CONCRETE CURBING TO REMAIN/ REPAIR
- NEW CONCRETE CURBING
- EXISTING CONSTRUCTION TO BE DEMOLISHED
- ▨ NEW STRIPING
- ▨ EXISTING LANDSCAPING TO REMAIN
- ▨ NEW LANDSCAPING
- ▨ EXISTING CONCRETE TO REMAIN
- ▨ NEW CONCRETE
- EXISTING LOT LIGHT



GENERAL NOTES

1. PROPOSED UTILITIES ARE SHOWN IN SCHEMATIC ONLY. EXACT LOCATIONS SHALL BE DETERMINED TO ALLOW FOR THE MOST ECONOMICAL INSTALLATION.
2. THE CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES TO DETERMINE EXACT POINT OF SERVICE CONNECTION AT EXISTING UTILITY. REFER TO THE BUILDING ELECTRICAL AND PLUMBING DRAWINGS FOR UTILITY SERVICE ENTRANCE LOCATIONS, SIZES, AND CIRCUITING.
3. FINISH WALK AND CURB ELEVATIONS SHALL BE 6" ABOVE FINISH PAVEMENT.

PAVING SPECIFICATION

(MINIMUM 3" TOTAL COMPACTED ASPHALT THICKNESS)

NOTE: MCDONALD'S ENGINEER RESERVES THE RIGHT TO REQUEST A COMPACTION TEST AND/OR A CORE SAMPLE. IF TESTS PROVE CORRECT, PER ABOVE SPECIFICATION, TESTS WILL BE AT THE EXPENSE OF MCDONALD'S. OTHERWISE, GC WILL BE CHARGED.

LOT LIGHTING RECOMMENDATION

EXISTING LOT LIGHTS TO REMAIN. VERIFY WITH MCDONALD'S CONSTRUCTION MANAGER IF CLEAN/RELAMP OR NO WORK DURING THIS PROJECT.

PARKING INFORMATION

TOTAL SPACES	STALLS	SPACING
29	14	9'-0" X 19'-0" SPACES @ 90°
	13	9'-0" X 15'-1" SPACES @ 60°
	2	ADA 9'-0" X 19'-0" SPACES @ 90°

UTILITY INFORMATION

	SIZE	TYPE	LOCATION
SANITARY SEWER		EXISTING	
WATER		EXISTING	
STORM SEWER		EXISTING	
ELECTRIC		EXISTING	
GAS		EXISTING	

SURVEY INFORMATION

PLAN SCALE: 1" = 20'	
STREET ADDRESS 225 NE NORTON LN	
CITY MCMINNVILLE	STATE OR
COUNTY YAMHILL	

REGIONAL DWG. NO. 36-0160	CORPORATE DWG. NO. 12714
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PREPARED FOR: McDonald's USA, LLC

DRAWN BY: JLT

REVIEWED BY: JLT

DATE ISSUED: AUG 2018

SHEET NO: 36-0160

TITLE: 2018 MRP REMODEL CORE 16

DESCRIPTION: WOOD FRAMING WALLS, WOOD FRAMING ROOF, BRICK WAINGLOT AND LAP SIDING EXTERIOR FINISH

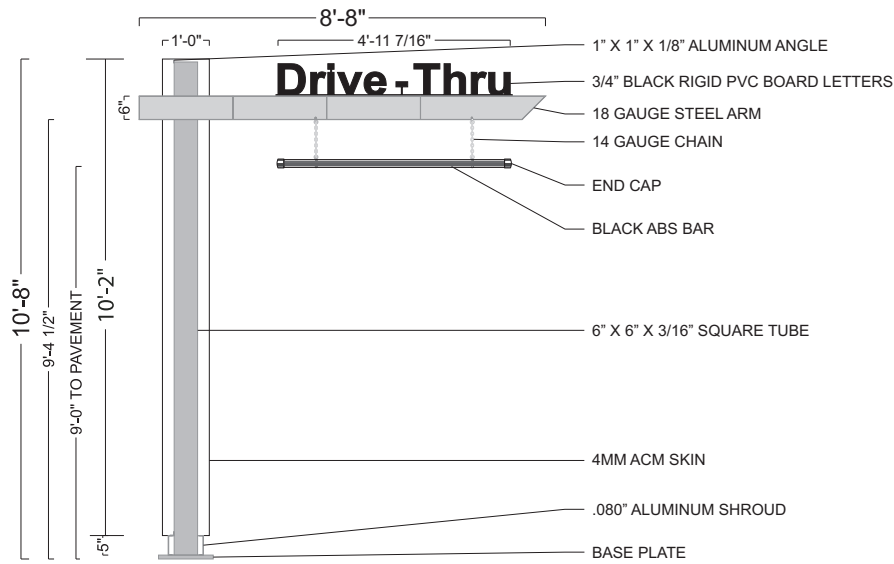
SITE ID: 225 NE NORTON LN MCMINNVILLE, OR, 97128

FREHEIT ARCHITECTURE

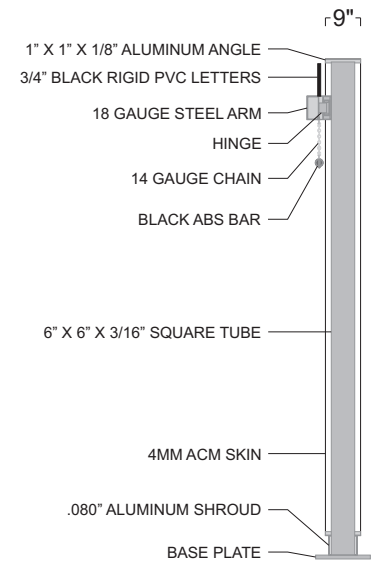
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REVISIONS:

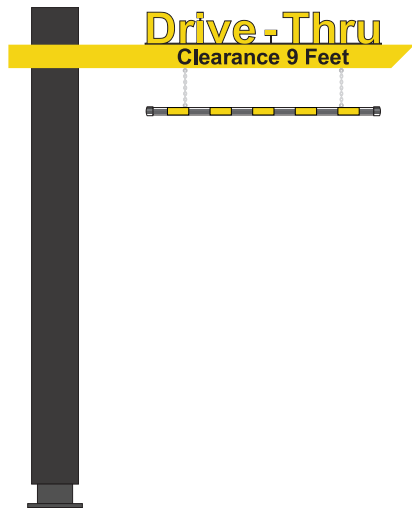
NO.	DATE	DESCRIPTION
2	6/11/2020	685 DRIVE THRU AND KITCHEN OWNER REVISION
1	06/09/2019	000T & CITY FRONTAGE PERMIT SUBMITTAL
	8/28/2018	LEGAL CLEARANCE
		REV. DATE



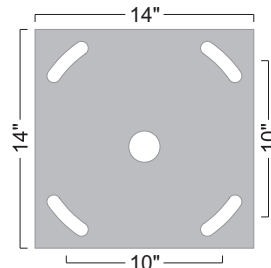
FRONT VIEW
SCALE: 1/4" = 1'-0"



END VIEW
SCALE: 1/4" = 1'-0"



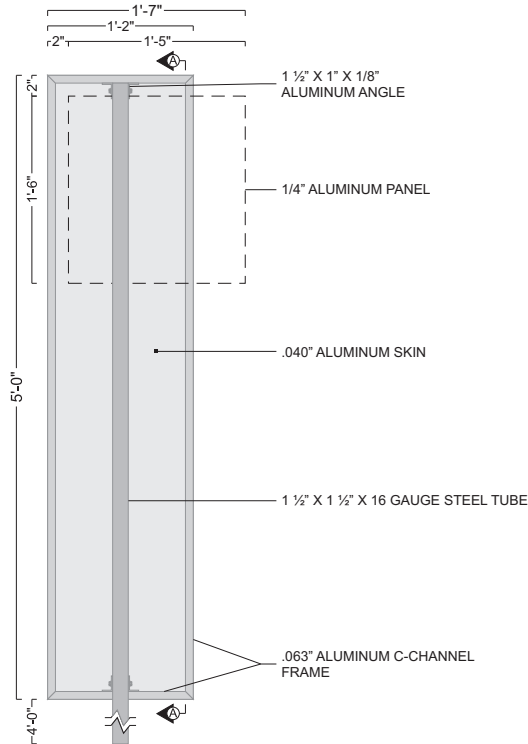
GRAPHIC DETAIL
SCALE: 1/4" = 1'-0"



BASE PLATE DETAIL
SCALE: 1" = 1'-0"

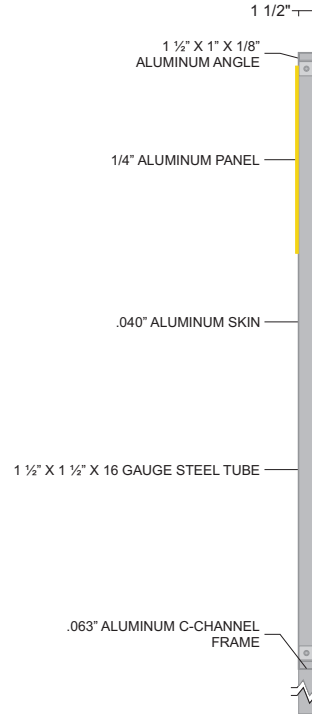
POLE COVER DETAIL
DESIGN FACTOR: TBD
4MM WHITE ACM POLE COVER
.080" ALUMINUM CAP AND SHROUD
EXTERIOR FINISH:
POLE COVER
GRAY FORD-MED DK PLATINUM
SHROUD AND BASE PLATE - MATCH
B.M. 1631 MIDNIGHT OIL
CLEARANCE BAR DETAIL
BLACK ACM PIPE W/ 7725-15 YELLOW
VINYL STRIPES APPLIED TO 1ST
SURFACE
SQUARE FEET: BOXED = 92.44
ACTUAL = 15.68

SWING ARM DETAIL
DESIGN FACTOR: TBD
18 GAUGE STEEL ARM WITH HINGE
W/ 1ST SURFACE PAINT AND VINYL
DECORATION:
MATCH PMS 109 C YELLOW - SWING ARM
7725-12 BLACK VINYL - "CLEARANCE 9 FEET"
COPY
"DRIVE-THRU" LETTER DETAIL
3/4" BLACK RIGID PVC BOARD ROUTED
LETTERS W/ 1ST SURFACE VINYL
DECORATION:
7725-15 BRIGHT YELLOW - "DRIVE-
THRU" COPY
BLACK - COPY OUTLINE



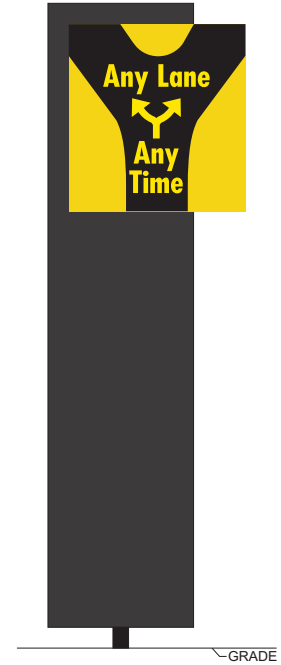
FRAME DETAIL
SCALE: 1" = 1'-0"

FRAME DETAIL
DESIGN FACTOR: TBD
1 1/2" X 3/4" X .063" ALUMINUM C-CHANNEL FRAME
.040" ALUMINUM SKINS
EXTERIOR FINISH: BONE WHITE
SQUARE FOOTAGE: BOXED = 7.92, ACTUAL = 6.46



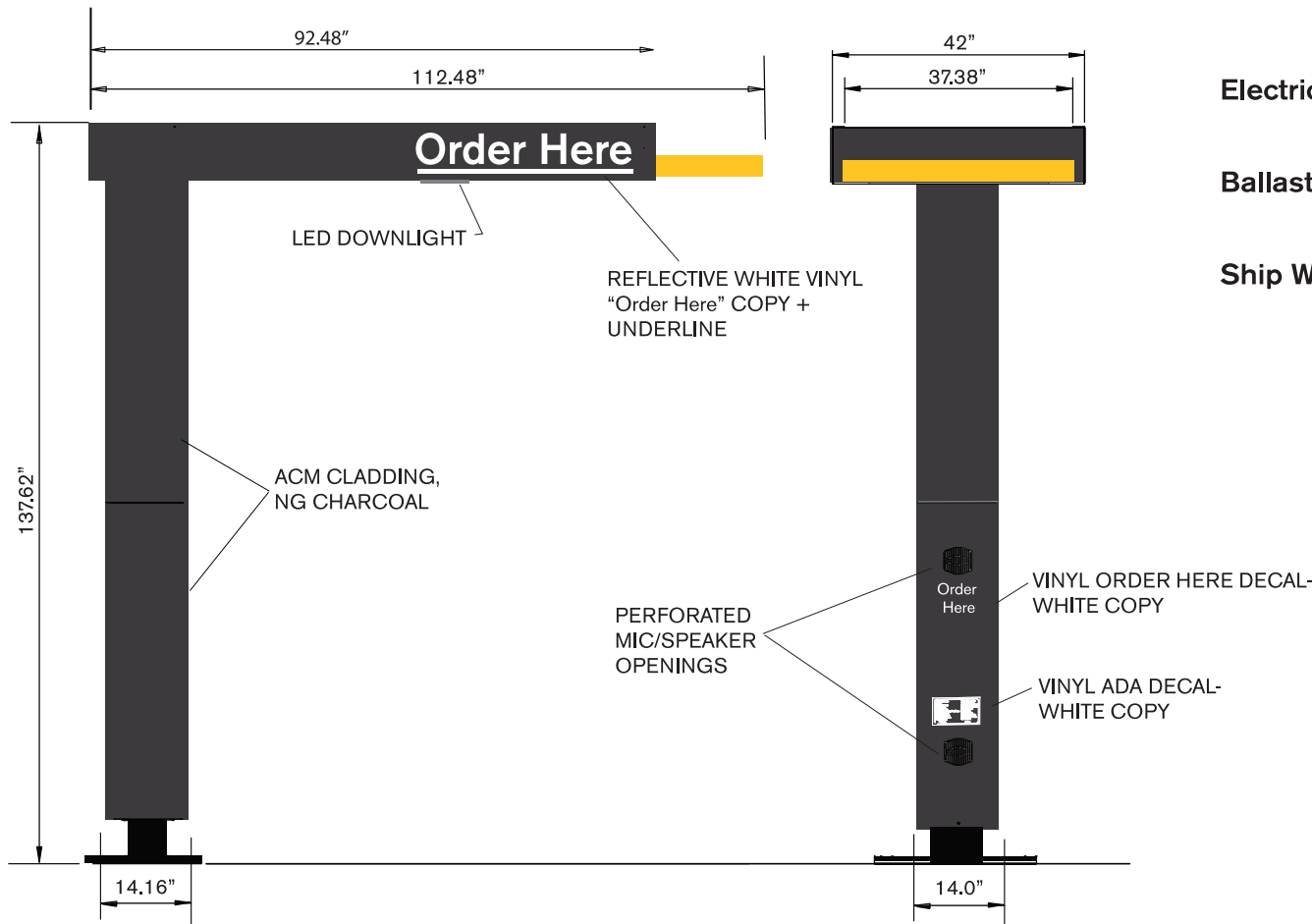
CROSS SECTION A-A
SCALE: 1" = 1'-0"

FACE DETAIL
1/4" ALUMINUM PANEL W/ 1ST SURFACE PAINT
& VINYL DECO.:
■ YELLOW DUPONT CENTARI 5000- BACK-
GROUND, COPY & ARROW
■ 3630-22 BLACK - LANES



GRAPHIC DETAIL
SCALE: 1" = 1'-0"

Slim Springboard Canopy w/Audio Only - Yellow



Illumination: LED Downlighting

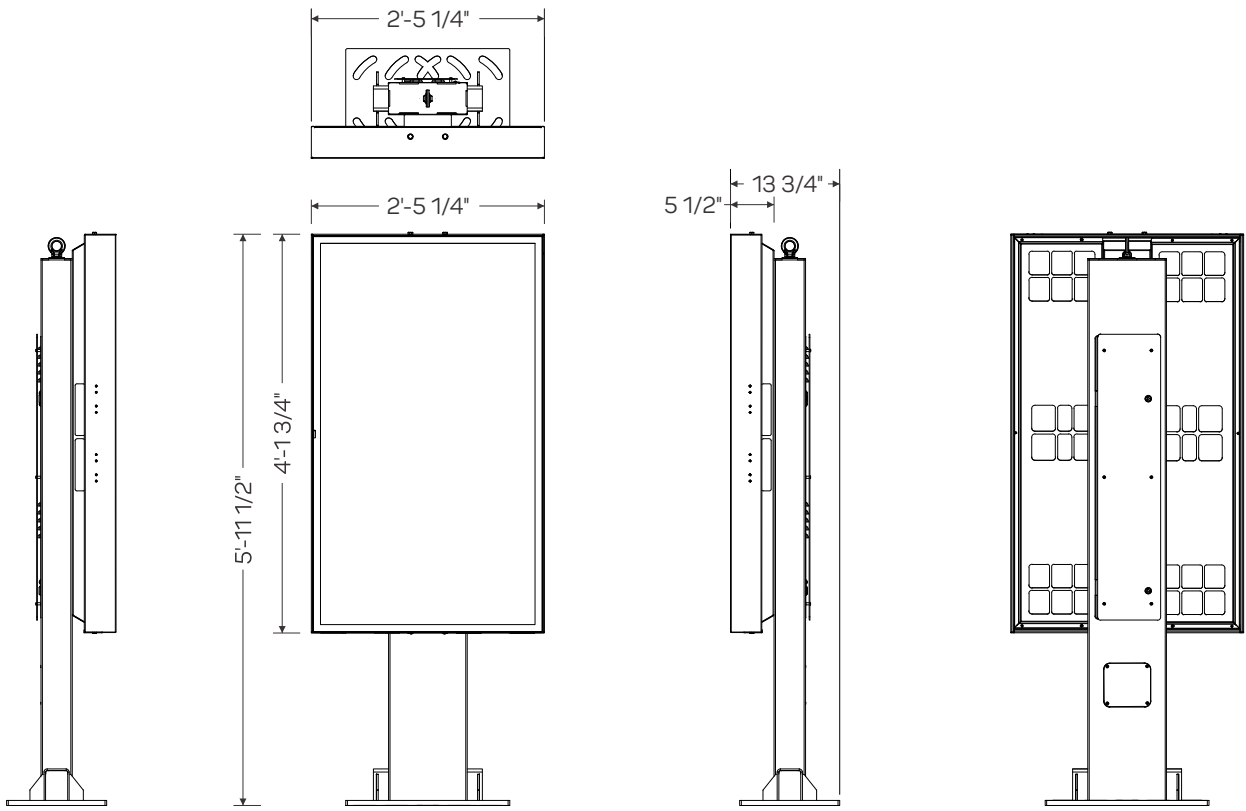
Electrical: 1.5 Amps 120 volt, 60 Hz

Ballast:

Ship Weight:

Frame	Hot dip galvanized + anti-graffiti powder coated steel
Brackets	Hot dip galvanized
Panels	Aluminium + anti-graffiti powdercoat
Access fasteners	Security Torx
Media player access	Dual camlock
Eyebolt	Stainless crane on
Baseplate	McDonalds spec triple mounting pattern option

ODMB 02 SINGLE	
Displays	Samsung OH55F
Hardware	Stratacache Spectra NG
Heating/Cooling	Watlow 100W Heater Sunon 120mm AC Fan
Power Supply Units	60W DC Media Player Power Supply
Power Cables	1 x IEC Power Cable
Electrical Components	Isolated Ground 2 x IG Receptacles 20A Circuit Breaker
Communication Cables	2 x HDMI 1 x RS232
Certification	UL Certified



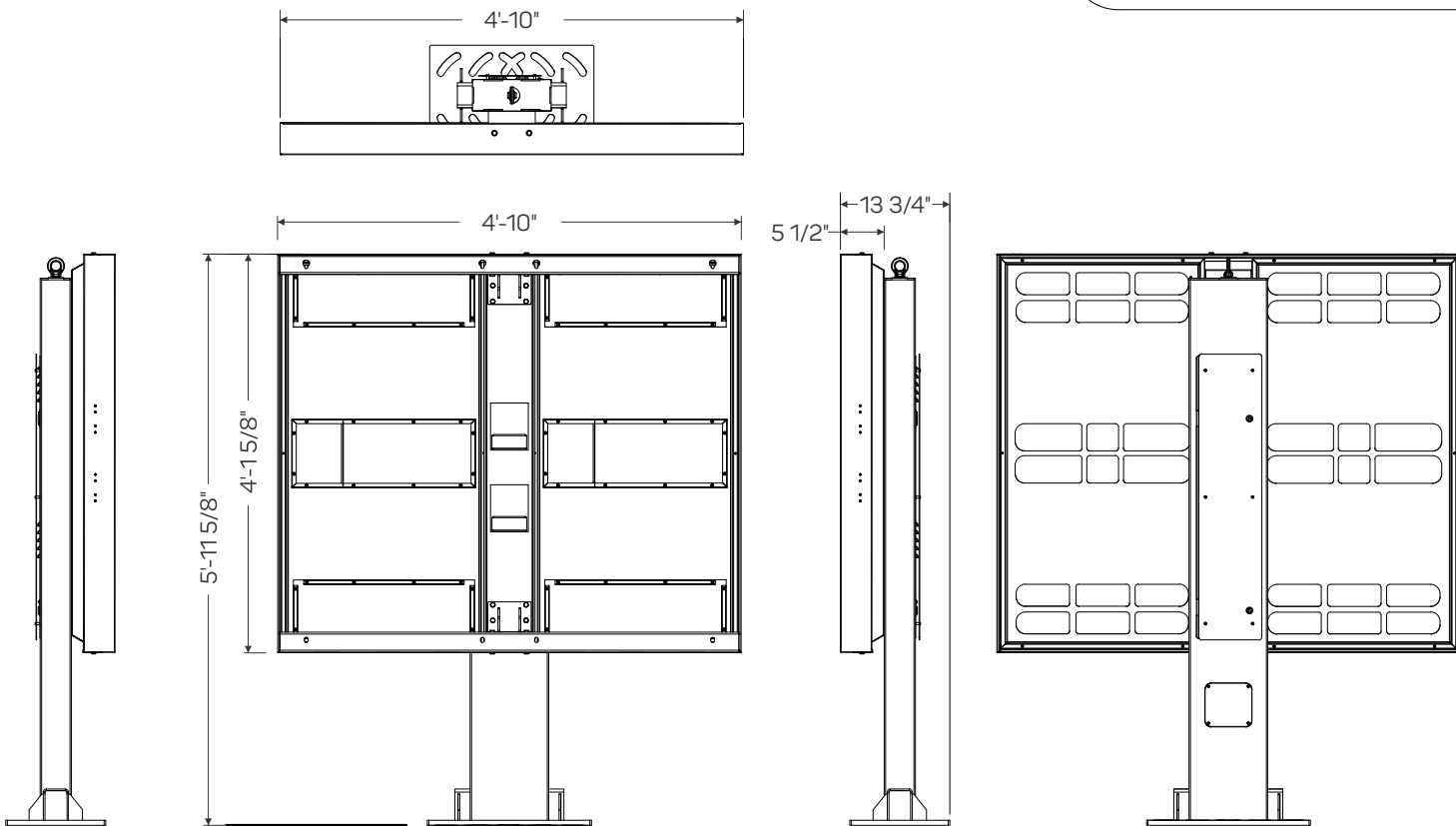
SCALE: 1/2" = 1'-0"



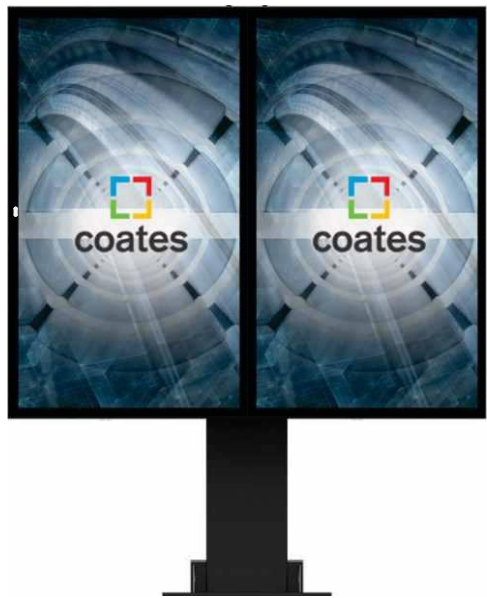
GRAPHIC DETAIL NOT TO SCALE

Frame	Hot dip galvanized + anti-graffiti powder coated steel
Brackets	Hot dip galvanized
Panels	Aluminium + anti-graffiti powdercoat
Access fasteners	Security Torx
Media player access	Dual camlock
Eyebolt	Stainless crane on
Baseplate	McDonalds spec triple mounting pattern option

ODMB 02 DOUBLE	
Displays	2 x Samsung OH55F
Hardware	2 x Stratacache Spectra NG
Heating/Cooling	Watlow 100W Heater Sunon 120mm AC Fan
Power Supply Units	2 x 60W DC Media Player Power Supply
Power Cables	2 x IEC Power Cables
Electrical Components	Isolated Ground 2 x IG Receptacles 20A Circuit Breaker
Communication Cables	4 x HDMI 2 x RS232
Certification	UL Certified



SCALE: 1/2" = 1'-0"



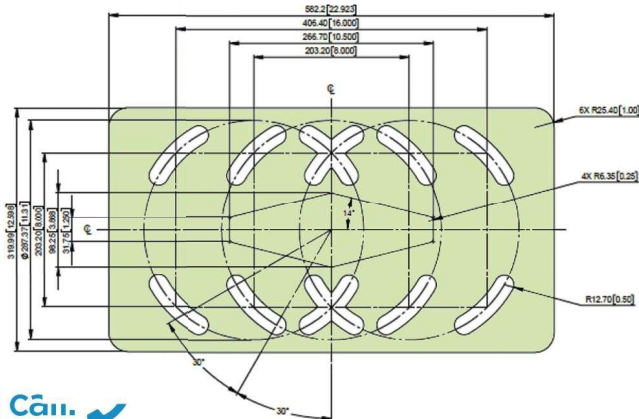
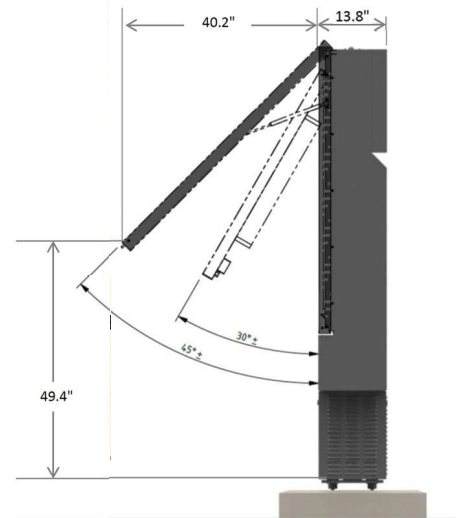
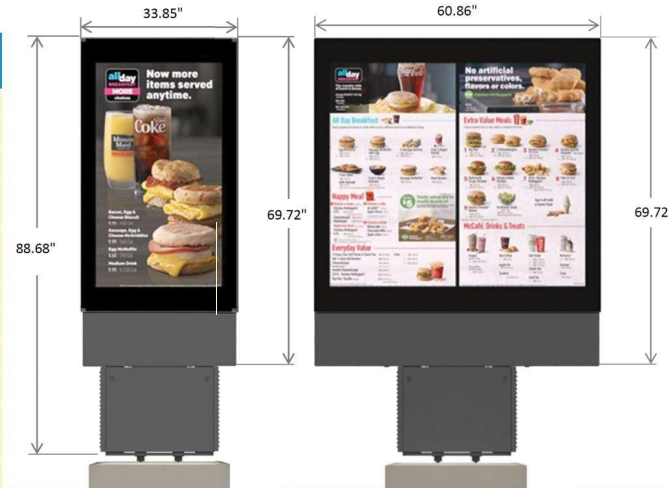
GRAPHIC DETAIL NOT TO SCALE

Customer:	Date:	Prepared By:	<p><small>Note: Color output may not be exact when viewing or printing this drawing. All colors used are PMS or the closest CMYK equivalent. If these colors are incorrect, please provide the correct PMS match and a revision to this drawing will be made.</small></p>
Location:	File Name:	Eng:	

The Elevate 55P Series Digital Menu Boards are designed to fully support outdoor rugged sunlight readable applications; a total solution that is 100% site-serviceable and out-of-box ready for immediate deployment. They offer affordability and best-in-class performance for your drive-thru menu board and other all weather exposed digital media applications. Best-in-class reliability with proven in-field installations exceeding 6 years of service.



	MODEL	Single Screen Presell (PN#: G1S-55-S)	Dual Screen Menu Board (PN#: G1S-55-D)	
Description	Configuration	1 x 1 Single Panel	1 x 2 Double Panel	
LCD Panel	Panel Size	1 x 55" diag	2 x 55" diag	
	Orientation	Portrait		
	Native Resolution (W x H)	1080 x 1920	2160 x 1920	
	Brightness (Typical)	3000/3500 cd/m ²		
	Contrast Ratio	1300:1		
	LCD Technology	IPS RGB+W with QLP - polarized eyewear compatible		
	Viewing Angle	178° x 178°		
	External Control	Yes - RS232C		
	Connectivity	Content	ActiVia for Media 4.0 - Select 3rd Party CMS Supported	
		Data Access	CAT 6 Standard Optional Wi-Fi / 4GLTE	
Audio	Speakers	Optional		
	Microphone	Optional		
Enclosure	Thermal Management	Direct Air Cooling System (DACs) US Patent# 8472174 B2		
	Cover Glass	Proprietary AR treated tempered safety glass (UL48 Listed)		
	Ingress Protection	IP56 - NEMA 4		
	Mount Design	Universal Base-Plate - flexible mounting options		
	Accessibility	Front access via Security Compression Latches		
Special Features	Dimensions (inches HxWxD)	88.68 x 33.85 x 13.8	88.68 x 60.86 x 13.8	
	Net Weight	600 lbs	850 lbs	
	Ambient Light Sensor	Yes - Auto Backlight Brightness Dimming (min. 300 NIT for night sky conditions)		
Power	Field Serviceability	All Components Fully Field Serviceable		
	Easy Access Module Swap	Yes - Front access door - Gas strut supported.		
	Remote Monitoring	Remote monitoring, diagnostics and control		
	Input Power	120/240V 50/60Hz		
Approval	Consumption (Typical/Max)	320W/500W	520W/800W	
	Cable Access	From bottom via sealed gland in free standing baseplate		
Environmental	Termination	Circuit breaker / Surge Suppression / EMI Filter		
	Safety	JL, cUL, UL48, TUV		
Approval	EMC	FCC Class "A" CE Mark		
	Operating Temperature	-22°F to +122°F Under full solar exposed conditions		
	Wind Load Rating	Per Florida Building Code IBC-20122 to 180 mph		
Approval	Operating Humidity	10% to 100% RH		



📞 For more information call. 📞

USA & Canada - +1.800.244.8915 Ext. 296 AUS - +61.8.8152.0455
EU - +44.20.3170.5543 India - +91.80.4623.0000



October 25, 2018

Freiheit Architecture
Attn: Matt Grinnell, Permit & Entitlement Coordinator
929 108th Ave NE, Suite 210
Bellevue, WA 98004

Re: McDonald's McMinnville, OR
225 NE Norton LN
McMinnville, OR
DEI Project #: 18-1351

Dear Mr. Grinnell:

Dibble Engineers, Inc. (DEI) has completed a review of the foundations for the Digital Menu Board, Digital Pre-Sell Board, Order Canopy, and Single Gateway Sign foundations proposed for installation at the McDonald's location above.

DEI calculated the wind and seismic loading for the signs based on weights and dimensions provided by Freiheit Architecture. Based on our review and calculations, the sign foundations are acceptable for installation with the dimensions, reinforcing, and sign anchorage shown on the attached redlined detail sheets DT1.1 and DT1.2. Note that the signs are proprietary designed, pre-manufactured units, and our scope did not include review of the signs themselves.

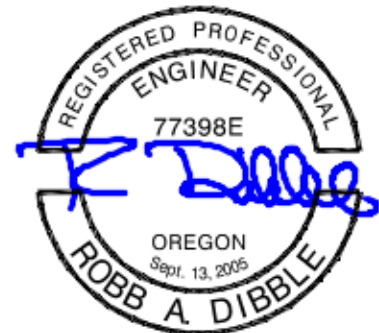
Supplemental structural calculations for the sign foundations and sign anchorage are attached with this letter.

Please contact us with any questions, and we will be happy to assist.

Sincerely,
DIBBLE ENGINEERS, INC.

TJ Hamad
Design Engineer
TJ@dibbleengineers.com

Attachments: Redlined Detail Sheets DT1.1 & DT1.2, Structural Calculations

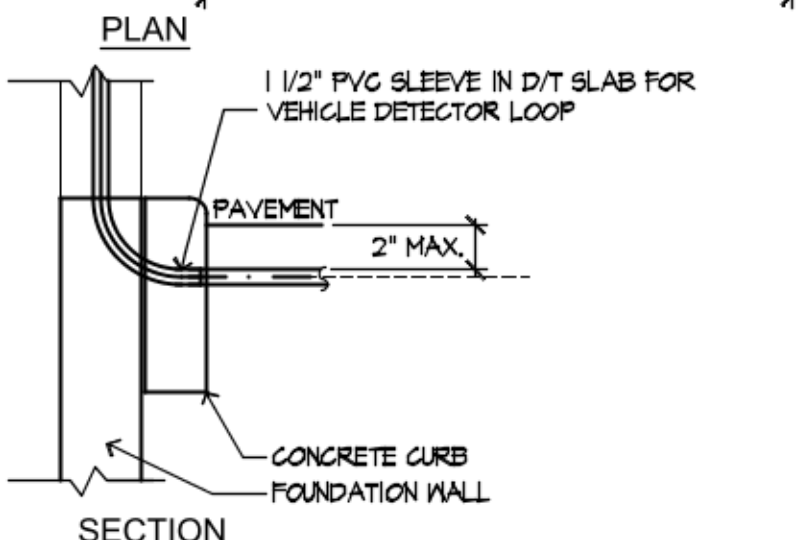
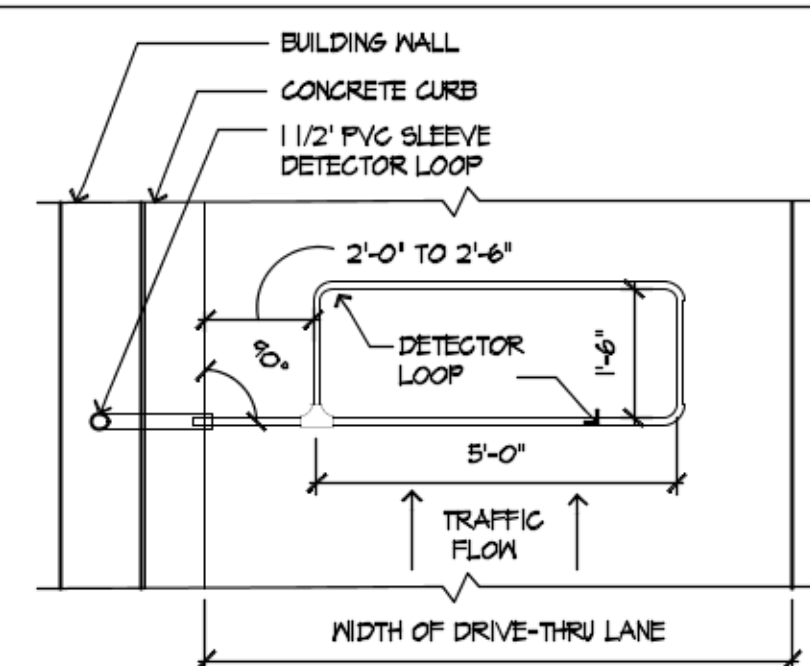


EXPIRES: DECEMBER 31, 2021

07/30/2020

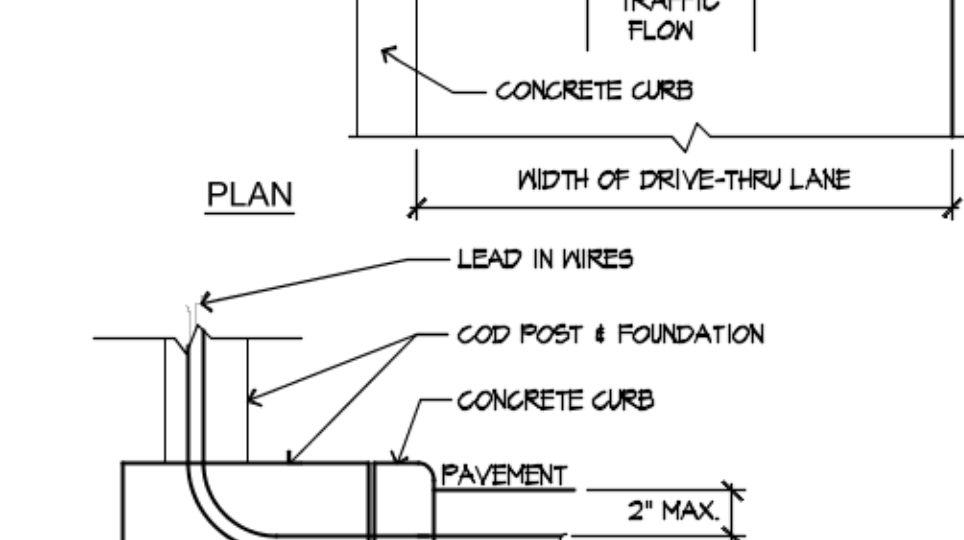
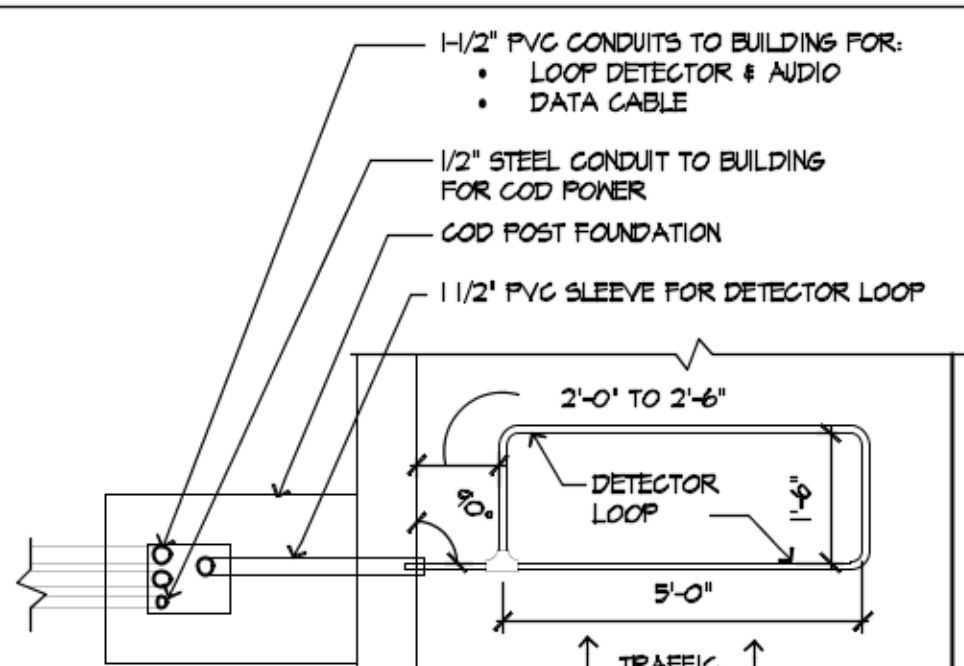
NOTES

- VERIFY CONDUIT SIZES AND LAYOUT WITH DETECTOR LOOP MANUFACTURER.
- CENTER VEHICLE DETECTOR LOOP IN DRIVE THRU LANE. INSTALL PER MFR. RECOMMENDATIONS.
- SEE SHEET DT1.0 FOR DIMENSIONS OF DRIVE-THRU LANE CONCRETE PAD FOR DETECTOR LOOP.
- NO STEEL (REBAR OR ELECTRICAL WIRE) SHALL BE USED WITHIN 2' OF LOOP. DETECTOR LOOP MANUFACTURERS: DETECTOR LOOPS MAY BE BY ONE OF THE FOLLOWING COMPANIES OR EQUAL.
- SM: 1-800-828-0033
HME: 1-800-848-4468



NOTE: DETECTOR LOOPS AT PRESENTATION AND ORDER WINDOWS ARE OPTIONAL. G.C. TO INSTALL PVC SLEEVES TO FACILITATE POSSIBLE FUTURE LOOP INSTALLATION.

B - DT WINDOW DETECTOR LOOP

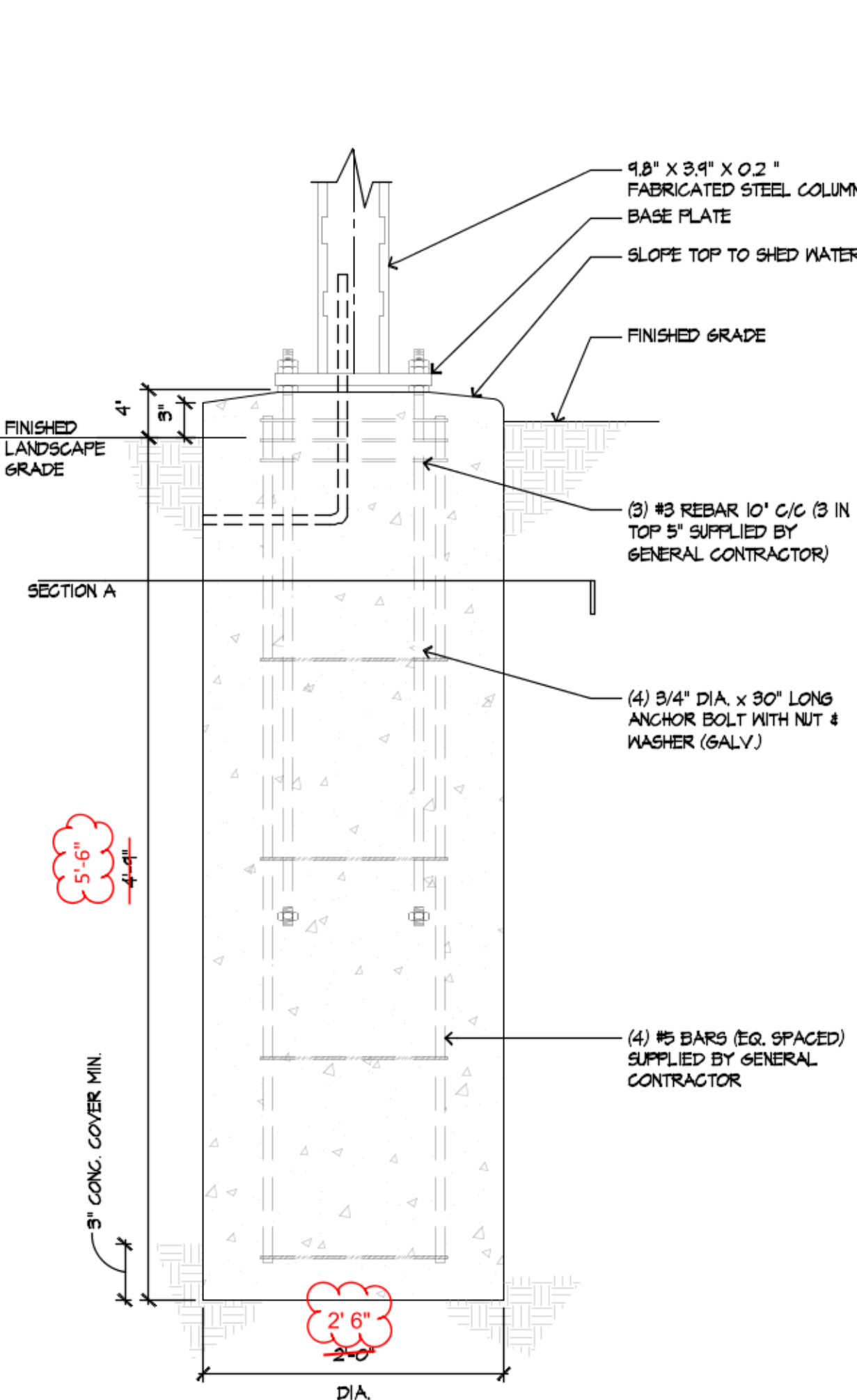


NOTE: DETECTOR LOOPS AT PRESENTATION AND ORDER WINDOWS ARE OPTIONAL. G.C. TO INSTALL PVC SLEEVES TO FACILITATE POSSIBLE FUTURE LOOP INSTALLATION.

A - C.O.D. DETECTOR LOOP

NOTES:

-TOP OF PIERS SHALL BE SLOPED SUCH THAT MOISTURE CANNOT ACCUMULATE.

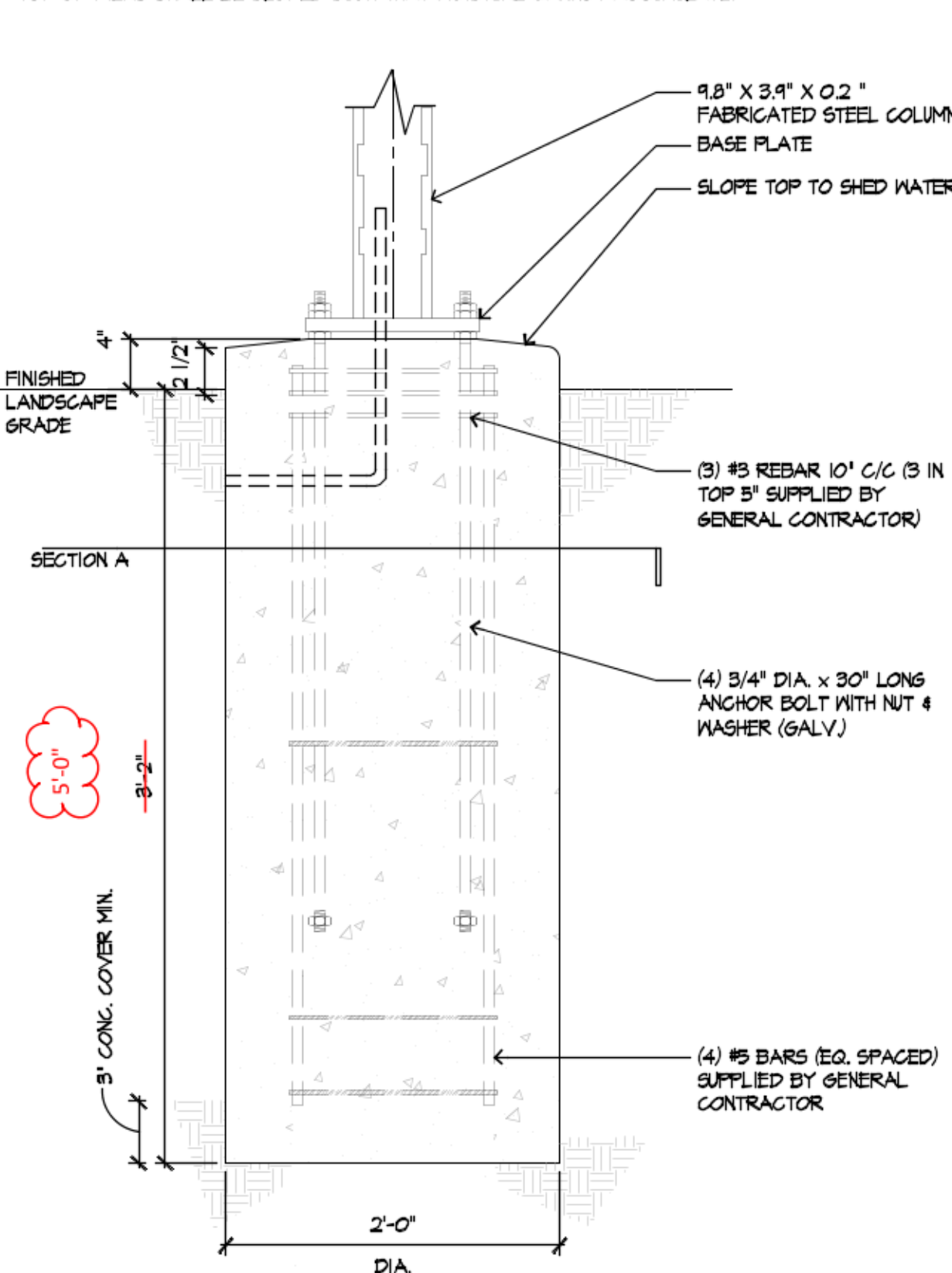


10 DT1.1 DIGITAL MENU BOARD

SCALE: NOT TO SCALE

NOTES:

-TOP OF PIERS SHALL BE SLOPED SUCH THAT MOISTURE CANNOT ACCUMULATE.

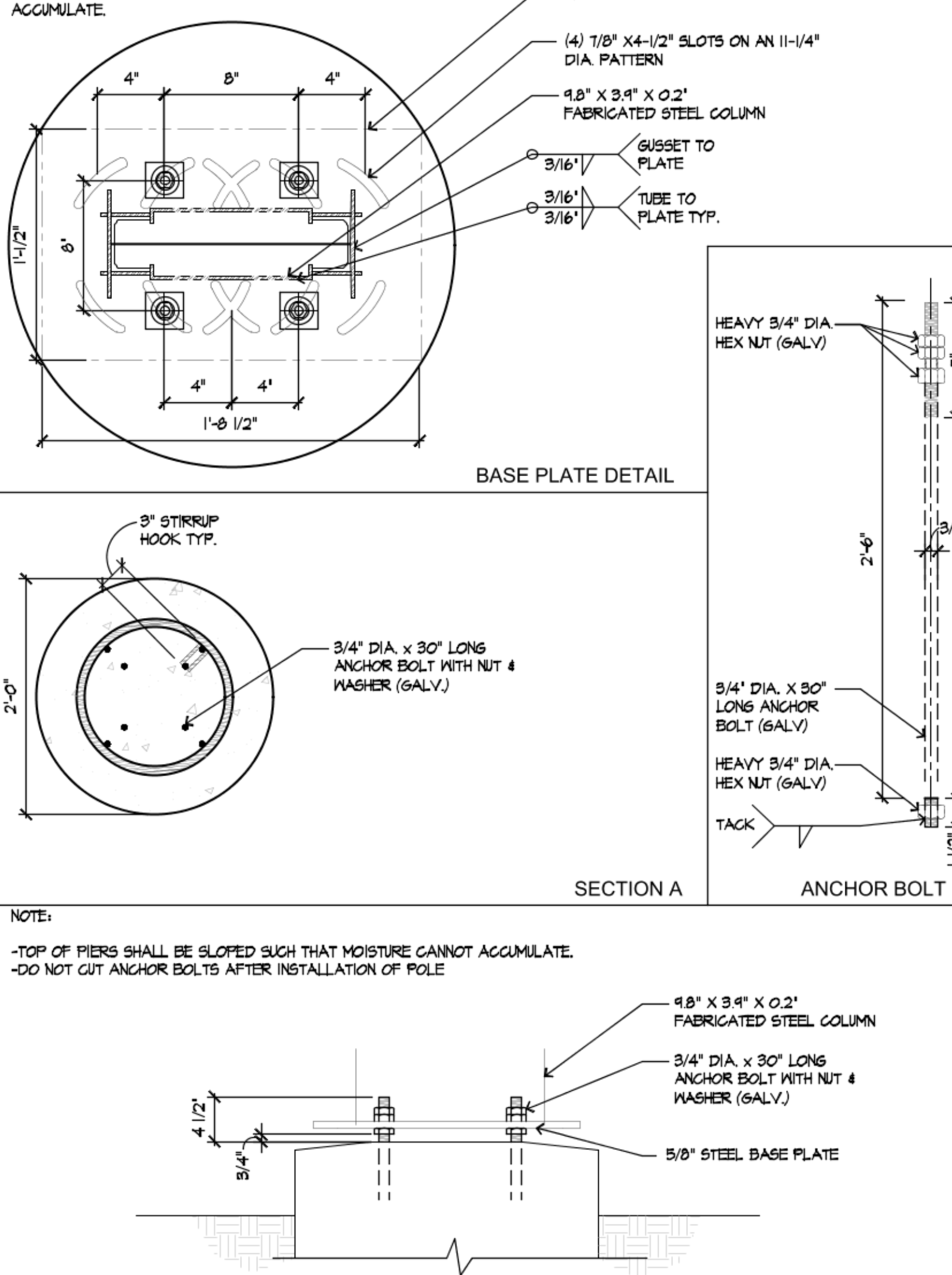


12 DT1.1 DIGITAL PRE-BROWSE BOARD

SCALE: NOT TO SCALE

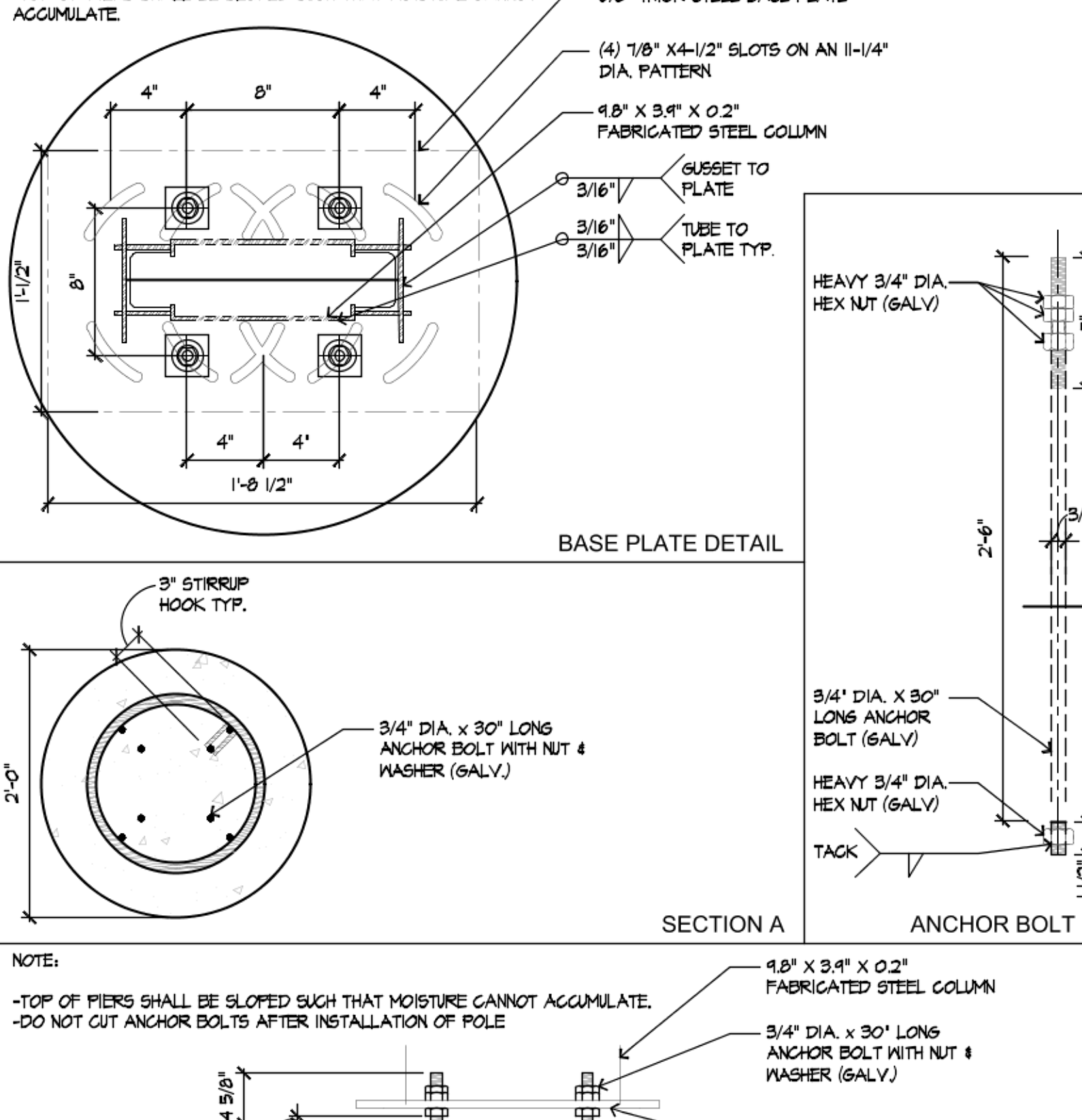
NOTES:

-TOP OF PIERS SHALL BE SLOPED SUCH THAT MOISTURE CANNOT ACCUMULATE.



NOTES:

-TOP OF PIERS SHALL BE SLOPED SUCH THAT MOISTURE CANNOT ACCUMULATE.

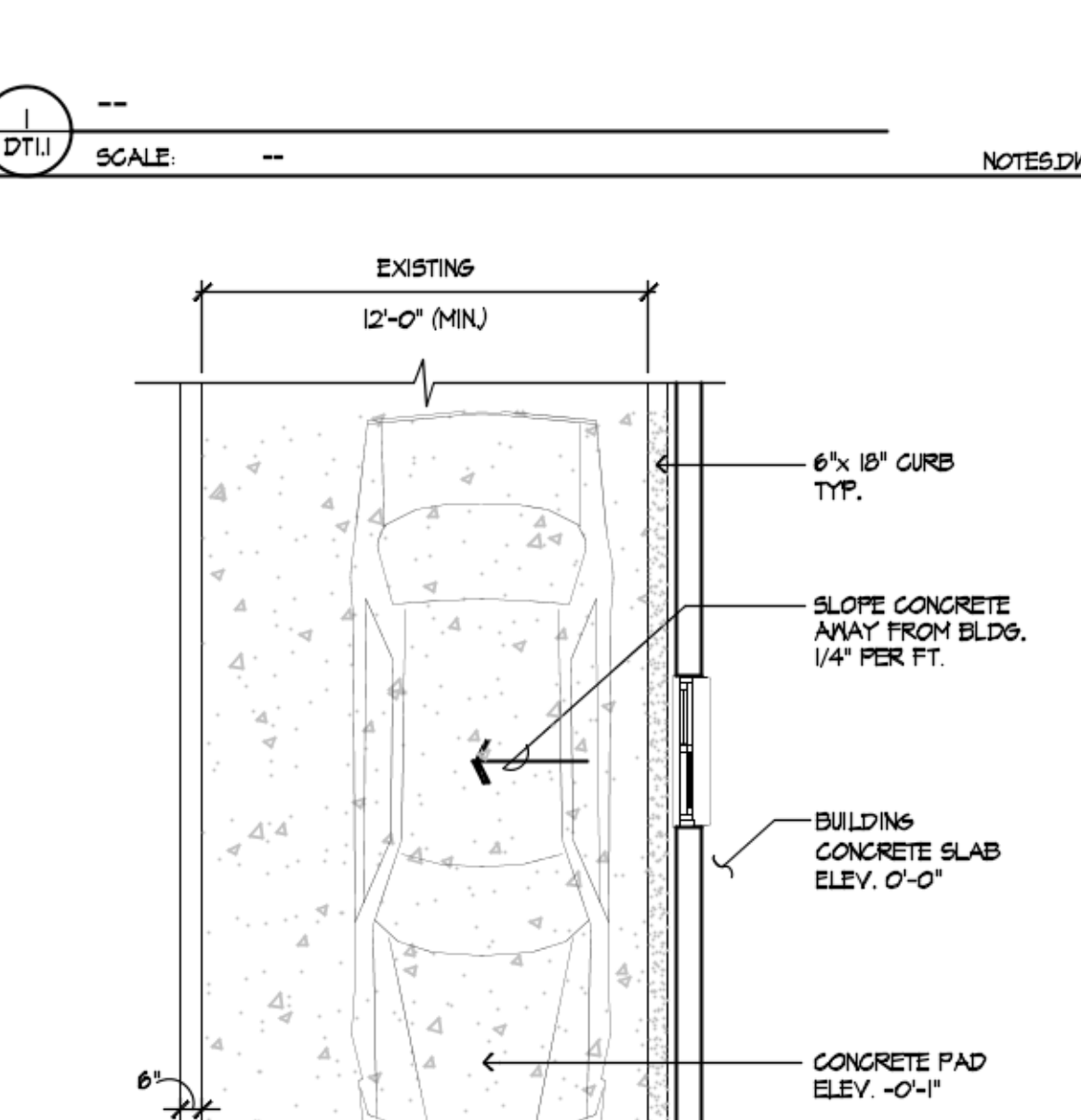


4 DT1.1 6" X 18" CURB SECTION

SCALE: 1/4" = 1'-0"

NOTES:

- G.C. TO COORDINATE THE RESPONSIBILITIES OF THE ELECTRICAL CONTRACTOR AND THE SIGN SUPPLIER.
- GENERAL CONTRACTOR TO INSTALL PRE-FORMED, PRE-WIRED VEHICLE DETECTOR LOOP.
- #2 GREEN GROUNDING CONDUCTOR MUST BE RUN WITH CIRCUIT CONDUCTORS FROM GROUND BUS IN PANEL LP TO ALL MENU BOARDS AND REFERENCE BOARDS.
- ALL UNDERGROUND CONDUIT FOR AUDIO TO BE P.V.C. NO METAL ALLOWED.
- ALL GALVANIZED ANCHOR BOLTS TO BE SUPPLIED BY THE CONTRACTOR.

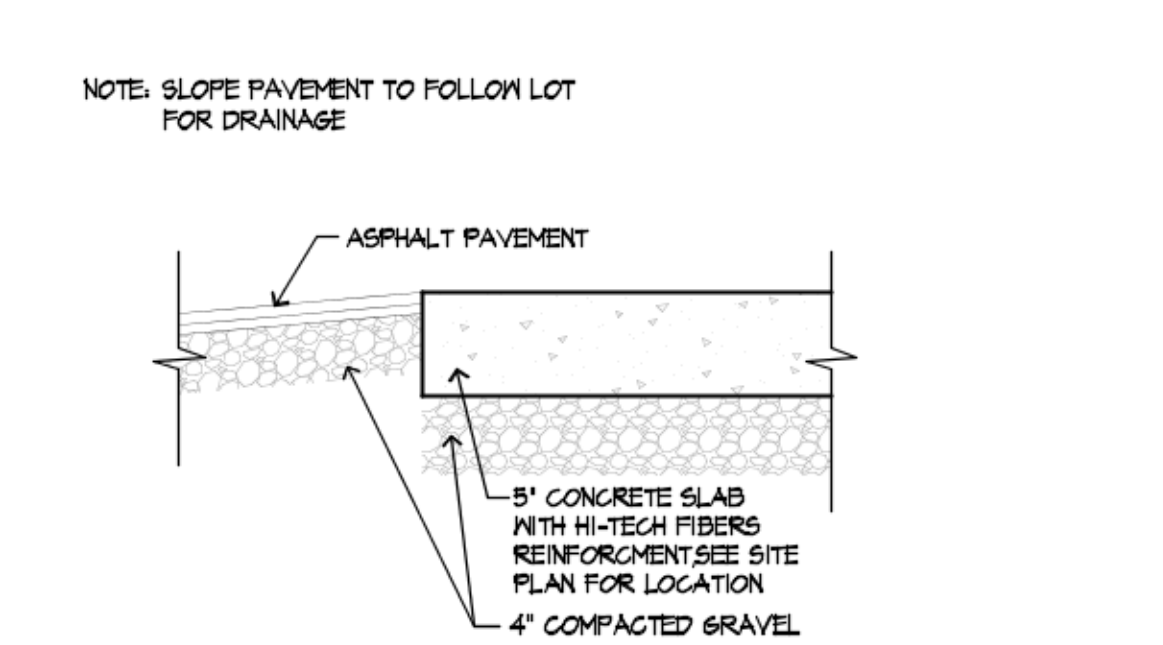


2 DT1.1 D/T CONCRETE PAD

SCALE: 1/4" = 1'-0"

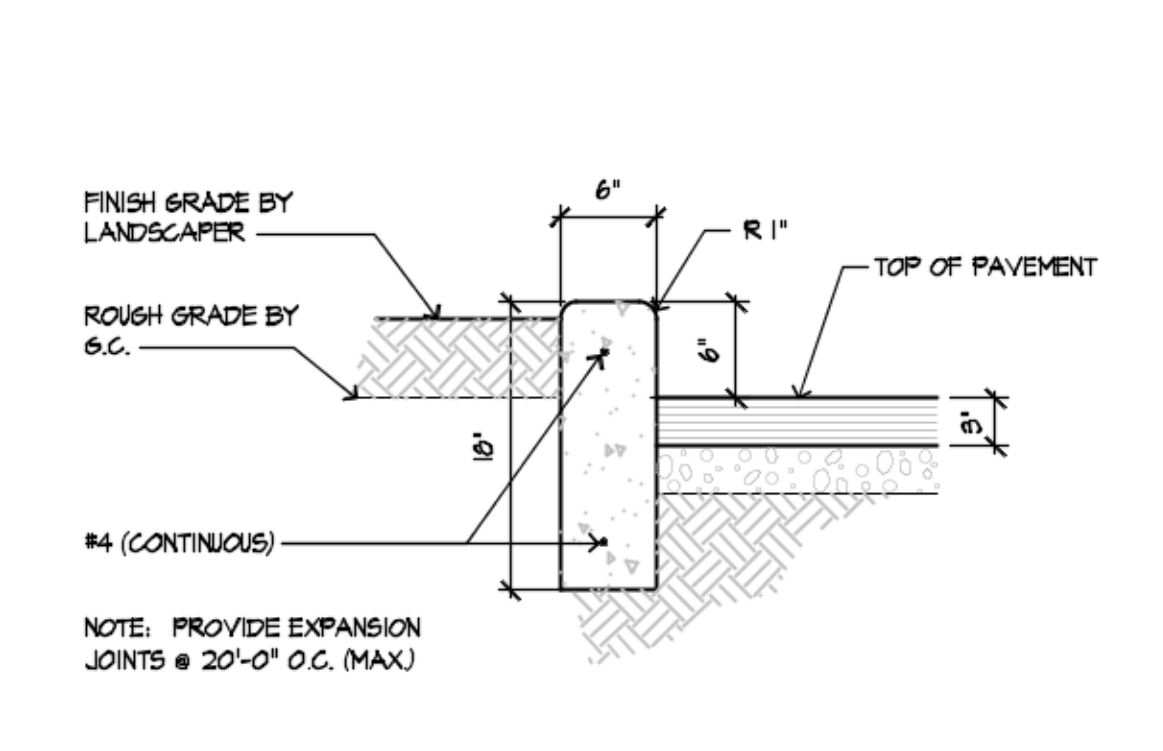
NOTES:

NOTE: SLOPE PAVEMENT TO FOLLOW LOT FOR DRAINAGE



3 DT1.1 DRIVE THRU PAD DETAIL

SCALE: 1/4" = 1'-0"



4 DT1.1 6" X 18" CURB SECTION

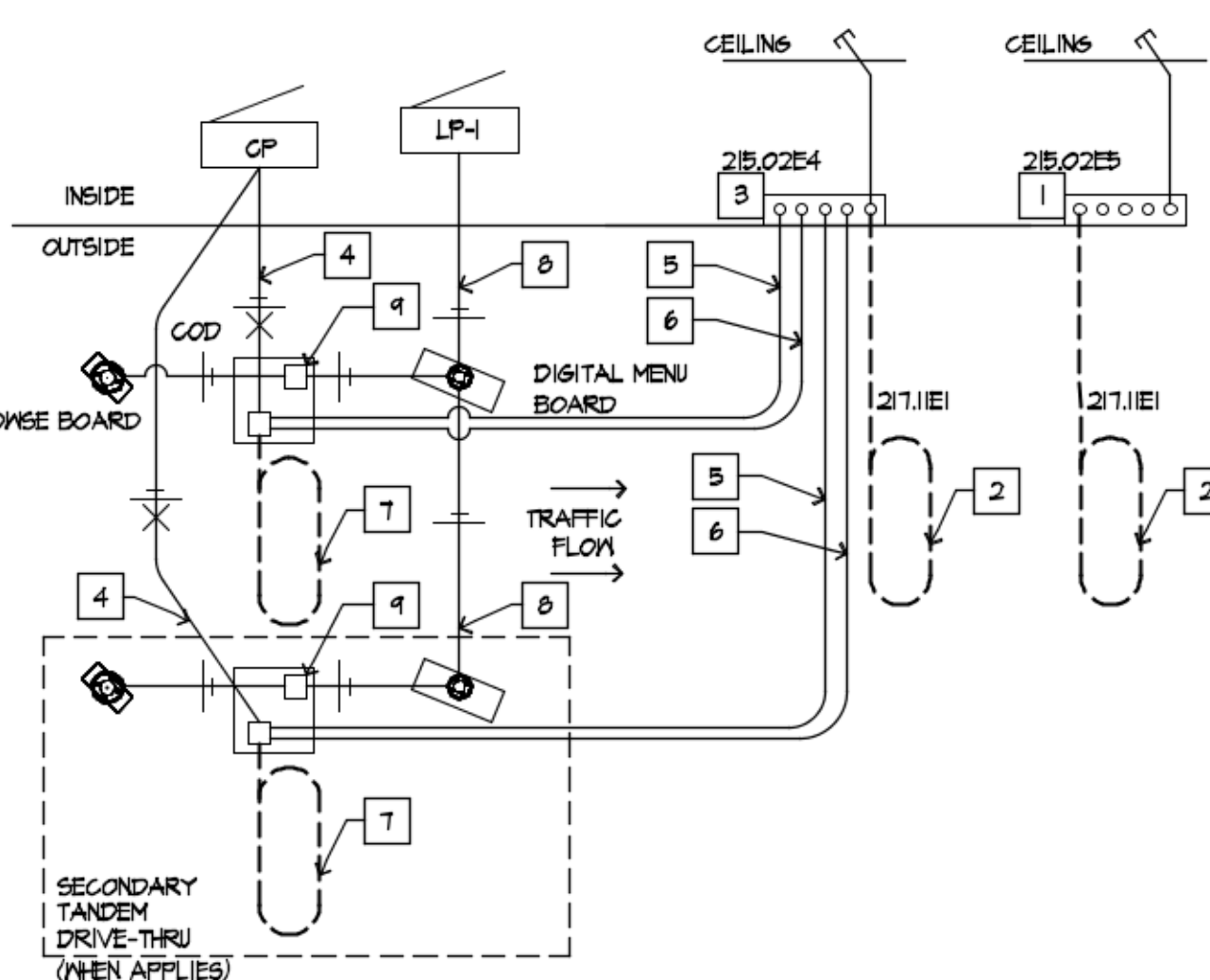
SCALE: 1/4" = 1'-0"

18 DT1.1 DETECTOR LOOP DETAIL

SCALE: NOT TO SCALE

KEYNOTES

- ELECTRICAL EQUIPMENT ITEM #215.02E5 IN PRESENTATION BOOTH. PROVIDE 2" HOLE & GROMMET IN JUNCTION BOX COVER PLATE FOR POS DATA CABLES. PROVIDE 2 1/2" ABOVE CEILING.
- OPTIONAL DRIVE-THRU WINDOW DETECTOR LOOP FOR ORDER & PRESENTATION WINDOWS - SEE 18/DT1.1. G.C. TO INSTALL PVC SLEEVES TO FACILITATE POSSIBLE FUTURE LOOP INSTALLATION.
- ELECTRICAL EQUIPMENT ITEM #215.02E4 IN ORDER/CASH BOOTH. PROVIDE 2" HOLE & GROMMET IN JUNCTION BOX COVER PLATE FOR POS DATA CABLES. PROVIDE 2 1/2" ABOVE CEILING.
- PROVIDE STEEL CONDUIT AND CONDUCTOR PER POS & COD ISOLATED GROUND/ DEDICATED CIRCUIT DETAIL. 1 1/2" C-2#12 & #12 GROUND & #12 ISOLATED GROUND. PROVIDE 1 CB PER COD.
- 1 1/2" PVC CONDUIT FOR REMOTE LOOP DETECTOR & COD AUDIO CABLES. EC TO PROVIDE 3 PULL STRINGS INSIDE CONDUIT.
- 1 1/2" FOR COD DATA CABLES. EC TO PROVIDE 3 PULL STRINGS INSIDE CONDUIT.
- CONDUIT FOR COD LOOP DETECTOR, ELECTRICAL EQUIPMENT ITEM #211.1E1 - SEE 10/DT1.1.
- 1 1/2" C-2#12 POWER FOR MENU BOARD ILLUMINATION.
- 'ORDER HERE' CANOPY LIGHTS.



20 DT1.1 DRIVE-THRU SITE WIRING DIAGRAM

SCALE: NOT TO SCALE

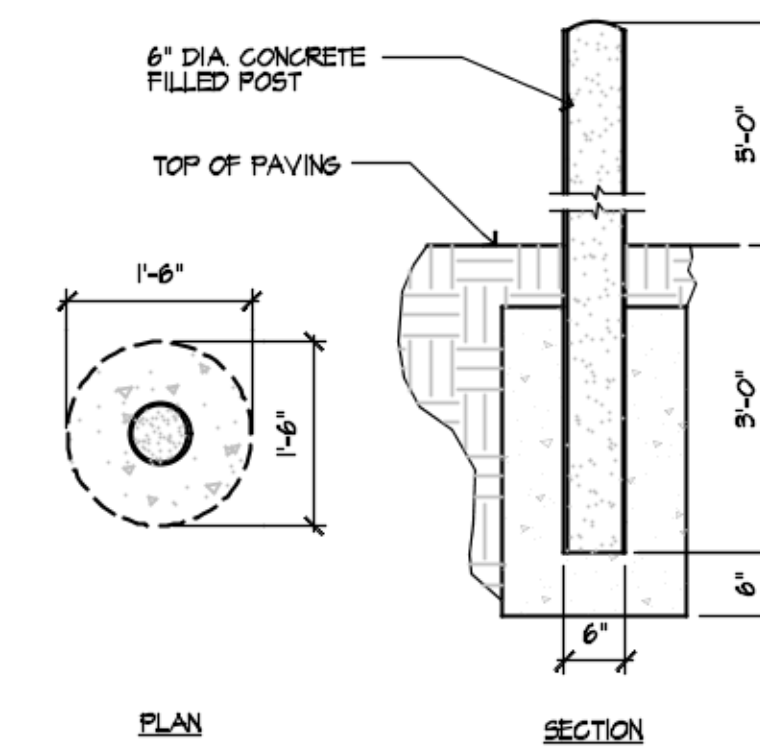
SITE_WIRING.DWG

PREPARED FOR	McDonald's USA, LLC	
PREPARED BY	McDonald's USA, LLC	
DRAWN BY		
STD. ISSUE DATE		
REVIEWED BY		
DATE ISSUED		
TITLE	2018 MRP REMODEL CORE 16	
DESCRIPTION		
SHEET NO.	DT1.1	
DRIVE THRU DTLs		
REV.	DATE	DESCRIPTION
FIC	BY	



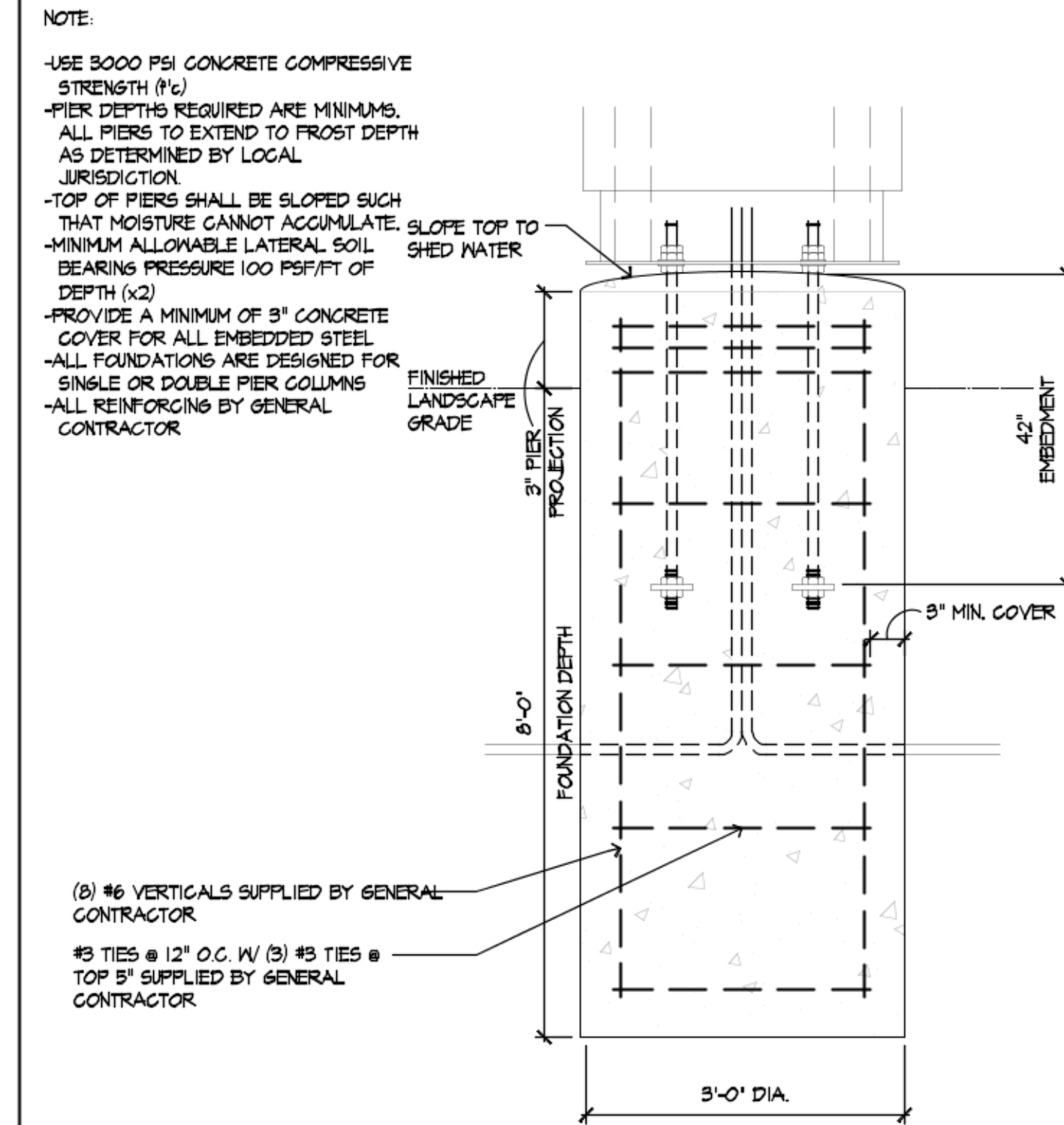
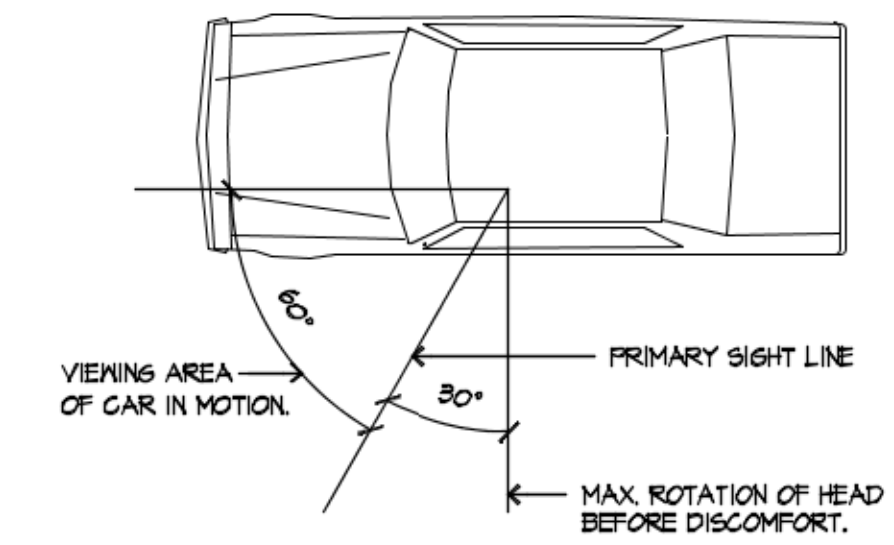
5 D/T POST DETAIL

DT1.2 SCALE: NOT TO SCALE DT_POST.DWG



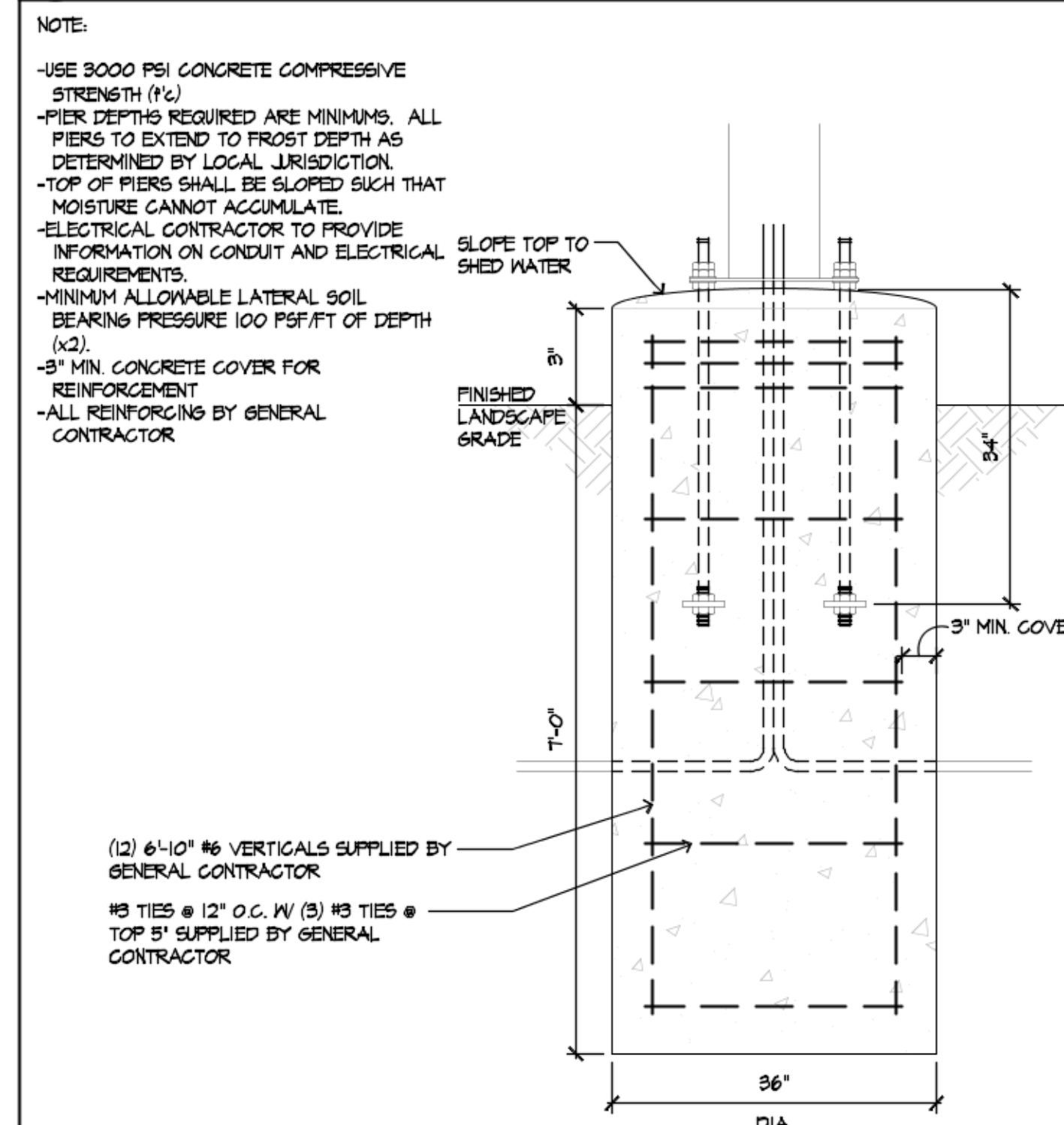
1 VEHICLE SIGHT LINES

DT1.2 SCALE: NOT TO SCALE VEHICLE_SIGHT_LINES.DWG



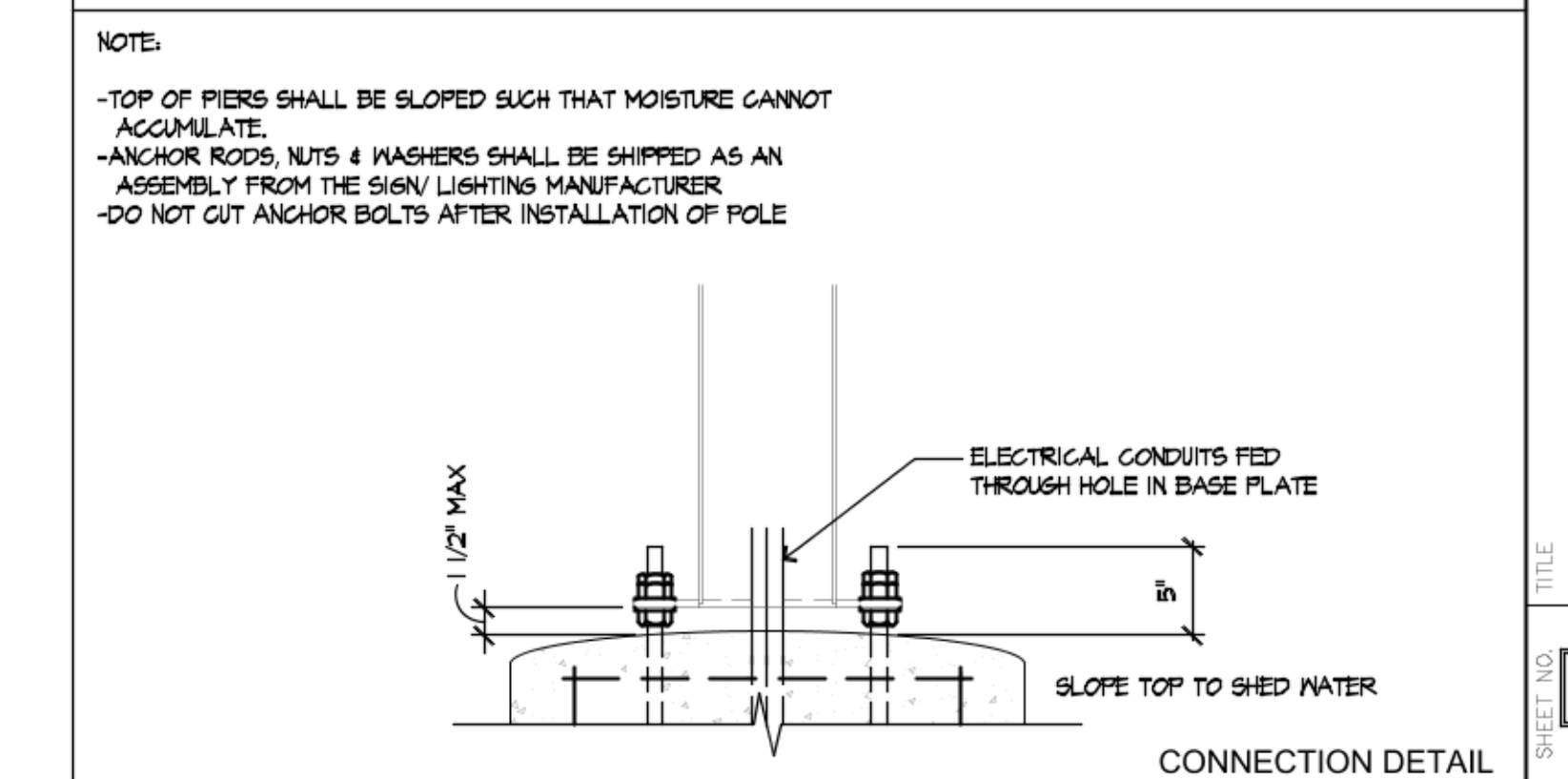
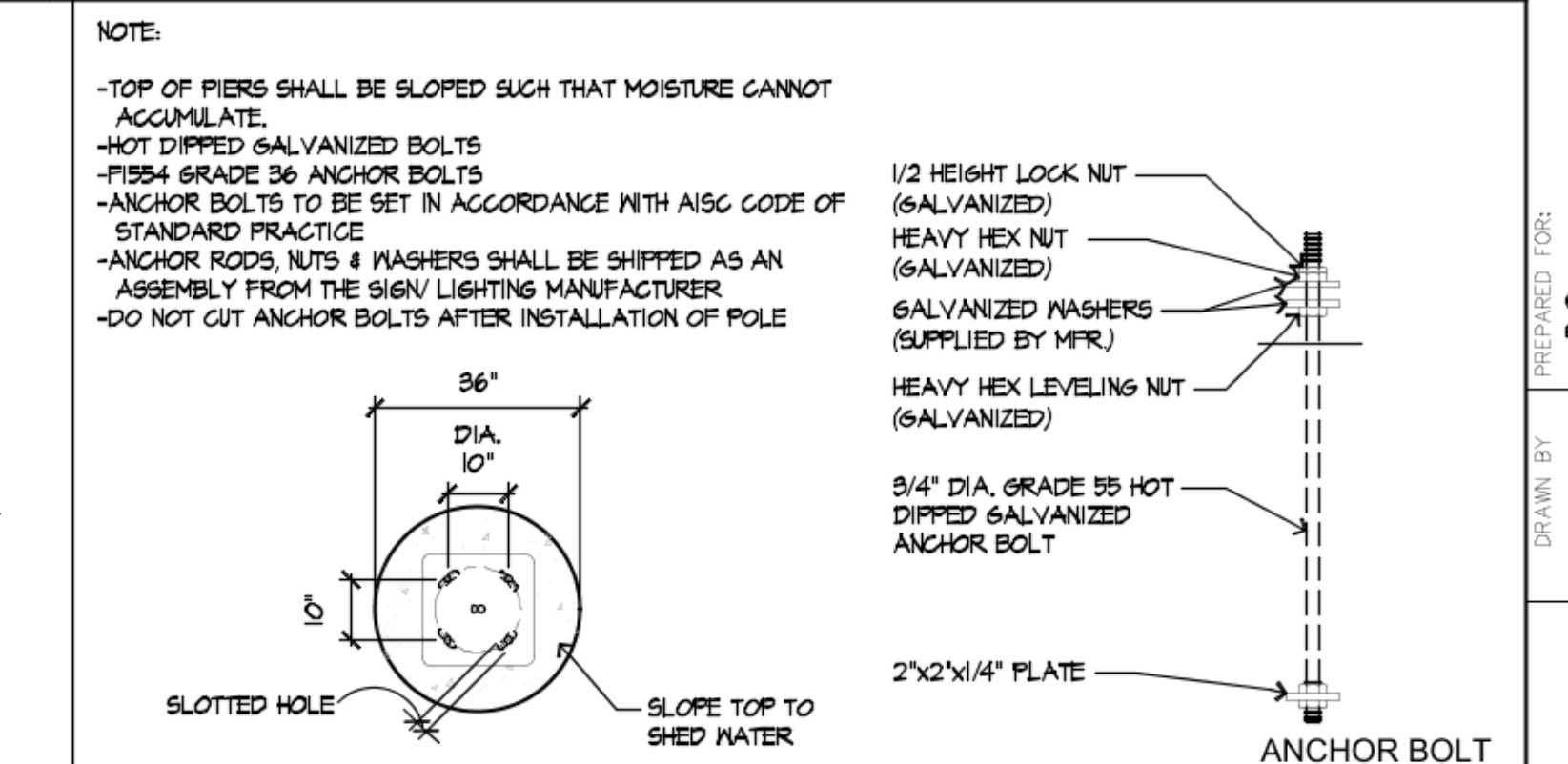
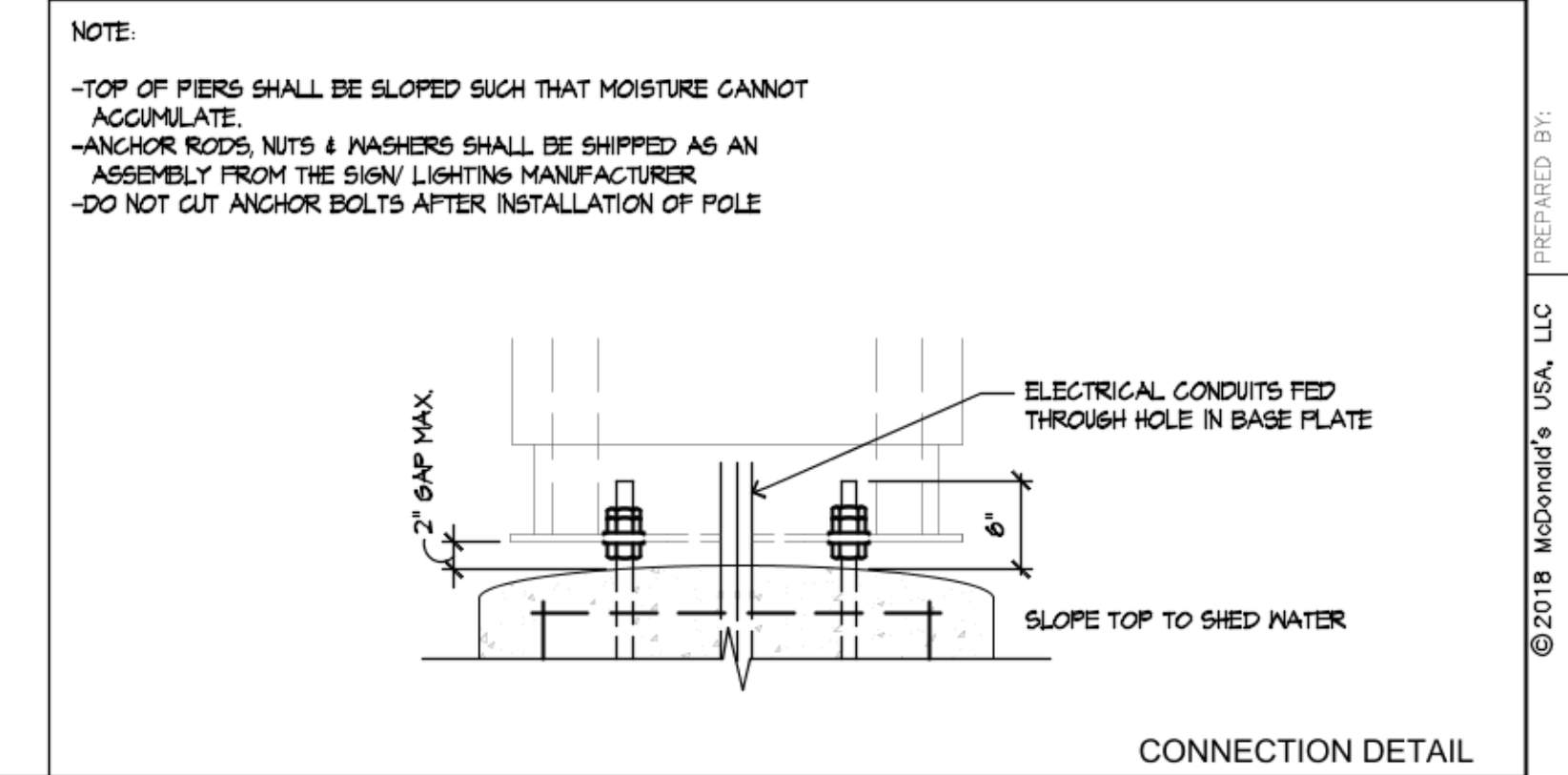
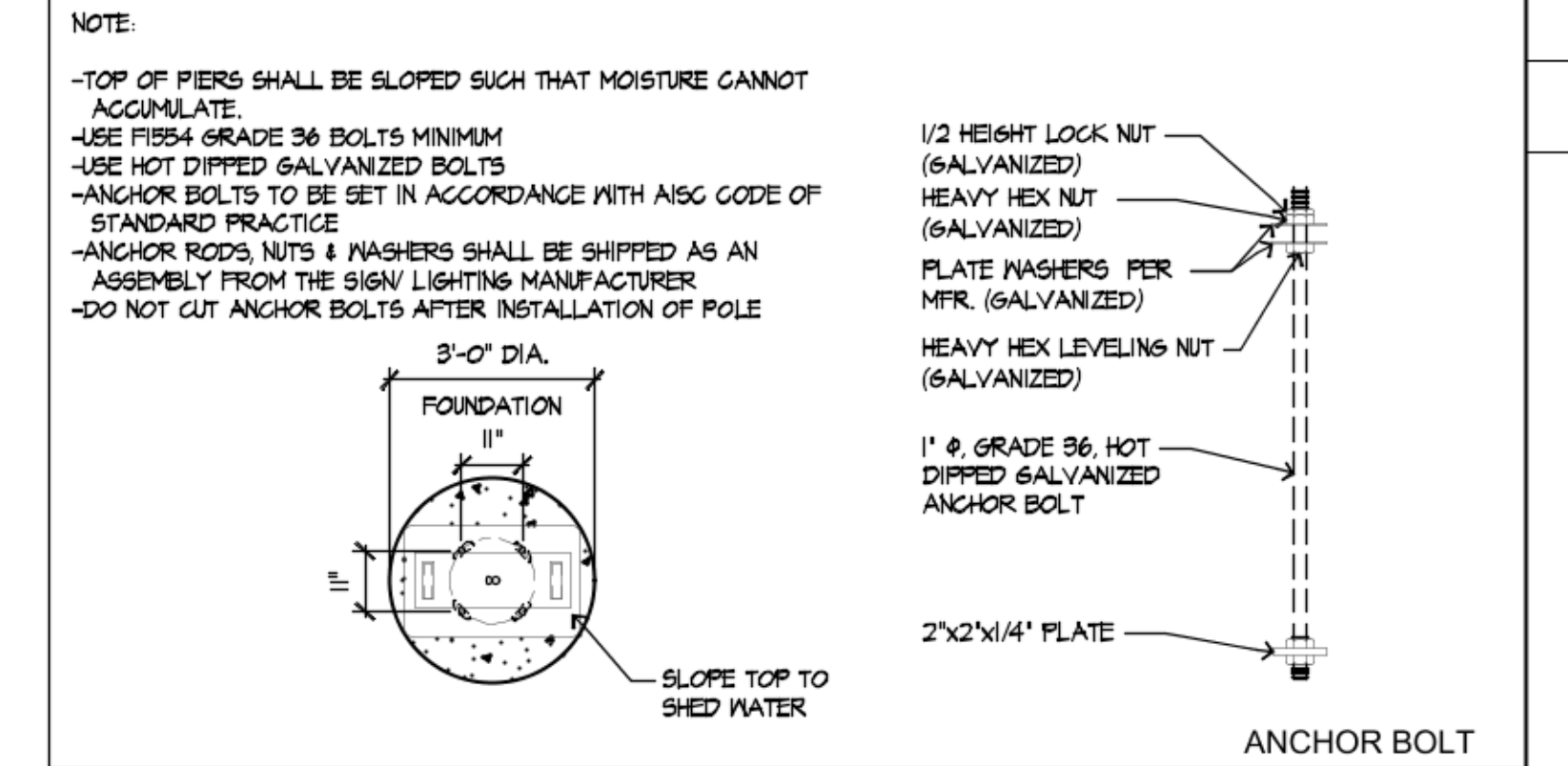
6 ORDER HERE CANOPY FOUNDATION

DT1.2 SCALE: NOT TO SCALE



6 GATEWAY SIGN FOUNDATION

DT1.2 SCALE: NOT TO SCALE



NO.	REV.	DATE	DESCRIPTION



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TITLE	2018 MRP REMODEL CORE 16
DRWN BY	
STD ISSUE DATE	
REVIEWED BY	
DATE ISSUED	
SHEET NO	DT1.2
DRIVE THRU DETAILS	



Dibble Engineers, Inc.		<i>Project No.:</i> 18-1351	<i>Sheet No.:</i>
<i>Project:</i> McDonald's McMinnville		<i>Date:</i> 11/15/2018	
<i>Subject:</i> Drive Thru Sign Footings - Summary Page		<i>By:</i> TJ	

Site Wind Properties:

V = 120 mph (ASCE 7 Figure 26.5-1A)
 Exposure = C - (ASCE 7 Section 26.7)
 K_{zt} = 1.00 - (ASCE 7 Section 26.8.2)

Site Seismic Properties:

S_{DS} = 0.731 g (USGS App.)
 S₁ = 0.478 g (USGS App.)

Site Snow Load:

S = 25 psf (ASCE 7 Figure 7-1 or Jurisdiction)

Sign Foundation Depths:

Sign (-)	Diameter (ft)	Depth		Δ (ft)	
		Assumed (ft)	Required (ft - inches)		
Digital Menu Board	2.5	5.25	5.25	5' - 3"	0.00
Digital Presell Board	2	4.61	4.61	4' - 7 1/3"	0.00
Order Canopy	3	6.36	6.36	6' - 4 1/3"	0.00
Gateway (Single)	3	4.10	4.10	4' - 1 1/5"	0.00
Total:				0.00	



Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Digital Menu Board		By: TJ	

Sign Properties:

Type =	Digital Menu -	(sign type)
Weight =	850 lbs	(per Freiheit Architecture)
Area =	32.76 ft ²	(per attached sign data)

Wind Loading:

I =	1.00 -	(ASCE 7 Table 1.5-2)
V =	120 mph	(ASCE 7 Figure 26.5-1A)
Exposure =	C -	(ASCE 7 Section 26.7)
K _z =	0.85 -	(ASCE 7 Table 29.3-1)
K _{zt} =	1.00 -	(ASCE 7 Section 26.8.2)
K _d =	0.85 -	(ASCE 7 Table 26.6-1, 0.85 for Solid Signs)
G =	0.85 -	(assumed per ASCE 7 Section 26.9.1)
C _f =	1.75 -	(ASCE 7 Figure 29.4-1)
q _n =	26.63 psf	(ASCE 7 Equation 29.3-1)
W =	1297.90 lb	(ASCE 7 Equation 29.4-1)
0.6W =	778.74 lb	(For ASD Load Combinations)

Seismic Loading:

S _{DS} =	0.731 g	(USGS App.)
S ₁ =	0.478 g	(USGS App.)
R =	3.0 -	(ASCE 7 Table 15.4-2, 3.0 for Signs)
I =	1.0 -	(ASCE 7 Table 1.5-2)
C _s =	0.244 -	(ASCE 7 Sections 12.8.1.1 and 15.4.1)
E =	207.12 lb	(ASCE 7 Equation 12.8-1)
0.7E =	144.98 lb	(For ASD Load Combinations)



Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Digital Menu Board		By: TJ	

Check Lateral:

$$d = 0.5A\{1 + [1 + (4.36h/A)]^{1/2}\}$$

IBC EQ 18-1

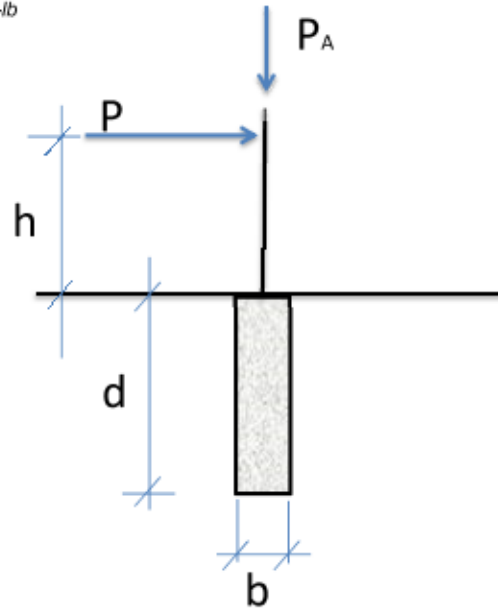
b =	2.5 ft	(post/footing diameter)
d_{assumed} =	5.25 ft	(assumed depth of embedment/footing)
h =	7.33 ft	(height of applied load above ground)
P =	778.74 lbs	(applied load per calculations above)
S =	100 psf/ft	(lateral soil bearing pressure per IBC table 1806.2)
l₂ =	2 -	(increase for poles that can deflect 1/2" per IBC 1806.3.4)
S₁ =	350.18 psf	(lateral soil bearing pressure at d/3)
A =	2.08 -	(2.34P/(S , b))
d =	5.25 ft	(Required depth of embedment. Shoud match assumed.)

Check Bearing:

P_A =	850 lbs	(vertical load)
A =	4.91 ft ²	(bearing area)
P/A =	173 psf	< 1500 psf, OK (bearing pressure - 1500 psf allowable per IBC table 1806.2)

LRFD Attachment Forces - Sign to Footing

V =	1298 lbs
M =	9514 ft-lb

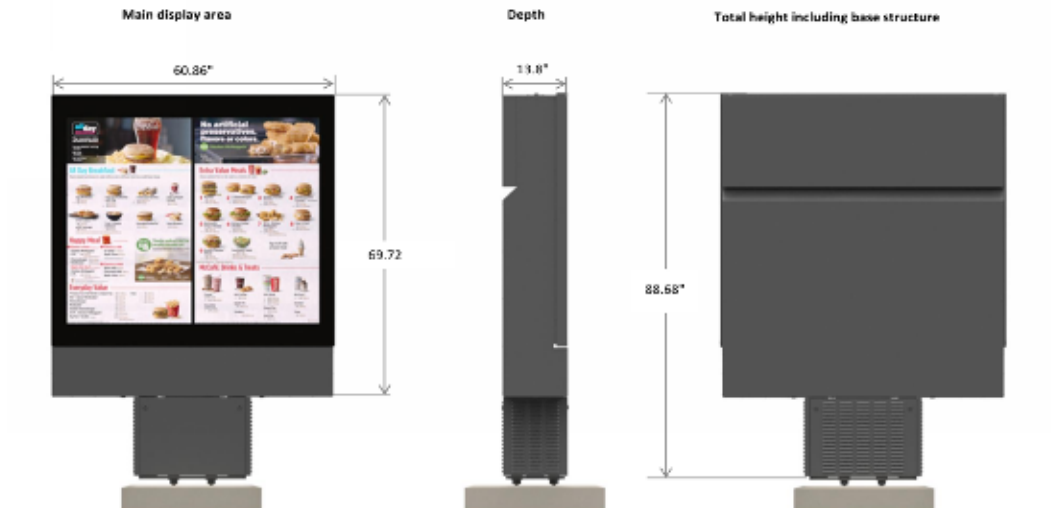




Dibble Engineers, Inc.	<i>Project No.:</i> 18-1351	<i>Sheet No.:</i>
<i>Project:</i> McDonald's McMinnville	<i>Date:</i> 11/15/2018	
<i>Subject:</i> Non-Constrained Embedded Post Footing - Digital Menu Board	<i>By:</i> TJ	

Weight and Dimensional Info per Freiheit Architecture:

Weight = 850 lbs





Dibble Engineers, Inc.	Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville	Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Digital Presell Board	By: TJ	

Sign Properties:

Type = Digital Presell - (sign type)
 Weight = 600 lbs (per Freiheit Architecture)
 Area = 19.75 ft² (per attached sign data)

Wind Loading:

I = 1.00 - (ASCE 7 Table 1.5-2)
 V = 120 mph (ASCE 7 Figure 26.5-1A)
 Exposure = C - (ASCE 7 Section 26.7)
 K_z = 0.85 - (ASCE 7 Table 29.3-1)
 K_{zt} = 1.00 - (ASCE 7 Section 26.8.2)
 K_d = 0.85 - (ASCE 7 Table 26.6-1, 0.85 for Solid Signs)
 G = 0.85 - (assumed per ASCE 7 Section 26.9.1)
 C_f = 1.65 - (ASCE 7 Figure 29.4-1)
 q_n = 26.63 psf (ASCE 7 Equation 29.3-1)
W = 737.75 lb (ASCE 7 Equation 29.4-1)
0.6W = 442.65 lb (For ASD Load Combinations)

Seismic Loading:

S_{DS} = 0.731 g (USGS App.)
 S₁ = 0.478 g (USGS App.)
 R = 3.0 - (ASCE 7 Table 15.4-2, 3.0 for Signs)
 I = 1.0 - (ASCE 7 Table 1.5-2)
 C_s = 0.244 - (ASCE 7 Sections 12.8.1.1 and 15.4.1)
E = 146.20 lb (ASCE 7 Equation 12.8-1)
0.7E = 102.34 lb (For ASD Load Combinations)



Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Digital Presell Board		By: TJ	

Check Lateral:

$$d = 0.5A\{1 + [1 + (4.36h/A)]^{1/2}\}$$

IBC EQ 18-1

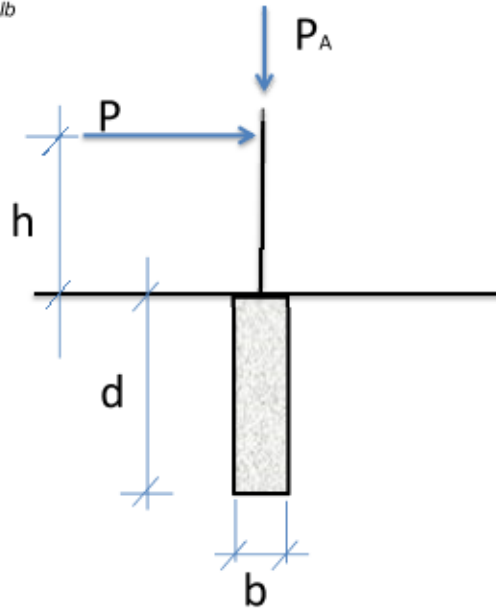
b =	2 ft	(post/footing diameter)
d_{assumed} =	4.61 ft	(assumed depth of embedment/footing)
h =	7.33 ft	(height of applied load above ground)
P =	442.65 lbs	(applied load per calculations above)
S =	100 psf/ft	(lateral soil bearing pressure per IBC table 1806.2)
l₂ =	2 -	(increase for poles that can deflect 1/2" per IBC 1806.3.4)
S₁ =	307.21 psf	(lateral soil bearing pressure at d/3)
A =	1.69 -	(2.34P/(S , b))
d =	4.61 ft	(Required depth of embedment. Should match assumed.)

Check Bearing:

P_A =	600 lbs	(vertical load)
A =	3.14 ft ²	(bearing area)
P/A =	191 psf	< 1500 psf, OK (bearing pressure - 1500 psf allowable per IBC table 1806.2)

LRFD Attachment Forces - Sign to Footing

V =	738 lbs
M =	5408 ft-lb

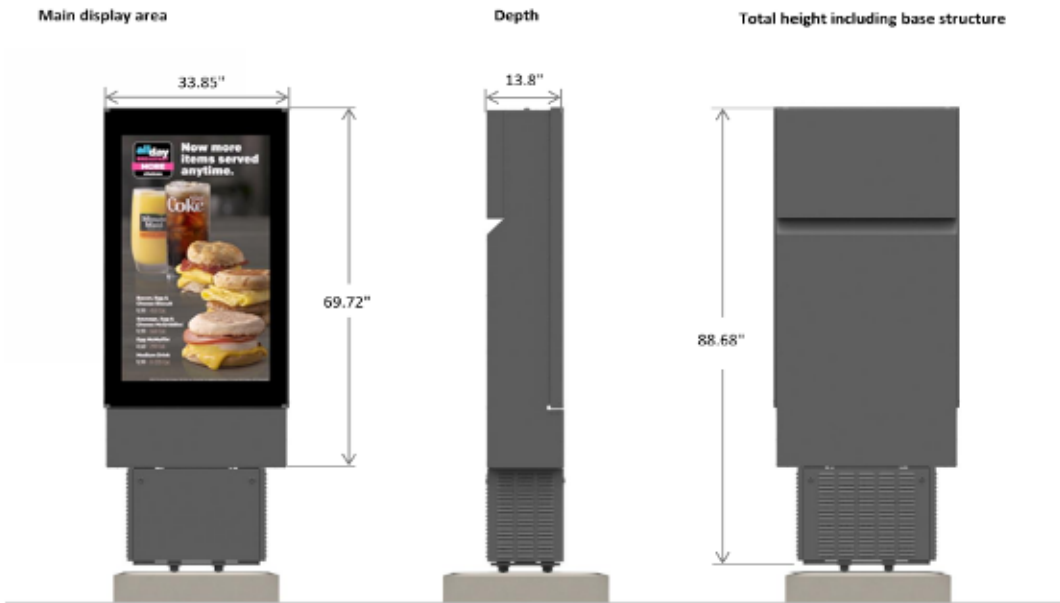


Weight and Dimensional Info per Freiheit Architecture:



Dibble Engineers, Inc.	<i>Project No.:</i> 18-1351	<i>Sheet No.:</i>
<i>Project:</i> McDonald's McMinnville	<i>Date:</i> 11/15/2018	
<i>Subject:</i> Non-Constrained Embedded Post Footing - Digital Presell Board	<i>By:</i> TJ	

Weight = 600 lbs





Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Order Canopy		By: TJ	

Sign Properties:

Type =	Order Canopy -	(sign type)
Weight =	850 lbs	(per Freiheit Architecture)
Area =	20 ft ²	(per attached sign data)
Roof Area =	36 ft ²	(per attached sign data)

Horizontal Wind Loading:

I =	1.00 -	(ASCE 7 Table 1.5-2)
V =	120 mph	(ASCE 7 Figure 26.5-1A)
Exposure =	C -	(ASCE 7 Section 26.7)
K _z =	0.85 -	(ASCE 7 Table 29.3-1)
K _{zt} =	1.00 -	(ASCE 7 Section 26.8.2)
K _d =	0.85 -	(ASCE 7 Table 26.6-1, 0.85 for Solid Signs)
G =	0.85 -	(assumed per ASCE 7 Section 26.9.1)
C _f =	1.85 -	(ASCE 7 Figure 29.4-1)
q _h =	26.63 psf	(ASCE 7 Equation 29.3-1)
W_H =	837.65 lb	(ASCE 7 Equation 29.4-1)

Vertical Wind Loading:

C _N =	1.90 -	(ASCE 7 Figure 27.4-4)
p =	43.01 psf	(ASCE 7 Equation 27.4-3)
W_V =	1548.51 lb	(vertical wind Load)
L =	4.50 ft	(canopy roof moment arm)
h =	7.79 ft	(height of applied wind/seismic load)
W_{veq} =	894.52 lb	(equivalent horizontal wind load)
0.6W_{H+Veq} =	1039.30 lb	(total equivalent horizontal wind load)

Seismic Loading:

S _{DS} =	0.731 g	(USGS App.)
S ₁ =	0.478 g	(USGS App.)
R =	3.0 -	(ASCE 7 Table 15.4-2, 3.0 for Signs)
I =	1.0 -	(ASCE 7 Table 1.5-2)
C _s =	0.244 -	(ASCE 7 Sections 12.8.1.1 and 15.4.1)
E =	207.12 lb	(ASCE 7 Equation 12.8-1)
0.7E =	144.98 lb	(For ASD Load Combinations)

Gravity Loading:

D =	15 psf	(dead load)
S =	25 psf	(snow load)
L =	4.5 ft	(canopy roof moment arm)
h =	7.79 ft	(height of applied wind/seismic load)
P_D =	312 lbs	(equivalent horizontal dead load)
P_S =	520 lbs	(equivalent horizontal snow load)



Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Order Canopy		By: TJ	

Load Combinations: (Equivalent Horizontal Force)

$P_3 =$	832 lbs	(D + S)
$P_5 =$	1351 lbs	(D + [0.6w or 0.7E])
$P_6 =$	1481 lbs	(D + 0.75S + 0.75[0.6W or 0.7E])
P =	1481 lbs	(worst-case equivalent horizontal force)

Check Lateral:

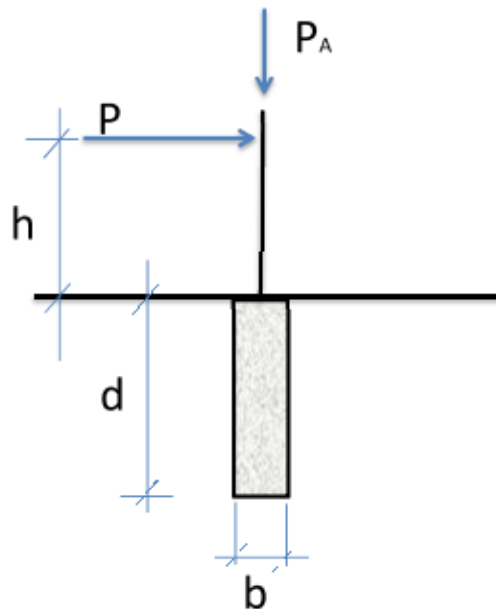
$d = 0.5A\{1 + [1 + (4.36h/A)]^{1/2}\}$		IBC EQ 18-1
b =	3 ft	(post/footing diameter)
d_{assumed} =	6.36 ft	(assumed depth of embedment/footing)
h =	7.79 ft	(height of applied load above ground)
P =	1481.34 lbs	(equivalent lateral load per calculations above)
S =	100 psf/ft	(lateral soil bearing pressure per IBC table 1806.2)
I₂ =	2 -	(increase for poles that can deflect 1/2" per IBC 1806.3.4)
S₁ =	424.07 psf	(lateral soil bearing pressure at d/3)
A =	2.72 -	(2.34P/(S ₁ b))
d =	6.36 ft	(Required depth of embedment. Should match assumed.)

Check Bearing:

P_A =	1750 lbs	(vertical load = Weight + Snow)
A =	7.07 ft ²	(bearing area)
P/A =	248 psf	< 1500 psf, OK (bearing pressure - 1500 psf allowable per IBC table 1806.2)

LRFD Attachment Forces - Sign to Footing

V =	838 lbs	(maximum of 1.0W and 1.0E)
M₁ =	3402 lb-ft	(1.4D)
M₂ =	16143 lb-ft	(1.2D + 1.6S + 0.5W)
M₃ =	18435 lb-ft	(1.2D + 1.0W + 0.5S)
M₄ =	5339 lb-ft	(1.2D + 1.0E + 0.2S)
M =	18435 lb-ft	(worst-case moment)

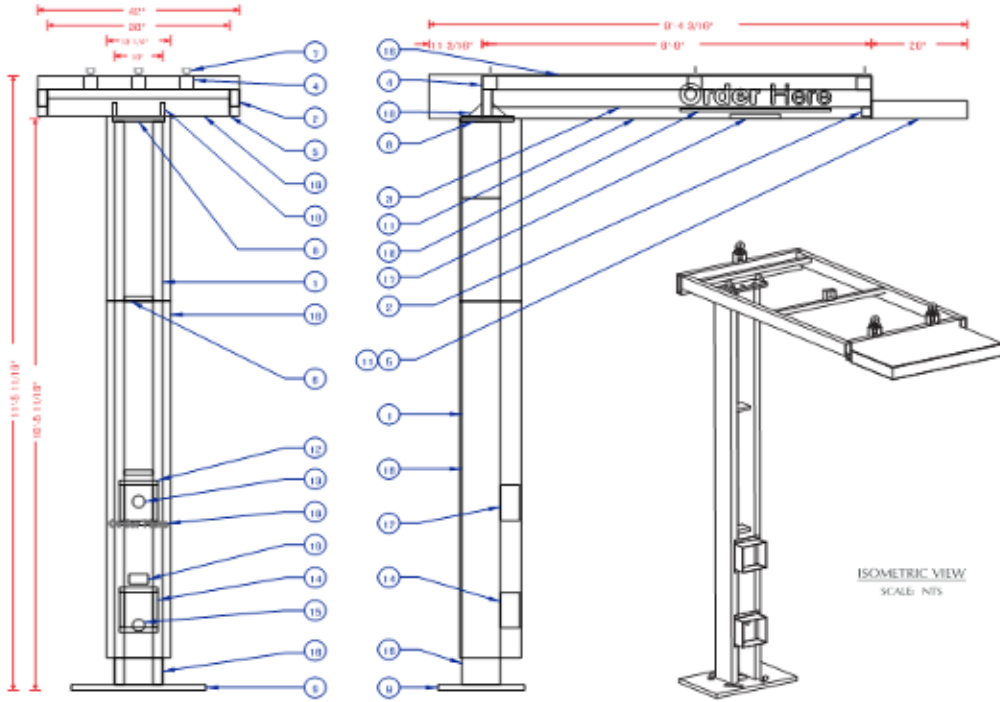




Dibble Engineers, Inc.		<i>Project No.:</i> 18-1351	<i>Sheet No.:</i>
<i>Project:</i> McDonald's McMinnville		<i>Date:</i> 11/15/2018	
<i>Subject:</i> Non-Constrained Embedded Post Footing - Order Canopy		<i>By:</i> TJ	

Weight and Dimensional Info per Freiheit Architecture:

Weight = 850 lbs





Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Gateway Sign (Single)		By: TJ	

Sign Properties:

Type = Gateway (Single) - (sign type)
 Weight = 450 lbs (per Freiheit Architecture)
 Area = 20 ft² (per attached sign data)

Wind Loading:

I = 1.00 - (ASCE 7 Table 1.5-2)
 V = 120 mph (ASCE 7 Figure 26.5-1A)
 Exposure = C - (ASCE 7 Section 26.7)
 K_z = 0.85 - (ASCE 7 Table 29.3-1)
 K_{zt} = 1.00 - (ASCE 7 Section 26.8.2)
 K_d = 0.85 - (ASCE 7 Table 26.6-1, 0.85 for Solid Signs)
 G = 0.85 - (assumed per ASCE 7 Section 26.9.1)
 C_f = 1.9 - (ASCE 7 Figure 29.4-1)
 q_n = 26.63 psf (ASCE 7 Equation 29.3-1)
W = 860.29 lb (ASCE 7 Equation 29.4-1)
0.6W = 516.17 lb (For ASD Load Combinations)

Seismic Loading:

S_{DS} = 0.731 g (USGS App.)
 S₁ = 0.478 g (USGS App.)
 R = 3.0 - (ASCE 7 Table 15.4-2, 3.0 for Signs)
 I = 1.0 - (ASCE 7 Table 1.5-2)
 C_s = 0.244 - (ASCE 7 Sections 12.8.1.1 and 15.4.1)
E = 109.65 lb (ASCE 7 Equation 12.8-1)
0.7E = 76.76 lb (For ASD Load Combinations)



Dibble Engineers, Inc.		Project No.: 18-1351	Sheet No.:
Project: McDonald's McMinnville		Date: 11/15/2018	
Subject: Non-Constrained Embedded Post Footing - Gateway Sign (Single)		By: TJ	

Check Lateral:

$$d = 0.5A\{1 + [1 + (4.36h/A)]^{1/2}\}$$

IBC EQ 18-1

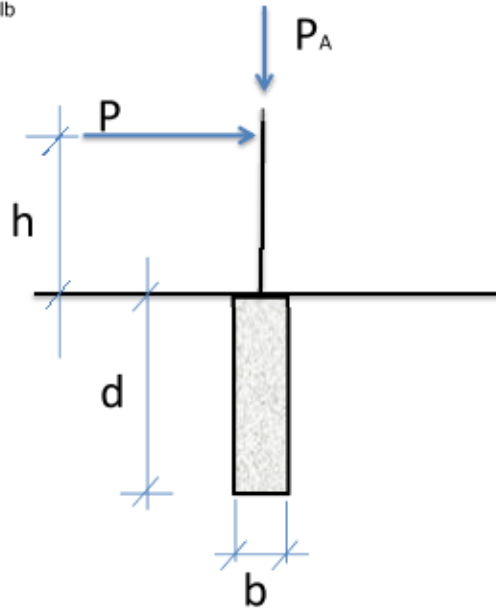
b =	3 ft	(post/footing diameter)
d_{assumed} =	4.10 ft	(assumed depth of embedment/footing)
h =	6.7 ft	(height of applied load above ground)
P =	516.17 lbs	(applied load per calculations above)
S =	100 psf/ft	(lateral soil bearing pressure per IBC table 1806.2)
l₂ =	2 -	(increase for poles that can deflect 1/2" per IBC 1806.3.4)
S₁ =	273.24 psf	(lateral soil bearing pressure at d/3)
A =	1.47 -	(2.34P/(S, b))
d =	4.10 ft	(Required depth of embedment. Should match assumed.)

Check Bearing:

P_A =	450 lbs	(vertical load)
A =	7.07 ft ²	(bearing area)
P/A =	64 psf	< 1500 psf, OK (bearing pressure - 1500 psf allowable per IBC table 1806.2)

LRFD Attachment Forces - Sign to Footing

V =	860 lbs
M =	5764 ft-lb

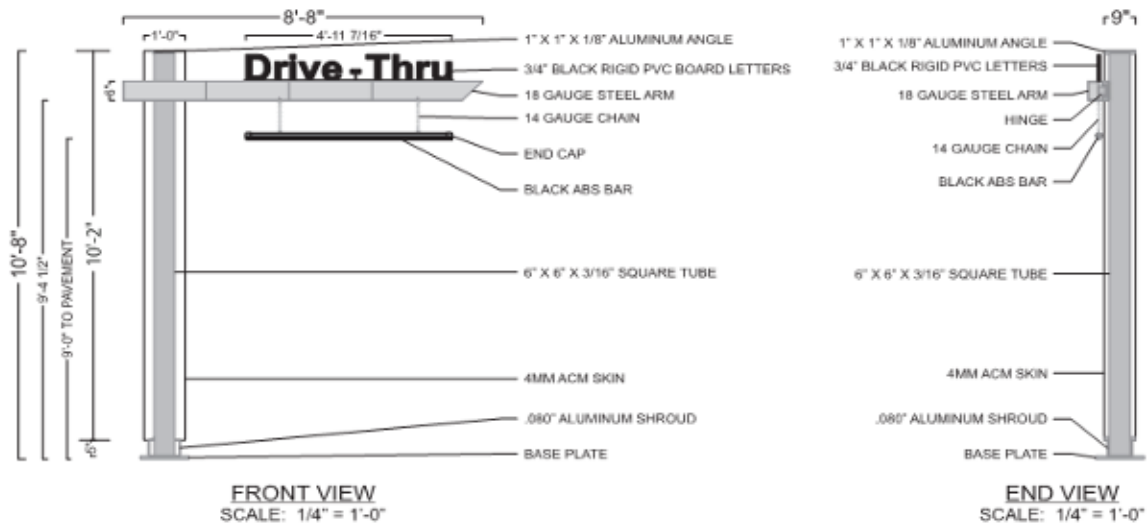


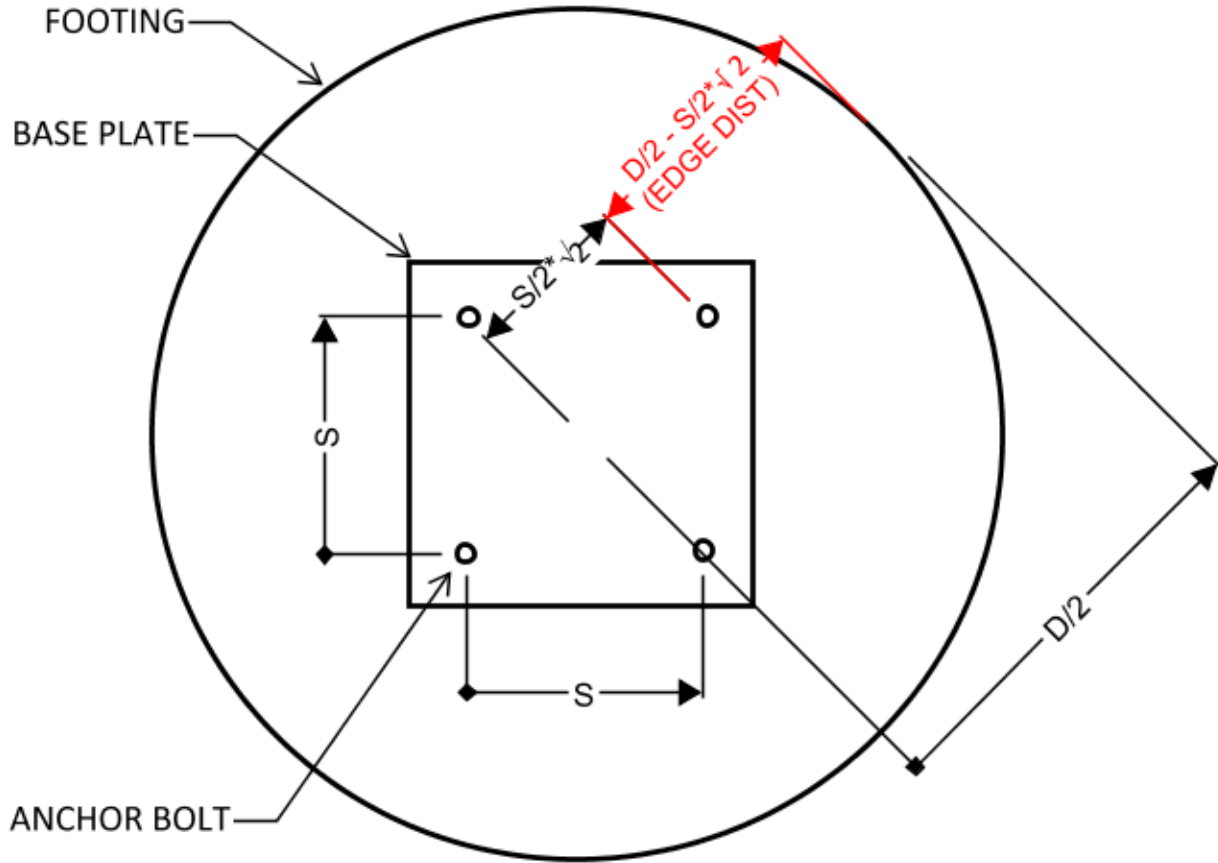
Weight and Dimensional Info per Freiheit Architecture:



Dibble Engineers, Inc.		<i>Project No.:</i> 18-1351	<i>Sheet No.:</i>
<i>Project:</i> McDonald's McMinnville		<i>Date:</i> 11/15/2018	
<i>Subject:</i> Non-Constrained Embedded Post Footing - Gateway Sign (Single)		<i>By:</i> TJ	

Weight = 450 lbs





Footing Diameter	Bolt Spacing	Edge Distance
D (in)	S (in)	E (in)
24	8	6.34
30	8	9.34
36	10	10.93
36	11	10.22



Company:	Dibble Engineers	Date:	8/16/2017
Engineer:		Page:	1/6
Project:	McD's		
Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

1. Project information

Customer company: Freiheit Architecture
 Customer contact name:
 Customer e-mail:
 Comment:

Project description: Digital Menu Board
 Location:
 Fastening description:

2. Input Data & Anchor Parameters

General

Design method: ACI 318-14
 Units: Imperial units

Anchor Information:

Anchor type: Cast-in-place
 Material: F1554 Grade 36
 Diameter (inch): 0.750
 Effective Embedment depth, h_{ef} (inch): 24.000
 Anchor category: -
 Anchor ductility: Yes
 h_{min} (inch): 25.50
 c_{min} (inch): 4.50
 s_{min} (inch): 4.50

Base Material

Concrete: Normal-weight
 Concrete thickness, h (inch): 50.00
 State: Cracked
 Compressive strength, f_c (psi): 3000
 $\Psi_{e,v}$: 1.0
 Reinforcement condition: B tension, B shear
 Supplemental reinforcement: Not applicable
 Reinforcement provided at corners: No
 Ignore concrete breakout in tension: No
 Ignore concrete breakout in shear: No
 Ignore 6do requirement: No
 Build-up grout pad: No

Base Plate

Length x Width x Thickness (inch): 12.00 x 12.00 x 0.50

Recommended Anchor

Anchor Name: Heavy Hex Bolt - 3/4"Ø Heavy Hex Bolt, F1554 Gr. 36





Company:	Dibble Engineers	Date:	8/16/2017
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Project:	McD's		
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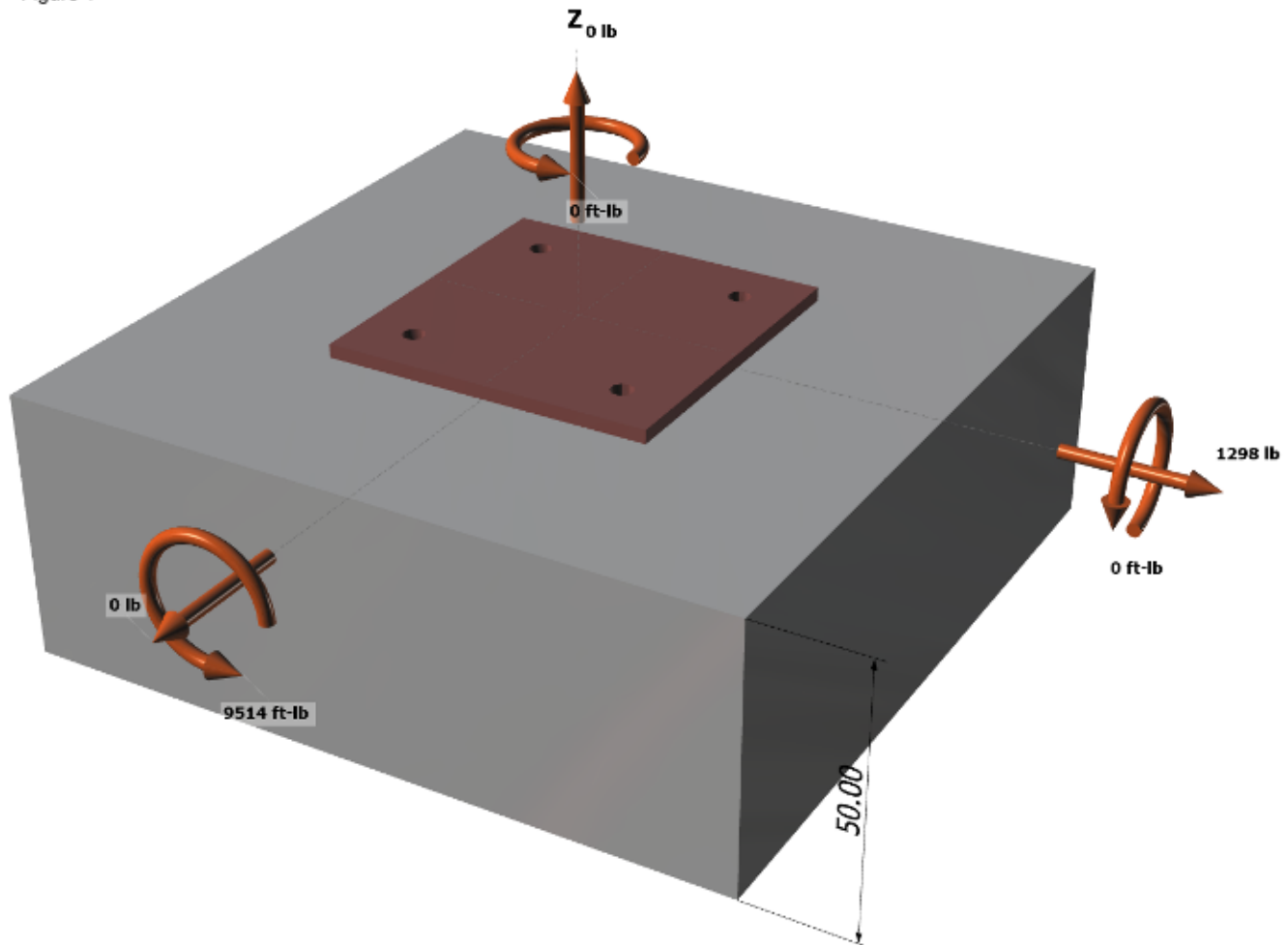
Load and Geometry

Load factor source: ACI 318 Section 5.3
Load combination: not set
Seismic design: Yes
Anchors subjected to sustained tension: Not applicable
Ductility section for tension: 17.2.3.4.3 (d) is satisfied
Ductility section for shear: 17.2.3.5.3 (c) is satisfied
 Ω_0 factor: not set
Apply entire shear load at front row: No
Anchors only resisting wind and/or seismic loads: No

Strength level loads:

N_{ua} [lb]: 0
 V_{ux} [lb]: 0
 V_{uy} [lb]: 1298
 M_{ux} [ft-lb]: 9514
 M_{uy} [ft-lb]: 0
 M_{uz} [ft-lb]: 0

<Figure 1>

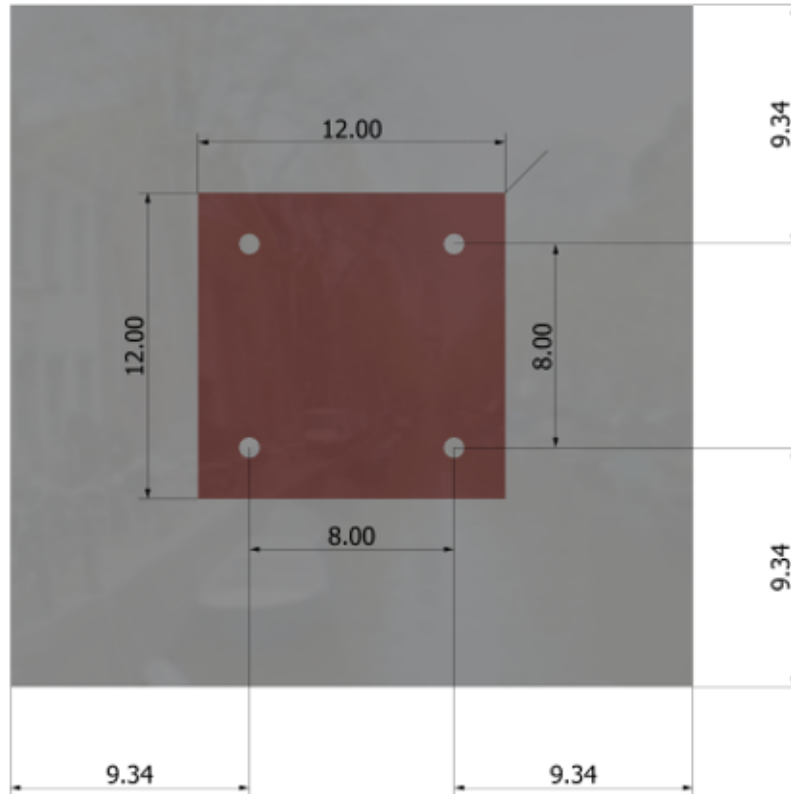


Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.



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Project:	McD's		
Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

<Figure 2>





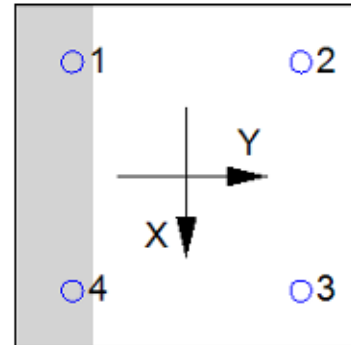
Company:	Dibble Engineers	Date:	8/16/2017
Engineer:		Page:	4/6
Project:	McD's		
Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

3. Resulting Anchor Forces

Anchor	Tension load, N_{ua} (lb)	Shear load x, V_{ux} (lb)	Shear load y, V_{uy} (lb)	Shear load combined, $\sqrt{(V_{ux})^2+(V_{uy})^2}$ (lb)
1	0.0	0.0	324.5	324.5
2	6281.7	0.0	324.5	324.5
3	6281.7	0.0	324.5	324.5
4	0.0	0.0	324.5	324.5
Sum	12563.5	0.0	1298.0	1298.0

Maximum concrete compression strain (%): 0.18
 Maximum concrete compression stress (psi): 764
 Resultant tension force (lb): 12563
 Resultant compression force (lb): 12563
 Eccentricity of resultant tension forces in x-axis, e'_{Nx} (inch): 0.00
 Eccentricity of resultant tension forces in y-axis, e'_{Ny} (inch): 0.00
 Eccentricity of resultant shear forces in x-axis, e'_{Vx} (inch): 0.00
 Eccentricity of resultant shear forces in y-axis, e'_{Vy} (inch): 0.00

<Figure 3>



4. Steel Strength of Anchor in Tension (Sec. 17.4.1)

N_{sa} (lb)	ϕ	ϕN_{sa} (lb)
19370	0.75	14528

5. Concrete Breakout Strength of Anchor in Tension (Sec. 17.4.2)

$N_b = 16\lambda_a^2 f_c h_{ef}^3$ (Eq. 17.4.2.2b)

λ_a	f_c (psi)	h_{ef} (in)	N_b (lb)
1.00	3000	11.560	51794

$0.75\phi N_{cbg} = 0.75\phi (A_{Nc} / A_{Nco}) \psi_{ec,N} \psi_{ed,N} \psi_{c,N} \psi_{cp,N} N_b$ (Sec. 17.3.1 & Eq. 17.4.2.1b)

A_{Nc} (in ²)	A_{Nco} (in ²)	$c_{a,min}$ (in)	$\psi_{ec,N}$	$\psi_{ed,N}$	$\psi_{c,N}$	$\psi_{cp,N}$	N_b (lb)	ϕ	$0.75\phi N_{cbg}$ (lb)
711.82	1202.70	9.34	1.000	0.862	1.00	1.000	51794	0.70	13866

6. Pullout Strength of Anchor in Tension (Sec. 17.4.3)

$0.75\phi N_{pn} = 0.75\phi \psi_{c,P} P N_p = 0.75\phi \psi_{c,P} 8A_{brg} f_c$ (Sec. 17.3.1, Eq. 17.4.3.1 & 17.4.3.4)

$\psi_{c,P}$	A_{brg} (in ²)	f_c (psi)	ϕ	$0.75\phi N_{pn}$ (lb)
1.0	0.91	3000	0.70	11479

Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.



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Address:	1029 Market St		
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7. Side-Face Blowout Strength of Anchor in Tension (Sec. 17.4.4)

$$0.75\phi N_{sb} = 0.75\phi \left\{ (1 + c_{a2}/c_{a1})/4 \right\} (1 + s/6c_{a1}) N_{sb} = 0.75\phi \left\{ (1 + c_{a2}/c_{a1})/4 \right\} (1 + s/6c_{a1}) (160c_{a1} \lambda A_{brg}) \lambda \phi_c \quad (\text{Sec. 17.3.1, Eq. 17.4.4.1 \& 17.4.4.2})$$

s (in)	c _{a1} (in)	c _{a2} (in)	A _{brg} (in ²)	λ _a	f _c (psi)	φ	0.75φN _{sb} (lb)
8.00	9.34	9.34	0.91	1.00	3000	0.70	23435

8. Steel Strength of Anchor in Shear (Sec. 17.5.1)

V _{sa} (lb)	φ _{grout}	φ	φ _{grout} φV _{sa} (lb)
11625	1.0	0.65	7556

9. Concrete Breakout Strength of Anchor in Shear (Sec. 17.5.2)

Shear perpendicular to edge in y-direction:

$$V_{by} = \min \left[7 \left(l_e / d_a \right)^{0.2} \lambda_a \lambda \phi_c c_{a1}^{1.5}; 9 \lambda_a \phi_c c_{a1}^{1.5} \right] \quad (\text{Eq. 17.5.2.2a \& Eq. 17.5.2.2b})$$

l _e (in)	d _a (in)	λ _a	f _c (psi)	c _{a1} (in)	V _{by} (lb)
6.00	0.750	1.00	3000	17.34	35594

$$\phi V_{cbgy} = \phi \left(A_{vc} / A_{vco} \right) \psi_{ec,v} \psi_{ed,v} \psi_{c,v} \psi_{h,v} V_{by} \quad (\text{Sec. 17.3.1 \& Eq. 17.5.2.1b})$$

A _{vc} (in ²)	A _{vco} (in ²)	ψ _{ec,v}	ψ _{ed,v}	ψ _{c,v}	ψ _{h,v}	V _{by} (lb)	φ	φV _{cbgy} (lb)
693.95	1353.04	1.000	0.808	1.000	1.000	35594	0.70	10322

Shear parallel to edge in y-direction:

$$V_{bx} = \min \left[7 \left(l_e / d_a \right)^{0.2} \lambda_a \lambda \phi_c c_{a1}^{1.5}; 9 \lambda_a \phi_c c_{a1}^{1.5} \right] \quad (\text{Eq. 17.5.2.2a \& Eq. 17.5.2.2b})$$

l _e (in)	d _a (in)	λ _a	f _c (psi)	c _{a1} (in)	V _{bx} (lb)
6.00	0.750	1.00	3000	9.34	14071

$$\phi V_{cbgx} = \phi (2) \left(A_{vc} / A_{vco} \right) \psi_{ec,v} \psi_{ed,v} \psi_{c,v} \psi_{h,v} V_{bx} \quad (\text{Sec. 17.3.1, 17.5.2.1(c) \& Eq. 17.5.2.1b})$$

A _{vc} (in ²)	A _{vco} (in ²)	ψ _{ec,v}	ψ _{ed,v}	ψ _{c,v}	ψ _{h,v}	V _{bx} (lb)	φ	φV _{cbgx} (lb)
373.79	392.56	1.000	1.000	1.000	1.000	14071	0.70	18757

10. Concrete Pryout Strength of Anchor in Shear (Sec. 17.5.3)

$$\phi V_{cp} = \phi k_{cp} N_{cb} = \phi k_{cp} \left(A_{Nc} / A_{Nco} \right) \psi_{ec,N} \psi_{ed,N} \psi_{c,N} \psi_{cp,N} N_b \quad (\text{Sec. 17.3.1 \& Eq. 17.5.3.1b})$$

k _{cp}	A _{Nc} (in ²)	A _{Nco} (in ²)	ψ _{ec,N}	ψ _{ed,N}	ψ _{c,N}	ψ _{cp,N}	N _b (lb)	φ	φV _{cp} (lb)
2.0	711.82	348.94	1.000	1.000	1.000	1.000	18469	0.70	52746

11. Results

Interaction of Tensile and Shear Forces (Sec. 17.6)

Tension	Factored Load, N _{ua} (lb)	Design Strength, φN _n (lb)	Ratio	Status
Steel	6282	14528	0.43	Pass
Concrete breakout	12563	13866	0.91	Pass (Governs)
Pullout	6282	11479	0.55	Pass
Side-face blowout	12563	23435	0.54	Pass
Shear	Factored Load, V _{ua} (lb)	Design Strength, φV _n (lb)	Ratio	Status
Steel	325	7556	0.04	Pass
T Concrete breakout y+	1298	10322	0.13	Pass (Governs)
 Concrete breakout x-	649	18757	0.03	Pass (Governs)

Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.



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Pryout	1298	52746	0.02	Pass	
Interaction check	$N_{ua}/\phi N_n$	$V_{ua}/\phi V_n$	Combined Ratio	Permissible	Status
Sec. 17.6..1	0.91	0.00	90.6%	1.0	Pass

3/4"Ø Heavy Hex Bolt, F1554 Gr. 36 with hef = 24.000 inch meets the selected design criteria.

12. Warnings

- Per designer input, ductility requirements for tension have been determined to be satisfied – designer to verify.
- Per designer input, ductility requirements for shear have been determined to be satisfied – designer to verify.
- Designer must exercise own judgement to determine if this design is suitable.



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E-mail:			

1. Project information

Customer company: Freiheit Architecture
 Customer contact name:
 Customer e-mail:
 Comment:

Project description: Digital PreSell Board
 Location:
 Fastening description:

2. Input Data & Anchor Parameters

General

Design method: ACI 318-14
 Units: Imperial units

Anchor Information:

Anchor type: Cast-in-place
 Material: F1554 Grade 36
 Diameter (inch): 0.750
 Effective Embedment depth, h_{ef} (inch): 24.000
 Anchor category: -
 Anchor ductility: Yes
 h_{min} (inch): 25.50
 c_{min} (inch): 4.50
 s_{min} (inch): 4.50

Base Material

Concrete: Normal-weight
 Concrete thickness, h (inch): 36.00
 State: Cracked
 Compressive strength, f_c (psi): 3000
 $\Psi_{e,v}$: 1.0
 Reinforcement condition: B tension, B shear
 Supplemental reinforcement: Not applicable
 Reinforcement provided at corners: No
 Ignore concrete breakout in tension: No
 Ignore concrete breakout in shear: No
 Ignore 6do requirement: No
 Build-up grout pad: No

Base Plate

Length x Width x Thickness (inch): 12.00 x 12.00 x 0.50

Recommended Anchor

Anchor Name: Heavy Hex Bolt - 3/4"Ø Heavy Hex Bolt, F1554 Gr. 36





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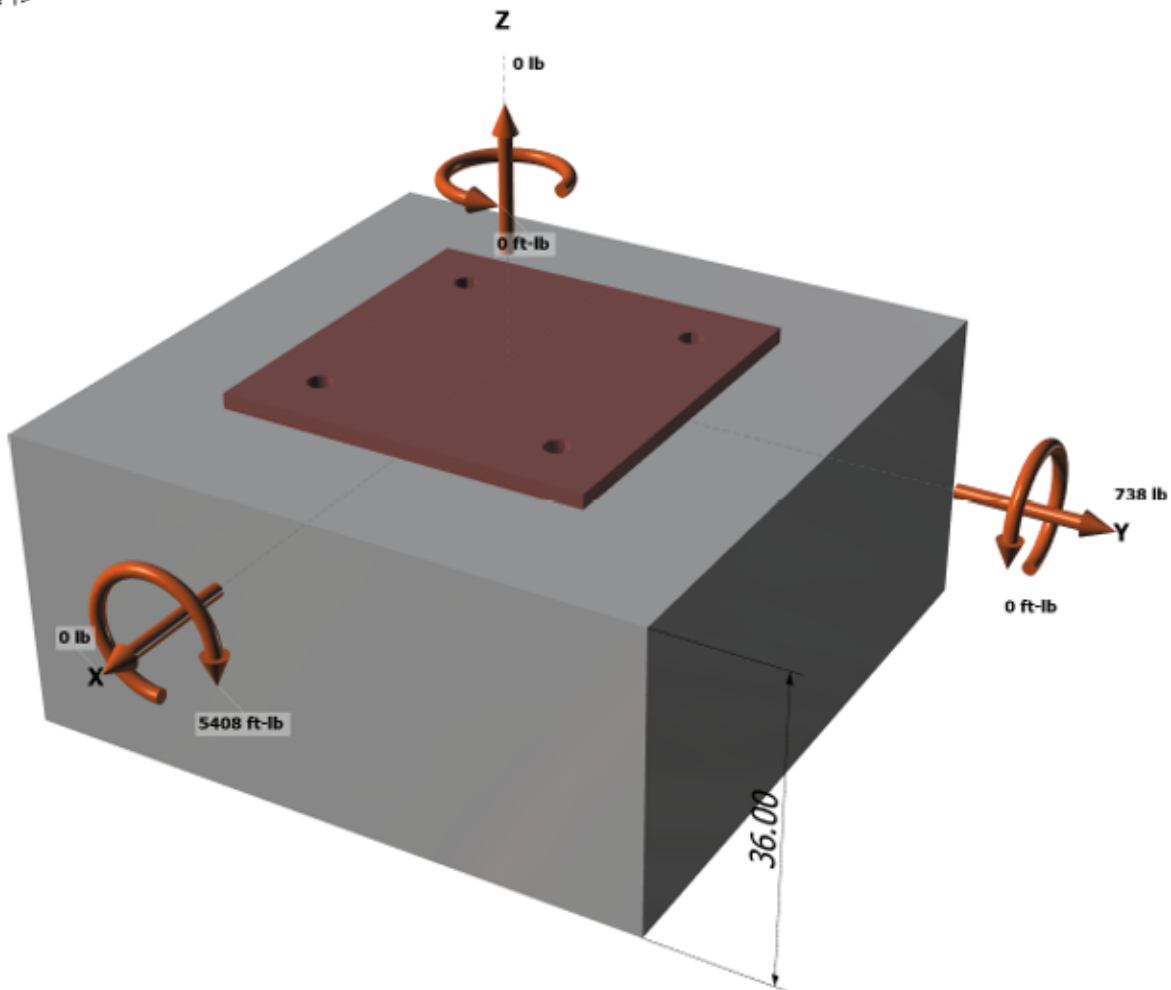
Load and Geometry

Load factor source: ACI 318 Section 5.3
Load combination: not set
Seismic design: Yes
Anchors subjected to sustained tension: Not applicable
Ductility section for tension: 17.2.3.4.3 (d) is satisfied
Ductility section for shear: 17.2.3.5.3 (c) is satisfied
 Ω_0 factor: not set
Apply entire shear load at front row: No
Anchors only resisting wind and/or seismic loads: Yes

Strength level loads:

N_{ua} [lb]: 0
 V_{ux} [lb]: 0
 V_{uy} [lb]: 738
 M_{ux} [ft-lb]: -5408
 M_{uy} [ft-lb]: 0
 M_{uz} [ft-lb]: 0

<Figure 1>

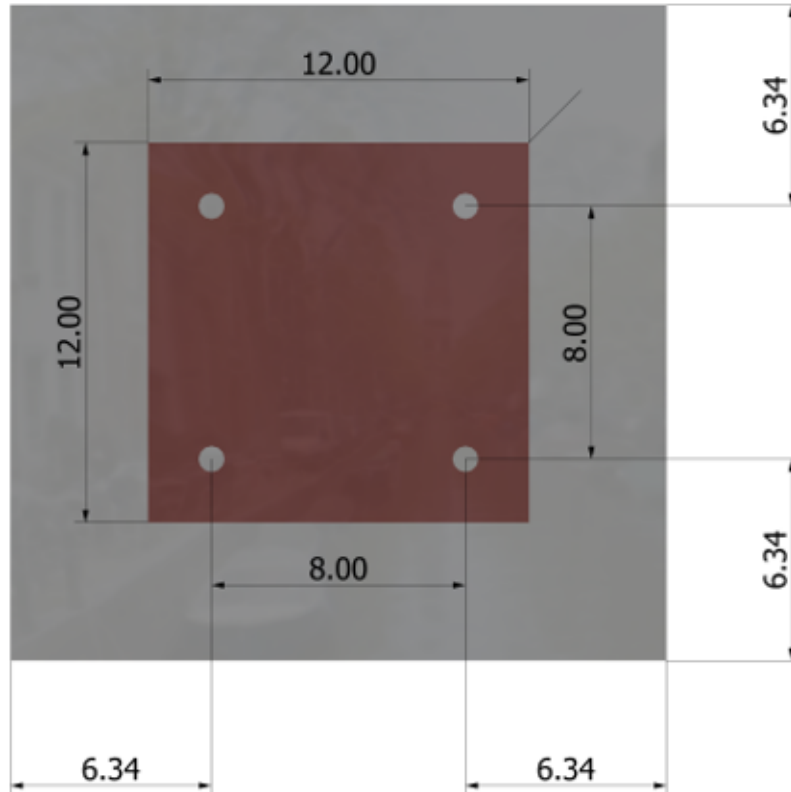


Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.



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<Figure 2>





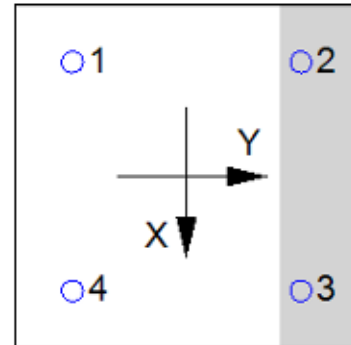
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3. Resulting Anchor Forces

Anchor	Tension load, N_{ua} (lb)	Shear load x, V_{ux} (lb)	Shear load y, V_{uy} (lb)	Shear load combined, $\sqrt{(V_{ux})^2 + (V_{uy})^2}$ (lb)
1	3571.2	0.0	184.5	184.5
2	0.0	0.0	184.5	184.5
3	0.0	0.0	184.5	184.5
4	3571.2	0.0	184.5	184.5
Sum	7142.3	0.0	738.0	738.0

Maximum concrete compression strain (%): 0.10
 Maximum concrete compression stress (psi): 434
 Resultant tension force (lb): 7142
 Resultant compression force (lb): 7142
 Eccentricity of resultant tension forces in x-axis, e'_{Nx} (inch): 0.00
 Eccentricity of resultant tension forces in y-axis, e'_{Ny} (inch): 0.00
 Eccentricity of resultant shear forces in x-axis, e'_{Vx} (inch): 0.00
 Eccentricity of resultant shear forces in y-axis, e'_{Vy} (inch): 0.00

<Figure 3>



4. Steel Strength of Anchor in Tension (Sec. 17.4.1)

N_{sa} (lb)	ϕ	ϕN_{sa} (lb)
19370	0.75	14528

5. Concrete Breakout Strength of Anchor in Tension (Sec. 17.4.2)

$N_b = 16\lambda_a^2 f_c h_{ef}^3$ (Eq. 17.4.2.2b)

λ_a	f_c (psi)	h_{ef} (in)	N_b (lb)
1.00	3000	9.560	37738

$0.75\phi N_{cbg} = 0.75\phi (A_{Nc} / A_{Nco}) \psi_{ec,N} \psi_{ed,N} \psi_{c,N} \psi_{cp,N} N_b$ (Sec. 17.3.1 & Eq. 17.4.2.1b)

A_{Nc} (in ²)	A_{Nco} (in ²)	$c_{a,min}$ (in)	$\psi_{ec,N}$	$\psi_{ed,N}$	$\psi_{c,N}$	$\psi_{cp,N}$	N_b (lb)	ϕ	$0.75\phi N_{cbg}$ (lb)
427.66	822.54	6.34	1.000	0.833	1.00	1.000	37738	0.70	8577

6. Pullout Strength of Anchor in Tension (Sec. 17.4.3)

$0.75\phi N_{pn} = 0.75\phi \psi_{c,P} N_p = 0.75\phi \psi_{c,P} 8A_{brg} f_c$ (Sec. 17.3.1, Eq. 17.4.3.1 & 17.4.3.4)

$\psi_{c,P}$	A_{brg} (in ²)	f_c (psi)	ϕ	$0.75\phi N_{pn}$ (lb)
1.0	0.91	3000	0.70	11479

Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.



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7. Side-Face Blowout Strength of Anchor in Tension (Sec. 17.4.4)

$$0.75\phi N_{sb} = 0.75\phi \left\{ (1+C_{a2}/C_{a1})/4 \right\} (1+s/6C_{a1}) N_{sb} = 0.75\phi \left\{ (1+C_{a2}/C_{a1})/4 \right\} (1+s/6C_{a1}) (160C_{a1} \lambda A_{brg}) \lambda \sqrt{f_c} \quad (\text{Sec. 17.3.1, Eq. 17.4.4.1 \& 17.4.4.2})$$

s (in)	C _{a1} (in)	C _{a2} (in)	A _{brg} (in ²)	λ _a	f _c (psi)	φ	0.75φN _{sb} (lb)
8.00	6.34	6.34	0.91	1.00	3000	0.70	16848

8. Steel Strength of Anchor in Shear (Sec. 17.5.1)

V _{sa} (lb)	φ _{grout}	φ	φ _{grout} φV _{sa} (lb)
11625	1.0	0.65	7556

9. Concrete Breakout Strength of Anchor in Shear (Sec. 17.5.2)

Shear perpendicular to edge in y-direction:

$$V_{by} = \min \left[7 \left(l_e / d_a \right)^{0.2} \lambda_a \sqrt{f_c} C_{a1}^{1.5}; 9 \lambda_a \sqrt{f_c} C_{a1}^{1.5} \right] \quad (\text{Eq. 17.5.2.2a \& Eq. 17.5.2.2b})$$

l _e (in)	d _a (in)	λ _a	f _c (psi)	C _{a1} (in)	V _{by} (lb)
6.00	0.750	1.00	3000	14.34	26769

$$\phi V_{cbgy} = \phi \left(A_{vc} / A_{vco} \right) \Psi_{ec,v} \Psi_{ed,v} \Psi_{c,v} \Psi_{h,v} V_{by} \quad (\text{Sec. 17.3.1 \& Eq. 17.5.2.1b})$$

A _{vc} (in ²)	A _{vco} (in ²)	Ψ _{ec,v}	Ψ _{ed,v}	Ψ _{c,v}	Ψ _{h,v}	V _{by} (lb)	φ	φV _{cbgy} (lb)
444.83	925.36	1.000	0.788	1.000	1.000	26769	0.70	7102

Shear parallel to edge in y-direction:

$$V_{bx} = \min \left[7 \left(l_e / d_a \right)^{0.2} \lambda_a \sqrt{f_c} C_{a1}^{1.5}; 9 \lambda_a \sqrt{f_c} C_{a1}^{1.5} \right] \quad (\text{Eq. 17.5.2.2a \& Eq. 17.5.2.2b})$$

l _e (in)	d _a (in)	λ _a	f _c (psi)	C _{a1} (in)	V _{bx} (lb)
6.00	0.750	1.00	3000	6.34	7869

$$\phi V_{cbgx} = \phi (2) \left(A_{vc} / A_{vco} \right) \Psi_{ec,v} \Psi_{ed,v} \Psi_{c,v} \Psi_{h,v} V_{bx} \quad (\text{Sec. 17.3.1, 17.5.2.1(c) \& Eq. 17.5.2.1b})$$

A _{vc} (in ²)	A _{vco} (in ²)	Ψ _{ec,v}	Ψ _{ed,v}	Ψ _{c,v}	Ψ _{h,v}	V _{bx} (lb)	φ	φV _{cbgx} (lb)
196.67	180.88	1.000	1.000	1.000	1.000	7869	0.70	11979

10. Concrete Pryout Strength of Anchor in Shear (Sec. 17.5.3)

$$\phi V_{cpg} = \phi K_{cp} N_{cbg} = \phi K_{cp} \left(A_{Nc} / A_{Nco} \right) \Psi_{ec,N} \Psi_{ed,N} \Psi_{c,N} \Psi_{cp,N} N_b \quad (\text{Sec. 17.3.1 \& Eq. 17.5.3.1b})$$

K _{cp}	A _{Nc} (in ²)	A _{Nco} (in ²)	Ψ _{ec,N}	Ψ _{ed,N}	Ψ _{c,N}	Ψ _{cp,N}	N _b (lb)	φ	φV _{cpg} (lb)
2.0	427.66	160.78	1.000	1.000	1.000	1.000	9683	0.70	36058

11. Results

Interaction of Tensile and Shear Forces (Sec. 17.6)

Tension	Factored Load, N _{ua} (lb)	Design Strength, φN _n (lb)	Ratio	Status
Steel	3571	14528	0.25	Pass
Concrete breakout	7142	8577	0.83	Pass (Governs)
Pullout	3571	11479	0.31	Pass
Side-face blowout	7142	16848	0.42	Pass
Shear	Factored Load, V _{ua} (lb)	Design Strength, φV _n (lb)	Ratio	Status
Steel	185	7556	0.02	Pass
T Concrete breakout y+	738	7102	0.10	Pass (Governs)
 Concrete breakout x-	369	11979	0.03	Pass (Governs)

Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.



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Pryout	738	36058	0.02	Pass	
Interaction check	$N_{ua}/\phi N_n$	$V_{ua}/\phi V_n$	Combined Ratio	Permissible	Status
Sec. 17.6..1	0.83	0.00	83.3%	1.0	Pass

3/4"Ø Heavy Hex Bolt, F1554 Gr. 36 with hef = 24.000 inch meets the selected design criteria.

12. Warnings

- Per designer input, ductility requirements for tension have been determined to be satisfied – designer to verify.
- Per designer input, ductility requirements for shear have been determined to be satisfied – designer to verify.
- Designer must exercise own judgement to determine if this design is suitable.



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E-mail:			

1. Project information

Customer company: Freiheit Architects
Customer contact name:
Customer e-mail:
Comment:

Project description: Order Here Canopy
Location:
Fastening description:

2. Input Data & Anchor Parameters

General

Design method: ACI 318-14
Units: Imperial units

Anchor Information:

Anchor type: Cast-in-place
Material: F1554 Grade 36
Diameter (inch): 1.000
Effective Embedment depth, h_{ef} (inch): 24.000
Anchor category: -
Anchor ductility: Yes
 h_{min} (inch): 25.75
 C_{min} (inch): 6.00
 S_{min} (inch): 6.00

Base Material

Concrete: Normal-weight
Concrete thickness, h (inch): 36.00
State: Cracked
Compressive strength, f_c (psi): 3000
 $\Psi_{e,v}$: 1.0
Reinforcement condition: B tension, B shear
Supplemental reinforcement: Not applicable
Reinforcement provided at corners: No
Ignore concrete breakout in tension: No
Ignore concrete breakout in shear: No
Ignore 6do requirement: No
Build-up grout pad: No

Base Plate

Length x Width x Thickness (inch): 15.00 x 15.00 x 0.50

Recommended Anchor

Anchor Name: Heavy Hex Bolt - 1"Ø Heavy Hex Bolt, F1554 Gr. 36





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Load and Geometry

Load factor source: ACI 318 Section 5.3

Load combination: not set

Seismic design: No

Anchors subjected to sustained tension: Not applicable

Apply entire shear load at front row: No

Anchors only resisting wind and/or seismic loads: No

Strength level loads:

N_{ua} [lb]: 0

V_{ux} [lb]: 0

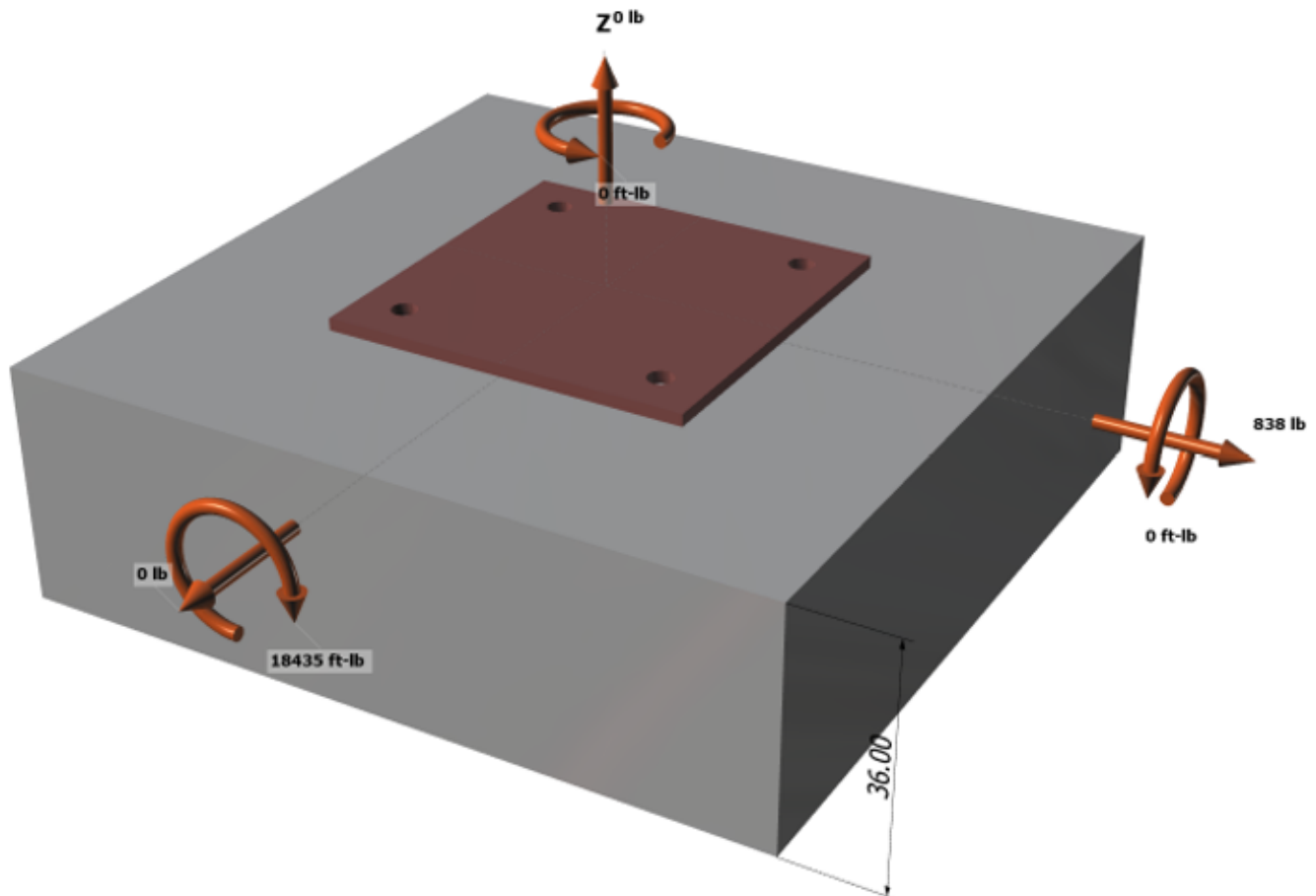
V_{uy} [lb]: 838

M_{ux} [ft-lb]: -18435

M_{uy} [ft-lb]: 0

M_{uz} [ft-lb]: 0

<Figure 1>



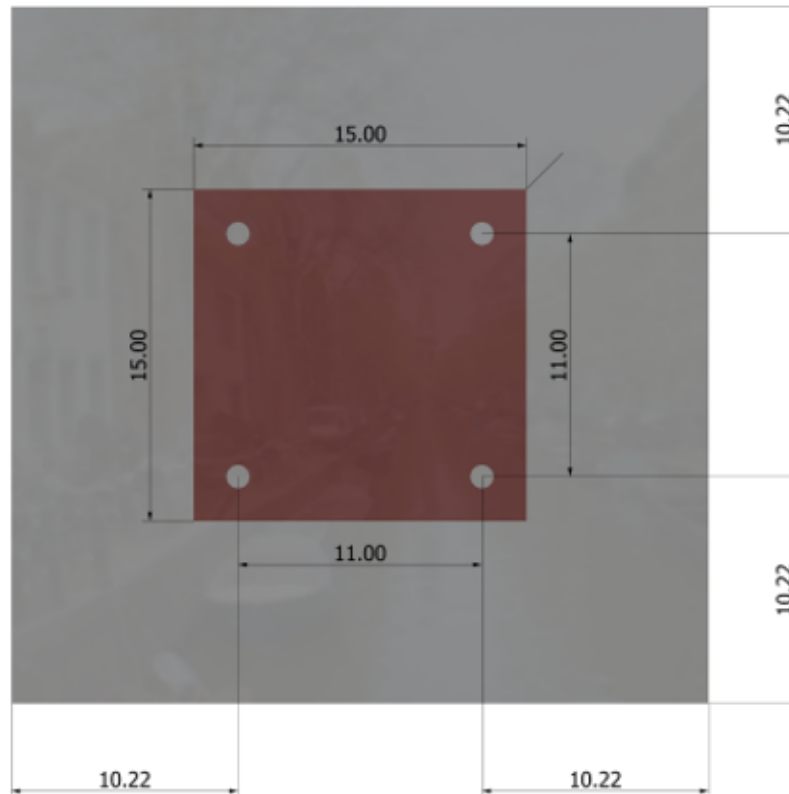
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Simpson Strong-Tie Company Inc. 5956 W. Las Positas Boulevard Pleasanton, CA 94588 Phone: 925.560.9000 Fax: 925.847.3871 www.strongtie.com



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E-mail:			

<Figure 2>





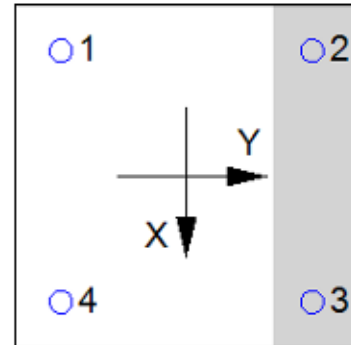
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3. Resulting Anchor Forces

Anchor	Tension load, N_{tias} (lb)	Shear load x, V_{uax} (lb)	Shear load y, V_{uay} (lb)	Shear load combined, $\sqrt{(V_{uax})^2 + (V_{uay})^2}$ (lb)
1	9407.8	0.0	209.5	209.5
2	0.0	0.0	209.5	209.5
3	0.0	0.0	209.5	209.5
4	9407.8	0.0	209.5	209.5
Sum	18815.6	0.0	838.0	838.0

Maximum concrete compression strain (%): 0.15
 Maximum concrete compression stress (psi): 673
 Resultant tension force (lb): 18816
 Resultant compression force (lb): 18816
 Eccentricity of resultant tension forces in x-axis, e'_{Nx} (inch): 0.00
 Eccentricity of resultant tension forces in y-axis, e'_{Ny} (inch): 0.00
 Eccentricity of resultant shear forces in x-axis, e'_{Vx} (inch): 0.00
 Eccentricity of resultant shear forces in y-axis, e'_{Vy} (inch): 0.00

<Figure 3>



4. Steel Strength of Anchor in Tension (Sec. 17.4.1)

N_{sa} (lb)	ϕ	ϕN_{sa} (lb)
35150	0.75	26363

5. Concrete Breakout Strength of Anchor in Tension (Sec. 17.4.2)

$N_b = 16\lambda_a^2 f_c h_{ef}^3$ (Eq. 17.4.2.2b)

λ_a	f_c (psi)	h_{ef} (in)	N_b (lb)
1.00	3000	14.147	72517

$\phi N_{cbg} = \phi (A_{Nc} / A_{Nco}) \Psi_{ec,N} \Psi_{ed,N} \Psi_{c,N} \Psi_{cp,N} N_b$ (Sec. 17.3.1 & Eq. 17.4.2.1b)

A_{Nc} (in ²)	A_{Nco} (in ²)	$c_{a,min}$ (in)	$\Psi_{ec,N}$	$\Psi_{ed,N}$	$\Psi_{c,N}$	$\Psi_{cp,N}$	N_b (lb)	ϕ	ϕN_{cbg} (lb)
988.47	1801.15	10.22	1.000	0.844	1.00	1.000	72517	0.70	23526

6. Pullout Strength of Anchor in Tension (Sec. 17.4.3)

$\phi N_{pn} = \phi \Psi_{c,P} N_p = \phi \Psi_{c,P} 8A_{brg} f_c$ (Sec. 17.3.1, Eq. 17.4.3.1 & 17.4.3.4)

$\Psi_{c,P}$	A_{brg} (in ²)	f_c (psi)	ϕ	ϕN_{pn} (lb)
1.0	1.50	3000	0.70	25217



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8. Steel Strength of Anchor in Shear (Sec. 17.5.1)

V_{sa} (lb)	ϕ_{grout}	ϕ	$\phi_{grout}\phi V_{sa}$ (lb)
21090	1.0	0.65	13709

9. Concrete Breakout Strength of Anchor in Shear (Sec. 17.5.2)

Shear perpendicular to edge in y-direction:

$$V_{by} = \min[7(l_e/d_a)^{0.2} \lambda_a \lambda_a f_c c_{at}^{1.5}; 9 \lambda_a f_c c_{at}^{1.5}] \text{ (Eq. 17.5.2.2a \& Eq. 17.5.2.2b)}$$

l_e (in)	d_a (in)	λ_a	f_c (psi)	c_{at} (in)	V_{by} (lb)
8.00	1.000	1.00	3000	21.22	48186

$$\phi V_{cbgy} = \phi (A_{vc} / A_{vco}) \psi_{ec,v} \psi_{ed,v} \psi_{c,v} \psi_{h,v} V_{by} \text{ (Sec. 17.3.1 \& Eq. 17.5.2.1b)}$$

A_{vc} (in ²)	A_{vco} (in ²)	$\psi_{ec,v}$	$\psi_{ed,v}$	$\psi_{c,v}$	$\psi_{h,v}$	V_{by} (lb)	ϕ	ϕV_{cbgy} (lb)
1000.74	2026.30	1.000	0.796	1.000	1.000	48186	0.70	13266

Shear parallel to edge in y-direction:

$$V_{bx} = \min[7(l_e/d_a)^{0.2} \lambda_a \lambda_a f_c c_{at}^{1.5}; 9 \lambda_a f_c c_{at}^{1.5}] \text{ (Eq. 17.5.2.2a \& Eq. 17.5.2.2b)}$$

l_e (in)	d_a (in)	λ_a	f_c (psi)	c_{at} (in)	V_{bx} (lb)
8.00	1.000	1.00	3000	10.22	16106

$$\phi V_{cbgx} = \phi (2)(A_{vc} / A_{vco}) \psi_{ec,v} \psi_{ed,v} \psi_{c,v} \psi_{h,v} V_{bx} \text{ (Sec. 17.3.1, 17.5.2.1(c) \& Eq. 17.5.2.1b)}$$

A_{vc} (in ²)	A_{vco} (in ²)	$\psi_{ec,v}$	$\psi_{ed,v}$	$\psi_{c,v}$	$\psi_{h,v}$	V_{bx} (lb)	ϕ	ϕV_{cbgx} (lb)
481.98	470.02	1.000	1.000	1.000	1.000	16106	0.70	23122

10. Concrete Pryout Strength of Anchor in Shear (Sec. 17.5.3)

$$\phi V_{cpg} = \phi k_{cp} N_{cbg} = \phi k_{cp} (A_{Nc} / A_{Nco}) \psi_{ec,N} \psi_{ed,N} \psi_{c,N} \psi_{cp,N} N_b \text{ (Sec. 17.3.1 \& Eq. 17.5.3.1b)}$$

k_{cp}	A_{Nc} (in ²)	A_{Nco} (in ²)	$\psi_{ec,N}$	$\psi_{ed,N}$	$\psi_{c,N}$	$\psi_{cp,N}$	N_b (lb)	ϕ	ϕV_{cpg} (lb)
2.0	988.47	417.79	1.000	1.000	1.000	1.000	21459	0.70	71080

11. Results

Interaction of Tensile and Shear Forces (Sec. 17.6.)

Tension	Factored Load, N_{us} (lb)	Design Strength, ϕN_n (lb)	Ratio	Status	
Steel	9408	26363	0.36	Pass	
Concrete breakout	18816	23526	0.80	Pass (Governs)	
Pullout	9408	25217	0.37	Pass	
Shear	Factored Load, V_{us} (lb)	Design Strength, ϕV_n (lb)	Ratio	Status	
Steel	210	13709	0.02	Pass	
T Concrete breakout y+	838	13266	0.06	Pass (Governs)	
 Concrete breakout x-	419	23122	0.02	Pass (Governs)	
Pryout	838	71080	0.01	Pass	
Interaction check	$N_{us}/\phi N_n$	$V_{us}/\phi V_n$	Combined Ratio	Permissible	Status
Sec. 17.6..1	0.80	0.00	80.0%	1.0	Pass

1"Ø Heavy Hex Bolt, F1554 Gr. 36 with hef = 24.000 inch meets the selected design criteria.



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Engineer:		Page:	6/6
Project:	McD's		
Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

12. Warnings

- Designer must exercise own judgement to determine if this design is suitable.



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Project:	McD's		
Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

1. Project information

Customer company: Freiheit Architecture
Customer contact name:
Customer e-mail:
Comment:

Project description: Single Gateway Sign

Location:
Fastening description:

2. Input Data & Anchor Parameters

General

Design method: ACI 318-14
Units: Imperial units

Anchor Information:

Anchor type: Cast-in-place
Material: F1554 Grade 36
Diameter (inch): 0.750
Effective Embedment depth, h_{ef} (inch): 24.000
Anchor category: -
Anchor ductility: Yes
 h_{min} (inch): 25.50
 C_{min} (inch): 4.50
 S_{min} (inch): 4.50

Base Material

Concrete: Normal-weight
Concrete thickness, h (inch): 36.00
State: Cracked
Compressive strength, f_c (psi): 3000
 $\Psi_{e,v}$: 1.0
Reinforcement condition: B tension, B shear
Supplemental reinforcement: Not applicable
Reinforcement provided at corners: No
Ignore concrete breakout in tension: No
Ignore concrete breakout in shear: No
Ignore 6do requirement: No
Build-up grout pad: No

Base Plate

Length x Width x Thickness (inch): 15.00 x 15.00 x 0.50

Recommended Anchor

Anchor Name: Heavy Hex Bolt - 3/4"Ø Heavy Hex Bolt, F1554 Gr. 36





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Load and Geometry

Load factor source: ACI 318 Section 5.3

Load combination: not set

Seismic design: No

Anchors subjected to sustained tension: Not applicable

Apply entire shear load at front row: No

Anchors only resisting wind and/or seismic loads: No

Strength level loads:

N_{ua} [lb]: 0

V_{ux} [lb]: 0

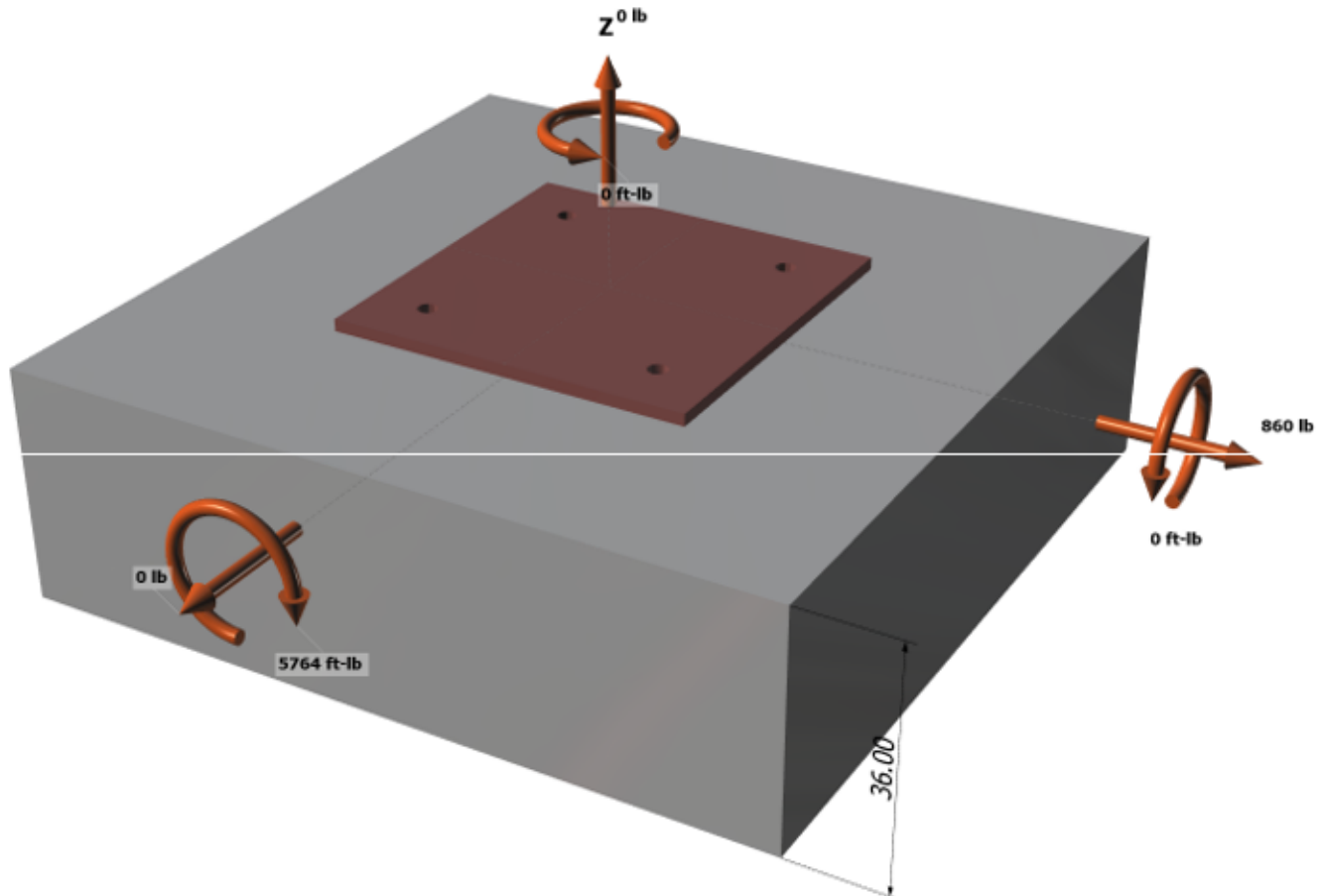
V_{uy} [lb]: 860

M_{ux} [ft-lb]: -5764

M_{uy} [ft-lb]: 0

M_{uz} [ft-lb]: 0

<Figure 1>



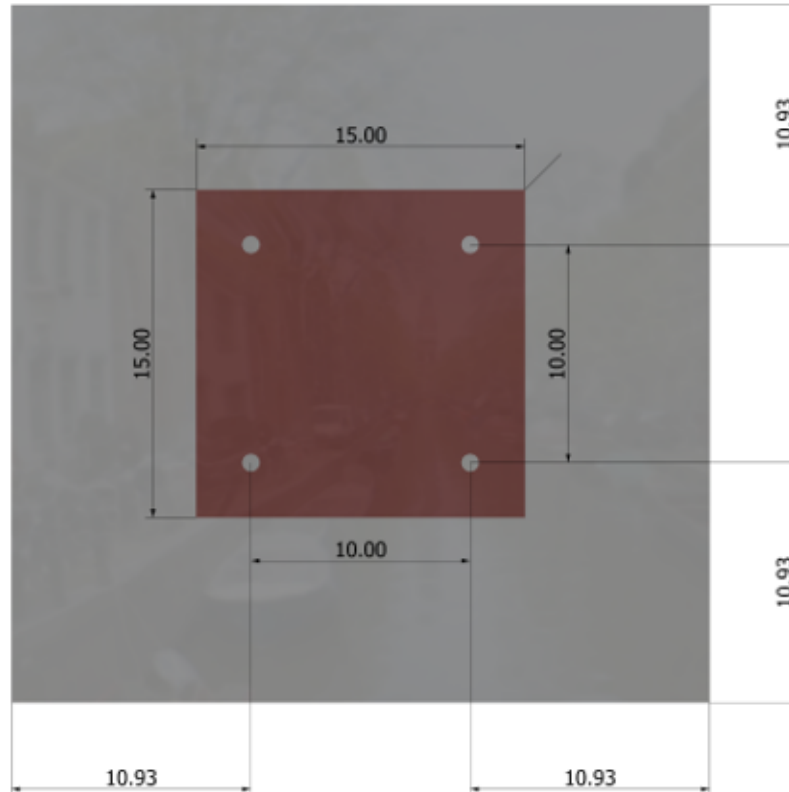
Input data and results must be checked for agreement with the existing circumstances, the standards and guidelines must be checked for plausibility.

Simpson Strong-Tie Company Inc. 5956 W. Las Positas Boulevard Pleasanton, CA 94588 Phone: 925.560.9000 Fax: 925.847.3871 www.strongtie.com



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<Figure 2>





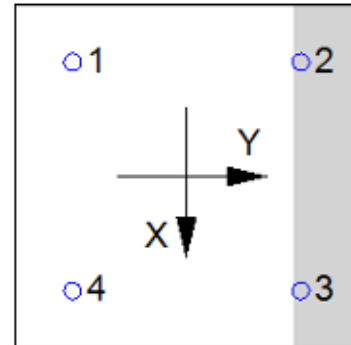
Company:	DEI	Date:	7/18/2017
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Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

3. Resulting Anchor Forces

Anchor	Tension load, N_{ua} (lb)	Shear load x, V_{ux} (lb)	Shear load y, V_{uy} (lb)	Shear load combined, $\sqrt{(V_{ux})^2+(V_{uy})^2}$ (lb)
1	2992.6	0.0	215.0	215.0
2	0.0	0.0	215.0	215.0
3	0.0	0.0	215.0	215.0
4	2992.6	0.0	215.0	215.0
Sum	5985.2	0.0	860.0	860.0

Maximum concrete compression strain (%): 0.06
 Maximum concrete compression stress (psi): 282
 Resultant tension force (lb): 5985
 Resultant compression force (lb): 5985
 Eccentricity of resultant tension forces in x-axis, e'_{Nx} (inch): 0.00
 Eccentricity of resultant tension forces in y-axis, e'_{Ny} (inch): 0.00
 Eccentricity of resultant shear forces in x-axis, e'_{Vx} (inch): 0.00
 Eccentricity of resultant shear forces in y-axis, e'_{Vy} (inch): 0.00

<Figure 3>



4. Steel Strength of Anchor in Tension (Sec. 17.4.1)

N_{sa} (lb)	ϕ	ϕN_{sa} (lb)
19370	0.75	14528

5. Concrete Breakout Strength of Anchor in Tension (Sec. 17.4.2)

$N_b = 16\lambda_a^2 f_c h_{ef}^3$ (Eq. 17.4.2.2b)

λ_a	f_c (psi)	h_{ef} (in)	N_b (lb)
1.00	3000	13.953	70872

$\phi N_{cbg} = \phi (A_{Nc} / A_{Nco}) \Psi_{ec,N} \Psi_{ed,N} \Psi_{c,N} \Psi_{cp,N} N_b$ (Sec. 17.3.1 & Eq. 17.4.2.1b)

A_{Nc} (in ²)	A_{Nco} (in ²)	$c_{a,min}$ (in)	$\Psi_{ec,N}$	$\Psi_{ed,N}$	$\Psi_{c,N}$	$\Psi_{cp,N}$	N_b (lb)	ϕ	ϕN_{cbg} (lb)
1015.06	1752.26	10.93	1.000	0.857	1.00	1.000	70872	0.70	24620

6. Pullout Strength of Anchor in Tension (Sec. 17.4.3)

$\phi N_{pn} = \phi \Psi_{c,P} N_p = \phi \Psi_{c,P} 8A_{brg} f_c$ (Sec. 17.3.1, Eq. 17.4.3.1 & 17.4.3.4)

$\Psi_{c,P}$	A_{brg} (in ²)	f_c (psi)	ϕ	ϕN_{pn} (lb)
1.0	0.91	3000	0.70	15305



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8. Steel Strength of Anchor in Shear (Sec. 17.5.1)

V_{sa} (lb)	ϕ_{grout}	ϕ	$\phi_{grout}\phi V_{sa}$ (lb)
11625	1.0	0.65	7556

9. Concrete Breakout Strength of Anchor in Shear (Sec. 17.5.2)

Shear perpendicular to edge in y-direction:

$$V_{by} = \min[7(l_e/d_a)^{0.2} \lambda_a \lambda_a f_c c_{at}^{1.5}; 9 \lambda_a f_c c_{at}^{1.5}] \text{ (Eq. 17.5.2.2a \& Eq. 17.5.2.2b)}$$

l_e (in)	d_a (in)	λ_a	f_c (psi)	c_{at} (in)	V_{by} (lb)
6.00	0.750	1.00	3000	20.93	47202

$$\phi V_{cbgy} = \phi (A_{vc} / A_{vco}) \psi_{ec,v} \psi_{ed,v} \psi_{c,v} \psi_{h,v} V_{by} \text{ (Sec. 17.3.1 \& Eq. 17.5.2.1b)}$$

A_{vc} (in ²)	A_{vco} (in ²)	$\psi_{ec,v}$	$\psi_{ed,v}$	$\psi_{c,v}$	$\psi_{h,v}$	V_{by} (lb)	ϕ	ϕV_{cbgy} (lb)
1000.24	1971.29	1.000	0.804	1.000	1.000	47202	0.70	13487

Shear parallel to edge in y-direction:

$$V_{bx} = \min[7(l_e/d_a)^{0.2} \lambda_a \lambda_a f_c c_{at}^{1.5}; 9 \lambda_a f_c c_{at}^{1.5}] \text{ (Eq. 17.5.2.2a \& Eq. 17.5.2.2b)}$$

l_e (in)	d_a (in)	λ_a	f_c (psi)	c_{at} (in)	V_{bx} (lb)
6.00	0.750	1.00	3000	10.93	17813

$$\phi V_{cbgy} = \phi (2)(A_{vc} / A_{vco}) \psi_{ec,v} \psi_{ed,v} \psi_{c,v} \psi_{h,v} V_{bx} \text{ (Sec. 17.3.1, 17.5.2.1(c) \& Eq. 17.5.2.1b)}$$

A_{vc} (in ²)	A_{vco} (in ²)	$\psi_{ec,v}$	$\psi_{ed,v}$	$\psi_{c,v}$	$\psi_{h,v}$	V_{bx} (lb)	ϕ	ϕV_{cbgy} (lb)
522.34	537.59	1.000	1.000	1.000	1.000	17813	0.70	24231

10. Concrete Pryout Strength of Anchor in Shear (Sec. 17.5.3)

$$\phi V_{cpg} = \phi k_{cp} N_{cbg} = \phi k_{cp} (A_{Nc} / A_{Nco}) \psi_{ec,N} \psi_{ed,N} \psi_{c,N} \psi_{cp,N} N_b \text{ (Sec. 17.3.1 \& Eq. 17.5.3.1b)}$$

k_{cp}	A_{Nc} (in ²)	A_{Nco} (in ²)	$\psi_{ec,N}$	$\psi_{ed,N}$	$\psi_{c,N}$	$\psi_{cp,N}$	N_b (lb)	ϕ	ϕV_{cpg} (lb)
2.0	1015.06	477.86	1.000	1.000	1.000	1.000	24001	0.70	71375

11. Results

Interaction of Tensile and Shear Forces (Sec. 17.6)

Tension	Factored Load, N_{us} (lb)	Design Strength, ϕN_n (lb)	Ratio	Status	
Steel	2993	14528	0.21	Pass	
Concrete breakout	5985	24620	0.24	Pass (Governs)	
Pullout	2993	15305	0.20	Pass	
Shear	Factored Load, V_{us} (lb)	Design Strength, ϕV_n (lb)	Ratio	Status	
Steel	215	7556	0.03	Pass	
T Concrete breakout y+	860	13487	0.06	Pass (Governs)	
 Concrete breakout x-	430	24231	0.02	Pass (Governs)	
Pryout	860	71375	0.01	Pass	
Interaction check	$N_{us}/\phi N_n$	$V_{us}/\phi V_n$	Combined Ratio	Permissible	Status
Sec. 17.6..1	0.24	0.00	24.3%	1.0	Pass

3/4"Ø Heavy Hex Bolt, F1554 Gr. 36 with hef = 24.000 inch meets the selected design criteria.



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Engineer:		Page:	6/6
Project:	McD's		
Address:	1029 Market St		
Phone:	425-828-4200		
E-mail:			

12. Warnings

- Designer must exercise own judgement to determine if this design is suitable.

ORDINANCE NO. 4131

THREE MILE LANE

An Ordinance enacting conditions for development in an area hereinafter described, and commonly known as Three Mile Lane; now, therefore,

THE CITY OF McMinnville ORDAINS AS FOLLOWS:

Section 1. Statement of Purpose. The City of McMinnville finds that the development of lands on the north and south sides of Three Mile Lane (Oregon Highway 18) is acceptable if provisions are made to integrate the dual functions of the highway as a bypass around the City and a land service arterial for adjacent properties. The City also finds that development along Three Mile Lane is desirable if it is of good quality and design, if the living and working environments developed are compatible with each other, and if the living environment includes open spaces, parks, and features buffering of residential uses from the highway. The City recognized, during the comprehensive planning process, that to insure that these concerns are met, proper designations on the McMinnville Comprehensive Plan and Zoning Maps must be made, and, further, that a planned development overlay must be placed over the area establishing specific conditions for development.

Section 2. Planned Development Overlay. The planned development overlay which is created and implemented by this ordinance shall be placed over areas to the north and south of Oregon Highway 18 from the eastern city limits west to the vicinity of the Three Mile Lane Spur intersection with Highway 18. The affected area is further described by map in Exhibit "A". Areas within and without the city limits shall carry this designation. The policies and procedures set by this ordinance shall be applied to all land use decisions under the jurisdiction of the City of McMinnville. The overlay shall not impinge the legal jurisdiction of Yamhill County for areas outside the city limits.

Section 3. Notation on Zoning and Comprehensive Plan Maps. The perimeters of the areas affected by this ordinance shall be noted on the official McMinnville Comprehensive Plan Map of 1980 and the official McMinnville Zoning Map of 1980.

Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit "A":

- (a) The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- (b) A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- (c) Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:

1. The minimization of entrances onto Three Mile Lane;
 2. The development of on-site circulation systems, connecting to adjoining properties;
 3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.
 4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.
- (d) Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- (e) Residentially designated areas covered by this ordinance shall be developed at R-1 zoning classification densities. Mixed housing-type residential developments shall be allowed and encouraged.
- (f) Residential developments shall provide parkland under the provisions of Section 35 of Ordinance No. 3702, unless safe and convenient pedestrian access is provided to an existing park. If no land is required, money in lieu of land shall be required.

The following policies shall apply to specified properties north and south of Three Mile Lane, described on the map in Exhibit "B". The written descriptions, uses, and written policies included below are to apply to the interpretation of the map and be of at least equal weight and importance.

Municipal Airport (Area 1)

The airport shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980 to accommodate aviation related facilities in keeping with the Airport Master Plan, as amended. Facilities identified in the plan for upgrading and expanding the aviation aspects of the airport shall be exempted from review by the provisions of the planned development overlay. Any private or public aviation related industrial development that may in the future be contemplated shall be subject to the provisions of this ordinance. The adoption of an airport zoning ordinance in the future may be appropriate, and additional requirements of such a zone shall be applied in conjunction with the requirements of this ordinance.

West of Municipal Airport (Area 2)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The City shall encourage the location of aviation related industries or industries requiring extensive use of airport services, in this area. Zone changes to M-L (Limited Light Industrial), to M-1 (Light Industrial) and AH (Agricultural Holding) shall be allowed. Zone changes to M-2 (General Industrial) may be allowed upon findings that the intended use of the property shall be an aviation related industry, or an industry which requires extensive use of the airport services for its operation.

West of Municipal Airport to Mobile Home Park (Area 3)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The City shall allow zone changes to M-L (Limited Light Industrial) and AH (Agricultural Holding) designations.

East of Pacific Avenue - Industrial Area (Area 4)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980 to accommodate the existing industrial uses. Expansion of those uses may take place if compatibility with surrounding properties is insured.

Airport Rendezvous (Area 5)

This area shall be designated as industrial on the McMinnville Comprehensive Plan Map of 1980. The areas designated AH (Agricultural Holding) on the map shall remain so designated until completion of the update of the Airport Master Plan. The adoption of an airport zoning ordinance, recommended land uses, and development controls for identified noise contours may be appropriate upon completion of the Airport Master Plan; any additional requirements enacted by the City shall be applied in conjunction with the requirements of this ordinance. Redesignation of the properties may be allowed after completion of the Airport Master Plan update and adoption of appropriate Master Plan recommendations, provided that the requirements of this ordinance are met, and upon determination that the proposed use is compatible with surrounding properties.

Remainder of Planned Development Area

The remainder of the properties within the area covered by this ordinance shall be developed according to the designations on the McMinnville Comprehensive Plan Map. Site plan review as required in Chapter 17.51 of Ordinance No. 3380 shall include conditions to insure compatibility of allowed developments with proposed uses in surrounding areas.

Section 5. Procedures for Review.

- (a) Annexation to the City of lands within the area covered by this ordinance shall meet the requirements of the annexation ordinance.
- (b) Comprehensive Plan Map amendments shall be processed under procedures set out in Ordinance No. 4127.
- (c) Zone changes and land divisions submitted for approval shall be processed under the requirements and standards of Chapter 17.51 of Ordinance No. 3380 (Zoning Ordinance). If standards and requirements of Chapter 17.51 differ with those established elsewhere by the City, the more restrictive standards and requirements shall be adhered to.
- (d) Land division requests shall also be processed under the requirements of Ordinance No. 3702 (Land Division Ordinance).
- (e) Amendments to this ordinance shall be processed under procedures set out in Chapter 17.51 of Ordinance No. 3380 (Zoning Ordinance).

Passed by the City Council on April 7, 1981.



2000 0 2000 Feet

EXHIBIT A
Three Mile Lane
Planned Development Overlay

Ordinance No. 4131

CITY OF McMINNVILLE

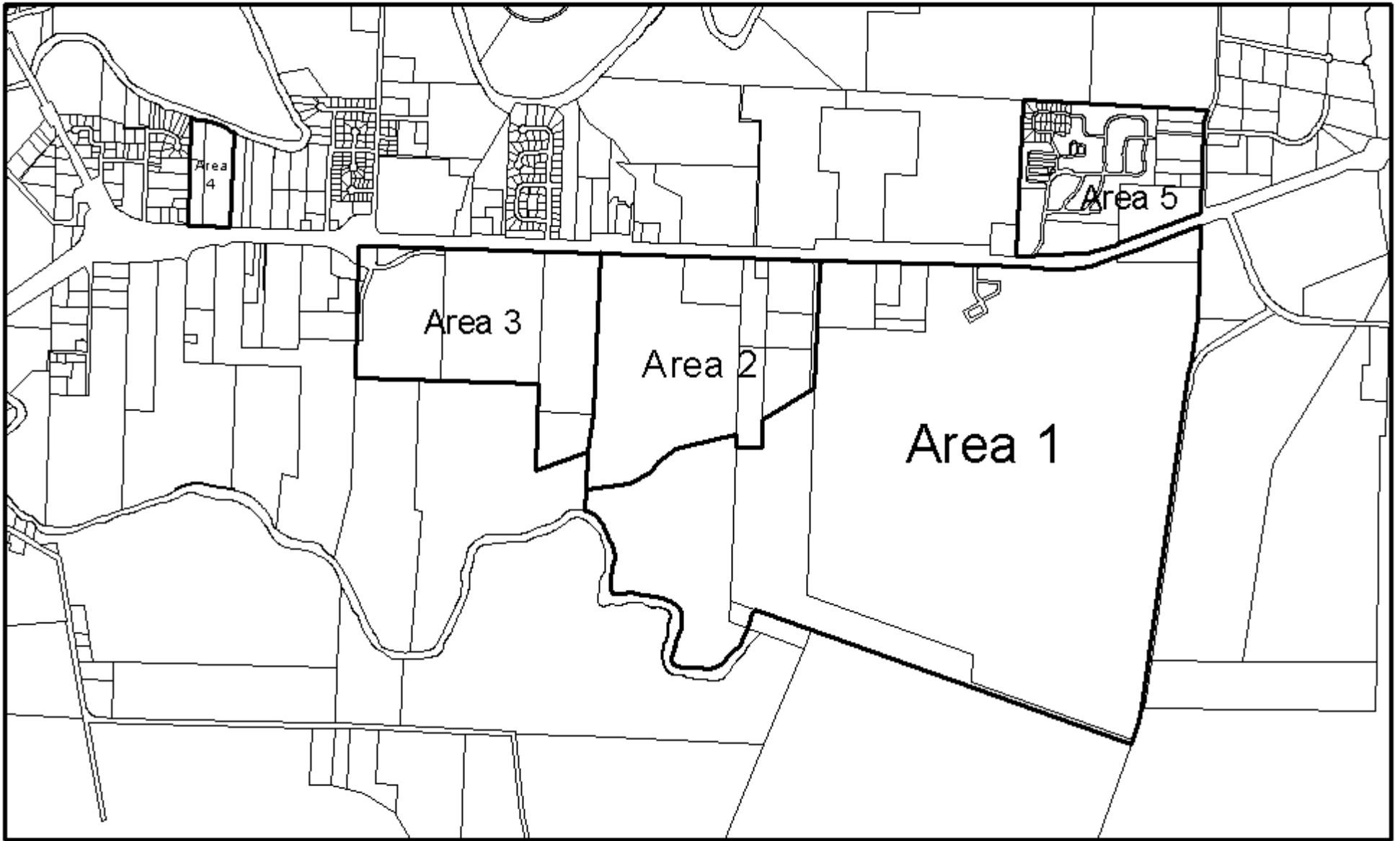


EXHIBIT B
Three Mile Lane
Planned Development Overlay

Ordinance No. 4131

CITY OF McMINNVILLE

ORDINANCE NO. 4572

An Ordinance amending Ordinance 4131 (Three Mile Lane Planned Development Overlay) by supplanting the terms and provisions of Sections 4 and 5 and by adding new sections, and amending ordinance 4506 (Commercial Lands) by repealing subsections 10(h) and 10(i).

RECITALS:

The City of McMinnville has found that certain terms and provisions of the Three Mile Lane Planned Development Overlay have become dated since the 1981 passage of the ordinance and are in need of replacement. The City Council believes it is also in the best interest of the citizens of McMinnville and of the community as a whole to regulate commercial signage along the Three Mile Lane corridor as it leads into McMinnville.

At the direction of the City Council, staff developed amendments to the Three Mile Lane Planned Development Overlay which included regulations governing the use of commercial signage. The McMinnville Planning Commission held a work session on September 8, 1994 and a public hearing on October 13, 1994 on the proposed amendments, after which they recommended that the amendments be approved; now, therefore,

THE CITY OF McMINNVILLE ORDAINS AS FOLLOWS:

Section 1. The terms and provisions of Section 4 of Ordinance 4131 are hereby supplanted with the following terms and provisions:

Section 4. Policies. The following policies shall apply to the property described on the map in Exhibit "A":

- A. The goals and policies of the McMinnville Comprehensive Plan, Volume II, and applicable regulations and standards in Volume III, and other City codes shall be adhered to.
- B. A one hundred twenty (120) foot setback from the centerline of Highway 18 shall be established both north and south of the highway.
- C. Access requirements adopted hereafter in an access plan for this area shall be adhered to. Provisions of the plan shall include:
 1. The minimization of entrances onto Three Mile Lane;
 2. The development of on-site circulation systems, connecting to adjoining properties, including public frontage roads;
 3. The provisions of acceleration-deceleration lanes and left-turn refuges when and where necessary and practicable.

4. The provision of bikeways along frontage roads or on-site circulation systems. Bikeway connections accessing Three Mile Lane shall be provided so that the frontage road or on-site circulation system can serve as an alternative route for cyclists traveling along Three Mile Lane.
- D. Landscaping and buffer strips along the highway frontage may be required including noise buffering methods, such as berms and/or plantings.
- E. Mixed housing-type residential developments shall be allowed and encouraged in those areas designated as residential.
- F. Temporary signage shall be allowed as per Section 17.62.060(B)(3) of the McMinnville Zoning Ordinance (No. 3380). (Ord. 4988 §1, 2015)

Section 2. The terms and provisions of Section 5 of Ordinance 4131 are hereby supplanted with the following terms and provisions:

Section 5. Signs. The following regulations apply to those properties designated as either industrial or commercial on the McMinnville Comprehensive Plan Map and which are located within the area described in Exhibit "A." All signage must be approved by the Three Mile Lane Design Review Committee and may not exceed the maximums established in this section. The Three Mile Lane Design Review Committee may take into account color selection; sign material; relationship to site and building design; size, form, mass, height, and scale of sign; the sign's location and proximity to existing signs on adjacent parcels; and other sign design elements appropriate to ensuring that the proposed sign is compatible with the objectives of this ordinance. The Three Mile Lane Design Review Committee may require a proposed sign to be redesigned as necessary to make it consistent with the objectives of this ordinance including, but not limited to, restricting the size and height of the sign, its location, form, color, and material.

The Three Mile Lane Planned Development Overlay area is hereby divided into three "sign zones" which are depicted on Exhibit "B" which is attached hereto and is by this reference herein incorporated. Signage shall be regulated within the various sign zones as follows:

- A. Zone 1. The following regulations apply to commercially or industrially designated properties which fall within zone 1:
 1. That only one free-standing sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit "B", subject to the following:
 - a) The maximum height of any free-standing sign shall be 15 feet.

- b) The maximum square footage per sign face is 24 square feet and there shall be a maximum of two sign faces per free-standing sign.
 - c) A free-standing sign is allowed only on properties with frontage on the Highway 18 frontage road.
2. In lieu of a free-standing sign as listed in subsection 1 above, one monument-type sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit "B", subject to the following:
- a) Maximum square footage per sign face shall be 50 square feet, and there shall be a maximum of two sign faces per monument sign.
 - b) Maximum sign height shall be six feet, and the top of the sign shall not be more than eight feet above the center line grade of the Three Mile Lane frontage road.
 - c) Monument signs shall be non-illuminated, indirectly illuminated, neon, or may have individually back-lit letters, providing that no plastic sign cabinets are allowed.
3. All other signs, with the exception of incidental directional signage, must be flush against the buildings and not protrude more than 12 inches from the building face, subject to the following:
- a) Total area of building signage may not exceed eight percent of the total area of the face of the building which is most closely parallel to Highway 18 or the frontage road and may not exceed four percent on the other sides.
 - b) The area of building signs shall be measured as the area around or enclosing each sign cabinet, or where sign cabinets are not used, the area shall be that within a single continuous perimeter composed of any straight line geometric figure which encloses the extreme limits of the advertising message.
4. If a parcel is divided after the date of the adoption of this ordinance, the sign allowances established in subsections 1. and 2 above will apply only to the original parcel or what remains of it and newly created parcels are limited to the sign allowances established in subsection 3 above except as provided in subsection (A) (5) below.
5. The Three Mile Lane Design Review Committee may allow the sharing of a single free-standing sign by an original parcel or what remains of it and parcels divided after the date of the adoption of this ordinance. The total sign size shall not exceed that allowed for the original parcel.

6. Portable signs and signs with flashing or moving parts are prohibited.
- B. Zone 2. The following regulations apply to commercially and industrially designated properties which fall within zone 2:
1. That only one free-standing sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit "B", subject to the following:
 - a) The maximum height of any free-standing sign shall be 30 feet, except as provided in subsection (B) (1) (c) below.
 - b) The maximum square footage per sign face is 64 square feet, and there shall be a maximum of two sign faces per free-standing sign, except as provided in subsection (B) (1) (c) below.
 - c) If two or more properties share a single free-standing sign, said sign shall be a maximum of 35 feet in height, and each property sharing the sign shall be entitled to up to 64 square feet of sign face per side on a maximum of two sides. The shared free-standing sign would exhaust the allowance for free-standing signs listed in subsection (B) (1) (a) above for all parcels represented.
 - d) A free-standing sign is allowed only on properties with frontage on the Highway 18 frontage road.
 2. In lieu of a free-standing sign as listed in subsection 1 above, one monument-type sign shall be allowed per parcel as they exist at the time of the passage of this ordinance and as they are represented on Exhibit "B", subject to the following:
 - a) Maximum square footage per sign face shall be 50 square feet, and there shall be a maximum of two sign faces per monument sign.
 - b) Maximum sign height shall be six feet, and the top of the sign shall not be more than eight feet above the center line grade of the Three Mile Lane frontage road.
 - c) Monument signs shall be non-illuminated, indirectly illuminated, neon, or may have individually back-lit letters, providing that no plastic sign cabinets are allowed.
 3. All other signs, with the exception of incidental directional signage, must be flush against the buildings and not protrude more than 12 inches from the building face, subject to the following:

- a) Total area of building signage may not exceed eight percent of the total area of the face of the building which is most closely parallel to Highway 18 or the frontage road and may not exceed four percent on the other sides.
 - b) The area of building signs shall be measured as the area around or enclosing each sign cabinet, or where sign cabinets are not used, the area shall be that within a single continuous perimeter composed of any straight line geometric figure which encloses the extreme limits of the advertising message.
4. If a parcel is divided after the date of the adoption of this ordinance, the sign allowances established in subsections 1 and 2 above will apply only to the original parcel or what remains of it and newly created parcels are limited to the sign allowances established in subsection 3 above except as provided in subsection (B) (5) below.
 5. The Three Mile Lane Design Review Committee may allow the sharing of a single free-standing sign by an original parcel or what remains of it and parcels divided after the date of the adoption of this ordinance. The total sign size shall not exceed that allowed for the original parcel.
 6. Portable signs and signs with flashing or moving parts are prohibited.
- C. Zone 3. The following regulations apply to commercially and industrially designated properties which fall within zone 3:
1. That free-standing signs are prohibited.
 2. That one monument-type sign shall be allowed per parcel, except that a second monument sign shall be allowed if the parcel has more than 600 feet of frontage on Highway 18 or on the Highway 18 frontage road, subject to the following:
 - a) Maximum square footage per sign face shall be 80 square feet and there shall be a maximum of two sign faces per monument sign.
 - b) Maximum sign height shall be eight feet, and the top of the sign shall not be more than ten feet above the center line grade of Highway 18 or the Highway 18 frontage road, whichever is applicable.
 - c) Monument signs shall be non-illuminated, indirectly illuminated, neon, or may have individually back—lit letters, providing that no plastic sign cabinets are allowed.

3. All other signs, with the exception of incidental directional signage, must be flush against the buildings and not protrude more than 12 inches from the building face, subject to the following:
 - a) Total area of building signage may not exceed eight percent of the total area of the face of the building which is most closely parallel to Highway 18 or the frontage road and may not exceed four percent on the other sides.
 - b) The area of building signs shall be measured as the area around or enclosing each sign cabinet, or where sign cabinets are not used, the area shall be that within a single continuous perimeter composed of any straight line geometric figure which encloses the extreme limits of the advertising message.
 - c) Signage for the third building in the Tanger Factory Outlet Mall shall be consistent in terms of sizes and percent of coverage with that on buildings one and two.
4. Portable signs and signs with flashing or moving parts are prohibited.

Section 3. That the following section is hereby added to Ordinance 4131:

Section 6. Procedures for Review:

- A. No development or signage may take place within the Three Mile Lane Planned Development Overlay without such development or signage gaining the approval of the Three Mile Lane Design Review Committee.
- B. Zone changes submitted for approval shall be processed under the requirements and standards of Chapter 17.51 of McMinnville Ordinance 3380 (Zoning Ordinance)

Section 4. Subsections 10(h) and 10(i) of Ordinance 4506 are hereby repealed in their entirety.

Section 5. That this ordinance shall be subject to the terms and conditions of Ordinance 3823 entitled "Initiative and Referendum" for a period of thirty days.

This Ordinance passed by the City Council this 22nd day of November 1994

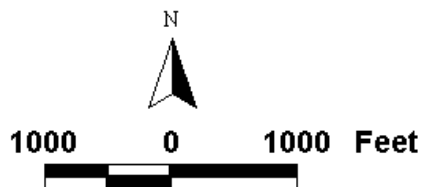


EXHIBIT A
Three Mile Lane Amendment
PLanned Development Overlay
Ordinance No. 4572

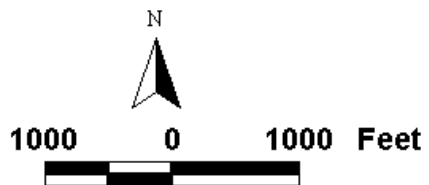
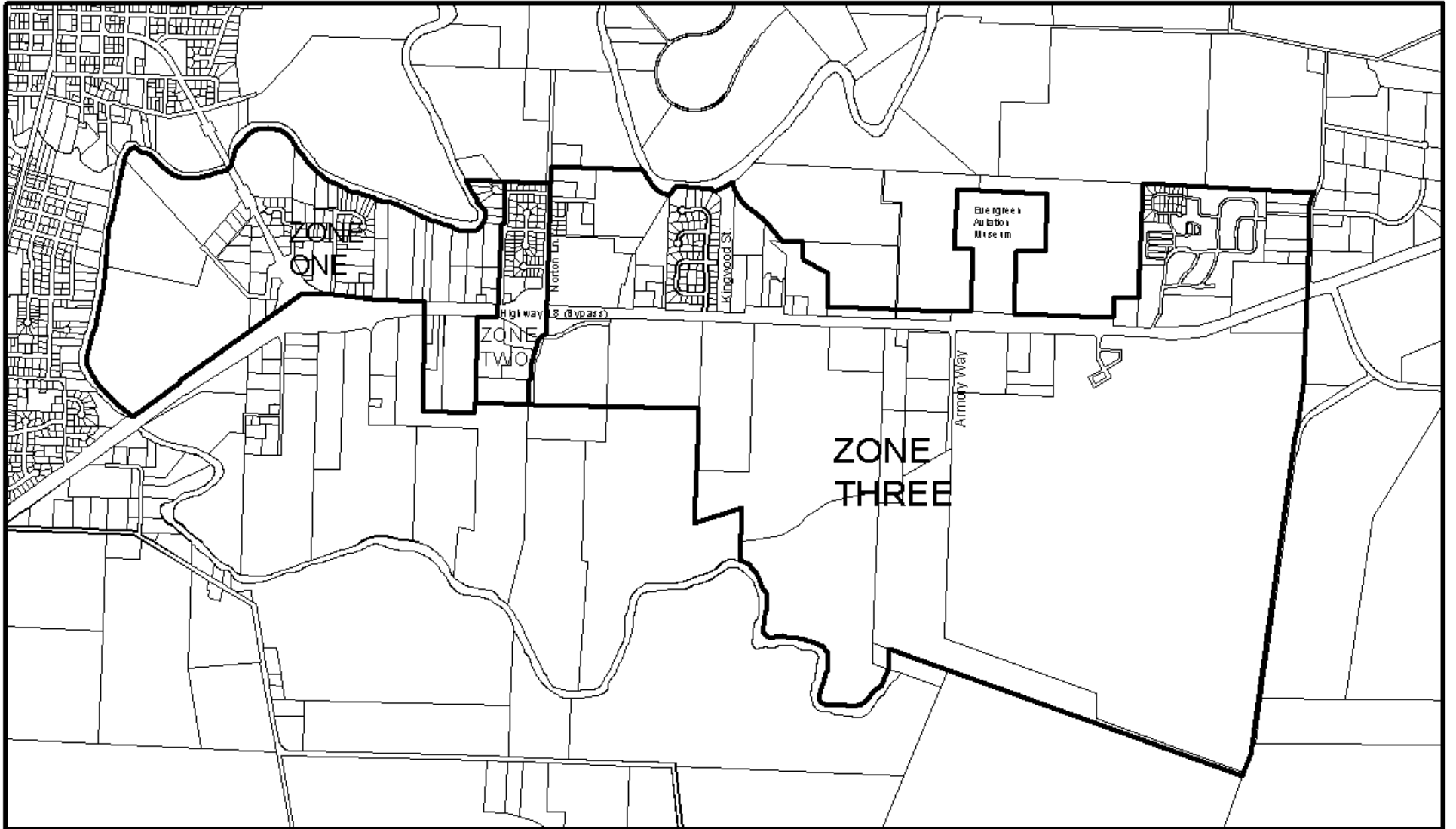


EXHIBIT B
Three Mile Lane Amendment
PLanned Development Overlay
Ordinance No. 4572